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VICTORIA.

COUNTRY ROADS BOARD.

SEVENTEENTH ANNUAL REPORT

FOR YEAR ENDED 30TH JUNE, 1930.

PRESENTED TO BOTH HOUSES OF PARLIAMENT PURSUANT TO ACT No. 2635.

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COUNTRY ROADS BOARD.

SEVENTEENTH ANNUAL REPORT.

Exhibition Building,
Carlton, N.3,
19th November, 1930.

*The Honorable J. P. Jones, M.L.C.,
Minister for Public Works, Melbourne.*

SIR,

In accordance with the requirements of section 96 of the Country Roads Act (No. 3662), the Board has the honour to submit herewith its Annual Report for the financial year ended 30th June, 1930.

FINANCIAL CONDITIONS.

Financial conditions during the past year made it impossible for the Government to provide the proportion of loan moneys authorized under the Country Roads Act. For this reason, the amount allotted by the Board at the beginning of last financial year for the construction of declared Main and Developmental Roads had to be curtailed, involving a reduction to 40 per cent. of the allotment made in July, 1929.

This has necessarily entailed some delay in carrying through the five-year programme of works for which Loan Funds have been authorized by Parliament up to the 30th June, 1930.

STATE HIGHWAYS.

Since the close of 1924, when the Highways and Vehicles Act was passed by Parliament, an enormous increase has occurred in motor traffic within the State of Victoria by the use of private, freight, and public passenger vehicles. With the progressive reconstruction and surfacing of the declared State highways, in accordance with the policy laid down by the Board in 1925, many townships and villages which, a few years ago, were isolated, are now connected by a system of good roads.

With the increase in the number and use of motor cars, sections of highways near populous areas, completed only within a comparatively short period, required replacement with higher grade materials and a more lasting form of construction. Many of the unimportant rural townships have acquired importance which justified improvements to the principal road or roads leading to them.

Only a short time ago, highways were considered sufficiently wide if two motor cars were enabled to pass each other at moderate speeds without the necessity of running off the paved surface. With the increasing motor traffic the edges of the pavement were gradually broken away, making the effective roadway narrow and dangerous.

Hence, several of the more important arterial roads which were constructed to a standard and width sufficient for the traffic at the time, did not fulfil requirements, with the result that it was necessary to undertake the strengthening of these roads as well as their widening up to 20 feet, according to the density of traffic. This applied particularly to State highways, on which a great deal of work of this description has been carried out during the past few years, and which has been made possible by the provision of moneys from the Board's maintenance fund, together with the assistance given under the Federal Aid Roads Agreement.

The greater improvement of the State highway system brings a larger increase in traffic, with the consequent necessity of maintaining longer lengths of roads. The results of these improvements are already apparent in reduced maintenance costs and increased road efficiency. Up to the present, the concern of the Board has been the improvement as quickly as possible of the highways as a whole, with the object of giving trafficable roads at all seasons.

The taking of a half-yearly census of traffic during the past few years has enabled careful investigations to be made of the growth and flow of traffic. These investigations, besides furnishing an adequate basis as to the need of the improvements required for each road, and the order in which such improvements should be carried out, have indicated the necessity of providing a road of sufficient width for the fast-moving motor vehicle, the importance of designing the work to safely carry modern traffic, and the desirability of providing for future widening and improvement, when the volume of traffic warrants

The last highways traffic census taken by the Board between 24th February and 2nd March last disclosed that the rapid increases in traffic indicated in previous censuses have been maintained, the average increase in respect of all highways being from 30 to 35 per cent. The increase is almost entirely accounted for by the pneumatic-tyred vehicle, the percentage of the solid-tyred motor and horse-drawn vehicles showing a decrease at every census. Details of the traffic included in this census are given in the Report of the Chief Engineer.

The type of construction, as in previous years, has been determined according to the nature and volume of traffic using the road, and the locality of the work. As the result of the policy continuously followed during the past five years, the principal highway system of the State, which comprises 1,512 miles, has been considerably improved. Of this mileage, 389 miles have been reconstructed, either in bituminous macadam, water-bound macadam, or with higher type materials, whilst 371 miles have been completed in gravel and lower class construction. Four hundred and sixty-one miles have been improved to the required standard by continuous and systematic maintenance. Reconstruction work completed during the last financial year extended over a length of 268 miles, whilst initial improvements to meet maximum traffic requirements were carried out to the extent of 18 miles.

The expenditure on the reconditioning and maintenance of highways during the year ended 30th June last was £892,129 3s. 2d. The mileage reconditioned under the direct supervision of the Board was 212, the balance having been dealt with by municipal councils.

All available funds were devoted to reconstruction, the betterment of initially constructed sections, and the improvement of existing surfaces. By this system of progressive or stage construction, the State highways are being gradually raised over their entire length to a standard capable of carrying traffic without hindrance throughout the year.

The making of tests, fixing of standards, and the compilation of systematic data, are material factors in determining the most suitable type of construction. Investigations have been made by the Board with the object of developing types of low-cost roads capable of carrying light forms of traffic at all seasons. Interesting experiments—the first to be tried in this State—were conducted on several highways in the construction of low-cost pavements.

The first on the Western Highway, between Horsham and Dimboola, consisted of the application of fuel oil to local fine sand and clay, to which a coat of bitumen was subsequently applied. The experiment so far has produced satisfactory results, inasmuch as a good riding surface has been obtained; dust has been eliminated during dry weather, and mud prevented during the winter.

On the Calder Highway, between Bendigo and Inglewood, within the shire of Korong, experimental work comprised the construction of pavement similar in type to asphaltic concrete, by mixing the gravel aggregate with tar on the road bed, the mixed material being bladed into shape and consolidated by traffic. The shape was maintained by the use of a grader, and the final surface secured by rolling. As soon as consolidation was obtained the surface was sealed with bitumen. The construction cost was £540 per mile, and the cost of maintenance of the pavement, shouldering, &c., is estimated at £50 per mile. Plate No. 1 shows the pavement in this section of road ready for consolidation and shouldering.

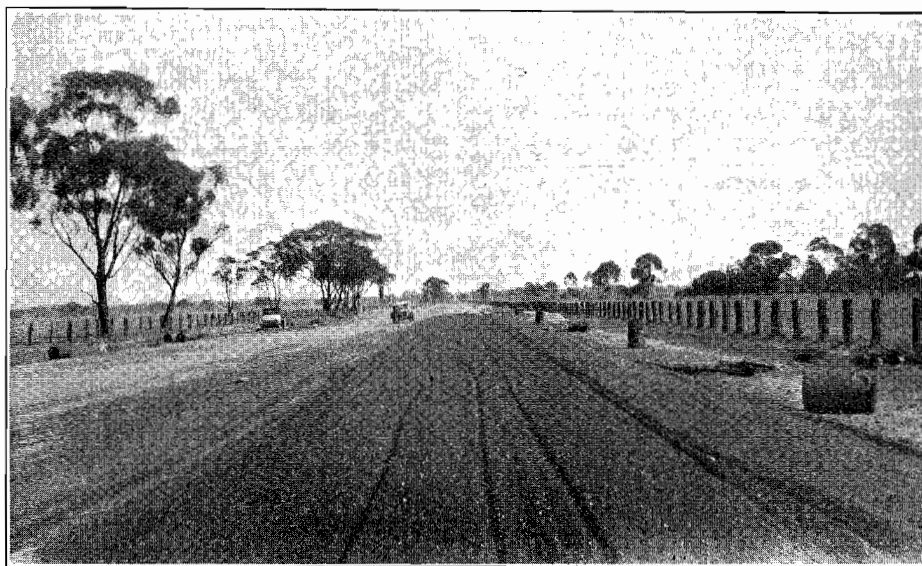


Plate No. 1.—Calder Highway. Mixing completed. Consolidation was carried out with a horse roller, as traffic would not use the pavement sufficiently.

On the western section of the Prince's Highway, between Panmure and Allansford, a test section was laid consisting of a modified macadam surface. The metal spread on a proper water-bound or gravel base was consolidated; an 8-inch wide strip along each edge of the road was then penetrated with bitumen, and $\frac{3}{4}$ -inch screenings applied uniformly over the whole of the pavement. These were rolled into the surface of the road and the voids filled by brooming and watering. After toppings were spread and the voids filled, cold tar was applied and the surface covered with screenings rolled in the pavement. Rolling was carried on for at least seven days, and the surface of the roadway then sealed with bitumen.

The cost of this type of pavement $2\frac{1}{2}$ inches thick and 20 feet wide was £1,580 per mile, and the estimated cost per mile for maintenance £50, including pavement and shouldering.

Apart from the advantage of having a suitable road constructed adequate for the traffic it is called upon to bear, considerable saving will be effected in the cost of future maintenance.

The experiment of using crushed rock for road construction was carried out on the Prince's Highway west, between Portland and Heywood. The material was spread in two even layers to a loose thickness of 3 to 4 inches, the lower coat being consolidated before spreading the top course. The whole was then consolidated by rolling or by traffic alone.

The cost per mile for a 20-foot wide pavement varied from £590 to £2,000 per mile, according to the thickness of the material. The estimated cost of maintenance is £60 per mile.

To date, the results of these investigations and experiments have provided types of roads suitable in every way to carry the traffic they are called upon to bear; they have proved an economical method of construction, capable of supplying a good surface that can be progressively improved as the volume of traffic justifies, and have already supplied a vastly superior road surface, which can be efficiently maintained at a much reduced cost. Careful checks will be made to ascertain the wearing qualities of these sections.

Full details of construction are set out in the Report of the Chief Engineer which is appended.

The far-seeing policy of our early Colonial Government in laying out the important arterial roads to a general width of 3 chains can, in these days of fast-moving traffic, be appreciated, as well as the fact that these highways were suitably located, obviating the necessity for extensive deviations or costly re-location. Where minor re-locations of the highway as originally built have been effected, they have generally been done to rectify alignment, improve the approaches to railway crossings, or reconstruct the roadway on an improved grade at a lower cost.

The reconditioning in gravel of that portion of the Prince's Highway between Moe and Sale has completed another important link. The work done is to a standard that will withstand the traffic for the next two years without the necessity of bitumen surface treatment. Included in this length was the realignment and widening for a distance of 3·8 miles over the Haunted Hills, which previously had to be negotiated with care, even by the experienced motorist.

Between Sale and Stratford a bitumen surface was applied to the highway, and this work is now well advanced towards Bairnsdale, with the prospect of completion during the present financial year.

In order to better serve the traffic using the western section of the Prince's Highway, a deviation was constructed near the Guiding Star Hotel, and the road thrown open to traffic on the 28th February last. The work included a box cutting to a depth of 10 feet, and the laying of a bituminous pavement with a concrete base. This was necessary to meet the demands of increasing traffic, which is of a particularly heavy nature on this portion of the highway, and was the only section constructed with first class paving materials during the period under review. Details of the type of work done are contained in the Chief Engineer's Report.

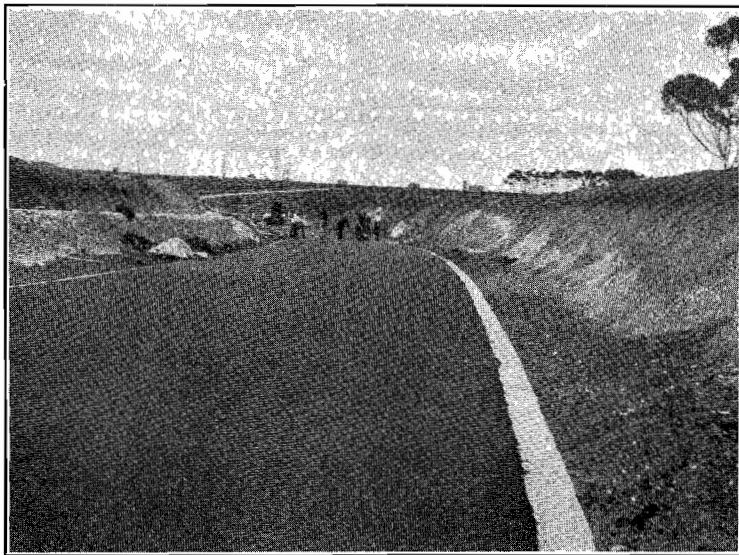


Plate No. 2.—Prince's Highway, deviation near Guiding Star Hotel.

The gradual improvement of the State highway system during the past five years, which, as already stated, has been accomplished by the expenditure of funds provided out of revenue, and from the Federal Aid Roads Grant, is now advanced to such a stage that the early completion of the system as a whole is not far distant. Even at this date, State highways have been improved to a standard that provides generally a good surface road all the year round. In 1925, rough or impassable sections of declared highways totalled 1,284 miles, or 85 per cent. of the entire length, whereas to date traffic can use them during all seasons, rough and unmade sections comprising only 125 miles, or $8\frac{1}{2}$ per cent. of the total. The length of State highways now in good condition totals 1,387 miles.

With the great improvement of the road surfaces, the necessity of coping with increased traffic requirements, and the fact that a number of bridges and culverts were either no longer fit, or wide enough to carry the traffic, or had outlived their usefulness, it became necessary to erect new structures or replace the existing ones on several of the highways at a total cost of £49,184.

The more important of these include a reinforced concrete bridge over the Burrumbeet Creek, on the Western Highway, 9 miles from Ballarat. The structure, consisting of four spans of a total length of $127\frac{3}{4}$ feet, with a width of 22 feet between kerbs, was erected under contract at a cost of £2,349. (See Plate No. 3).

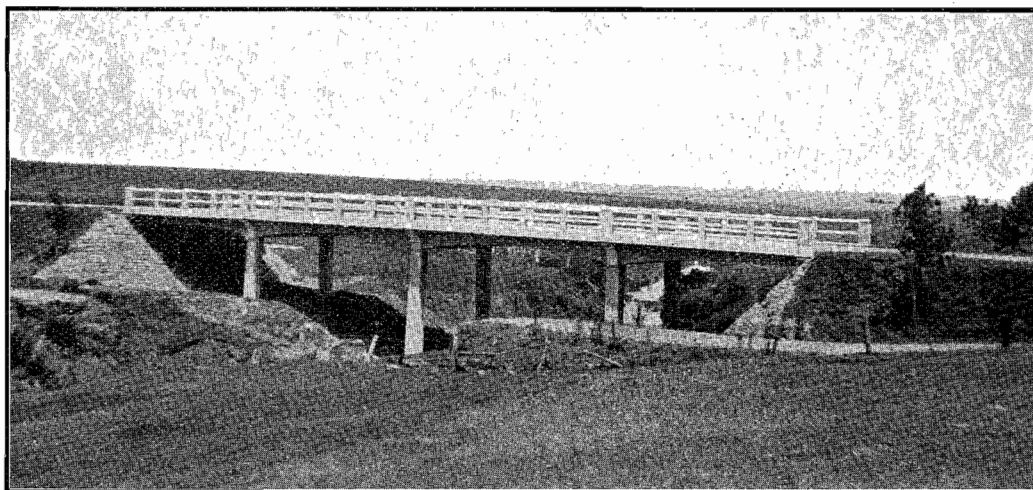


Plate No. 3.—Western Highway. Bridge over Burrumbeet Creek.

On the section of the Western Highway between Stawell and Horsham, known as Dads-well's, three new timber bridges were erected to replace worn-out timber structures, and two small reinforced concrete bridges were constructed. With the exception of the approaches to the bridges, the gravelling of this length of highway has now been completed, and at an early date a good serviceable road available to traffic at all seasons of the year will have been completed.

The completion of the reinforced concrete bridge at Pyke's Creek Reservoir, near Ballan, forms another important link on the Western Highway. This bridge was erected in substitution of an old timber structure over the reservoir, which it was necessary to replace on account of the raising of the level of the water by the State Rivers and Water Supply Commission.

The new bridge, of which details of construction were given in the Board's last Annual Report, has a total length of 246 feet and a width of 27 feet between kerbs. A roadway 22 feet wide is provided, together with a footway of a width of 5 feet.

The total expenditure to 30th June, 1930, was £13,495 12s. 5d., including the cost of constructing the approaches on an improved alignment.

The completed structure is shown in Plate No. 4.

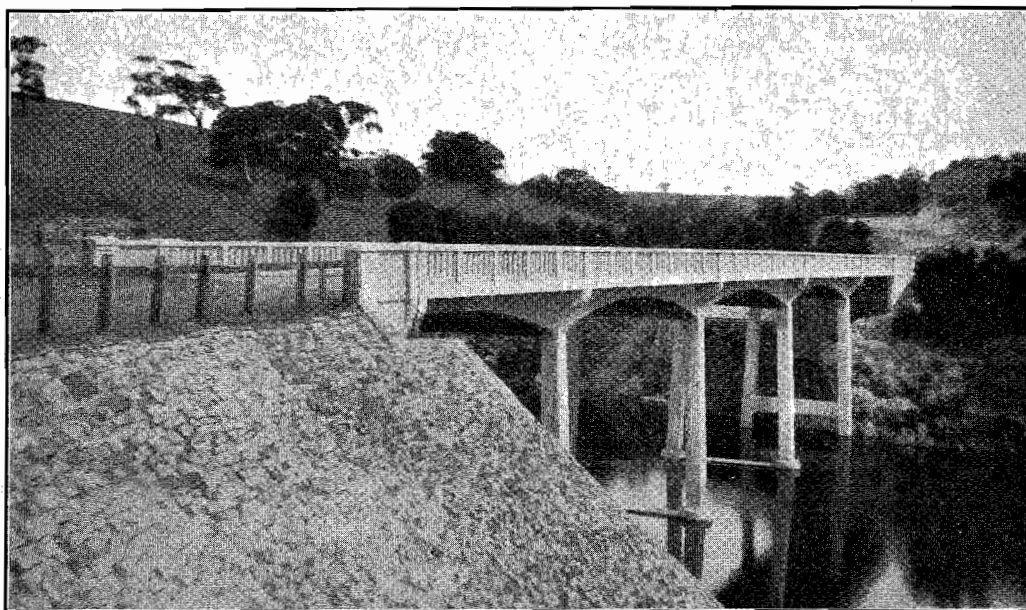


Plate No. 4.—Western Highway. Bridge at Pyke's Creek Reservoir.

Another important highway timber structure completed under contract during the year was that over the Bunyip River, near its junction with the Tarrago River. The new structure replaced two worn out bridges erected many years ago when the two rivers originally joined below the site of the bridge crossing, but owing to scour the river bed is now 20 feet lower than it was at the original level. The cost of this bridge was £2,373.

On the Prince's Highway, 2 miles east of the township of Morwell, a three-span reinforced concrete bridge, 75 feet in length, was erected to replace an old structure in an advanced state of decay. The cost was £1,776 12s. 6d.

Between Traralgon and Rosedale a timber bridge, 61 feet in length, was constructed on a re-located section of roadway at a cost of £859 14s. 4d.

Consequent on the widening of the Prince's Highway between Melbourne and Geelong, it became necessary to extend or reconstruct most of the culverts. No less than 47 structures of this nature, which consist of the pipe or box type, were so dealt with during the year.

To replace the old wooden structure which collapsed in 1925, a new bridge over the Tambo River at Swan Reach was designed. As a preliminary, a contract was let for the erection of the substructure, and this work is now in hand. As soon as the completion of this contract is in sight, another contract will be entered into for the erection of the bridge superstructure, and the construction of the necessary approaches.

The necessity of replacing a number of old timber structures on the Hume Highway involved the construction of 40 pipe culverts from 1 foot to 4 feet diameter, and thirteen reinforced concrete box culverts. The work was done by contract, with the exception of three box culverts, which were constructed by the Board's expert gang.

MAIN ROADS.

During the past year 173·68 miles of new construction works were added to those completed in previous years. The expenditure involved, including the Commonwealth contribution of £6,790 14s. 2d., amounted to £189,416 9s. 1d., of which £177,138 10s. 2d. was spent by municipal councils in constructing 130·82 miles of roads and £12,277 18s. 11d., represented the expenditure by the Board on works under its direct supervision, including twelve bridges with approaches.

Compared with that of previous years, expenditure on construction was much lower on account of the curtailment of Loan Funds. The expenditure was, therefore, confined to the carrying out of works considered to be of the most urgent character, such as the replacement of old bridges, or the erection of new ones on newly constructed roads, the linking up of gravelled or metalled road sections, or the extension of construction works where necessary to serve settlement.

The need for greater mileages of constructed roads has impressed the Board with the necessity of adopting as a general practice the low-cost surface type for country roads. By this method, an economical policy of construction is laid down, under which the initial cost is not too great, the cost of upkeep is not excessive, and a road surface is procured, capable of being improved from time to time with higher type materials, as the requirements of traffic dictate.

The reconditioning of the Geelong-Queenscliff-road, for a distance of 8 miles, to which reference was made in the last Annual Report of the Board, is typical of the class of work above referred to, and has proved satisfactory in all respects for the traffic using it. During the last summer, the surface was resealed as the final stage of reconstruction. It is anticipated that the cost of maintenance as compared with the cost of similar work prior to reconstruction, will be considerably reduced.

The formation and gravelling of sections of the Yarra Glen-Glenburn-road between Yarra Glen and Steele's Creek, and from West's to the northern side of Mount Slide, which was completed under the Board's direct supervision at a cost of £34,433 has provided a suitable road for the use of settlers on both sides of the Dividing Range, by which they will be enabled to cart their produce to the railway and the Melbourne markets by an all-weather road. This road forms an important connexion between the Upper Goulburn River Valley at Yea and Melbourne.

The construction in sealed water-bound macadam of nearly 4 miles of the road between Whittlesea and Kinglake has given the settlers in the district an excellent road to the railway system. This work involved an expenditure of £9,530.

Of 5,692 miles of declared main roads, surveys were made by municipal councils for a total length of 148·46 miles, plans prepared for 147·08 miles, and contracts entered into for forming, gravelling, or metalling, 142·94, as against 173·92 miles for the previous year. The decrease in the last mentioned figures is accounted for by the necessity of curtailing expenditure owing to shortage of Loan Funds.

A summarized statement of expenditure on construction and maintenance is contained in Appendix D.

In the maintenance of the highways and main roads, the patrolman plays a very important part. Efficient maintenance can only be expected from the patrolman who is capable of adapting himself to circumstances and intuitively doing what is required at the proper time. This particularly applies to gravelled roads, on which attention to drainage is of importance to prevent the formation of ruts and disintegration, and where dragging of the surface must be carried out at suitable times.

The necessity of patrol maintenance is being more generally realized by shire councils, and it is gratifying to report that councils have in most instances adopted and extended the system on the lines laid down by the Board. The results indicate that main roads under the care of municipalities, maintained by patrolmen, are kept in good condition, and that councils are standing up to their responsibilities in efficiently maintaining the main roads up to the limit of their financial ability.

Roads constructed to a proper standard and efficiently maintained have an important bearing on the number of motor cars in use, inasmuch as they further the demand for additional roads, which involves increased responsibilities for maintenance. On the other hand, the adequately maintained road plays an important part in relation to the running cost of the motor vehicle, its depreciation, and the wear and tear on its tyres.

For the year ended 30th June last, the Board was in a position to allot the whole of the amounts which municipalities asked for as being necessary for the maintenance of declared main roads. Of the total sum of £804,178 estimated by municipalities as being required, £680,177 3s. 9d. was expended, which is the largest amount expended under this heading in any one year. The increase over last year's expenditure on maintenance is £170,089.

The necessity for the greater expenditure is due to the need of meeting the increased demands of traffic on longer lengths of constructed roads. It is felt, however, that where main roads traversing municipal territory carry a big proportion of traffic not of local origin, the municipality is entitled to greater relief in the payment of its contribution towards the cost. Such relief has been given by the Board as far as funds will permit, and this practice will be extended from time to time as the Board's financial position will allow.

During the last financial year, a total sum of £17,392 was expended in erecting new bridges on main roads to replace worn out structures.

The bridge over the Loddon River at Eddington on the Main-road between Bendigo and Maryborough, which is the most important of the new structures, and the longest reinforced concrete bridge ever constructed in this State, consists of fifteen spans of reinforced concrete, each 45-ft. 4-in. in length, the total length of the bridge, therefore, being 680 feet. The total cost of the work, including approaches, was £16,353 10s. 6d.

Details of construction are shown in the appended Report of the Chief Engineer.

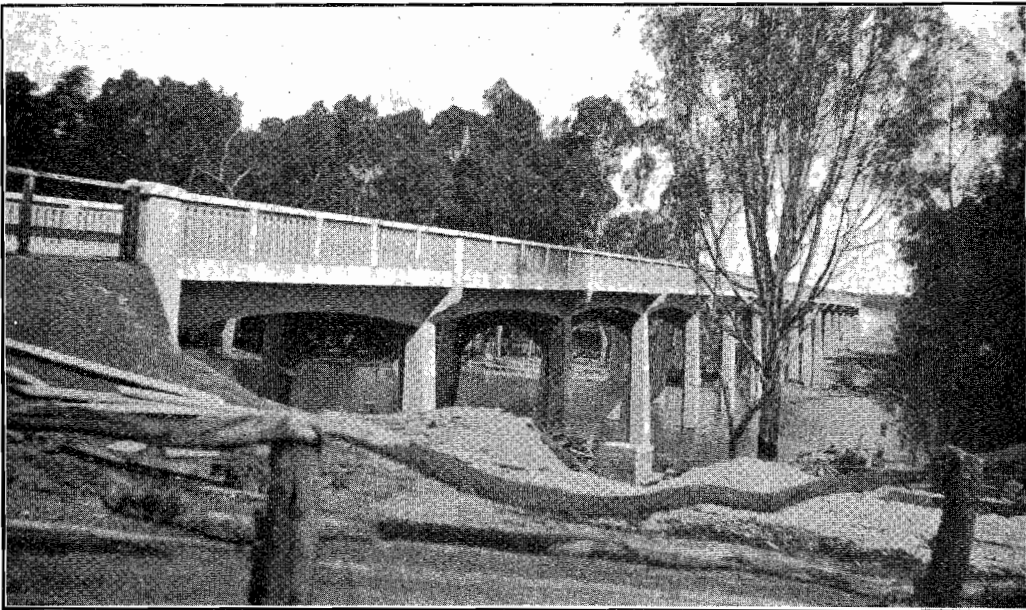


Plate No. 5.—Reinforced Concrete Bridge over Loddon River at Eddington.

A reinforced concrete bridge built in the township of Pyalong, over Mollison's Creek, on the Bendigo-Heathcote-Kilmore-road, replaced a timber structure erected 70 years ago. The new bridge has a length of 180 feet, comprising five reinforced concrete spans, 20 feet wide between kerbs. It was built by contract at a cost of £3,721 9s. Particulars are set out in the Chief Engineer's Report.

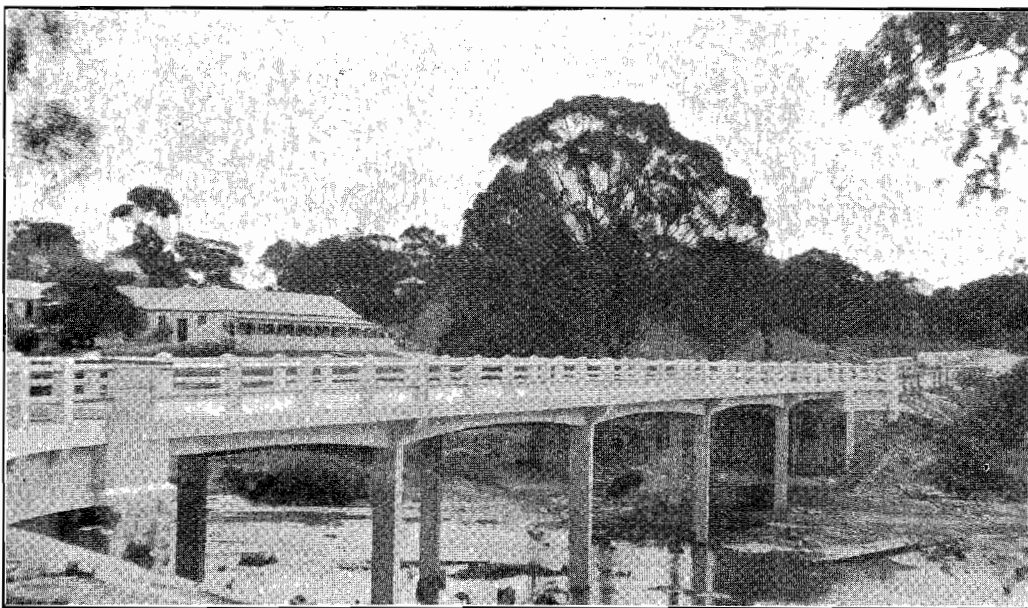


Plate No. 6.—Reinforced Concrete Bridge at Pyalong.

DEVELOPMENTAL ROADS.

The *Country Roads Act 1928* defines a developmental road as one which is considered by the Board to be of sufficient importance and will serve to develop an area of land by providing access to a railway station or to a main road leading to a railway station.

Many roads of this description which have been declared by the Board with a view to their construction have not yet been completed, and on that account settlers experience great difficulties in traversing the unconstructed sections during the winter months. For this reason, the Board is endeavouring to conserve as much of its Loan Funds as possible in completing these roads, and extending them into settled areas.

It is obvious that, unless these roads are serviceable throughout the year, the settler is greatly handicapped in transporting his produce to the railway, just as much as is the farmer who lives on a good trafficable road which connects with an unconstructed highway or main road leading to the railway system. As already stated, the State highways of this State are now within measurable distance of completion, and the construction and improvement of developmental roads tributary to the main system and serving the primary producer are of the utmost importance in giving access to producing areas and providing means of communication between those areas and the nearest railway or market town.

With the decreased value of primary produce, it is now more essential to the producer than ever before that reasonable road facilities be available under all weather conditions, thereby reducing the cost of transport.

Whilst considerable improvements were effected under the construction programme for last year, it is unfortunate that curtailment of expenditure, with the consequent diminution in the work, had to be resorted to. Under normal conditions, an amount of £952,584, less discounts, would have been available for expenditure on developmental roads, but for the reasons stated it was necessary to reduce the State expenditure to £452,917 14s. 8d., which is £78,243 less than the amount expended during the previous year.

The mileage of developmental roads initially constructed or completed by shire councils during last year was 266·2, whilst 190·58 miles were dealt with under the Board's direct supervision.

A large part of the work accomplished comprised the construction of roads in undeveloped areas, and in the north-western Mallee, in order to give improved facilities direct to the railway or to the State highways or main roads leading to the rail head.

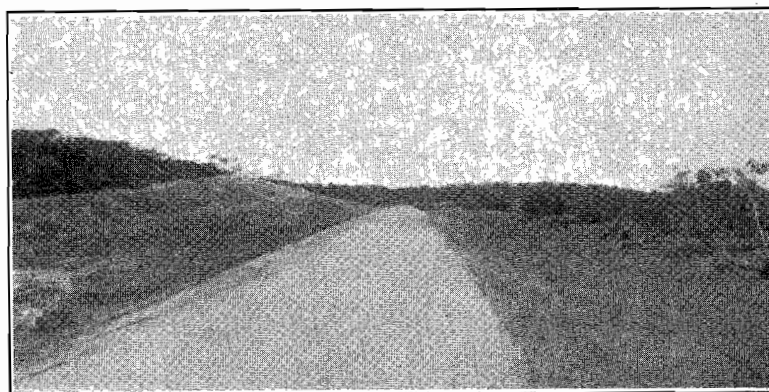


Plate No. 7.—Showing limestone rubble road constructed near Hattah.

In view of the drought conditions existing in the Mallee, special efforts were made to provide suitable work for the settlers by putting in hand as much road work as possible, with the result that an appreciable amount of work was carried out.

Of the works constructed under the Board's direct supervision may be mentioned the gravelling of the Main Ridge-road within the shire of Alberton, from the Tarra Valley to the junction of Whitelaw's Track with the Jeeralang West-road, covering a distance of nearly 4 miles. This was the only unsurfaced portion between the township of Mirboo North and Carrajung, serving an area eminently suitable for dairying and root crops, and forming another important link in the now completed road system between the main Gippsland and the South Gippsland railway lines.

The sanding and metalling of 7 miles of the Morwell River-road within the Morwell shire completes the whole of the surfacing of this road, thereby giving an excellent road into a valuable dairying district.

Work is at present proceeding by day labour on the eastern branch of the Morwell River, and when completed will be the means of opening up and providing access for existing settlement, eventually connecting with the Boolarra-Welshpool-road.

On the Callignee Estate, recently purchased by the Closer Settlement Board, roads were formed during the year for a distance of 5·10 miles, leaving only 2 miles to be constructed to the junction of the Traralgon-Yarram-road, near Balook.

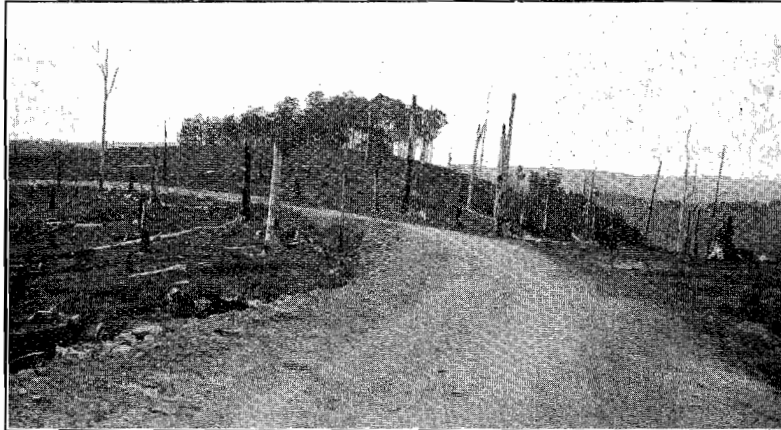


Plate No. 8.—Newly constructed road in Callignee Estate.

A number of roads, originally constructed as developmental, have had the effect of attracting other than local traffic, and, therefore, assumed sufficient importance to be classified as main roads. The maintenance of roads under this category is a serious problem for the municipalities, and to assist shire councils, the Governor in Council, on the recommendation of the Board, approved during the twelve months of 58·5 miles of roads of this character being declared main roads, thereby allowing the Board to share in the maintenance to the extent of two-thirds of the cost. It is proposed to follow this practice as and when additional funds become available.

As a general principle, developmental roads are being constructed with the low-cost materials, such as suitable sand-gravel or coarse gravel obtainable in the district, these materials being considered adequate for the average traffic conditions on these roads. The Board has, therefore, concentrated on the expenditure of available funds in constructing a greater mileage of roads of the light type rather than a short mileage of higher class surfaces.

The cost of these light pavements ranges from £500 to £1,500 per mile, according to the locality of the work, not including new bridges and bitumen surfacing. The average cost per mile of developmental road works (including formation, rock excavation and pavements), carried out last year by shire councils and directly by the Board, was £1,897, excluding bridges.

With the low-cost road, the municipal contribution towards interest on the capital is reduced to a minimum. On the other hand, the increased cost of a road constructed with a higher type pavement would involve the payment of a larger contribution by the municipality. Provided, however, intelligent methods and continuous and a well-organized system is applied in the maintenance of the low-cost road, such a road will be found to be economical and serviceable.

Thus, a gravelled road, costing £1,000 per mile to construct, would cost approximately £30 per mile per annum to maintain. The municipal contribution towards interest on the cost at 2 per cent., the average rate required to be contributed by shire councils under the provisions of the Country Roads Act, would amount to £20 per annum. The total sum payable by the municipality would be, therefore, £50 per mile per annum.

A road of a higher type, such as bitumen penetration, costing, say, £4,000 per mile, would require maintenance at approximately £20 per mile per annum. The municipal contribution towards interest, averaging 2 per cent. per annum, would be £80, so that the total financial responsibility of the municipality would be £100 per mile per annum.

As traffic increases on a gravelled road, surfacing with bituminous material will provide a pavement capable of carrying additional traffic, with a decrease in the total annual maintenance charges. For this reason, roads of this type are of special advantage to shire councils who are solely responsible for the maintenance of constructed developmental roads.

In the construction of a number of new bridges and culverts on developmental roads, the sum of £19,579 was expended during the financial period. These structures embraced several bridges up to a total length of 50 feet, constructed in accordance with the Board's standards.

FEDERAL AID ROADS GRANT.

The Federal Aid Roads Act, which has operated in this State as from the 1st July, 1926, provided that the Commonwealth Government shall in each year during the period of ten years, pay to the States a total amount of £2,000,000 per annum, for the construction of roads classified as Main Developmental Roads, Trunk Roads, and Arterial Roads, on condition that the States provide an amount equivalent to 15s. for each £1 contributed by the Commonwealth Government, the amount referred to being apportioned to each State on the basis of area and population. No portion of this money was to be expended on maintenance of roads, for which the State was to be solely responsible.

At a meeting of Federal and State Ministers, held at Canberra in February last, the heavy responsibilities placed upon the States of maintaining Federal aid roads, as well as making provision for the contribution of 15s. for every £1 of Commonwealth money, was discussed, with the result that it was decided to remove the restrictions on the use of funds provided by the Commonwealth Government by allowing the money to be used either for construction or maintenance, without any obligation on the part of the State to provide its quota.

The effect of this decision as far as Victoria is concerned is that the State will not now be called upon to provide £270,000 per annum against the amount of £360,000 provided each year by the Federal Government, and the State will now be free to use the Commonwealth funds on construction, reconstruction, or maintenance, as considered necessary by the State. With these concessions, part of the difficulty of maintaining roads, especially those carrying through traffic, which might legitimately be classified as State highways, will be met, additional funds will be available for the maintenance of roads already constructed, and the system of Federal assistance will become more flexible.

The expenditure under the Federal Aid Roads Act for the past year amounted to £693,578 14s. 4d., including the State's contribution of £297,248 0s. 5d. The amount expended on works under the Board's direct supervision was £519,172 6s., whilst shire councils supervised works totalling £174,406 8s. 4d.

An amount of £7,333 14s. 5d., which was paid by the State into the Sinking Fund on the amount of loan moneys included in the contribution by the State, was expended on road construction works. This was done in accordance with the decision of the Federal Aid Roads Board that, in the event of any State having provided for the redemption of its loans by establishing its own Sinking Fund, such State should pay into a separate account an amount equivalent to 15s. for every £1 paid by the Commonwealth into the National Debt Sinking Fund, and that the accumulated amount so contributed by the State should be expended on road construction works. The amount paid by the State into the separate account in connexion with the making of surveys, preparation of plans and supervision of works, was dealt with in a similar manner. The total additional amount thus expended amounted to £17,136 18s. 4d., which was utilized in the extension of developmental work on the Orbost-Delegate-road, Buchan-Ensay-road, and the road between Benambra and Corryong.

Reference to the statement of accounts will show that a total amount of £693,578 14s. 4d. was expended on Federal aid roads for the twelve months, of which £158,138 16s. 8d. was spent on main roads, £11,883 14s. 9d. on trunk roads, and £523,556 2s. 11d. on arterial roads. Main roads to the number of 37 shared in the full expenditure, the works being situated in various parts of the State.

The definition applied to a main road under the Federal Aid Roads Act is one which opens up and develops new country, and in this respect the Board has no hesitation in affirming that on completion of these roads, not only will new country be developed and further settlement encouraged, but opportunities will be made available to the present settlers to market their produce by giving improved facilities to convey it to the railway or the market.

Keeping in mind the necessity of constructing greater lengths of developmental roads in rural areas, rather than a short mileage of expensive work, the Board has confined itself to construction by the low-cost methods with the use of local materials. Consequently all constructed roads enumerated in the statement were built in waterbound macadam or gravel, and where traffic warranted, the surfaces were treated with bitumen.

Of the trunk roads, the Casterton-Mount Gambier-road in the shire of Glenelg was formed and metalled for a distance of 7,500 feet under the supervision of the municipal council. A further length of 15,000 feet of gravelling was completed under the Dunmunkle shire's supervision on the Horsham-Murtoa-Minyip-road, and 16,977 feet of gravel construction was completed on the Marnoo-Donald-road, a section of the work being supervised by the shire of Dunmunkle, and the remainder by the Stawell shire council. The expenditure on Trunk Roads amounted to £11,883 14s. 9d.

The work done on arterial roads, which are designed to carry concentrated traffic from developmental, main, trunk, and other roads, consisted of widening and reconditioning to meet traffic demands. With the expenditure of Federal aid and State funds for the twelve months ended 30th June, the completion of the State highway system to a suitable standard is now within sight.

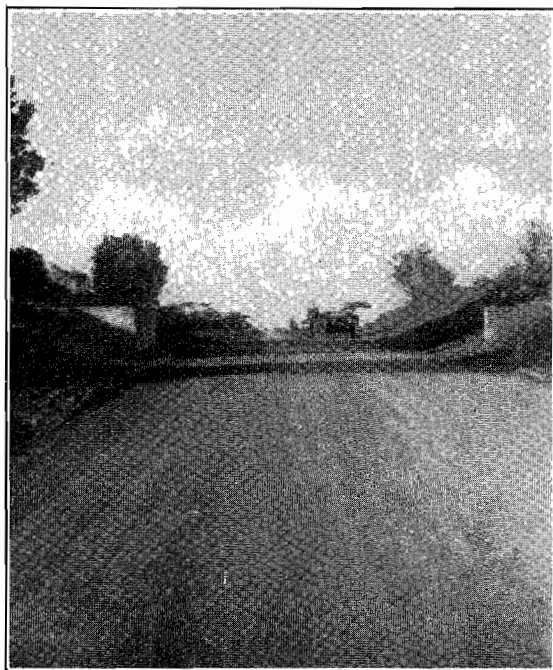


Plate No. 9.—Calder Highway. Deviation between Red Cliffs and Irymple. Rubble foundation ready to receive top-course metal.

Last year's operations added the following mileage to Federal aid roads constructed in previous years:—

Cleared and formed	..	..	..	30·1 miles
Gravelled	..	..	..	70·3 "
Metalled	..	..	..	34·4 "
Bitumen surfaced	..	..	..	5 "

FEDERAL UNEMPLOYMENT RELIEF GRANT.

For the purpose of putting in hand works for the relief of unemployment, a conference of State and Federal Ministers held at Canberra on the 9th December last unanimously decided that—

- (1) £1,000,000 of accumulated balances of road moneys (provided under the Federal Aid Roads Act) be made available to the States for expenditure on roads of the classes defined by the Federal Aid Roads Agreement.
- (2) That any State may adjust its works programme for the current year so as to make the best use of its total funds to relieve unemployment.
- (3) That to give effect to this decision, the Commonwealth will interpret the Agreement as favorably as possible, so that further roads which are eligible for inclusion in the scheme may be added to the programme of roads already laid down, and money properly expended on any such road since the commencement of the Agreement may form part of the State's quota of 15s.
- (4) The Commonwealth undertakes to continue the Agreement until £1,000,000 has been added to the total fund, so as to ensure that the programme laid down will be carried out.

Under this scheme, the total amount allotted to the State of Victoria was £180,000. Works were distributed over 34 roads, as shown on the list included in Appendix B, all roads being developmental in character, with the exception of seven trunk roads which connect important towns.

Whilst recognizing the immediate necessity of providing employment, the Board at the same time took every precaution to ensure the utmost economy in executing the works. Only such works as were considered suitable for day labour were put in hand by that method, and in all other cases contracts were entered into. In selecting the areas to be served by the developmental roads, special consideration was given to the future development of the districts by prospective additional settlement and consequent increased production.

The construction of the Sugarloaf Reservoir by the State Rivers and Water Supply Commission involved the submergence of the road up the Goulburn Valley for about 12 miles from the weir. The location and construction of a new road from Eildon township, in Alexandra shire, to the new bridge over the Goulburn below the junction of the Howqua River in Mansfield shire, was carried out by the Country Roads Board, in conjunction with the shire council, on behalf of the Commission.

The route generally follows the margin of the reservoir, but the Big River, which forms a long arm, is crossed, at a narrow gorge near Darlingford, by a bridge 240 feet long, and an embankment for 450 feet. This crossing enabled the distance to be shortened by $13\frac{1}{2}$ miles, and as it was possible to take advantage of a low stage in the reservoir to build the foundations, the cost of construction was greatly reduced below estimates based on the route around the valley of Big River. The bridge is being constructed by day labour, and will consist of high concrete piers supporting six spans of rolled steel joists, with timber deck.

The new route is $18\frac{1}{2}$ miles long. A length of $5\frac{1}{2}$ miles from Eildon to Wilson's Creek was constructed by day labour at a cost of approximately £10,300, and contracts are in progress over the remaining lengths. Funds for these works were provided from the Federal Unemployment Relief Grant and the State Rivers and Water Supply Commission.

The total amount expended on Federal relief works to the end of the financial year was £106,585 13s. 4d., of which £86,224 16s. 7d. represents expenditure on day labour works, and £20,360 16s. 9d. on works executed under contracts.

Commitments, including liabilities on contracts, &c., as at 30th June, amounted to £73,414 6s. 8d. Employment had been found for 2,291 men to that date.

STATE UNEMPLOYMENT RELIEF WORKS.

In addition to the provision just referred to, the State Government granted an amount of £35,000 for the purpose of putting road works in hand for relieving the acute unemployment position. During the financial period, £33,020 2s. was expended on developmental roads, roads for isolated settlers, and a road in an orchard district. When completed these works will have a beneficial influence on the districts they were designed to serve. The balance was expended on meeting liabilities carried forward from the previous financial year. The list of the roads and summary of expenditure are given in the Appendix B.

Out of the Unemployment Relief Fund, established under the Unemployment Relief Act (No. 3866) passed by Parliament in June last, an amount of £154,300 was allotted to the Board, of which £12,515 6s. was expended to the 30th idem. This expenditure was distributed over eighteen roads, of which nine are developmental and nine main roads carrying traffic not of local origin. (See Appendix B.)

As the developmental works are situated in districts in which the establishment or extension of settlement largely depends upon provision of roads, they will fulfil a very useful and necessary purpose, as well as having provided employment for a number of men. It is already apparent that production has been stimulated in many of the areas served by roads constructed under this scheme.

On the main roads, which link important towns, it will now be possible to effect much needed improvements in the way of new construction and renewals, which for lack of funds could not be done either by the Board or the councils concerned for some time. These works will, therefore, be of considerable advantage to the municipalities in not only assisting them in reconstructing the roads, but in effecting improvements that will result in a free interchange of traffic between important towns.

MIGRATION GRANT.

On the roads under construction in the Millewa area, it was not possible to make much progress during the year owing to the prolonged drought; consequently many of the roads which under normal conditions would have been completed at an earlier date, could not be finalized.

The whole of the roads in this area, which run at right angles to the Red Cliffs to Merringur railway, are essential to the success of the extensive new wheat growing district west of Mildura. Although their construction has not yet been completed, the work done will enable wheat to be transported to the railway without difficulty.

The amount expended was £13,830 4s. 4d., of which £3,457 11s. 1d. was paid out of Developmental Loan Account,

In the Childers Settlement, a sum of £1,554 17s. was spent in sanding lengths of formed roads in the southern portion of the settlement.

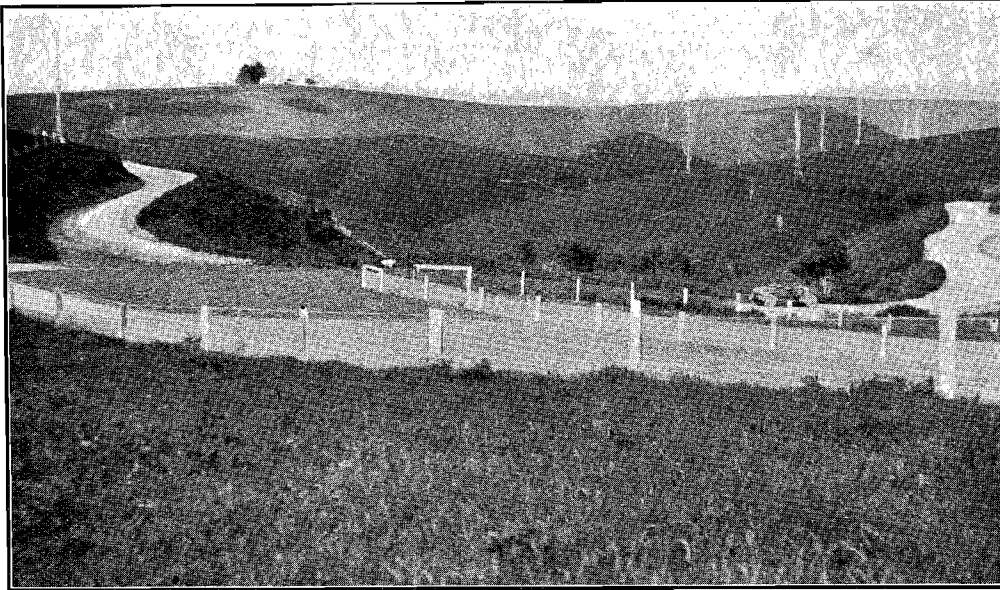


Plate No. 10.—Showing roads constructed in Childers Settlement.

GREAT OCEAN ROAD.

The work of constructing the above road has been continued under the supervision of the Country Roads Board, on behalf of the Great Ocean Road Trust. The completed work now extends from Anglesea to Lorne, for a distance of 19 miles, and from Lorne as far as the Wye River, a further distance of 11 miles.

Up to date a sum of £25,000 contributed by the public has been expended, practically the whole of it in labour, thereby providing employment for a large number of returned soldiers.

The work done is now within a mile of being linked up with the developmental road constructed by the Board from Apollo Bay to the Jamieson River. A further amount of £2,000 is required to make the short connexion between Mount Defiance and the Jamieson River, and so complete the through road between Melbourne and Apollo Bay.

The road will not only be of great advantage to contiguous settlements and to considerable areas of unalienated Crown lands suitable for settlement, but will open up unsurpassed coastal and mountain scenery, and provide tourist attractions second to none in the State.

PROTECTION OF ROADS.

The policy of building lightly constructed roads suitable for present day needs has thrown upon the Board and the municipalities the responsibility of protecting these roads against damage by occasional heavy loads and concentrated traffic, especially during the winter months.

The Board has, therefore, been compelled, in collaboration with municipal councils, to use its power under the Country Roads Act by closing a number of main roads during wet periods against certain classes of traffic, but in nearly all cases permits were issued to carry restricted loads under conditions that would safeguard the road against excessive damage.

On lightly constructed or partially completed sections of State highways, it was necessary for the Board to use its statutory power by prohibiting the use thereon of motor vehicles which, with the load, exceeded 5 tons in weight. Similar action was taken in the case of several main roads which were serviceable for local traffic during dry weather.

These precautions were taken for the protection of the roads against damage by heavy loads in the general interests of the road user.

Some idea of the serious effect on roads that may be occasioned by excessive loads may be gathered from the illustration in Plate No. 11, which depicts extensive damage to a formed section of the main road between Minyip and Warracknabeal, in the shire of Borung, caused by the unscrupulous driving of a heavy motor truck over the road during wet weather.



Plate No. 11.—Showing damage on Main Road, between Minyip and Warracknabeal, caused by excessive loads.

REGULATION OF ADVERTISING HOARDINGS.

In the Board's Report for the year ended 30th June, 1929, it was mentioned that the decision of the State Full Court was then awaited on an order *nisi* taken out by an advertising firm to show cause why the by-law made by the Board dealing with the erection of advertising hoardings and the exhibition of advertisements on or in the vicinity of any State highway should not be quashed wholly or in part.

The Full Court having quashed the by-law, an appeal against the decision was made to the High Court, which resulted in the by-law being declared valid.

In administering the by-law, the Board in the first place gave special attention to hoardings erected at dangerous points, and many were removed. Owing, however, to the validity of the by-law being tested in the Courts, action was suspended pending the decision of the High Court, but it was found that in the interim a number of new hoardings were erected in conspicuous places in the vicinity of the highways.

Following the High Court judgment, fresh notices were served on the owners of advertising hoardings erected in rural districts on land abutting State highways outside populous areas, requiring the removal of the boards by the 31st December next. This, it is considered, will allow of reasonable time for the advertisers to make the necessary arrangements.

Hoardings and advertisements are designedly erected where they will attract most attention. They therefore tend to distract the attention of the motor driver when he should concentrate on the road and the careful driving of his vehicle. In addition, they frequently obscure the landscape or mar the beauty of the surroundings. In most cases, the advertisements, which are not asked for nor required by the public, are a repetition of what has already been properly displayed through the medium of the press or other agencies. The attached plate showing hoardings erected on the Prince's Highway illustrates how the landscape has been obscured.

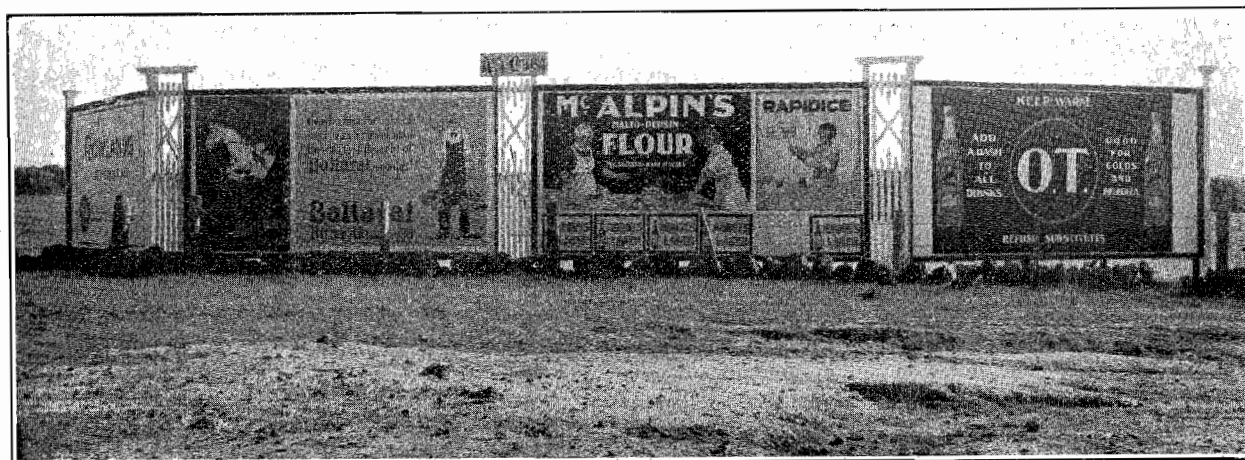


Plate No. 12.—Showing hoardings on Prince's Highway West.

Many advertisers have strongly supported the Board in carrying out the administration of the by-law, and have expressed their approval of the uniform action being taken for the removal of all forms of highway advertisements, thereby placing them and their competitors on the same footing, and eliminating unnecessary expense. A recent complaint was received that advertising hoardings were being used as targets by persons driving along the highway, and in one instance a valuable horse grazing near a hoarding was so badly injured by a shot that it had to be destroyed.

In other countries, the need of reform in the direction of prohibiting unrestrained advertising has long been recognized. In 1907, legislation was passed in England to control the activities of advertisers. The Design and Industries Association of England has recently been exerting its influence by urging, in addition to other reforms, the necessity for a greater measure of control over the activities of out-of-doors advertisers, and by issuing valuable propaganda in support of its plea for the abolition of the practice of conspicuously displaying blatant commercial advertisements. In its *Year-Book* for 1929-30, entitled "The Face of the Land," the Association draws public attention to the necessity of making such radical changes in the present methods as will restore to the country-side its former charm and beauty.

The latest information available indicates that in the United States of America, 42 States have legislation to deal with outside advertisements. In the Annual Report issued by the Bureau of Public Roads for the fiscal year ended 30th June, 1928, it is stated that the disfigurement of the landscape by roadside advertisements is a national disgrace, and it is hoped that means may be found by suitable legislation to effect their complete elimination upon all roads constructed in part with money appropriated by the National Government.

Some time ago, the Board adopted standard warning signs in the form of a triangle fitted with reflectors for erection at dangerous points on the highways, such as at railway crossings, intersecting roads, &c., and a large number of these have been placed in suitable positions. The presence of advertisements near these signs, particularly when placed on fences at railway crossings, constitutes a grave menace to the safety of the travelling public, inasmuch as the advertisement tends to divert the driver's attention from the warning sign, which was erected at public expense for the purpose of cautioning him of the danger ahead.

For this reason, the Board is very definitely opposed to advertisements being exhibited in any form at or near railway crossings, and cannot too strongly emphasize the necessity of removing this source of danger.

TREE PLANTING.

The planting of trees for the improvement of roadsides and beautification of the landscape has been, during the past few years, enthusiastically taken up by a number of Shire Councils, Progress Associations, and other bodies.

The Calder Memorial Avenue Committee was appointed more than two years ago for the purpose of carrying out a scheme of tree-planting on the Melbourne-Geelong section of the Prince's Highway, to perpetuate the memory of the late Mr. William Calder, the first Chairman of this Board.

To the 30th June last, 3,000 permanent trees and 60,000 "cover" trees were planted under expert supervision, the cost of planting being met by subscriptions from the public, whilst a large number of trees were donated by the Nurserymen and Seedsmen's Association of Victoria. Since that date, the plantations have been extended as far as the Little River Bridge, and the planting of the section between the railway crossing at 17.3 miles and the Werribee township has been completed with funds provided by the Werribee Shire Council. The completion of the project will, it is considered, provide, within the next few years, an avenue second to none in the State.

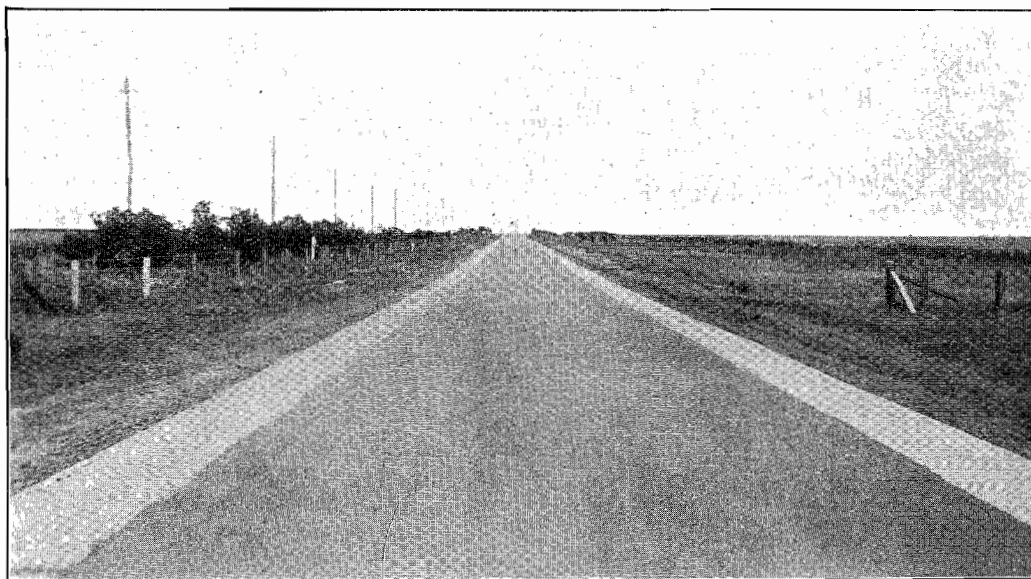


Plate No. 13.—Prince's Highway West, showing New Plantations on Calder Memorial Avenue.

The commendable efforts of Cr. James Railton in co-operation with the Gisborne Shire Council, local Progress Association, and the Tree Planters' and the Nurserymen's Associations, in extending the scheme of planting along the Calder Highway on the Melbourne side of the Gisborne township, are worthy of special mention. During the last planting season 500 trees were planted in continuation of the planting done in previous years.

Roadside improvements of this description not only contribute to the pleasure of the travelling public, but will provide suitable shelter in the future and be a decided advantage to adjacent properties.

PAYMENT OF INTEREST AND SINKING FUND ON LOANS.

Prior to the introduction of the Bill passed by Parliament during last session, the Board gave very careful consideration to this matter and recommended that the Country Roads Board Fund should meet the payment of portion of interest and sinking fund payable on the State's proportion of loan moneys expended under the Country Roads Acts.

The principle of meeting the Interest and Sinking Fund payment on loans raised for road construction from the revenue collected for road purposes, without any charge on the Consolidated Revenue of the State, is sound, and has also been adopted in New South Wales, Queensland, and South Australia. It is also advocated in a report issued by the Commonwealth Transport Committee in May, 1929, in which the recommendation is made that main roads and developmental roads should, in future, be financed entirely from the taxation of road users and the contributions of local authorities, that any further loans for the roads financed by State Treasurers be without charge on Consolidated Revenue, and that wherever legislation does not provide for this, consideration be given to the advisability of introducing amending legislation to give effect to this principle.

Act No. 3844 gave effect to the Board's recommendation by providing that, in addition to any moneys required to be paid under the Principal Act into any Sinking Fund and for interest in respect of loans, there may, if the Treasurer thinks fit, be applied in the financial year beginning on the 1st July, 1929, out of the Country Roads Board Fund and paid into Consolidated Revenue to recoup the Consolidated Revenue, such amount as the Treasurer determines, but not exceeding the total amount payable in that year on account of Interest and Sinking Fund, less £50,000.

In accordance with this authority, the sum of £271,473 2s. 6d. was transferred from the Board's Fund in respect of the year ended 30th June, 1930, to recoup Consolidated Revenue.

COSTING OF WORKS.

To be effective, a road-costing system should be so designed that an explicit record is made of quantities and of the cost of each unit. The person responsible for the framing of these records should be familiar with the details of the works proposed to be carried out, and must not lose sight of the fact that the system must be simple, economical in its operation, and suited to the particular works it is intended to record.

In the case of works being performed by day labour, especially those of construction or reconstruction, involving the employment of a number of men and purchasing of materials, it is essential that up-to-date accurate costs and quantity data be kept so that the Supervising Engineer shall be able to compare at any time the costs of the various units of work with his estimates. He will thus be in a position to account for any undue inflation in the cost of any particular item, prevent waste and needless expense, and so exercise a rigid supervision over the work to ensure that on completion the actual cost favorably compares with the approved estimate.

The Board appreciates that many Shire Councils supervising road works carried out with funds provided by it recognize the value and necessity of accurate costing, this fact being revealed in the statements of costs furnished by them to the Board from time to time. It is regretted, however, that others do not appear to sufficiently realize the importance of the matter, and the Board cannot too strongly urge upon those Councils the necessity of recording the cost of works of the character referred to in a suitable form.

So that the Council may be properly informed, a cost statement should be furnished to it by its Engineer at each meeting, and a copy of such statement transmitted to the Board for its information.

A sample cost statement in use by the Board is furnished hereunder.

C.R.B. FORTNIGHTLY COST STATEMENT.

Form 2078.
 Total Estimated Cost .. £746 4 6
 Expenditure to date
 (Final) 684 13 10
 Balance 61 10 8

Total of Inwards Suspense
 Return £684 13 10
 Total of Outwards Suspense
 Return 684 13 10
 Balance
 (Represents Materials, &c., on hand.)

Job No. 553HM 79.
 Statement No., 3 and Final.
 Date Lodged, 5th May, 1930.
 Highway—Hume (Section 3).
 Shire—
 Fortnight Ended, 1st May, 1930.
 Sheet Prepared by J.T.
 Overseer, W.J.

Work Commenced, 24th March, 1930.
 Estimated date of Completion, 24th
 April, 1930.
 In progress, five weeks.

Total Cost to date of
 Unproductive Sub Jobs
 ("U3." excluded) .. £22 0 2
 Percentage of same to
 "Productive" Labour.. 3 per cent.

Sub Job No.	Sub Job.	Unit.	Estimate for Completed Work.			Cost for Fortnight.			Progressive Cost to Date.			Estimated Cost to Date.			Comparison.	
			Quantity or Measurement.	Unit Cost.	Amount.	Quantity or Measurement.	Unit Cost.	Amount.	Quantity or Measurement.	Unit Cost.	Amount.	Quantity or Measurement.	Unit Cost.	Amount.	Gain on Estimate.	Loss on Estimate.
			£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	
G. 1	Quarrying..	c. yd.	968	0 5 0	242 0 0	424½	0 4 9	33 11 0	1059	0 4 6	76 0 0	..	..	..	0 0 1	..
G. 1A	Carting ..	c. yd.	968	0 0 9	36 6 0	424½	0 0 7 6	68 13 11	1059	0 0 8 6	165 19 11	..	..	..	0 1 2	..
G. 2	Spreading ..	c. yd.	968	0 0 9	36 6 0	424½	0 0 8	13 11 8	1059	0 0 8 6	36 4 10	..	..	..	..	..
G. 4	Watering ..	c. yd.	Not provided for			424½	0 0 8	15 16 11	1059	0 4 5	21 13 5	Work	..	..	21 13 5	5
G. 5	Shouldering	100l. ft.	3227	5 0 0	161 7 0	1000	4 0 2	40 2 1	3227	4 19 3	160 1 4	complete	..	..	1 5 8	..
G. 6	Trimming ..	100l. ft.	3227	3 0 0	96 16 3	3227	1 19 0	63 13 9	3227	2 17 6	92 15 4	(See	..	..	4 0 11	..
G. 7	Rolling ..	c. yd.	968	0 2 0	96 16 0	424½	0 0 6	11 2 6	1059	0 1 1	61 1 4	Estimate	746 4 6	..	35 14 8	..
G. 8	Scarifying ..	100l. ft.	3227	3 0 0	96 16 3	600	2 3 8	13 2 2	3227	1 7 5	44 5 3	for	..	..	52 11 0	..
M.	Maintenance	100l. ft.	3227	0 10 0	16 3 0	1500	0 9 2	7 0 0	3227	0 9 6	15 6 7	completed	..	..	0 16 5	..
S.D.	Making Side Drains	Item	Not provided for			Item	..	11 5 10	Item	..	11 5 10	work)	..	..	..	11 5 10
			746 4 6			277 19 10			684 3 10			746 4 6			94 9 11	32 19 3

Supervising Engineer's Report:—Job complete. Work consisted of reforming and gravelling 3,227 lineal feet. Final inspection made 2nd May, 1930. Gain on Estimate—£61 10s. 8d.

H.P.W.,
 District Engineer.

LICENSING OF COUNTRY MOTOR OMNIBUSES.

In the Annual Report of the Board for the year ended the 30th June, 1929, reference was made to the fact that the Government then had under consideration the question of competition with the Railways by motor vehicles carrying less than six passengers.

In December last, legislation was enacted by the passing of the Light Motor Omnibus Act, under which all vehicles carrying less than six passengers at separate and distinct fares must be licensed by the Country Roads Board.

The licence entitles the omnibus to operate anywhere in Victoria, except upon specified State highways, but any vehicle which operated regularly upon these roads during the period of three months ended on the 31st December last, may, on application, be granted a licence to operate also on such highways until 31st December, 1930.

With the obligations now imposed under Part II. of the *Motor Omnibus Act* 1928 and the *Light Motor Omnibus Act* 1929, it can be claimed that the regulation and control of motor vehicles carrying passengers for hire in country districts have been placed on a sound footing. Unnecessary competition has been reduced to a minimum and provision has been made for preventing the overlapping of passenger services.

The following statement indicates the number of licences issued, routes prescribed, &c., from the 1st July, 1929, to the 30th June, 1930:—

				Fees Payable.	
				£	s. d.
Stage Motor Omnibuses—					
Licences issued and renewed	..	216	..	578	8 7
Permits issued	.. ..	11	..	5	10 0
Routes prescribed	.. ..	17	..	..	..
Touring Motor Omnibuses—					
Licences issued and renewed	..	76	..	408	7 0
Permits issued, &c.	.. ..	2	..	1	0 0
Light Motor Omnibuses—					
Licences issued	.. ..	221	..	1,009	4 3
Driver's Licences issued	.. ..	399	..	99	15 0
Total	.. ..		..	£2,102	4 10

For various offences under the Motor Omnibus Acts, proceedings were instituted in 77 cases, fines and costs totalling £587 14s. 8d.

SUMMARY OF EXPENDITURE.

Statement of expenditure on road construction, including expenditure under special appropriations, is submitted below in a summarized form, from which it will be seen that the total for the year was £2,555,221 2s. 5d.

			Under Direct Supervision of the Board.		Under Supervision of Municipalities.		Total.	
	£	s. d.	£	s. d.	£	s. d.	£	s. d.
1. State Highways—								
Construction	2,495	18 10						
Maintenance and reconditioning ..	889,633	4 4						
			745,313	15 11	146,815	7 3	892,129	3 2
2. Main Roads—								
Construction	189,416	9 1						
Maintenance	680,177	3 9						
			74,216	7 11	795,377	4 11	869,593	12 10
3. Developmental Roads—								
Construction	543,282	15 7	127,219	4 0	416,063	11 7	543,282	15 7
4. State Unemployment Relief .. .. .	..	..	40,979	15 6	..	..	40,979	15 6
5. Roads to develop Tourists Resorts ..	..	..	1,179	14 0	..	..	1,179	14 0
6. Roads for Isolated Settlers .. .. .	..	..	39	15 0	1,638	7 0	1,678	2 0
7. Migration Grant, Childers Settlement Road .. .. .	..	..	1,554	17 0	..	..	1,554	17 0
8. Migration Developmental Roads .. ..	..	..	*10,372	13 3	..	..	10,372	13 3
9. Orchardists' Relief .. .. .	..	..	2,877	10 6	..	..	2,877	10 6
10. Great Ocean Road .. .. .	..	..	5,377	6 9	..	..	5,377	6 9
11. Grants to Municipalities, Act 3662 ..	..	..	..	..	43,742	0 0	43,742	0 0
12. Federal Unemployment Relief .. .. .	..	..	103,869	11 1	2,716	2 3	106,585	13 4
13. Federal Trust .. .. .	..	..	17,136	18 4	..	..	17,136	18 4
14. Experimental roads .. .. .	..	..	3,671	5 0	826	12 2	4,497	17 2
15. Bethanga Connexion .. .. .	..	..	11,669	13 4	..	..	11,669	13 4
16. Miscellaneous .. .. .	..	..	2,563	9 8	..	..	2,563	9 8
			1,148,041	17 3	1,407,179	5 2	2,555,221	2 5

* £3,457 11s. 1d. additional expenditure was paid out of Developmental Roads Loan Fund.

Towards the expenditure on the conditioning and reconditioning of State Highways, and for the construction of main and developmental roads, the Commonwealth Government contributed an amount of £396,330 13s. 11d., under the provisions of the Federal Aid Roads Act.

APPORTIONMENT OF COSTS.

In accordance with provisions of Section 28 of the *Country Roads Act* 1928, the cost of permanent works and maintenance for the year ended 30th June, 1929, was apportioned, the amount of £110,978 being charged to municipalities in respect of permanent works expenditure, and £151,887 on account of expenditure on maintenance.

MOTOR REGISTRATION.

Motor cars to the number of 153,856 and motor cycles numbering 25,405 (including renewals in each case) were registered under the Motor Car Act during the year, as compared with 143,407 motor cars and 24,554 motor cycles during the preceding year.

The net revenue received was £1,137,912 15s. 5d.

A comparison of the number of motor cars registered in Victoria during the past five years discloses a very substantial increase during that period. For the year ended the 30th June, 1925, 89,458 cars and cycles were registered, as compared with 179,261 cars and cycles during the financial year ended 30th June, 1930, representing an increase of more than 100 per cent. In 1925, there was one motor car to 18·8 persons of the population, whilst in 1930 the figures show one car to every 9·9 persons.

The increase in the population of the State during the five-year period from 1st January, 1925, was from 1,684,017 to 1,777,065. It is, therefore, obvious that the remarkable increase in the number of motor cars in use in this State is not due to increased population, but rather to the growing popularity of the motor and to the road facilities provided for its use.

We have the honour to be, Sir,

Your obedient servants,

W. T. B. McCORMACK, Chairman.

F. W. FRICKE, Member.

W. L. DALE, Member.

R. F. JANSEN, Secretary.

CHIEF ENGINEER'S REPORT.

Country Roads Board,
Exhibition Buildings,
Melbourne, 18th November, 1930.

The Chairman,

SIR,

I have the honour to submit herewith my report on work carried out under the direct supervision of the Board during the year ended 30th June, 1930. The total expenditure supervised was £1,148,041 17s. 3d.

Unemployment Relief.—The expenditures shown under the headings Unemployment Relief, Federal Trust, Federal Relief, and Orchardists' Relief were all for the relief of unemployment, making a total expenditure for this purpose of £164,861. Except for small sections of reconstruction on main roads practically all this money was expended in the construction or improvement of developmental roads. Already it has been noticed that, following the road construction, new areas of land are being put into production along these works.

Tourist Vote.—A short section of the Ocean Road near Lorne, and the completion of the Mount Victory section of the Hall's Gap-Wartook-road were carried out with funds provided from the Tourist Vote.

Migration Grant.—The Childers Settlement works shown consisted of surfacing with sand some sections of formation at the southern end of the settlement.

Migration Developmental.—The expenditure under this heading was on works in the Millewa Settlement, the roads in all cases serving the Red Cliffs-Meringur railway. The works comprised the practical completion of the surfacing of the sand hills—the most important stage in the construction of these roads—and the forming and surfacing of the worst of the flats. The road construction in this area, although far from complete, has now reached a stage where no serious transport difficulty will be met with in transporting wheat to the railway sidings along the line.

Bethanga Connexion.—This road is being constructed to connect the settlers in the Bethanga district with the new bridge over the Hume reservoir, the construction of which has submerged the roads previously serving this area. Further works in connexion with the project are now in hand, and will progress with the increase in height and corresponding storage area of the reservoir.

Experimental Works.—The main work under this heading was the re-sheeting of the Ballarat-Creswick-road in gravel mixed-in-place on the road with tar. The development of this type of construction will enable use to be made of large supplies of washed mine gravel which is readily available in different parts of the State, but which so far has not been considered suitable for road construction.

Great Ocean Road.—The expenditure on this road has been mainly on the heavy rock section between Lorne and the Wye River, where work has continued on from that done on Mount Defiance last year.

CONSTRUCTION METHODS AND MATERIALS.

Methods of construction have been again modified towards the more general use of low cost methods, using local materials as widely as possible. With experience gained in the past two years particularly, more confidence has been felt in these methods, and experience has again widened the field of choice of materials, so that greater mileages have been constructed than were previously contemplated with the funds available. Fine crushed rock has been widely used in lieu of waterbound macadam, and it has been found very satisfactory, particularly as it prevents the subgrade from working up into the body of the pavement. This frequently occurs with waterbound macadam, and in the past has not only led to failure of the road, but it very considerably reduces its value as a base for building on as traffic increases, even if actual failure is not apparent. The policy of providing a layer of gravel, loam, crushed rock, or similar fine granular material as the initial base for new pavements on any but sound soils has been extended, and in this connexion increased use has been made of the laboratory for soil analysis. The results of these tests have been in many instances as surprising as they have been valuable, and have shown that, even with considerable experience, engineers are apt to be badly misled by the appearance of soils, and an extension of soil analyses as routine tests for new pavement construction is accordingly contemplated.

The commendable activity of local manufacturers in improving the quality of tar products has been watched and assisted, with the result that, by mutual co-operation, a considerable extension of the Board's use of these materials has taken place. The result has been that the demand for tar, which a few years ago could not be sold, is now greater than the production.

STATE HIGHWAYS.

The State highways have now generally reached the stage where they are readily trafficable at all times of the year by all classes of traffic. Reconstruction works now in hand and proposed are not, therefore, expected to increase the capacity of the roads in respect to volume, comfort, speed, or load-carrying ability, but are required for the purpose of making for increased safety, for reducing the cost of maintenance, and for reducing the cost of vehicle operation. The widening, superelevating, and benching of curves, erection of guide posts, and provision of warning signals, for instance, make for greater safety. The widening of pavements to reduce heavy edge maintenance, and the sealing of gravel, crushed rock, and such surfaces to reduce dragging costs and to prevent loss of materials, considerably reduce maintenance costs.

These works, however, are desirable only when the total capital charges do not exceed the reduction in maintenance costs, which include costs of shoulder maintenance, drainage, &c., common to all classes of pavements. This point is frequently lost sight of by advocates of very high class (and high cost) pavements.

Hard, smooth pavements reduce tire wear and petrol consumption, and general wear and tear on the vehicle, and their provision is economical provided again that the saving is worth the initial cost. Here "traffic counts" and "roughometer surveys" must be checked against tables of vehicle operating costs on various types of roads, for which we are indebted to American experimenters. The latter comparisons are only approximate, but are sufficiently accurate when conservatively applied to assist in determining the design of pavements, or desirable improvements.

Many other features than those mentioned are taken into account when considering new improvements, but the general nature of these considerations is as indicated in the foregoing discussion. Some details of interest are given later.

DISTRICTS.

With the extension of work on the highways into the outlying areas the amount of work carried out under the direct control of District Engineers has increased considerably. The repeated changes in design due to experience with low cost roads, made still more apparent the very great value of the District Engineers' co-operation with the Shire Councils. By no other means could effective dissemination of information gained by the Board be passed on rapidly and accurately to Shire Engineers. An engineer who may view a printed circular or specification with conservative suspicion is generally ready to try new methods when verbal explanation and argument can be presented.

SALE DISTRICT.

On the sections of the Prince's Highway East between Moe and Sale, the reconditioning in gravel was completed to such a standard that the pavement can be surface-treated with bitumen without further strengthening, provided that this is not postponed for more than one or two years, according to the thickness and nature of the gravel. The most important part of this work was the reconditioning carried out between Moe and the Traralgon Shire boundary. This included the re-alignment and widening carried out over 3.8 miles on the Haunted Hills, thus eliminating the one section between Melbourne and Lakes Entrance which might be considered dangerous by the inexperienced driver.

During the year the highway was declared along the north side of the railway through the township of Morwell, and the section constructed. This eliminated two dangerous turns over the railway line.

At Flynn's Creek a very dangerous corner was cut out by replacing the old bridge, which was in very bad condition, by a new structure on a new alignment.

The bituminous surface treatment between Sale and Stratford was completed and similar work commenced east of the latter. Owing to the high speed of traffic and the nature of the local gravel, the cost of maintaining the untreated pavement between Stratford and Bairnsdale has been very high and the condition poor in dry weather in spite of the expenditure. The reconditioning of the section was commenced, and funds are available for completing the work and applying a surface treatment during the season 1930-31. The very dangerous "S" bridge on this section has been replaced by a 4-cell culvert with approaches having radii of 1,000'.

East of Bairnsdale a contract has been let for the substructure of the Swan Reach bridge, and a number of dangerous bends have been eliminated by reconstructing the worst section of the road.

The whole of the highway from Moe to the New South Wales border, a distance of 250 miles, is supervised from the district office at Sale, the office also directly supervising the Omeo highway in the Tambo Shire.

Unemployment Relief Works—

Several camps were established for the relief of unemployment, and the works undertaken comprised the following :—

Orbost Shire—

- (1) Deddick River Road, between Bonang and Tubbut. This road will connect with the Orbost-Delegate road at Bonang, and by serving the Tubbut country an area of many thousands of acres of good grazing land will be made accessible. This area has been subdivided, and is well watered by permanent streams, only its isolation preventing it from being developed before this.

The area will be on the average 75 miles from Orbost, which represents a three hours' run in an average car.

- (2) Orbost-Delegate Road.—A length of road has been constructed on the old road, extending north from Scanlon's Creek, along the valley of Martin's Creek for a distance of 5 miles, and north from Bonang over a length of 6 miles, making a total of 11 miles.

At these places the old road was rough and uneven, with steep grades and narrow cuttings, and the bridges and culverts were old and dangerous.

The new work has been completed to the Board's standard, and when the through road is completed will carry increased traffic to the railhead at Orbost.

Tambo Shire—

- (3) On the Buchan-Ensay Road, the work comprised new construction, extending into the Timbarra country, where a large area of good land is awaiting development.

The length of road constructed is 3·82 miles, and the work is to be continued for another 3 or 4 miles.

Omeo Shire—

- (4) Benambra-Corryong Road.—The work commenced at the end of the existing road at King's accommodation house, and extends north along the valley of the Gibbo River, and comprises new formation. The work is extending to a road leading to Corryong on the Upper Murray, and when this connexion is made, it will be a direct benefit to Omeo, Benambra, and Corryong. Benambra at present is virtually a dead end, although it contains many thousands of acres of beautiful country, and Corryong is an exceptionally fine area, so that the possibilities of the through road for developmental purposes and interchange of stock and produce are being looked forward to by the pioneers of this remote area.

BENDIGO DISTRICT.

A year of drought conditions over most of the district militated against both road-making and maintenance where waterbound or earth surfaces were made or kept. Particularly in the north-western Mallee, limestone roads in construction did not get the moisture so necessary for their chemical set, and the maintenance periods of contracts had to be extended until rain had fallen. The effect was felt to a less extent on gravelled roads, but many of the existing roads made of this material disintegrated to a greater or less extent, and high costs were incurred in dragging to keep the material from scattering away.

These conditions accentuated the sand drift trouble in the northern Mallee. The sand was personally observed travelling for half a mile; it is reported credibly to have come for miles. Under these conditions, short of belts of timber for wind breaks, the only palliation of conditions is the provision of hard surfaces kept clear of sand by scooping.

A method of surfacing new to the district has been the "mixed-in-place" tarred gravel surface. It is applicable to any hard-surfaced road as an alternative to penetration macadam. After the existing surface has been made good to cross-section, a suitable clean and well-graded gravel (typical gradings are shown graphically elsewhere) is spread on the road to a depth of 2 or 3 inches, preferably between side boards. Such a material as Duratar No. 0 is poured on the loose gravel in two applications, totalling about 0·7 gallons to the square yard. After each application the loose material is turned over and over on itself, preferably outwards each way from the centre for the first turn, by a mechanical grader until a uniform mixture with the tar is obtained. It is then roughly shaped to cross-section and thrown open to traffic without rolling. The grader is used during this traffic period to keep the shape until the mixture sets.

Then rolling is carried out, taking care that the material is not being pushed before the roller. If this happens, rolling should be discontinued until further setting has occurred. Back-rolling is advisable at intervals until the material is firm. When consolidation has taken place, a bitumen seal is given.

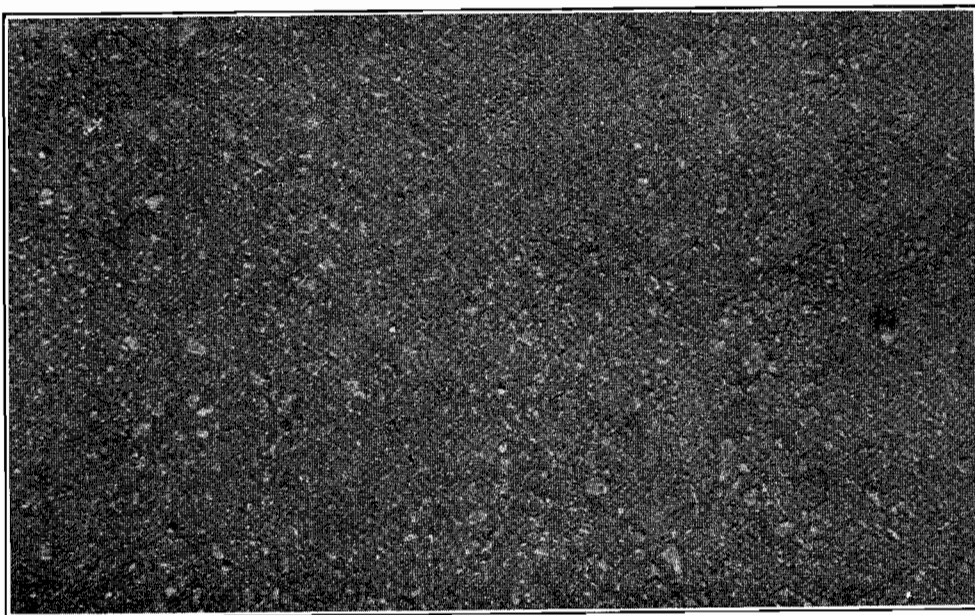


Plate 14.—“Mixed-in-Place” gravel. Surface of Pavement before sealing.

Examples of this class of construction are to be seen on the Calder Highway between Bridgewater and Wedderburn (carried out by the Engineer of Korong Shire) (see Plate No. 14), and on the Castlemaine–Maryborough Road, east of Carisbrook, carried out by the Board. Cost (gravel at 5s. delivered) is about 1s. 3d. per square yard, exclusive of preparation of base and sealing, for 2½-inch loose gravel depth.

The result is a very smooth riding surface. A “roughometer survey” from Melbourne to Sea Lake showed this construction to be at least as smooth as the best penetrated and sealed work south of Castlemaine.

Attention is drawn to the design of earthworks in sandy country in the dry Mallee. The conventional formation designed to keep pavement foundations dry and to shed water is out of place in these conditions, resulting as it does very often in a loose heap of sand, or worse still, of sand and marl lumps. On the Murray Valley Road, a useful practice has been adopted by the Shire Engineers of removing the sand down to the marl or clay which is generally below, and carving out a cross-section in the underlying material with a cross fall from the centre of about 1 in 20 to 1 in 30. The result is cheap, smooth, and reasonably durable, and is very easily maintained by grading off defects of surface.

Materials.

Large supplies of river sand are available at Echuca, and are being utilized for sand clay construction on the Murray River Valley Road.

The use of granite gravel has been extended with satisfactory results in the Kerang and Cohuna Shires, in the former chiefly as a base course.

Owing partly to high freights and partly to the cost of maintenance in dry climates, the use of road-metals for waterbound roads has been almost discontinued, except in the case of the Goulburn Valley shires served by the Dookie Municipal Quarry. The Board's two gravel quarries near Korong Vale and White Hills, Bendigo, have been in active operation. The output of the former has been improved by the lessee to supply the large works on the highway and on main roads north of Charlton. A tender for a five years' lease of White Hills Quarry was accepted at a reduced rate, the Board having acquired a lease from the Railways of the spur siding into the pit. The output of this quarry is also substantially increased.

Highways.

Calder Highway.—Between Harcourt and Castlemaine, works are in progress comprising widening, straightening, and reducing curvature with a tar penetrated road, sealed with bitumen. A contract for the reconstruction of the remainder of the length between Bendigo and Marong is making rapid progress, while towards the end of the year, tenders were invited for the reconstruction from Marong to Bridgewater.

A detailed examination was made of the old timber bridge over the Loddon at Bridgewater for information as to the repairs necessary.

North of Bridgewater through the Korong Shire, the improvement of the surfacing has been continued in long lengths, and is practically completed.

Between Charlton and Wycheproof the 4 miles north of Teddywaddy reported last year as gravelled, was sealed with bitumen, and 7 miles of gravelling further north completed to join with the hard-surfaced road.

North of Wycheproof the surfacing of the road is now let so as to be continuous to Dumosa, making a continuous hard surface from Melbourne, a length of 190 miles. Between there and Sea Lake, limestone pavements, totalling about 27 miles, have been constructed, or are under construction and nearly complete.

North of Sea Lake surfacing is almost completed to Mittyack, with the exception of contracts in progress (about 3 miles long) and of a length near Nandaly Cemetery. In general, it may be said that between Wycheproof and Mittyack there is no serious trouble to be anticipated by traffic, either on account of sand or rain, when existing contracts are completed about the end of 1930.

From Mittyack to Ouyen several long lengths of limestone construction have been made.

Between Ouyen and Mildura continuous progress has been made with surfacing, particularly in the sandy lengths west of Lake Hattah, and in extension and connexion of existing lengths up to Red Cliffs. A deviation was acquired and constructed to eliminate two railway crossings south of Ginquam Avenue, between Red Cliffs and Irymple.

In the length from Mittyack to Mildura, there is now no severe sand with which traffic has to contend, and there is no serious inconvenience in wet weather.

The whole length of the highway has been systematically and continuously maintained.

Northern Highway.—The permanent construction has been considerably advanced by continuing on the banked and gravelled road from Goornong to Avonmore and from Elmore to Rochester in two contracts now practically completed. This ensures an almost continuous hard-surfaced road between Bendigo and Echuca, although portions of the highway, especially beyond Rochester, need attention yet by way of widening and re-sheeting. This highway has been continuously patrolled, together with a long detour from Goornong to Elmore, which was for long distances reformed for the comfort of traffic during the construction of the large contracts.

Murray River Valley Road.—This road in this district extends from Echuca through Cohuna, Kerang, Swan Hill to Lake Powell, thence to Bannerton and Hattah; and again from Mildura to the South Australian border.

A contract has been let west of Echuca for 2 miles of sand-clay and gravelled road; surfacing through the Cohuna Shire is completed, and several extensive contracts have been completed north and south of Kerang. Down river from Swan Hill, the banking and surfacing through the portions below the flood level of the Murray have been continued, and near Swan Hill a certain amount of bitumen treatment (both spraying and semi-penetration) has been given. The natural surface of the road has been improved below Nyah to past Piangil by scooping to the marl and by surfacing. Between Lake Powell and Lake Hattah a commencement has been made with several contracts for clearing and forming.

Downstream from Mildura clearing and forming with some limestoning have been continued, and the road is now fairly good, except immediately after heavy rains, as far at least as Lake Cullulleraine.

Mount Camel Estate Road.—The value of the work done on this route has been greatly increased by the construction of a two-span timber bridge at Lady's Pass, replacing the old ford. Further north, improvements of surface have been continued.

Castlemaine-Maryborough Road.—The Board took over the direct control of this road between Maryborough and Joyce's Creek. Besides maintenance throughout, the portion east of Carisbrook has been given a new base course with a "mixed-in-place" finish not yet completed.

Township Signs, Direction Posts, &c.—At each entrance to townships on the highways these are being erected for the information of travellers, and the erection of signposts is still proceeding.

BENALLA DISTRICT.

Hume Highway. Sections 2 and 3.

Patrol Maintenance.—Patrol maintenance has been carried out continuously throughout the year on the length of approximately 129 miles between Seymour and the Murray River at Wodonga. Twenty (20) patrolmen were employed, each having a horse, dray, and road drag, an extra horse being used in the drag if necessary.

In addition to the regular patrol work numerous repairs to both road and bridges have been carried out by a small day labour gang, and two "Wehr" power graders have also been employed for light scarifying and shaping.

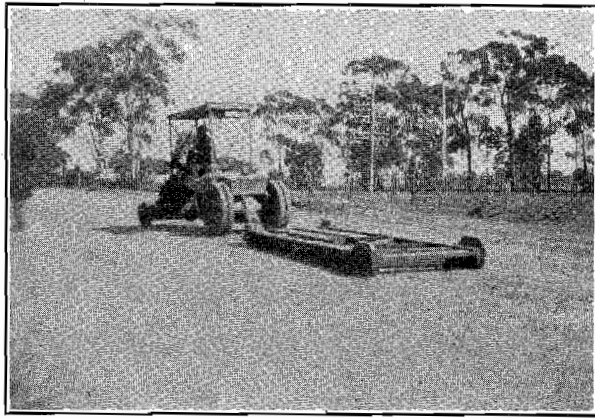


Plate No. 15.—Wehr Grader pulling Road Planer.

By the above methods the Highway within this district has been kept in reasonable order throughout the year.

Reconstruction.—Contracts were let for the reconstruction of the remaining portions of old road between Seymour and Avenel (3.45 miles), including a new deviation to eliminate the railway level crossing north of Seymour. This involves the construction of a large embankment to an overhead bridge constructed by the Railway Department, and will, when completed, be one of the notable improvements to the Highway.

The length of 2.9 miles is also under construction through Old Longwood, the pavement being of granitic sand, while the Avenel Township has been reconstructed with penetration macadam, using a base course of Mangalore gravel, and Broadford stone for the wearing coat.

The section of highway between Melbourne and Euroa has now been reconstructed practically throughout, with the exception of approximately 5 miles from Longwood turnoff to Euroa. The township sections through Euroa and Violet Town have also been re-sheeted in penetration macadam.

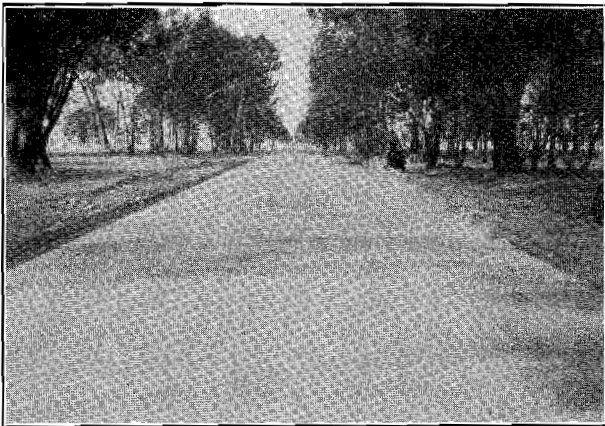


Plate No. 16.—Euroa Township, showing Base (left), Penetration (right).

A rough section of $5\frac{1}{2}$ miles between Winton and Glenrowan was reformed, widened, and treated with granitic sand, and north of Wangaratta a length of 10,000 feet between Bowser and Springhurst was reconstructed by contract. Material for this pavement was Porepunkah gravel, which was supplied by the Board on the roadside. A contract was also let for forming only of the existing tracks between Bowser and Springhurst.

The remainder of the old road between Bowser and Springhurst, consisting of 4 miles, was reformed, widened, and treated with granitic sand by day labour; so that the road between Wangaratta and Springhurst has now been entirely reconstructed. The granitic sand for this work was obtained at Springhurst and has given very satisfactory results on what was one of the roughest sections between Melbourne and Albury.

An important section of the highway between Wodonga and the Murray River has been constructed in penetration macadam with the exception of a short length between the Murray bridge, which has been left until the new bridge has replaced the first existing timber one on the

Victorian side. The construction of this section has provided an impressive entrance to the State, and a new guard fence has been provided throughout, as it is an embanked formation across the Murray Flats.

An experimental section using the "mixed-in-place" method of construction was put down south of Chiltern. The method of construction was to prepare a hill gravel base to template and spread thereon quartz gravel and quartz tailings, obtainable from mine dumps, to a loose depth of approximately 3 inches. Three applications of Duratar No. 0 were applied, using a total of 0.8 gallons per square yard for the three applications. After each application the tar was mixed with the tailings by means of blading from one side of the road to the other with the "Wehr" grader, the material being bladed back to correct longitudinal shape before each application.

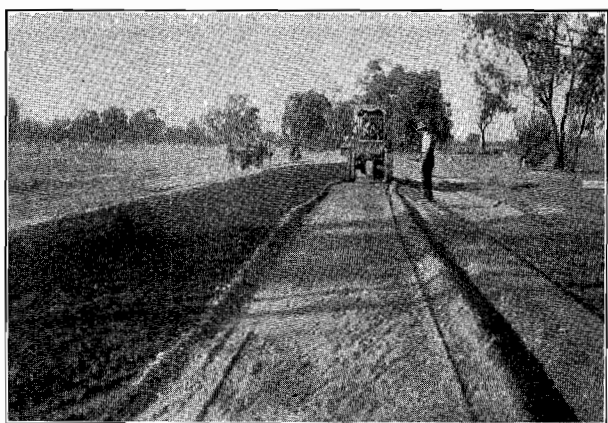
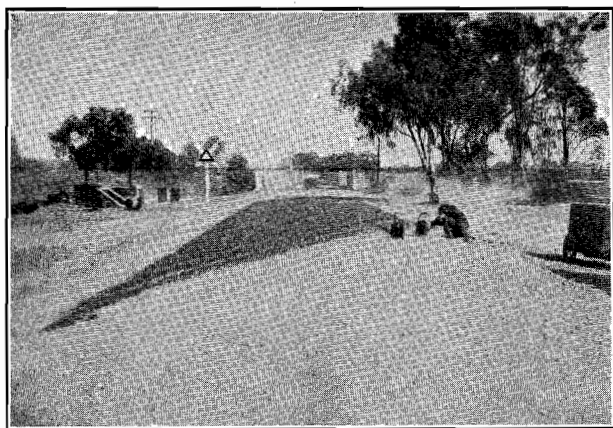


Plate No. 17.

Tar and Gravel being mixed with Grader.

Tar being applied. "Mixed-in-Place" Method

Consolidation after the third application was obtained by allowing traffic over it, the surface being continually dragged, and later a seal coat of 85/100 bitumen was applied and covered with quartz tailings.

Satisfactory results were obtained on this section, which is notable for its good riding quality.

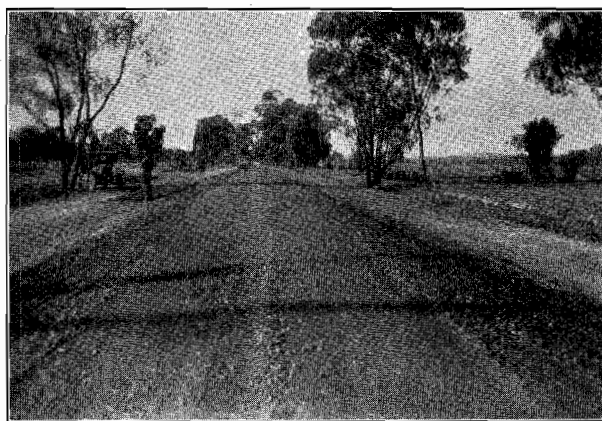


Plate No. 18.—"Mixed-in-Place" Section under Traffic.

Bridges and Culverts.

A considerable number of old timber structures have been replaced with new reinforced concrete box and pipe culverts. Forty pipe culverts, between 12 inches and 4 feet, have been constructed, mostly with concrete head walls, and thirteen box culverts, up to double 8 feet 8 inches x 8 feet 8 inches, have been constructed, together with a reinforced concrete bridge at Mangalore.

Another is in course of construction at the foot of the hill at Old Longwood, and this will eliminate a very narrow existing timber structure.

Three of these box culverts, and the one at present under construction, have been carried out by a small day-labour gang which has been found very satisfactory, as much time and supervision is saved. The price of the work compares favorably with contracts, and the finish obtained is better.

Spraying Operations.

The total length of road sprayed last summer was $24\frac{1}{2}$ miles, 19 of which were on the Hume Highway between Seymour and Longwood, the remainder being between Wangaratta and Springhurst.



Plate No. 19.—Sprayed Road Pavement between Avenel and Longwood, consisting of granite or natural earth selected from borrow pits on the roadside during forming.

The pavements sprayed consisted of Mangalore gravel, granitic sand, and Porepunkah gravel.

Twenty-one and a half ($21\frac{1}{2}$) miles were treated with a primer of cold tar, which was found satisfactory, and a seal coat of bitumen; and three (3) miles were given a seal coat only.

The covering material used between Seymour and Longwood was crushed gravel from the Goulburn River, and quartz tailings from a mine dump in the vicinity of Chiltern were used between Wangaratta and Springhurst.

One of the most difficult parts of this work was the preparation of the gravel surface in order to spray it, and most of it was done with a Fordson roller fitted with a grader blade in front.



Plate No. 20.—Fordson Roller Grader

It is of interest to note that the cold tar primer was left in position for periods varying from a few days to two months before the seal coat was applied.

The above distance sprayed is in addition to 4.5 miles in the townships of Avenel, Euroa, Violet Town, and between Wodonga and the Murray River, which were sealed after penetrating.

WORKS OTHER THAN ON STATE HIGHWAYS

Benalla-Shepparton Road. Shires of Benalla and Shepparton.

A start was made this year on the construction of the unformed portions of this road, which connects two important north-east towns. This road normally is impassable during the winter months, and its construction will be of great benefit generally. The work being carried out is forming and gravelling, and local gravel is being used. Up to the end of the financial year $8\frac{1}{2}$ miles had been let by contract.

Running Creek Road. Yackandandah Shire.

The remaining gap in this road has been completed by contract, which was for forming only, so that settlers in the Kiewa Valley and Dederang District now have a good summer road to Myrtleford. This will prove of great benefit to the settlers concerned, and will also be popular with tourists, as there is considerable scenic beauty from Myrtleford over this new road to the Kiewa Valley, thence on to Wodonga.

Tolmie-Whitfield Road. Shire of Oxley.

Steady progress has been made this year with reforming and either metalling or gravelling of this road, which will be an important one when completed, as it will link up Mansfield and Whitfield, traversing the Tolmie tableland. Up to the end of the financial year approximately 9 miles had been completed, commencing at Whitfield Railway Station, and contracts have been let for the construction of 13·13 miles altogether. It has been noted that settlement is extending along the road almost as fast as it is being completed.

Material being used on the last 6½ miles of the length under contract consists of a sandy conglomerate obtained locally, and this makes an excellent road at comparatively low cost.

Toombullup Road. Shires of Benalla and Oxley.

This road is an important connexion between Tolmie and the rail head at Tatong, and considerable progress has been made this year with the reforming and gravelling of the existing formed road.

Seven (7) miles of gravelling have been completed, and a further 4½ miles of existing metal pavement have been re-sheeted with gravel in order to stop it ravelling in the summer.

There are now approximately 4 miles of this road remaining to be gravelled or metalled between Tolmie and Tatong.

Mount Buffalo Road. Bright Shire.

Widening of this road between McKay's Lookout and the Chalet was carried out over a distance of approximately 5 miles. This enabled all control gates to be abolished, and has resulted in considerable convenience to the travelling public, and a saving in cost of upkeep.

Chiltern-Howlong Road. Shire of Rutherglen.

This is an important connexion between the interstate bridge and the Murray Valley Road. An important section of this road is across the Murray River Flats, and there is a considerable number of bridges therein. Most of the bridges are of timber and urgently in need of repair, and a start has been made with this work by day labour.

Bright-Omeo Road. Bright Shire.

Patrol maintenance has been carried out on this road between Harrietteville and Hotham Heights. It was found necessary after last winter to put on a small gang repairing landslides, &c.

STAWELL DISTRICT.

Western Highway.—On the Western Highway the length of the bituminous surface between Ballarat and Stawell has been increased by 23 miles. Four short sections only are now uncompleted, their total length being about 10 miles.

In this work the use of tar and tar products for penetration and priming has been adopted for practically the whole length. A short length was penetrated with ¾ gallon tar and consolidated without the use of water, and has proved quite successful. (Modified macadam.)

On Dadswell's Section three new timber bridges have been erected, replacing the old timber structures, and also two small concrete bridges, completing all bridge work likely to be required for some time to come. The whole length has been gravelled with the exception of approaches to bridges, which are now in progress. The effect of the above work is to provide an all weather outlet for practically the whole of the Wimmera.

Between Horsham and Dimboola experiments have been made in covering the clay formations with bitumen, and so far have proved quite successful, a little over 2,000 feet having been put down. It withstood all the traffic during a moderately wet winter, and it is expected that this work will be continued next year over the sand clay formations between Horsham and Pimpinio, which were completed during the year.

The Little Desert Road, connecting Kaniva and Carpolac, has been completed across the desert and through the settlement at the south end, but to make this fit for winter traffic there are still nearly 4 miles of black country to be surfaced in the Lawloit Shire.

Rupanyup and Minyip have been connected with an all-weather road, although it is mostly a foundation course only, constructed with local sandstone. This connects Minyip with the main line at Murtoa.

The newly proclaimed cross-country main road from Murtoa to Horsham will eventually prove a very great convenience. At the time of proclamation there was practically no construction, but during the year a long length in the Dunmunkle Shire was gravelled. The portion in the Shire of Wimmera is nearly all through black country, and with the exception of a few formations, it has never been touched. This is the only cross-country main road, going north, till the cross connexion between Beulah and Rainbow is reached.

Further progress has been made with the Grampians Tourist Roads. The Hall's Gap-Dunkeld Road has been further improved, and a connexion completed between the Mount Victory Road and the McKenzie Creek Falls, so that tourists can now, crossing the mountains, travel in summer all the way between Horsham and Hall's Gap. This is not possible in the winter time owing to the lack of construction of Shire roads.

Developmental roads throughout the district have nearly all been pushed out a little bit further; on most of them there is still a great deal to be done. The construction of these roads is proving of very great value to settlers, in many cases providing the only outlet available. This is particularly noticeable in such cases as the Trawalla soldier settlement, as it would have been impossible to keep the settlers on the east side of this estate without the construction of these roads. In the Wimmera and Mallee these roads are also proving of great value in spite of long lengths yet to be taken in hand; construction in nearly every case starting at the railway station.

WARRNAMBOOL DISTRICT.

From 1st July, the Board assumed direct control of the Prince's Highway West through the Shires of Colac and Hampden, so that now the construction and patrol work on the whole length from Birregurra to the South Australian border are supervised by the Board's Engineer resident at Warrnambool.

During the year two contracts let in the preceding year for the construction of 5 miles of penetration macadam through the Stoney Rises between Pirron Yallock and Stoneyford, and $5\frac{1}{4}$ miles between Camperdown and Boorcan, were completed, and the remaining $26\frac{1}{4}$ miles of waterbound macadam in bad order from Stoneyford to Camperdown, Boorcan to Terang, and Terang to Garvoc were widened to 20 feet and re-surfaced with bituminous macadam, the materials required being obtained by contract, and the construction work carried out by day labour. Also, work was commenced on the widening and re-surfacing with bituminous macadam of $5\frac{1}{2}$ miles of old waterbound macadam between Panmure and Allansford.

On the completion of the latter work there will be a continuous bitumen surfaced road from Birregurra to Yambuk, a township 10 miles beyond Port Fairy.

A rough section of $4\frac{1}{4}$ miles of limestone macadam across the Eumerella Flats, half-way between Port Fairy and Portland, was widened to 16 feet, and re-surfaced with ironstone gravel obtained locally, and the existing timber culverts on this section were replaced by reinforced concrete structures.

Between Dartmoor and the South Australian border the contract let last year for the forming and metalling in limestone of the gaps in the existing metalled road was completed, $2\frac{1}{2}$ miles being constructed as waterbound macadam, and later sealed, using a single application of road oil, and limestone screenings as covering material, the other $3\frac{1}{4}$ miles being constructed as semi-penetration bituminous macadam, using road oil as binder.

Also, $4\frac{1}{4}$ miles of existing limestone macadam in very rough order at the South Australian border was widened to 16 feet and re-surfaced with semi-penetration bituminous macadam, using limestone obtained locally.

The opportunity has been taken of making the highway much safer for motor traffic on the sections reconstructed, by the construction of the sharper curves to bigger radii, and by benching where necessary to provide good visibility. This improvement is particularly noticeable through the Stoney Rises, where some of the more tortuous sections have been completely re-aligned.

The chief developmental road work for the year has been in the Heytesbury Forest. A new road has been grubbed and cleared between Timboon and Curdie's Vale, and a commencement made with the construction work. This road will open up for settlement a considerable area of Crown land.

Construction works are in progress on the Corriejong and Eastern Creek roads, to provide access to the land recently thrown open for settlement between Timboon and Port Campbell.

A new road has been formed and graded between Port Campbell and Peterborough, and $2\frac{1}{2}$ miles of road have been formed from Port Campbell towards Princetown. These roads, which form part of the Great Ocean-road, are located to give first-class panoramic views of the magnificent coastal scenery between Cape Otway and Peterborough, while still forming important links in the developmental road system of the area.

Several shires have adopted semi-penetration bituminous construction in lieu of waterbound construction for use in the re-sheeting of main roads. This has been done on account of the difficulty experienced in maintaining waterbound macadam, constructed of necessity in the winter, until the surface can be sealed.

Experiments were conducted on the Prince's Highway between Panmure and Cudgee with a view to the construction of bitumen surfaced macadam roads in one operation. These experiments have proved successful, and the type of construction called "modified macadam" is to be widely adopted and is described in detail later.

HEAD-QUARTERS DISTRICT.

The work in this district is somewhat differently organized to that in the other districts as regards work carried out under the direct control of the Board. The Inspecting Engineer in charge is concerned with certain definite works of maintenance and construction, and with supervision of council works and co-operation with the Shire Engineers. The Highways and Bridge Engineers supervise directly the relevant works of their sections in this area. Brief details of some of the more important works in respect to magnitude and interest which have been carried out in the district are as follow:—

Ferguson-Charley's Creek Road.—The formation has been completed to Ferguson Railway Station. This route will open up country along the Charley's Creek and provide a graded road from Beech Forest to Gellibrand.

Apollo Bay-Hordern Vale Road.—Formation was pushed through, providing a dry weather connexion between Hordern Vale and Apollo Bay. This road opens up a large area of good land for settlement, and will provide the Hordern Vale settlement with access to Apollo Bay Butter Factory.

Laver's Hill-Glenaire Road.—A contract has been partially completed for the sanding of 5 miles of road over a very bad clay section. The sand used was a fine reddish sand available cheaply, and it was used to stabilize the clay base preparatory to further surfacing with metal or fine crushed rock.

Hordern Vale Road.—A further length of grading and formation has been carried out. This continues the work of providing a graded access from the Hordern Vale settlement to the Laver's Hill Railway.

Laver's Hill-Chapplevale Road.—A length of 3 miles has been sanded between Laver's Hill and Chapplevale. A base course of 6 inches was laid, using a white cemented sand as a base course on the clay. This sand is liable to scour under the action of water, and it is proposed to lay a top coat of good quality gravel. This method will produce a first class road at less cost than would be possible had the whole job been done with first class material.

Mount Sabine-Laver's Hill Road.—8 miles of metalling have been completed between Beech Forest and Mount Sabine, and settlers in Beech Forest district have, for the first time, an all-weather road to Colac.

Bituminous surfacing was carried out between Beech Forest and Laver's Hill, and the whole of this length has now been treated.

Main Warburton Road.—4½ miles of the Main Warburton Road from Launching-place to Wesburn were re-sheeted with semi-penetration bituminous macadam, and is the first stage in the reconstruction of a very bad section of road.

Monbulk Road.—Under an agreement between this Board, the Shire of Lillydale, and the Melbourne and Metropolitan Board of Works, the reconstruction of the road between Silvan and Mount Evelyn was carried a further 4,800 lineal feet towards Mount Evelyn.

Geelong-Queenscliff Road.—The 8 miles of road from the Wallington Hotel to the Queenscliff borough boundary sanded and sealed last year, were resealed as the final operation in the reconstruction of this length, and a further 5,500 lineal feet from the Geelong city boundary towards Queenscliff were reconstructed in semi-penetration bituminous macadam.

Geelong-Portarlinton Road.—A commencement was made with the reconstruction of this rough road, and the 2 miles from the Moolap State School easterly to Leopold were reconstructed in semi-penetration bituminous macadam.

Whittlesea-Kinglake Road.—The construction of this road was continued and nearly 4 miles were dealt with during the year. Settlers have now an all-weather road to the railway system at Hurstbridge and Whittlesea.

Yarra Glen-Glenburn Road.—The completion of the construction and surfacing of this road by the forming and gravelling of the sections between Yarra Glen and Steele's Creek, and between the bridges near West's and the northern side of Mount Slide, has now connected the settled area on both sides of the Dividing Range. It has also provided a hard surfaced road from Melbourne to Yea, via Mount Slide.

STATE HIGHWAYS (DETAILS).

Maintenance.—Pending reconstruction the continuous patrol maintenance of the State highways has been of such a varied nature that uniformity of organization has not been possible. With the extension of "black" pavements, however, it has been possible to extend the use of truck patrols on the inlying sections in place of the horse and dray patrols previously used. These truck patrols, consisting of a light truck and three or four men, normally look after a section 40-50 miles long as against 5-8 miles in case of man, horse and dray patrols. The experience at present gained indicates that the maintenance is as satisfactory and the expense slightly less than with individual patrolmen, in addition to which the cost of effective engineering supervision is smaller. It is anticipated that further reductions in the cost of patrol maintenance by truck patrols will be obtained as the gangs become accustomed to the organization and the use of mechanical devices with the trucks is developed.

The appointment of roadmasters on certain of the outlying sections was mentioned in last year's Report. The system has proved satisfactory. Its success is shown by the distinct improvement in the average condition of the sections under their oversight.

Reconditioning.—The year saw the completion of that portion of the reconditioning of the State highways where the volume of traffic or the availability of materials involved the use of a pavement of waterbound, penetration or semi-penetration macadam. A great part of this was carried out on the Prince's highway west, between Colac and Warrnambool, where experimental sections of "modified macadam" were successfully constructed. It has been felt for some time that with traffic of the comparatively light nature which the highways are called upon to carry beyond the outer metropolitan area the chief advantage of a penetration macadam over a waterbound pavement was the possibility of completing the construction of the pavement without trouble due to ravelling. The "modified macadam" type of construction was evolved with the intention of obtaining this advantage with a smaller expenditure on the bituminous binder. The saving is approximately 3d. per square yard. Those portions of the specification for this type of construction which are of technical interest are given below.

SPECIFICATION FOR MODIFIED MACADAM.

Metal, screenings and toppings.—The metal, screenings and toppings to be supplied and consolidated shall be crushed from clean spalls, to be of dense, hard, basalt, or other approved igneous rock, having a French co-efficient of wear of not less than ten. It shall be clean and free from honeycomb, scoriaceous, weathered, or disintegrated stone, and from thin or elongated pieces, and shall be uniform in quality and grading throughout.

When tested with laboratory screens having circular openings of the sizes stated, the grading shall be within the following limits:—

Material.	Percentage Passing Screens.					
	2½ inches.	2 inches.	1½ inches.	¾ inches.	½ inches.	¼ inches.
Metal ..	100	55-90	15-45	0-10	..	Nil
Screenings ..	..	..	..	100	..	0-2
Toppings ..	..	..	..	..	100	..

MODIFIED MACADAM TOP COURSE.

Base.—Spreading of metal for the modified macadam surfacing shall not be carried out until the base has been tested and passed for consolidation, cross section and longitudinal shape by the Board's Superintending Officer.

The waterbound macadam or gravel base, constructed or reconstructed as previously specified, shall be swept clear of all excess screenings, toppings, binding gravel, dust or loose material.

Side Boards.—Immediately before the metal is spread, boards not less than 2 inches wide, and of a depth equal to the loose depth of metal required, in lengths of 10 feet or over, shall be fixed true to line, and level along each side of the boxing, and shall be secured in position by approved spikes driven firmly into the base. As the metal is spread, and before the boards are removed, the shoulders of the road shall be made up to the full height of the top of the boards and firmly rammed and consolidated behind them. Sufficient boards shall be kept on the works and placed in position at least 60 feet in advance of the new metal spreading. On all curves shorter boards shall be used in order to maintain the correct alignment. The distance between boards, inside to inside, shall be equal to the specified pavement width.

Spreading Metal.—The metal shall be dumped on to metal spreading sheets and spread to a uniform loose thickness of 4 inches. Each load or pile shall be spread outside of the area upon which it is dumped, in such a manner that segregation of sizes is prevented and uniformity of consolidation and distribution of voids is obtained when rolling. Any accretions of fine particles shall be removed by forking or other means before rolling.

Before rolling is begun the surface of the loose metal shall be tested by means of templates, for cross section and longitudinal shape.

Rolling.—The metal spread as above shall be rolled with a mechanical roller weighing not less than 10 tons, and having a load of not less than 350 lb. per inch width of rear wheel. Rolling shall be carried out longitudinally, starting at the sides and working towards the centre line of the road. It shall be continued until no vertical movement of the pavement occurs, and until the stones are firmly interlocked and present a uniform mosaic appearance.

If any irregularities appear during or after rolling they shall be remedied by loosening the surfaces and removing or adding metal, after which the area disturbed shall be rolled until the whole surface is satisfactorily consolidated to the desired surface. Any metal which becomes coated or mixed with dirt or foreign matter prior to the application of the cold tar or bitumen and oil mixture, shall be removed, replaced, and consolidated as specified.

The compacted surface shall be tested by the Superintending Officer for cross sectional and longitudinal shape by means of template, spirit level, boning rods and straight edge, and no pouring shall be done until the work has been passed as satisfactory by him.

Penetration of Pavement Edges.—After the consolidation of the metal as specified above, and before the application of the screenings, a strip 8 inches wide along each edge of the pavement shall be penetrated with 85-100 penetration bitumen applied at the rate of one gallon to every square yard of surface. The road shall be thoroughly dry when the bitumen is applied, and the temperature shall be not less than 50° F. for at least three hours prior to the application. Suitable thermometers should be provided by the contractor for determining the air temperatures. The bitumen shall be applied uniformly by means of hand pouring pots which shall be approved by the engineer. The temperature at the time of application shall be not less than 300° F.

The bitumen shall be heated in kettles designed to secure uniform heating of the whole of the contents to a temperature of 325° F. to 350° F. Suitable thermometers for determining the temperature of the material in the heaters shall be kept on the works at all times.

Care shall be taken to avoid overheating the bitumen or maintaining it at a high temperature for long periods. Any bitumen of which the penetration (77° F. 100 g. 5 secs.) is less than 80 immediately prior to application will be rejected and shall be replaced by the contractor at his own cost.

Filling the Voids.—After the edges have been penetrated with bitumen as specified above, screenings shall be spread thinly and uniformly, at a rate not exceeding 1 cubic yard to 120 square yards of surface, and rolled, broomed and watered in, more screenings being gradually added and rolled, broomed and watered as before, until the voids are well filled to the level of the top of the coarse metal, and to the satisfaction of the Superintending Officer.

Care shall be taken to avoid forming a mat on the surface by using excess screenings.

Toppings shall then be spread and broomed and rolled in without watering until the smaller voids are in turn well filled to the surface of the metal, and the surface voids in particular are completely filled. Should any excess toppings lie on the surface of the metal they shall be swept off to expose the faces of all the coarse stones.

Application of "Cold Tar" or Bitumen and Oil Mixture.—"Cold Tar" or bitumen and oil mixture shall then be applied at the rate of 0.4 gallons per square yard, and allowed to penetrate the road until no free tar or mixture is observed on the surface.

No heating of "Cold Tar" will be found necessary in hot weather. In cool weather, however, it shall be warmed to a temperature not exceeding 100° F. in order to facilitate the application.

Where a mixture of oil and bitumen is used as the binder the bitumen shall be first completely melted at as low a temperature as possible. If in this process the temperature exceeds 250° F. the bitumen shall be allowed to cool to this temperature before adding the oil. The oil shall then be added in small quantities, the mixture being stirred after each addition until a homogeneous mixture is obtained before further addition is made to the bitumen. The whole mixture shall then be brought to a temperature between 180° F. and 200° F. The proportions of oil and bitumen will be fixed by the engineer.

COVERING.

A light layer of screenings shall then be spread at the rate of about 1 cubic yard to every 120 square yards, and rolled into the road, the whole of the road being traversed twice a day for seven days. Traffic should be allowed on the road during this period unless mud is being brought on to the surface.

SEALING.

After seven days, and preferably within 24 days, as weather conditions permit, a seal coat of 85-100 penetration bitumen shall be applied by machine at the rate of .3 gallons per square yard as hereafter specified.

Considerable progress was made with the lighter forms of construction considered adequate on the more outlying sections of the highways. As the final purpose of many of these forms of construction is to provide an adequate support for surface sealing, a brief specification for the latter is given below.

Following the success of experimental sections, this sealing, which was originally used for the first seal of waterbound macadam and gravel pavements, was successfully applied to natural sand clay or granitic sand pavements, and to formations consisting of gravelly material such as are found on the Hume highway.

BRIEF SPECIFICATION FOR BITUMINOUS SURFACE TREATMENT.

Preparation of Existing Road.—Before any bituminous material is applied the existing surface shall be brought into first class condition by intensive maintenance, so that it can be swept to expose a hard surface true to grade and cross section.



Plate No. 21. Maintenance of Gravel Road with Road Planer after Scarifying and Grading until Surface Sealing is carried out.

In the case of a metalled road this shall consist of careful patching should this be necessary. Gravel roads, sand, clay, or natural formations shall be scarified and reshaped with a long wheel base grader during the last wet weather before treatment, and from then until the primer is applied continuously maintained with a road planer or sledge drag so that corrugations are never allowed to develop.

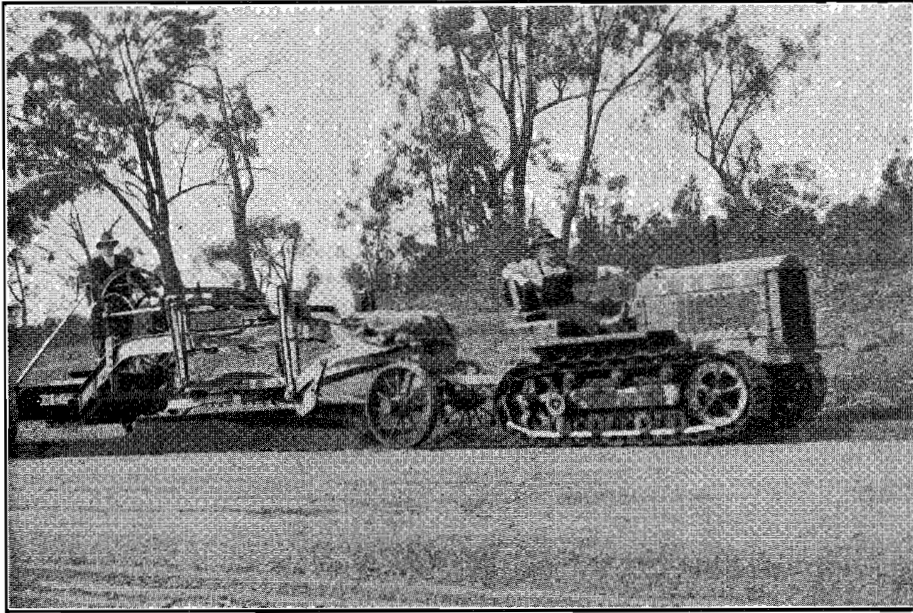


Plate No. 22.—Preparation of Gravel Road for Spraying.

Primer.—

(a) The primer shall consist of an intermediate tar (“Cold Tar”) containing not more than 2 per cent. water and of Specific Viscosity (Engler 40° C.) of from 4 to 10;

or

(b) The primer shall consist of a 50-50 mixture of 85-100 bitumen and a residual oil having a flash point not less than 180° F., and a specific gravity not less than 0.89.

Bitumen.—The bitumen shall have a penetration of from 85 to 100 and shall be fluxed to a penetration varying from 85-100 in hot weather to 220 in cold weather.

Covering Material.—Screenings shall be crushed from spalls having a French co-efficient of wear of not less than 10. The gravel shall consist of hard durable particles and shall contain not more than 3 per cent. removable by elutriation. They shall be graded as follows:—

Material.	Percentage Passing Screens.					
	$\frac{1}{2}$ inch.	$\frac{3}{4}$ inch.	1 inch.	10 mesh.	20 mesh.	200 mesh.
Screenings ..	100%	50-95	20-80	0-30	0-5	..
Gravel ..	100%	50-90	20-65	0-30	..	0-3

Application.—The primer shall be applied uniformly by hand or machine at the rate of 0.15 to 0.3 gallons per square yard. The road shall then be closed to traffic until the seal coat is applied.

Seal Coat.—The bitumen shall be uniformly applied by machine at the rate of 0.3 gallons per square yard.

Immediately after application of the bitumen, covering material shall be spread uniformly at the rate of 1 cubic yard per 100 square yards of road surface, and rolled in. The road shall then be opened to traffic and maintained by being kept covered with covering material until the bitumen has absorbed as much as possible.

Maintenance material shall be left by the roadside at the rate of 1 cubic yard per 300 square yards of road surface.

Following observation of an experimental section constructed during the previous season, an artificial sand clay pavement, obtained by the addition of sand to the existing soil, was constructed between Horsham and Pimpinio. This section was previously very bad during wet weather.

The only high-class pavement constructed during the year was at the “Guiding Star” deviation, on the Prince’s highway west, about 8 miles from Melbourne. A cutting 10 feet deep was necessary to get down to the level of the deck of the bluestone arch bridge over Kororoit Creek. The maximum grade is 3 per cent., and the curve has a radius of 1,170 feet. The pavement consists of a concrete base with a 2-inch bituminous concrete wearing surface. The

base is 7 inches thick at the centre and 9 inches at the sides and is insulated from the clay foundation by a layer of cinders 4 inches thick. A concrete kerb 8 inches wide and 11 inches deep was cast as part of the base. The mechanical analyses of three of the clays, which are very bad foundation materials, are given below:—

				Typical Surface Soil, Yellow-Brown.	Brown Clay.	Grey Clay.
				Per cent.	Per cent.	Per cent.
Pass No.	10 sieve	..	..	100	100	100
..	20	..	..	100	99.6	99.6
..	30	..	..	100	98.5	99.3
..	40	..	..	99.2	98.2	99.1
..	50	..	..	97.2	97.1	98.9
..	80	..	..	95.0	94.9	98.3
..	100	..	..	93.2	93.9	98.0
..	200	..	..	87.0	88.5	96.5
Clay content		..	..	45.0	65.7	81.5
Lower liquid limit		..	..	21.4	52.7	58.6
Lower plastic limit		..	..	14.3	29.3	30.8
Plasticity index		..	..	7.1	23.4	27.8

The base was specified to contain five bags of cement per cubic yard of finished concrete. The proportion of cement to aggregates (measured separately) was about 1 : 7.35, and with a slump of less than $\frac{3}{4}$ inch, an average twenty-eight day strength of 4,800 lb. a square inch was obtained. Reinforcement was not provided, but painted edge bars, $\frac{3}{4}$ -in. diameter, were put in to act as dowels at transverse cracks.

The wearing coat consists of a coarse graded bituminous concrete with a premixed seal coat.

A typical grading taken on 11th February last is given below. Bluesone dust was used for filler in the first 500 feet and the last 170 feet, limestone dust being used in the remainder of the work.

TYPICAL MECHANICAL ANALYSES OF BITUMINOUS CONCRETE.

	Percentage of Dry Material passing Sieves.								
	1½"	1"	¾"	½"	No. 4.	No. 10.	No. 40.	No. 80.	No. 200.
Filler, bluestone dust	..	..	..	..	..	100	92	89	80
Filler, limestone dust (not used 11th February, 1930)	..	..	..	..	..	..	100	96	79
Total sand	..	..	..	..	..	100	59	23	..
Total dry mix	100	94	76	58	41	32	22	13	6

Filler in concrete	..	..	..	..	..	7 per cent. of total dry mix
Filler in seal coat	..	..	..	..	..	12 per cent. of total dry mix
Bitumen in concrete	..	..	..	..	..	5.1 per cent. of total wet mix
Bitumen in seal coat	..	..	..	..	..	10.2 per cent. of total wet mix
Amount of seal coat used per square yard	..	..	..	..	..	44 lb.

Particular care was taken to obtain a surface free from waves, and to this end the concrete base was screeded with a stiff screed 16 feet long, after consolidation with a heavy tamper. This screeding removed transverse irregularities and long waves but left the surface too smooth to afford a grip for the bituminous concrete. Accordingly transverse indentations were made in the green concrete with a steel angle. The following is the specification for the finish of the wearing surface:—

The finished surface of the road shall be such that the deviation from a straight edge laid parallel to the centre line shall be not more than 1/16th inch for every foot of length of the straight edge used, with a maximum deviation from a 10-ft. straight edge of $\frac{1}{4}$ inch.

Any portion that does not pass the foregoing test shall be rectified by cutting out and replacing, or by any other method which the engineer may approve.

The pavement will be tested at the end of each 500-ft. length completed, and at the end of the maintenance period. No additional 500-ft. length shall be commenced till the previous 500-ft. length has been tested and found or made satisfactory.

The work was carried out by Victoria Roads Ltd. under Contract 501H79, the cost of pavement, including base and wearing surface only, being 23s. 2d. a square yard. The new deviation was opened to traffic on 28th February, 1930.

Following the year's work the highways consist of practically continuous black roads from Melbourne to the following points :—

Highway.	Point to which Black Road Extends.	Miles from Melbourne.
Prince's Highway West	Yambuk	192
Prince's Highway East	Moe	83
Western Highway	Great Western	137
Calder Highway	Bendigo	95
Hume Highway	Longwood	83

EXPERIMENTAL WORK.

The improvement of very poor subgrades by surface sealing with the object of lessening the amount of gravel needed to carry traffic has been carried out successfully in Canada. Following these results, experimental sections were constructed between Horsham and Dimboola. The treatment was as specified under "Surface Sealing" except that the quantity of primer (cold tar or a 50/50 mixture of fuel oil and bitumen) was varied from 0·4 to 1·0 gallons per square yard and the quantity of bitumen was increased to 0·4 gallons per square yard. The seal coat was covered with from 1 to 3 inches of gravel and the sections have since been maintained as a gravel road. A third section of artificial sand clay was treated with a surface seal. Following the success of these sections, it is proposed to surface seal the artificial sand clay between Horsham and Pimpinio during the coming season and to extend the treatment to the formations from Pimpinio to Dimboola when funds are available.

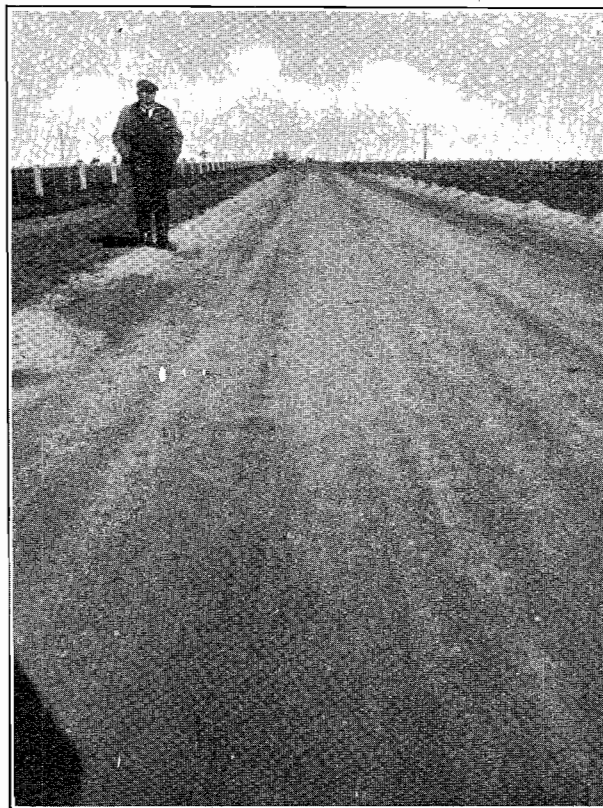


Plate No. 23.—Western Highway. Experimental Surface Seal on Artificial Sand Clay between Horsham and Pimpinio.

It is anticipated that this surface seal alone will carry the present traffic. If, however, failure occurs on the earth formations, the treatment will be valuable as a subgrade waterproofing, because only 2 to 3 inches of gravel will then be necessary instead of 7 inches which would otherwise be required to carry traffic. There are great possibilities for this method of construction

in localities where road-making materials are scarce. In this instance the gravel cost is about £1 per cubic yard delivered on the road.

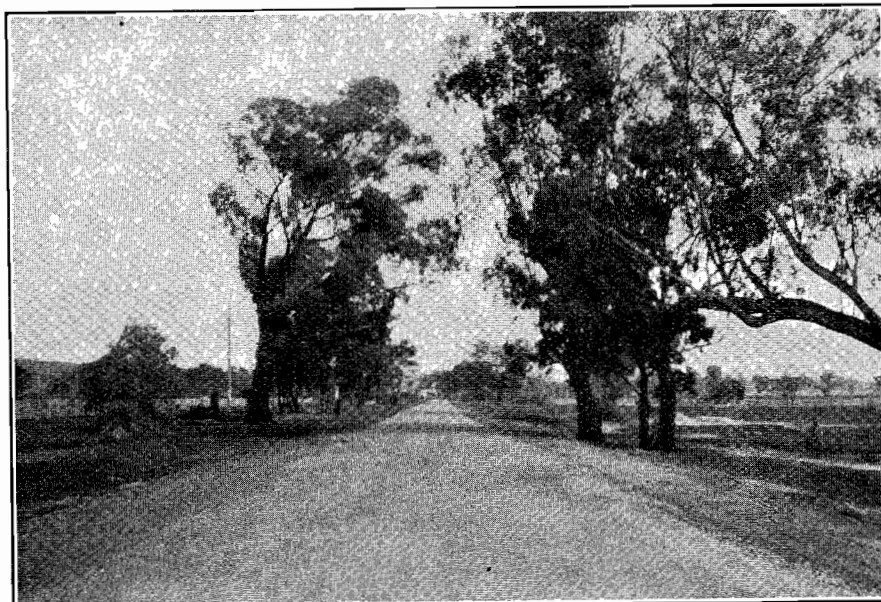


Plate No. 24.—Hume Highway. Completed mixed-in-place Pavement north of Chiltern Railway Crossing.

Two sections of mixed-in-place bituminous pavement have also been constructed, both of these being similar in general nature to work described in detail elsewhere in this report. One of these is on the Calder Highway north of Bridgewater, and the other on the Hume Highway north-east of the Chiltern railway crossing. Both are at present superior in riding qualities to any “black” road constructed on the State highways. This is due to the use of grader before and during consolidation. Graphical representation of the grading of the gravel used on these jobs, and on similar work carried out on the Ballarat-Creswick and Castlemaine-Maryborough roads, is shown in Figure 1.

TYPICAL GRADINGS OF MATERIALS USED IN MIXED IN PLACE SURFACING

C.R.B. 1930

GRADING OF ALL MATERIALS PASSING 10 MESH SIEVE EXCEPT FILLER					
SAMPLE N ^o	3726	3279	3834	3765	3920
% passing 10 mesh retained on 40 mesh	81.9	63.2	90.8	54.6	63.0
% " 40 " " 80 "	16.8	28.4	7.2	31.8	21.3
% " 80 " " 200 "	1.3	8.3	2.0	13.6	15.7

Sieves used - Lab. Set. A*

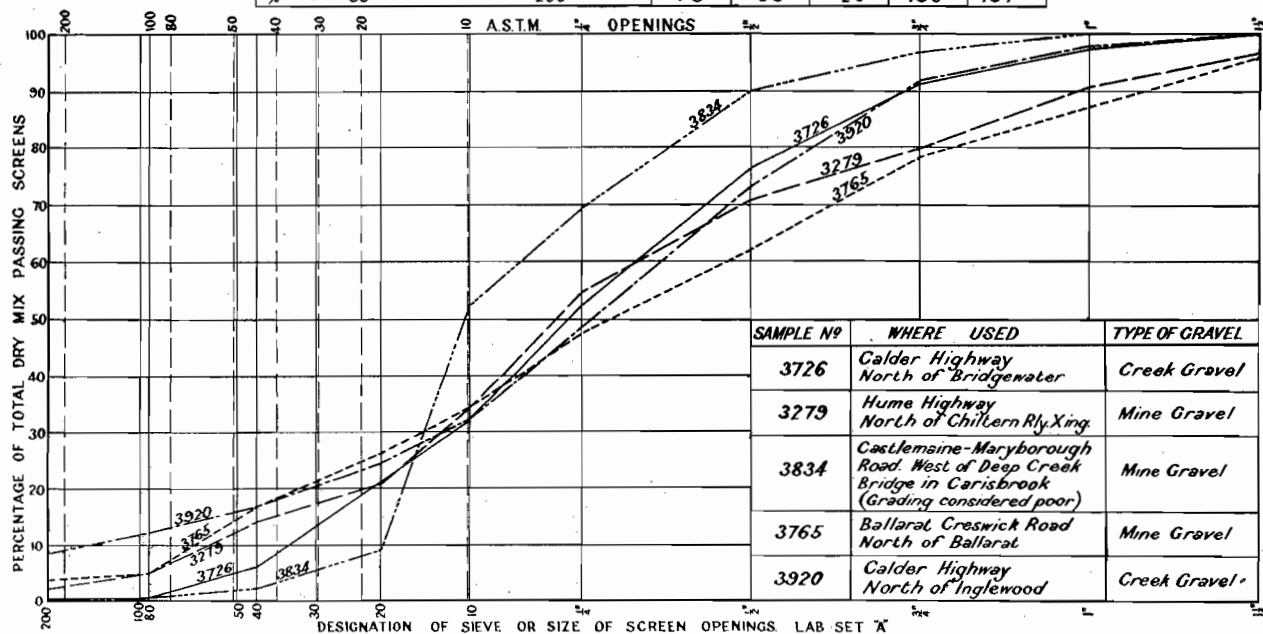


Figure No. 1.

TRAFFIC COUNTS.

A count of the traffic using the State highways was taken twice during the season. The principles upon which this was organized were stated in the last report. The records for the Hume Highway are shown graphically on Figure 2.

The following tables show the average increase since systematic counts were taken, and the percentages of the main classes of vehicles:—

TABLE 1.—INCREASE IN TRAFFIC, 1928-29-30.
TABLE 2.—PERCENTAGES OF TYPES OF VEHICLES OBTAINED FROM ANALYSIS OF TOTAL VEHICLES RECORDED ON DAY OF COUNT.

This shows the volume of traffic recorded at subsequent counts relative to the volume recorded when the first systematic count was taken. The traffic recorded at the first count is represented by 100.

(To nearest whole number.)

Highway.	Sec.	Average over whole Section.					At Seven-day Station.					Trucks with a Rated Carrying Capacity exceeding 2 tons, and Passenger Carrying Vehicles excluding Touring Cars with Pneumatic Tyres.	Trucks with a Rated Carrying Capacity of 2 tons or less and Touring Cars and Motor Cycles with Pneumatic Tyres.	Solid Tyred Trucks.			Steel Tyred Horse-drawn Vehicles.										
		Aug., 1928.	Feb., 1929.	Oct., 1929.	Feb., 1930.	Average Increase per Annum, Summer to Winter.	Aug., 1928.	Feb., 1929.	Oct., 1929.	Feb., 1930.	Average Increase per Annum, Summer to Winter.			Aug., 1928.	Feb., 1929.	Oct., 1929.	Aug., 1928.	Feb., 1929.	Oct., 1929.	Aug., 1928.	Feb., 1929.	Feb., 1930.					
P.H.W.	1	100	115	107.5	134	12	100	101	109	107	8	84	78	78	9	11	8	1	8	7	3	7	8	4	4	4	
P.H.W.	2-5	100	121	114	133	12	100	147	178	179	50	74	64	68	3	4	4	2	7	4	2	22	30	15	20	1	
P.H.E.	1	100	122	172	217	75	100	113	153	138	39	89	87	88	2	4	2	5	5	2	6	5	8	6	1	1	
P.H.E.	2-6	100	113	100	149	..	..	100	100	107	..	80	80	88	6	8	6	6	..	5	4	..	..	..	11	9	7
Western	1	100	113	117	169	33	100	128	118	148	17	82	82	83	7	6	7	3	83	6	3	86	1	1	8	8	7
Western	2-5	100	112	141	141	12	100	100	111	117	11	87	87	86	3	2	3	1	87	2	3	1	1	..	11	10	10
Calder	1	100	227	137	243	22	100	105	107	111	6	89	92	..	6	6	1	..	89	1	1	..	..	4	4	2	2
Calder	2-6	100	100	100	109	9	100	100	100	105	5	80	76	..	3	5	3	3	80	1	1	..	..	19	14	11	11
Hume	1	100	128	145	190	47	100	122	157	178	52	78	78	..	6	6	8	8	78	6	8	..	..	15	15	14	14
Hume	2-3	100	157	174	201	51	100	113	170	150	51	85	88	83	4	4	4	4	85	3	4	5	5	7	4	5	5
Northern	..	..	100	84	135	35	..	100	92	162	62	86	88	94	6	7	6	6	86	3	4	10	10	7	7	7	7
Omeo	..	..	Insufficient information available.	..	..	..	..	..	..	..	..	95	85	..	4	4	5	5	85	4	5	..	..	..	1	10	10

ROUGHOMETER RECORDS.

The value of the State Highways to the State is indicated by the amount of traffic using them and the quality of the pavements. Two years ago an instrument similar to that used by the American Bureau of Public Roads for giving a quantitative value to the roughness of a pavement was made locally. The instrument measures the sum of the successive movements of the front axle of the car to which it is fitted towards the engine block while travelling over any length of road.

During the season the necessary preliminary arrangements were carried out and records were made of the relative roughness of each of the State Highways from Melbourne outwards. A graphical representation of the record for the Calder Highway from Melbourne to Wycheproof is shown on Figure 3.

The systematic taking of these roughometer surveys and their examination, coupled with the examination of the traffic counts, will help very considerably towards estimating the life of the various improvements, and thus indicating the provision that needs to be made for future replacement.

BRIDGES.

The work done during the year includes a further step towards the gradual improvement in bridges and culverts on State Highways and Main Roads, and the construction of a number of new bridges and culverts on developmental roads.

In addition to the replacement of old timber structures which had become so weakened from decay that they were no longer able to carry the loads now using the roads, attention has been directed to extending culverts which, in common with narrow bridges, are a potential source of danger, particularly with the improvement in road surfaces and greater widths.

Forty-seven minor drainage structures, varying in size from 12-in. diameter pipes up to fairly substantial double cell reinforced concrete box culverts, have been extended or reconstructed to enable traffic to take full advantage of the 40-ft. width of formation between Melbourne and Geelong on the Prince's Highway West. Increased traffic speed has required radii in the vertical curves in the approaches to culverts or inverts in flat country to be considerably modified. It has been ascertained that a vertical curve of 1,000 feet radius is the minimum to produce a smooth riding surface at the highest speeds.

The bridge over the Loddon River at Eddington, on the road between Bendigo and Maryborough, was completed in September, 1929. The original bridge at this site was 70 years old when finally replaced. This is a remarkable tribute to the long lasting qualities of red gum and grey box. The construction of the Laanecoorie Weir by the State Rivers and Water Supply Commission required the bridge, as originally constructed at low bank level, to be raised over the level of impounded water. The raised level was still well below flood level, which was 3 feet above the top handrail. The valley of the Loddon River between the high banks is very wide at the site of the old bridge, but is constricted by a sandstone bluff half a mile upstream to a much narrower width. As a new structure above flood level was considered advisable, this narrower crossing, which involved considerably less earthwork, was selected. From run-off calculations and recorded maximum flood conditions, the new bridge was required to pass a flood flow of 50,000 cusecs. and the length of the bridge required was 680 feet. Bores showed that the sandstone outcrop on the Bendigo bank extended right across the valley at depths up to 35 feet below natural surface. Foundations consist of driven timber piles capped with concrete blocks at 6 feet below ground level. The new bridge consists of fifteen spans, each 45 ft. 4 in. long, of reinforced concrete. The unyielding rock foundation permitted the use of a continuous structure, but, owing to the necessity for providing for expansion and contraction, this economy could not be fully used. Expansion joints were placed at every third pier, which was split at ground level.

The new bridge site involved the construction of 4,000 feet of new gravelled approaches. The total cost of bridge and approaches was £16,353 10s. 6d.

The bridge over Mollison's Creek at Pyalong, on the Bendigo-Heathcote-Kilmore road, was a similar age to the old bridge at Eddington. It had granite abutments and wingwalls, while the balance of the substructure and superstructure was timber with an overall length of 180 feet. The timber portion was replaced by a five-span reinforced concrete bridge 20 feet wide between kerbs. The granite abutments, which were generally sound, were re-used. As, however, the width available between the solid granite masonry wingwalls was only 18 feet, it was unfortunately necessary to break these down to the level of the beam seatings and build them up from that level with reinforced concrete of much thinner section, slightly cantilevered to give the necessary width. As solid rock occurs over this section, this superstructure was made continuous.

The total cost of the contract was £3,721 9s.

C.R.B.
CALDER HIGHWAY
 ROUGHOMETER SURVEY

MAY 1930

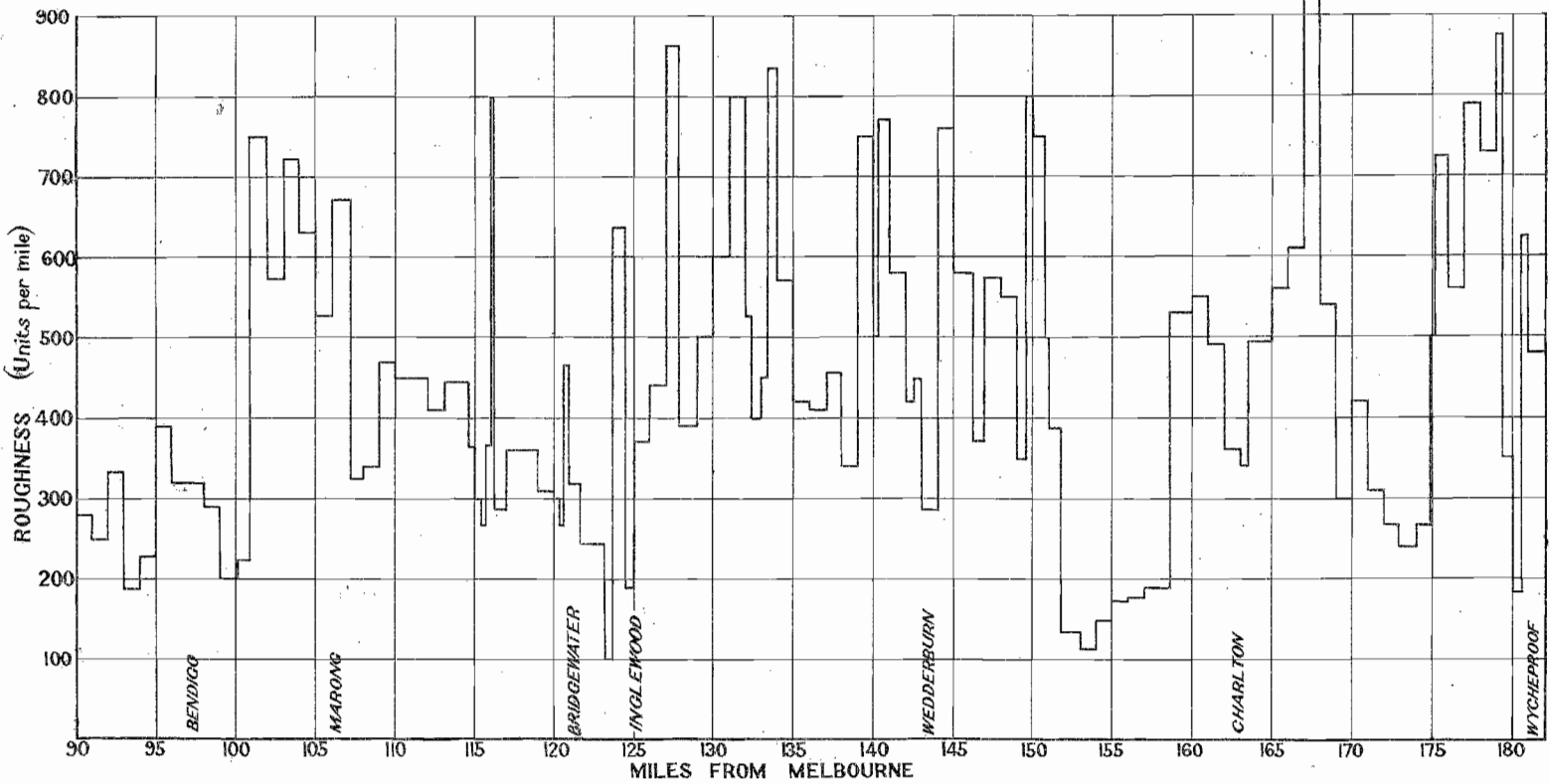
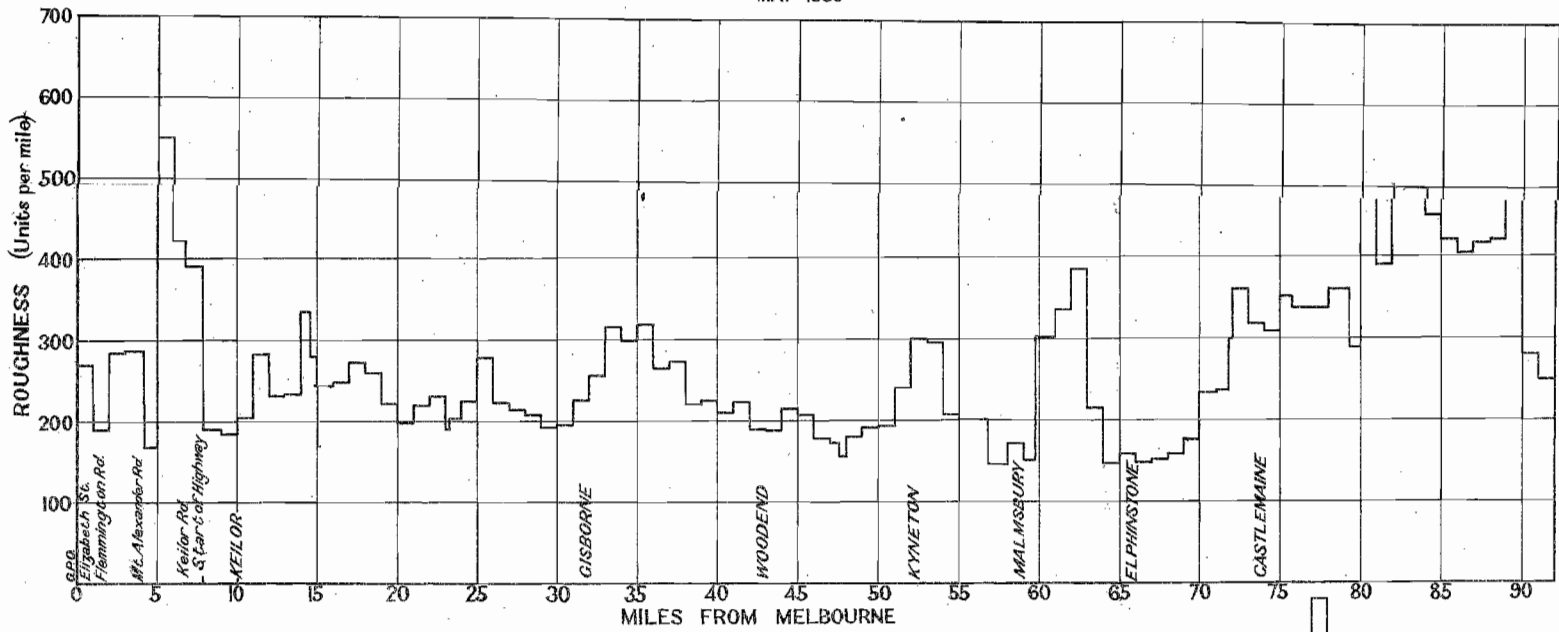


FIGURE No. 3.

Burrumbeet Creek Bridge is on the Western Highway approximately 9 miles west of Ballarat. This was of a similar type to the original bridge at Pyalong, but the abutments and wingwalls, which were constructed of basalt masonry, were not carried down to the rock level, and as the mortar was failing they could not be used for the new structure. A level floor of basalt, which is practically exposed in the creek bed, extends across the site. The new structure consists of a four-span reinforced concrete bridge $127\frac{3}{4}$ feet long and 22 feet wide between kerbs. Due to the large depth from deck level to rock level, the more economical type of buried pier abutments was used and masonry blocks from the old abutments were used to pitch the spilled-through filling against scour from flood water.

To prevent a bump at the junction of the concrete deck and the approaches due to settlement of the filling, a reinforced concrete slab was cast at each end of the bridge 6 feet long. One edge rests on the curtain wall of the buried pier and the balance of the slab is supported on the filling. Any settlement of the filling therefore only produces a slight tilt in the slab and does not affect the smooth riding qualities of the road.

The cost of this bridge alone was £2,349, which, for a superficial deck area of 2,810 square feet, shows the cost to be £0·84 per square foot.

The alteration of the Prince's Highway East from the road paralleling the railway between Nar-Nar-Goon and Longwarry to the original Sale Road necessitated the construction of a new bridge over the Bunyip River immediately below its junction with the Tarrago River. There has been very considerable stream bed alteration at this site. Originally the two rivers joined below the site of the bridge crossing and there were two separate bridges. Bed scour has occurred due to drainage channels in the Koo-wee-rup Swamp, and the Bunyip River bed is now 20 feet lower than it was a generation ago and now joins the Tarrago immediately upstream from the crossing. Bed scour has been uniform at the rate of a foot a year and the rapid velocity in the stream indicates that it will go on for some time until the bed is lowered sufficiently to reduce velocity.

A concrete bridge was designed for this site, but the cost of foundations to provide for future scour caused tender prices to be unduly high. Subsequently alternative tenders were called for timber and reinforced concrete, with the result that the lowest tender for a timber bridge was £2,373, and for a concrete bridge £7,300. The timber bridge was, therefore, more economical and has the merit that at the end of its life the river channel should be fairly stable. To prevent bank erosion, willow cuttings protected by rock filled cribs were planted along the toes of the banks.

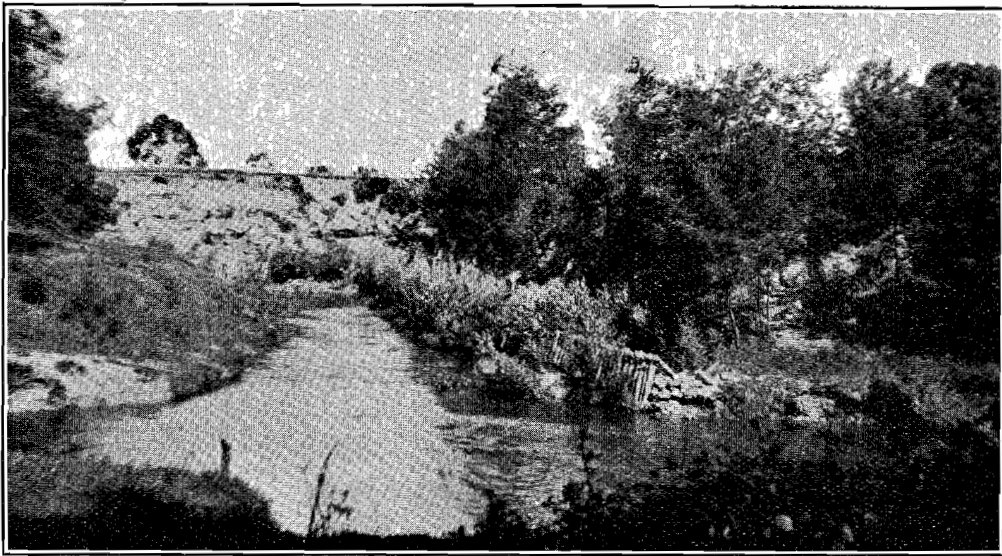


Plate No. 25.—Shows the growth made by the willows at the edge of the stream in twelve months.

Waterholes Creek is crossed by the Prince's Highway East 2 miles east of Morwell. The old timber bridge was 2 feet below flood level, very narrow, and in an advanced stage of decay. It has been replaced by a new three-span reinforced concrete bridge 75 feet long and 22 feet wide between kerbs. Pier foundations consist of a row of four reinforced concrete piles driven directly under each beam stem. These were broken down at ground level, and cast in place stems of the same section carried up and terminated in a tie beam under the beam seatings.

In conjunction with the reconstruction of the Western Highway in the length near Dadswells, between Stawell and Horsham, the main channel and floodways of Mount William Creek were bridged with three new timber bridges 22 feet wide, having a total length of 240 feet. The total cost of the three bridges was £2,900.

A sharp bend with bad visibility was eliminated, where the Prince's Highway East crosses Flynn's Creek between Traralgon and Rosedale, by relocating the road on an easy curve. A new bridge 61 feet long was constructed on the curve. In the length of the bridge, the difference between the arc and the chord was so small that the bridge was built straight and made 6 inches wider than the standard 22-foot highway width. Superelevation of 1 in 20 was provided, and the outer pile of the piers was raked 1 in 5 in place of the usual 1 in 8. In this bridge a variation in the standard cross section was made, and the deck planks were put longitudinally instead of transversely, and are supported by 9 in. × 6 in. cross-beams spaced at 4-ft. centres. The evolution of this type has been as follows:—

The crudest pioneering type of timber bridge consists of two round stringers supported on a bed log or on the bank. This is covered with cross decking together with a kerb. This type of structure was enlarged by the addition of more stringers where greater width was required. It was found that the contact area between the deck and stringers was usually damp, and the spike holes, together with the damp conditions, were very conducive to rotting of the stringer. This phenomenon resulted in the adoption of a spiking plank which was spiked to the stringers at about 4 ft. spacing, and into which the deck spikes were driven.

In addition, a layer of bituminous felt was put over the stringer under the spiking rail to keep the stringer dry. This type of construction was almost universally used for slow moving horse-drawn traffic, but inequalities of wear and shrinkage in the deck planks produced a very uneven riding surface with considerable impact on the planks, which shortened their life, loosened deck spikes, and made the structure rattle as vehicles passed over. To overcome these disadvantages with the increase in motor traffic, 2-in. thick running planks, were laid longitudinally over the transverse deck planks. This quietened the bridge very considerably and made an easy riding surface. These improvements, however, considerably increased the quantity of timber required, and moreover many surfaces were in contact, which, for bridge timbers exposed to weather, accelerate rotting. Structures so constructed were not satisfactory from a maintenance standpoint as the renewal of deck planks required the removal of running planks, and then, unless the kerb was removed, the planks had to be pushed out under the kerb. To replace a stringer required the removal of the whole of the deck. Because of this maintenance difficulty, timber bridges have been often allowed to become quite unserviceable, whereas with minor annual maintenance they can be given an almost indefinite life.

The longitudinal deck enables each deck plank to be replaced as required without disturbing any other part of the structure, and should a major repair, such as a stringer replacement, be necessary, the bridge can be readily converted into a single traffic bridge while a stringer and new half length cross-beams are put in. The ends of deck planks are staggered on the cross-beams so as to avoid an excess of coach screws into any cross-beam. The ends of planks are fastened down with two coach screws, each 9 inches long by $\frac{1}{2}$ -in. diameter, screwed into a hole drilled $\frac{3}{32}$ inch smaller than the screwed portion. The hole above this is of the same section as the unscrewed portion of the coach screw. The heads of coach screws are countersunk flush with the surface. The total economy in materials is approximately 15 per cent. Stringers only have to be checked out for cross-beams instead of continuous trimming for seatings. Except for cross-beam seatings, and end bearings on cross heads, stringers are completely surrounded by air, and should, therefore, have a longer life.

The bridge over Jackson's Creek, in the township of Gisborne on the Calder Highway, was constructed approximately 40 years ago with masonry abutments and piers, and with a timber superstructure. The masonry is sound except that there was a slight decay in the timber platforms on which the piers were constructed, due to a slight bed scour. It was, therefore, inadvisable to increase the dead load on foundations by a concrete deck, and the superstructure was renewed in timber to provide a 20-ft. wide roadway and a 4-ft. wide footway. The overall length was 92 feet and the total cost of the new superstructure was £550. The specification for reinforced concrete for use in structures was modified from the

previous specification. The new specification provided for a fixed quantity of cement per cubic yard of finished concrete, and has been arranged for the different classes of concrete as follows :—

Reinforced concrete piles	8 bags (752 lb.) per cubic yard.
Reinforced concrete	6 bags (564 lb.) per cubic yard.
Mass concrete	5 bags (470 lb.) per cubic yard.
Un-reinforced footings	4 bags (376 lb.) per cubic yard.

Previously the mixes were based on the ratio between the volume of the cement and the total volume of the aggregates. This latter method, while being generally satisfactory for crushed stone and sand as aggregates was found to give widely varying yields per bag of cement when applied to gravels or combinations of gravels. The new specification, which follows standard American practice, obviates this difficulty, and also has been found to give greater regularity in concrete strengths.

BITUMINOUS SURFACING OF ROADS.

Owing to the successful results given by a 400-gallon spraying unit mounted on a Thornycroft 3-ton chassis, four further units were obtained mounted on the remaining four chasses owned by the Board. These sprayers are lighter, faster and more mobile than the larger 800-gallon steam sprayers and are specially suited for treating sand and fine gravel surfaces.

Fourhundred-gallon heaters equipped with solid rubber tires, on wheels fitted with roller bearings, and with a firing system comprising oil burners capable of burning tar oil or fuel oil. were specially designed for use with the Thornycroft sprayers, and these heaters have proved more efficient, more economical and much more mobile than the usual wood fired heaters.

A complete spraying outfit consisting of a Thornycroft sprayer hauling two rubber tyred, oil fired heaters, a road broom and a Furphy cart averages 50 miles per day in moving from one work to another. In the case of the 800-gallon steam spraying units, the average shift is 10 miles per day.

During the past season six 800-gallon steam sprayers, five 400-gallon Thornycroft sprayers, and five 300-gallon roller or tractor drawn sprayers were in use. Of these, one 400-gallon unit and one 300-gallon unit were engaged on reconstruction works on the Prince's Highway West, and one 300-gallon unit was engaged on the bituminous surface mixing works on the Ballarat-Creswick Road.

The length treated with bituminous surface treatments and the respective costs are shown in the following table :—

	Single Coat.			Double Coat.		
	Length.	Total Cost.	Cost per Square Yard.	Length.	Total Cost.	Cost per Square Yard.
	Miles.	£	d.	Miles.	£	d.
Six 800-gallon sprayers	145·2	39,406	6	45·6	13,769	9½
Four 400-gallon sprayers	22·0	8,731	8	67·6	21,796	9½
Three 300-gallon sprayers .. ..	..	..	..	30·9	11,684	10¼
Totals	167·2	48,137	..	144·1	£47,249	..

311·3 miles treated at a total cost of £95,386.

In the case of single coat treatments by 400-gallon sprayers the average cost per square yard of 8d. is due to the employment of these machines on special works. Bitumen containing a natural filler and a tar product with an added filler were sprayed on lengths totalling 3·5 miles, and the average cost of these particular works was 11d. per square yard.

The rates of application of bituminous materials were as follows :—

Single coat treatment—

First seal	0·3 gallons bitumen per square yard
Reseal	0·20–0·25 gallons bitumen per square yard

Double coat treatment—

Primer	0·2 gallons cold tar per square yard
Seal coat	0·3 gallons bitumen per square yard

Since the previous season a local tar product "Cold Tar" has been developed for primer coat work on waterbound macadam gravel and sand roads. Cold tar at ordinary temperatures is less viscous than the 50-50 bitumen oil mixture previously used for primer coat work, and is capable of penetrating more easily and more quickly into the road materials. Owing to its low viscosity, cold tar can be used without heating in normal summer weather. The supply of this material is definitely limited, and during the year the Board obtained the maximum quantity that was available.

The technical specification of cold tar is as follows:—

Specific gravity	..	..	1·05 to 1·12
Water	..	..	Less than 2 per cent.
Total bitumen soluble in CS ₂	..	..	Not less than 95 per cent.
Specific viscosity Engler 40° C.	..	..	4 to 10
Naphthalene	..	..	Not more than 1 per cent.
Free carbon	..	..	Not more than 5 per cent.

Distillation.

0-170° C.	..	..	..	Not more than 5 per cent. by volume
170°-235° C.	..	..	..	Not more than 18 per cent. by volume
235°-270° C.	..	..	..	Not more than 18 per cent. by volume
270°-300° C.	..	..	..	Not more than 12 per cent. by volume
Total oils to 300° C.	..	..	..	Not more than 50 per cent. by volume

Pitch Residue.

Softening point	..	..	Not less than 110° F.
Ductility 25° C.	..	..	Not less than 35 cm.
Penetration 50g. 5s. 25c.	..	..	Not more than 150.

At the beginning of the year the use of 85-100 penetration bitumen for seal coat works was adopted.

Previously 60-70 penetration bitumen was used for seal coat work but it was found that great difficulty was experienced in properly filling such seal coats with gravel or screenings owing to the fact that 60-70 penetration bitumen at normal temperatures is quite hard. By the use of the softer grade of bitumen, the covering material is more easily incorporated in the seal coat, and the dangers of "bleeding" in hot weather or of picking up under steel tyred traffic are greatly minimized.

During the year the Board has used experimentally a tar product developed by the Metropolitan Gas Company and to which has been given the name "Bitural." The material produced at first was found to possess a pitch more ductile than that of ordinary distilled tar, but this material was very similar to tar in deterioration by weathering. The later product, in which the tar oils have been replaced with asphaltic oils, appears very promising and has been extensively used in penetration works.

EXPERIMENTAL CONCRETE ROAD—OAKLEIGH.

This experimental road was constructed in 1922 and extends from Atherton-street to about 300 feet past the turnoff to Fern-tree Gully. There are five different sections, three of unsurfaced concrete, one of penetration macadam, and one—Section "A."—of bituminous concrete on a concrete base. For various reasons this latter surface became unsatisfactory, and it was decided to strip off the carpet and examine the base, and then resurface with bituminous concrete. This work was completed in May, 1930.

The base is 5 inches thick at the edges and 7 inches at the centre, with the bottom flat and the top crowned to an arc of a circle. The mix was 1 cement, 2 sand, 0·5 3-in. toppings, 0·8 ¾-in. screenings, 3·2 2½-in. metal, with approximately five bags of cement per cubic yard of concrete. The slump was not measured, but a minimum quantity of water was used. Curing was with wet sand, and was satisfactory. Work started on 9th January, 1922, and was finished by 23rd March, 1922 (there being an interval of over nine weeks). This work was carried out by day-labour, and cost 15s. per square yard, excluding formation, &c., of the subgrade.

The bituminous concrete was laid under contract at 7s. 9d. per square yard, being completed by 18th May, 1922. No tests were made of the bituminous concrete when it was laid, but results of analyses of samples taken when it was being removed recently are given hereafter.

The bituminous concrete was laid in damp weather, and the base was not painted, but only sprinkled with bituminous paint.

In May, 1930, the bituminous concrete was removed by the Council, and a new wearing surface laid under contract at a cost of 10s. 9d. per square yard, plus 6d. per square yard for maintenance for five years. Before laying the new surface the concrete was swept and then painted with bitumen dissolved in benzol.

The following table gives details of the bituminous concrete laid in May, 1922, and in May, 1930. The analyses of the old concrete were made by extracting the bitumen from samples taken from the road in May, 1930. Those on the new material are routine tests made at the mixing plant during construction. All tests are based on A.S.T.M. standard sieves.

TABLE .

A.S.T.M. Sieves.	May, 1922.		May, 1930.		
	A.	B.	C.	D.	
<i>Total Dry Material Passing Sieves.</i>					
1½ inches .. ..	..	..	100	100
1 inch	100	100	99.6	99.3	
¾ inch	96.1	94.3	89.4	88.7	
½ inch	86.5	84.1	67.9	68.0	
¼ inch	67.8	62.9	40.4	41.0	
No. 10	55.4	49.5	37.1	37.1	
No. 40	31.6	30.0	31.8	32.4	
No. 80	17.2	18.3	12.2	11.8	
No. 200	8.9	9.9	5.1	4.9	
<i>Total Sand Passing Sieves.</i>					
No. 10	100	100	100	100	
No. 40	48.8	50.6	83.5	85.4	
No. 80	17.8	21.2	22.9	21.4	
No. 200 .. ..	..	..	..	..
<i>Bitumen, Percentage of Total Wet Mix.</i>					
Asphaltic Concrete ..	11	10	5.6	5.6	
Seal Coat	(Includes flush seal coat.)		9.0	9.1	

As an examination of the concrete base was the primary reason for resurfacing the section, it was carefully surveyed after stripping the wearing surface. The results are given in the accompanying plan, (Figure 4). It had been obvious that the poor condition of the surface was due in part to lack of adhesion of the bitumen to the concrete, and this had been ascribed to too smooth a finish of the concrete surface. That this was not the case was discovered during the operation of stripping, for, although a large area was separate from the concrete, there was considerable adhesion over the rest of the surface, and in places small portions of the bituminous concrete still adhered to the concrete after the surrounding mass had been chipped away.

It was noticed that there was no adhesion along the edges of the pavement and immediately adjacent to all cracks or joints in the concrete; also that there was a marked difference in the colour of the concrete at these places. This change in colour was particularly noticeable along the cracks, which looked as though they had been rendered with cement mortar in an attempt to fill them. This change in appearance is thought to have been caused by water entering between the concrete and the wearing surface—by capillarity up the cracks from the subgrade which was not drained, and from the waterbound macadam shoulders which were never waterproof. This separation had taken place to a much greater extent along the edges where there was more free water than in the subgrade. A defect in the original design was the omission of any kerb against which the bituminous concrete could be consolidated. The waterbound shoulders were partly consolidated, the bituminous concrete spread, and then both consolidated together. In the work carried out recently a line of pitchers was laid to grade before the bituminous concrete was placed.

Since the pavement was opened in 1922, it has carried fairly heavy traffic without serious failure. Several patches of the wearing surface have had to be replaced, but, as indicated above, this was due to lack of drainage of the subgrade and the omission of a kerb rather than to inherent weakness of the base. That this base is still capable of performing its proper functions for at least five more years (total more than thirteen years) is the opinion of two well-known contracting companies, who were each prepared to guarantee their work for five years for 6d. per square yard (about 4½ per cent. of the price of the wearing surface). As the concrete is thicker at the centre than at the edges, the edge thickness—5 inches—may be considered when comparing it with other pavements.

Yours obediently,

L. F. LODER,
Chief Engineer.

EXPERIMENTAL SECTIONS — OAKLEIGH.

SURVEY OF CONCRETE BASE OF SECTION "A", MAY 1930.

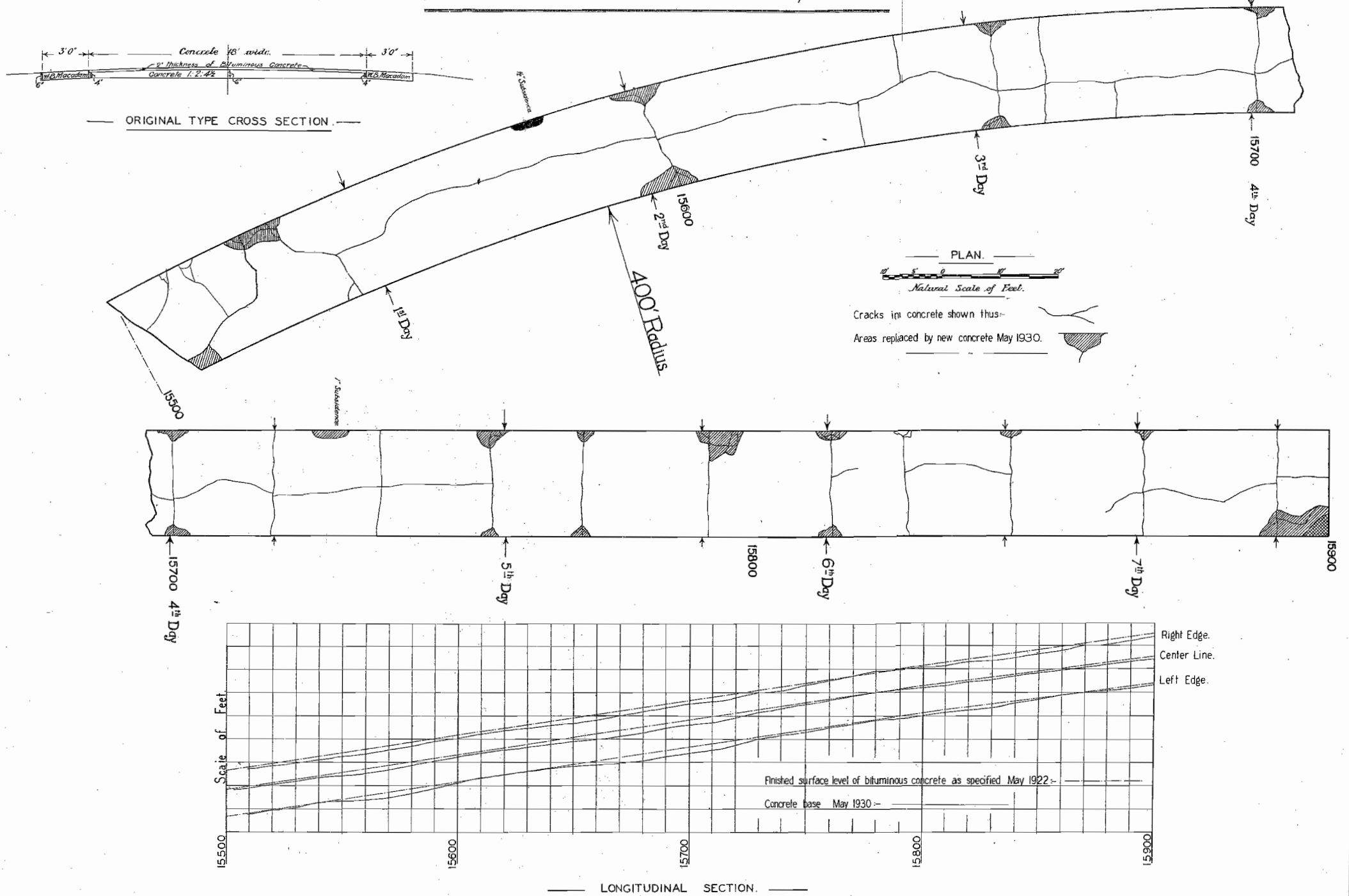


FIGURE No. 4.

APPENDIX A.

COUNTRY ROADS BOARD FUND.

Dr.	Cr.	RECEIPTS.		PAYMENTS.		Cr.	
		£	s. d.	£	s. d.	£	s. d.
1929.	1930.						
July 1.	June 30.						
To Balance	By Maintenance—(Appendix)	192,116	3 2	1,289,974	18 4	1,270,635	9 3
Motor Car Act No. 3741—	Less Refunds			19,339	9 1	720,320	12 3
Registration Fees	Miscellaneous	1,117,845	19 3			1,179	14 0
Licence Fees	Advance under Act No. 3235—Item 4, and Division 65/13/9—Tourist Vote	57,709	2 6				
Fines	Advance, Division 66/4—For Construction of Roads to Relieve Unemployment	15,498	7 0				
Less Refunds and Cost of Administration	Less Refunds	1,191,053	8 9				
		53,140	13 4				
Unused Roads and Water Frontages—	Advances Act No. 3866 for Construction of Roads to Relieve Unemployment						
Licence Fees Act 3730	Less Refunds	22,860	19 5	14,291	9 6		
Less Refunds and Cost of Administration		2,168	12 9	1,776	3 6	12,515	6 0
Motor Omnibus Act No. 3742—	Advances, Division 66/4—For Construction of Roads for Isolated Settlers					1,678	2 0
Fees and Fines	Advances for Construction of Roads under Migrants' Grant Closer Settlement Board	2,061	16 10				
Less Refunds	Less Refund	21	5 10	1,701	17 6		
				147	0 6		
Country Roads Act No. 3662—	Advances for Construction of Migration Developmental Roads—Act No. 3828	1,464	0 0			1,554	17 0
Registration of Traction Engines	Less Refund	643	3 0	13,932	5 9		
Width of Tyres Administration—				102	1 5		
Fees and Fines	Less Transfer to Developmental Roads Loan Act No. 3662						
				13,830	4 4		
Acts Nos. 3662 and 3742—Costs				3,457	11 1	10,372	13 3
Municipalities Repayments—	Advance under Division 66/4—For Construction of Roads for Relief of Unemployment in Thrip-infested Areas					2,955	3 1
Permanent Works	Less Refund					77	12 7
Maintenance	Advances, Division 46/6—For Extension of Great Ocean Road—Mount Defence Section					3,897	8 3
	Less Refunds					14	1 6
Appropriation for Maintenance	Advances for Extension of Great Ocean Road—Manifold's Gift						
Hire of Plant	Less Refund					1,119	2 11
Stores and Materials						119	2 11
Sundries	Advance for Great Ocean Road—Airey's River Bridge					536	11 11
	Less Refund					42	11 11
Advances on Account of Expenditure under Act No. 3235—Item 4, and Division 65/13, Item 9—Tourist Vote—Refunded	Advances for Expenditure—F.A.R. Trust					20,036	9 5
On Account of Expenditure to Relieve Unemployment	Less Refund					2,899	11 1
Advances Act 3866—On Account of Expenditure to Relieve Unemployment—Refunded	Advances—Caldor Memorial Avenue Tree Planting						
Advances—Division 66/4—Construction of Roads for Isolated Settlers—Refunded	Balance					17,136	18 4
Division 67/2—Roads and Bridges						31	17 0
						23,676	3 2
Carried forward	Carried forward					2,095,820	19 0

APPENDIX A—continued.

COUNTRY ROADS BOARD FUND—continued.

RECEIPTS—continued.		PAYMENTS—continued.	
Dr.	£ s. d.	£ s. d.	Cr.
1930.			
June 30.	Brought forward	1,936,642 9 5	2,095,820 19 0
	To Advances for Constructing Roads under Migrants' Grant—Childers Settlement Road—Refunded ..		
	Advances for Construction of Migration Developmental Roads Act 3828—Refunded	1,547 1 11	
	Advances—Division 66/4—Construction of Roads to Relieve Unemployment in Thrip-infested Areas—Refunded	31,988 5 8	
	Advances for Extension of Great Ocean Road—Division 46/6—Mount Defiance Section	3,000 0 0	
	Manifold's Gift	4,066 17 8	
	Airey's River Bridge	1,000 0 0	
	Advance F.A.R. Trust Recoup	494 0 0	
	Advance from Public Account	17,082 4 4	
		100,000 0 0	
		<u>2,095,820 19 0</u>	

RECONCILIATION STATEMENT.

Balance as per Country Roads Board Accounts	£	s. d.	£	s. d.
Adjustment Outstanding Transfers	23,676	3 2	8,542	8 5
	635	0 9	635	0 9
	<u>23,041</u>	<u>2 5</u>	<u>7,907</u>	<u>7 8</u>
			41,980	3 10
			18,639	1 5
			<u>23,041</u>	<u>2 5</u>

REVENUE ACCOUNT, 30TH JUNE, 1930.

Dr.	£ s. d.	Cr.	£ s. d.
1930.			
June 30.	To Maintenance Works—General	664,007 7 7	
	Maintenance Works—		
	Wood's Point Road	3,486 17 5	
	Wood's Point Road	1,423 2 6	
	Walhalla Road	1,544 13 0	
	Mount Buffalo Road	9,715 3 3	
	State Highways	590,458 5 6	
		<u>606,628 1 8</u>	
	Contribution to Sinking Fund		1,270,635 9 3
	Interest on Loans	29,670 12 3	
		89,011 17 7	
		<u>118,682 9 10</u>	
	Recoup to Revenue Act 3844—		
	Interest—		
	Main Roads	124,260 1 4	
	Developmental Roads	124,690 9 4	
		<u>248,950 10 8</u>	
	Balance as per Treasury Books		£
	Add Outstanding Transfers		s. d.
	Adjustment		£
			s. d.
			1,137,912 15 5
	Deduct Accounts in Transit		
			20,692 6 8
			1,464 0 0
			<u>265 2 8</u>

APPENDIX A—continued.

REVENUE ACCOUNT, 30TH JUNE, 1930—continued.		1930.		1930.		1930.		Cr.		
Dr.	June 30.	£	s.	d.	£	s.	d.	£	s.	d.
To Sinking Fund—										
Main Roads	10,202 11 9									
Developmental Roads	12,320 0 1									
		22,522	11	10	271,473	2	6			
Audit Fee		473	4	6						
Experimental Section		4,497	17	2						
Federal Aid Commission, 2 per cent.		9,032	18	6						
Instruments Account		252	0	9						
Insurance of Employees	114 6 2									
Highway Robbery	60 0 0									
Motor Expenses		174	6	2						
Mount Hotham Telephone Line		4,470	7	5						
New Offices, Exhibition Buildings		175	2	8						
New Storeyard		721	7	11						
Office Expenses		377	13	4						
Office Furniture		1,830	13	1						
Patrolmen's Cottages		2,093	4	9						
Plans Purchase		1,830	15	9						
Plant Purchase		10,132	12	9						
Postages and Telegrams		1,710	8	3						
Quarry Sites and Metal Investigation		5,086	11	4						
Printing and Stationery		2,735	5	0						
Record Survey		1,335	13	6						
Salaries		46,676	10	6						
Storage Sites		373	3	10						
Testing Materials		824	1	9						
Travelling Expenses		2,981	15	2						
Timber Revenue Account	96 7 9									
Less Receipts	46 4 1									
Width of Tires and Motor Omnibus		50	3	8						
Act		5,522	3	3						
Less Fees, Fines, and Costs		2,797	9	6						
Direction Posts and Warning Signs		2,724	13	9						
Traffic Census		4,002	12	0						
Airey's River Bridge Supervision		10	2	11						
Advertising, Government Gazette		41	7	9						
Valuations		236	10	3						
Investigation Surveys		121	0	0						
Rents—		67	5	10						
Quarry Site, Swan Hill		50	10	0						
Garage Site, Broadford		0	15	0						
Storage Site, Bendigo		3	8	6						
Works Film		227	8	0						
F.A.R. Trust, Expenditure					106,079	0	7			
Balance					17,136	18	4			
					327,203	15	1			
					2,111,210	15	7			
								1,534,790	11	2

APPENDIX A—continued.

BALANCE-SHEET AT 30TH JUNE, 1930.

LIABILITIES.		ASSETS.	
	£ s. d.		£ s. d.
Contractors' Deposits	19,108 14 4	Country Roads Board Fund	23,676 3 2
Sundry Liabilities	7,735 5 0	Transfers Outstanding	635 0 9
Sinking Fund	322,723 16 8	Maintenance Expenditure—	
Advance from Public Account	100,000 0 0	Contributions Payable by Municipalities	198,121 16 7
Revenue Account	327,203 15 1	Contributions in Arrears	521 2 0
		Permanent Works—	
		Contributions Payable by Municipalities	132,957 19 7
		Contributions in Arrears	2,486 19 8
		Federal Aid Roads Act 1926—	
		Special Advance	22,500 0 0
		Commission Account, 1½ per cent.	31,883 13 5
		State Payment to Sinking Fund	8,767 8 10
		Less Expenditure F.A.R. Trust	40,651 2 3
			17,136 18 4
			23,514 3 11
		Outstanding Accounts	46,014 3 11
		Materials—Stock—	14,077 4 1
		Storeyard	16,268 19 0
		Branches	1,449 12 10
		Investment Account for Redemption of Loans	17,718 11 10
		Trust Account	322,723 16 8
			19,108 14 4
			776,771 11 1

COUNTRY ROADS BOARD LOAN ACCOUNT, ACT No. 3662/2635, ETC.

RECEIPTS.		PAYMENTS.	
	£ s. d.		£ s. d.
1929.		1930.	
July 1. To Balance	29,361 4 9	June 30. By Permanent Works (Appendix)	185,698 2 9
1930.		Deduct Refunds	576 9 0
June 30. Proceeds of Loans	129,883 6 0		
Public Account	9,542 16 3		
Transfers	15,693 7 0		
Balance Cr.	640 19 9		
	185,121 13 9		

RECONCILIATION.

	£ s. d.
Accounts in Transit	1,092 0 1
Outstanding Credits	451 0 4
Balance Cr.	640 19 9
	185,121 13 9

APPENDIX A—continued.

DEVELOPMENTAL ROADS INTEREST, ACT No. 3662, (Nos. 2944, 2985, AND 3255).

RECEIPTS.		EXPENDITURE.	
1930. June 30.	To Interest Contributed by Municipalities— Acts Nos. 3662 and 2944 Act No. 2985 Act No. 3255	1930. June 30.	By Payments to Treasury
£	s. d.	£	s. d.
..	8,919 0 2	..	..
..	26,873 5 10	..	..
..	57,805 14 9	..	..
	<u>93,598 0 9</u>		
	93,598 0 9		93,598 0 9

AUDITOR-GENERAL'S CERTIFICATE.

The Accounts have been audited and compared with the books, with which they agree. Recon-
ciliations have also been made with the books of the Treasury. I certify that the statements submitted
are correct.

J. A. NORRIS,
Auditor-General,

20th November, 1930

E. J. HICKS, Accountant,
19th November, 1930.

APPENDIX B.

TOURIST VOTE FOR IMPROVING AND MAKING ACCESSIBLE PLACES OF INTEREST—TOURIST RESORTS, ETC.—“A.” ACT No. 3235, ITEM 4. “B.” DIVISION 65, SUBDIVISION 13, ITEM 9.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

		£	s.	d.
Hall's Gap-Wartook Road	.. Construction of Roads into and through Grampians	979	14	0
Great Ocean Road	.. Extension of Great Ocean Road from Cumberland River to George River*	150	0	0
..	.. Extension of Great Ocean Road to Carisbrook Falls*	50	0	0
		<hr/>		
		1,179	14	0

“A,” £979 14s.

* “B,” £200.

SPECIAL GRANT FOR RELIEF OF UNEMPLOYED—DIVISION 66/4.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

Shire.	Road.	£	s.	d.
Alberton	.. Dingo Creek	2,151	0	7
Buln Buln and Narracan	.. Willowgrove-Fumina	4,055	10	11
Orbost	.. Bonang-Delegate	5,873	19	11
Otway	.. Beech Forest-Apollo Bay	1,225	3	9
..	.. Ferguson-Charley's Creek	6,593	19	1
Rosedale	.. Merriman's Creek	8,488	7	5
..	.. Great Ocean Road	76	7	10
		<hr/>		
		28,464	9	6

UNEMPLOYMENT RELIEF WORKS ACT 3866.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

		£	s.	d.
Ballarat-Daylesford	.. .. .	12	7	0
Benambra-Corryong	.. .. .	225	3	10
Buchan-Ensay	.. .. .	3,469	14	1
Castlemaine-Daylesford	.. .. .	1	13	0
Deddick River	.. .. .	3,189	3	4
Dollar-Dumbalk	.. .. .	929	6	8
Geelong-Ballarat	.. .. .	89	13	4
Great Ocean Road	.. .. .	867	11	3
Kennedy's Creek	.. .. .	29	19	0
Melbourne-Ballarat	.. .. .	133	2	1
Melbourne-Geelong	.. .. .	9	13	3
Morwell River	.. .. .	142	11	0
Mt. Buffalo	.. .. .	548	0	7
Serpentine-Durham Ox	.. .. .	15	19	6
Skene's Creek	.. .. .	2,253	3	9
St. Arnaud-Charlton	.. .. .	5	11	5
Whitfield-Tolmie	.. .. .	18	18	11
Wild Dog-Strezlecki	.. .. .	573	14	0
		<hr/>		
		12,515	6	0

APPENDIX B—continued.

ROADS FOR ISOLATED SETTLERS, 66/4.—ORCHARDISTS' RELIEF, 66/4.—MIGRANTS' GRANT.—
CHILDERS SETTLEMENT ROAD AND GREAT OCEAN ROAD EXTENSION.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

Shire.		Road.		£	s.	d.
Alberton	Binjinwarri—Albert River	97	10	0
"	Golding's	95	0	0
"	Livingstone	185	13	2
Buln Buln	Toronga Valley	85	7	2
Glenlyon	Balharries	100	0	0
Healesville	Old Chum Creek	99	14	4
Heytesbury	Cameron's Hill—Newfield	97	4	6
"	Glenfyne	100	0	0
Kyneton	Bowden—McKeon's	98	0	0
Lilydale	Head's	29	4	6
Omeo	Bingo Creek	100	0	0
Otway	Old Hordernvale	39	15	0
"	Osborne and Rose's Access	100	0	0
"	Johanna Creek	50	0	0
"	Hall's	50	0	0
South Gippsland	McCartin's	124	5	11
"	Old Boolarra	86	5	0
"	Schmidt's Outlet	49	19	10
Tambo	Bills'	40	2	7
"	Butcher's Creek	50	0	0
				1,678	2	0

ROAD FOR RELIEF OF UNEMPLOYED IN THRIP-INFESTED AREA.

Eltham Shire	Hurstbridge—Kingleake East	2,877	10	6
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MIGRANTS' GRANT.

Narracan Shire	Childers Settlement Road	1,554	17	0
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GREAT OCEAN ROAD.

Great Ocean Road Extension	Mt. Defiance Section (Division 46/6)	3,883	6	9
"	..	..	Airey's River Bridge	494	0	0
"	..	..	Mr. E. Manifold's Gift	1,000	0	0
				5,377	6	9

MIGRATION DEVELOPMENTAL ROADS.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

Shire.		Road.		£	s.	d.
Mildura	Bambill North	23	14	8
"	Bambill South	5	4	8
"	Benetook North	9	13	11
"	Benetook South	745	3	3
"	Karawinna North	1,020	2	2
"	Karawinna South	1,834	3	4
"	Meringur North	2,207	8	0
"	Meringur South	3,160	15	4
"	Merrinee North	1,088	15	2
"	Merrinee South	1,344	0	7
"	Pirlta North	227	3	0
"	Pirlta South	83	0	8
"	Werrimull North	733	1	7
"	Werrimull South	147	2	0
"	Yarrara North	18	10	0
"	Yarrara South	1,182	6	0
				*13,836	4	4

* This expenditure has been incurred under provisions of Migration Agreement for Construction of Roads on the Red Cliffs to Meringur Line. The total estimated expenditure is £120,000—£30,000 to be borne by Country Roads Board and £90,000 by Migration Commission. One quarter of expenditure to 30th June, 1930, has been transferred to Developmental Roads Loan Act No. 3662-3255, and three quarters has been borne by State Treasury under provisions of Act No. 3828.

APPENDIX B—*continued.*

FEDERAL TRUST.

STATEMENT FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

	£	s.	d.
Orbost-Delegate	5,000	0	0
Buchan-Ensay	5,470	0	0
Benambra-Corryong	6,666	18	4
	17,136	18	4

FEDERAL AID ROADS ACT 1926.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

	£	s.	d.
<i>Main Developmental Roads.</i>			
1. Morwell River	4,014	18	5
2. Turton's Creek .. .. .	..	..	..
3. Fumina	2,963	12	4
4. Forrest-Apollo Bay	3,293	0	0
6. Allambee Estate	908	12	2
7. Yarra Glen-Glenburn	3,215	8	5
8. Orbost-Delegate	13,874	19	2
9. Jeeralang West	226	9	11
12. Apollo Bay-Wye River	2,616	17	1
14. Timboon-Nirranda	2,452	18	9
16. Murray River Valley—Cohuna	10,806	1	10
" " " Cohuna and Rochester	573	1	4
" " " Kerang	7,223	12	8
" " " Swan Hill	6,244	2	0
" " " Mildura	1,595	10	5
" " " Rochester	2,060	14	3
" " " Upper Murray	883	16	2
17. Toombullup	6,903	2	11
22. Lismore-Pittong	1,225	18	4
29. Boolarra-Foster .. .. .	..	..	..
30. Shepparton-Rochester-Wycheproof-Rainbow-Rochester	801	3	6
" " " Rodney	4,350	19	1
33. Laver's Hill-Princetown	10,616	15	0
34. Mt. Vernon and Boorool—Mirboo	1,907	8	10
" " " Woorayl .. .. .	..	..	..
35. Carlisle-Gellibrand .. .. .	..	..	..
36. Boolarra-Mirboo—Mirboo	3,125	0	0
" " " Morwell	2,169	8	0
38. Cann Valley	2,696	14	4
39. Bonang-Gelantipy	15	0	0
42. Hordern Vale-Apollo Bay	9,480	19	8
43. Kinglake	3,895	10	3
44. Little Desert	1,453	9	9
45. Mt. Camel Estate—McIvor	4,128	11	5
" " " Waranga	1,053	0	0
46. Mt. Sabine-Laver's Hill	9,850	2	9
47. Orbost-Buchan	2,744	9	3
48. Panitya North	2,466	1	0
49. Peterborough-Port Campbell	2,899	0	6
50. Sale-Foster	4,655	2	7
51. Vite Vite—Hampden	498	0	0
" " " Mortlake	1,604	10	0
52. Walhalla-Matlock	2,366	5	3
53. Whitfield-Tolmie	4,041	5	6
55. Ouyen-Piangil	1,650	14	4
56. Timboon-Scott's Creek	2,074	5	8
57. Laver's Hill-Glen Aire	1,315	9	1
21. Murray River-Tallangatta	5,196	14	9
	158,138	16	8

APPENDIX B—continued.

FEDERAL AID ROADS ACT 1926—continued.

		£	s.	d.
<i>Trunk Roads.</i>				
501. Casterton—Mt. Gambier	.. .. .	2,563	18	5
502. Horsham—Murtoa—Minyip	.. .. .	4,868	10	5
503. Marnoo—Donald—Stawell	.. .. .	1,580	13	11
" " Dunmunkle	.. .. .	2,870	12	0
		11,883	14	9
<i>Arterial Roads.</i>				
Prince's Highway West	.. .. .	196,092	14	1
Prince's Highway East	.. .. .	82,720	16	10
Western Highway	.. .. .	102,270	16	1
Calder Highway	.. .. .	47,904	12	6
Northern Highway	.. .. .	15,532	14	8
Hume Highway	.. .. .	63,129	19	9
Omeo Highway	.. .. .	15,904	9	0
		523,556	2	11

FEDERAL AID ROADS ACT 1926.

SUMMARY OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

		£	s.	d.
Main Developmental Roads	.. .. .	158,138	16	8
Trunk Roads	.. .. .	11,883	14	9
Arterial Roads	.. .. .	523,556	2	11
		693,578	14	4

FEDERAL UNEMPLOYMENT RELIEF.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1930.

		£	s.	d.
Callignee Estate	.. .. .	4,855	17	5
Buchan—Ensay	.. .. .	5,000	0	0
Morwell River	.. .. .	5,470	0	0
Murray Valley Road	.. .. .	63	0	0
Kinglake—Glenburn	.. .. .	1,833	2	3
Wye River Extension	.. .. .	10,000	0	0
Sunnyside	.. .. .	4,000	0	0
Fumina—Hill End	.. .. .	3,530	0	0
Wild Dog—Strezlecki	.. .. .	6,000	0	0
Emerald—Gembrook	.. .. .	4,458	3	0
Timboon—Curdie's Vale	.. .. .	2,789	1	3
Deddick River	.. .. .	10,000	0	0
Winnap—Drik Drik	.. .. .	168	13	3
Chum Creek—Kinglake	.. .. .	111	18	3
Woori Yallock—Cockatoo	.. .. .	145	2	0
Sunny Creek	.. .. .	361	7	6
Mildura—Wentworth	.. .. .	2,422	5	9
Echuca—Gunbower	.. .. .	15	6	3
Benalla—Shepparton	.. .. .	819	1	3
Eildon Weir—Jamieson	.. .. .	17,290	10	9
Casterton—Mt. Gambier	.. .. .	395	10	6
Newfield—Port Campbell	.. .. .	569	1	8
Murray River Valley—Hume Weir	.. .. .	3,515	7	5
Castlemaine—Maryborough	.. .. .	8,435	7	4
Chiltern—Howlong	.. .. .	539	5	10
Tawonga—Bright	.. .. .	2,000	0	0
Echuca—Wyuna	.. .. .	149	0	0
Geelong—Ballarat	.. .. .	5,215	1	0
Spargo Creek	.. .. .	1,052	4	8
Whittlesea—Kinglake	.. .. .	1,210	3	6
Grubbed	.. .. .	14	1	8
Barnawatha—Howlong	.. .. .	19	3	10
Highlands	.. .. .	4,014	17	0
Porcupine Ridge	.. .. .	123	0	0
		106,585	13	4

APPENDIX B—continued.

TRUST FUND (ACT No. 3662)—GRANTS TO MUNICIPALITIES.
EXPENDITURE UNDER THE SUPERVISION OF MUNICIPALITIES, YEAR ENDED 30TH JUNE, 1930.

	Trust Fund.		Special Appropriation.	
	£	s. d.	£	s. d.
Box Hill City	440	0 0
Brighton City	*980	0 0
"	973	0 0
Brunswick City	*1,857	0 0
"	1,918	0 0
Camberwell City	*1,439	0 0
"	1,540	0 0
Caulfield City	*2,314	0 0
Chelsea City	243	0 0
Coburg City	1,225	0 0	..	..
Collingwood City	1,129	0 0
Essendon City	1,459	0 0
Footscray City	1,698	0 0
Hawthorn City	1,129	0 0
Kew City	873	0 0
Malvern City	*1,591	0 0
Melbourne City	3,530	0 0
Mordialloc City	341	0 0
Oakleigh City	373	0 0
Port Melbourne City	455	0 0
Prahran City	1,786	0 0
Richmond City	1,460	0 0
Sandringham City	706	0 0	..	..
South Melbourne City	1,619	0 0
St. Kilda City	*1,504	0 0
"	1,460	0 0
Williamstown City	*850	0 0
"	842	0 0
Blackburn and Mitcham Shire	217	0 0	..	..
Braybrook Shire	*226	0 0
Broadmeadows Shire	178	0 0
Doncaster and Templestowe Shire	103	0 0
Eltham Shire	152	0 0
Heidelberg Shire	885	0 0	865	0 0
Moorabbin Shire	*416	0 0
"	537	0 0
Werribee Shire	209	0 0	..	..
Ballarat City	1,289	0 0
Bendigo City	*952	0 0
Geelong City	569	0 0
Geelong West City	457	0 0
Warrnambool City	275	0 0
Mildura Town	*217	0 0
Newtown and Chilwell Town	317	0 0	..	..
Sale Town	*144	0 0
	3,559	0 0	40,183	0 0

* Grant for 1928-29 Financial Year.

SUMMARY OF BOARD'S ASSETS AT 30TH JUNE, 1930.

	£	s. d.	£	s. d.
Workshop Fittings, &c.	2,617	17 10	..	..
Furniture and Fittings	10,111	0 10	..	..
Instruments (Survey)	441	7 6	..	..
Instruments (Pistols)	44	0 0	..	..
Testing Laboratory	342	3 6	..	..
Works Film	227	8 0	..	..
Patrolmen's Cottages	11,102	15 0	..	..
Storeyard No. 1	5,316	5 0	..	..
Storeyard No. 2	1,296	0 0	..	..
Motor Cars and Motor Cycles, including Police Motor Cycles	5,973	6 6	..	..
			37,472	4 2
Working Plant .. .. .	..	..	65,371	15 0
Total .. .. .	..	..	102,843	19 2

E. J. HICKS,
Accountant.

19th November, 1930.

STATEMENT OF APPORTIONMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF
MAIN ROADS, ETC.—*continued.*

Name of Municipality.	Permanent Works.		Maintenance.	Name of Municipality.	Permanent Works.		Maintenance.
	Principal.	Interest.	Amount.		Principal.	Interest.	Amount.
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
Brought forward	68,869 17 8	1,277 19 4	108,546 19 0	Brought forward	89,414 13 1	1,539 9 7	130,159 14 7
Portland Shire ..	519 18 3	10 9 5	960 14 10	Tullaroop Shire ..	2,172 12 2	40 0 2	907 16 7
Preston City ..	..	..	579 5 8	Tungamah Shire ..	1,693 2 8	21 6 11	344 16 6
Pyalong Shire ..	..	..	504 4 2	Upper Murray Shire	1,108 12 11	23 4 2	291 7 11
Queenscliff Borough	2 1 7*	0 0 8	505 18 2	Upper Yarra Shire	..	..	401 16 4
Ringwood Borough	117 0 11	2 15 5	1,315 6 7	Violet Town Shire	1,099 0 0	9 19 5	189 9 4
Ripon Shire ..	..	..	1,312 11 5	Walpeup Shire ..	814 7 11	18 14 11	324 16 5
Rochester Shire ..	989 7 11	3 15 11	309 19 8	Wangaratta	11 5 8	0 2 2	465 2 8
Rodney Shire ..	826 8 4	12 1 10	1,560 0 3	Borough	..	..	..
Romsey Shire ..	..	..	678 16 8	Wangaratta Shire	483 9 8	10 8 9	312 8 11
Rosedale Shire ..	446 18 1	9 1 1	481 10 4	Wannon Shire ..	6 8 7*	0 0 11	1,039 10 7
Rutherglen Shire..	168 7 1	1 1 6	854 19 0	Waranga Shire ..	1,867 11 4	41 14 3	2,239 0 11
Sale Town ..	..	..	340 11 4	Warragul Shire ..	426 10 6	12 12 10	2,082 18 8
Sebastopol Borough	..	..	524 4 1	Warrnambool Shire	..	..	3,936 6 9
Seymour Shire ..	1,702 14 6	14 18 8	259 6 9	Werribee Shire ..	..	..	150 15 3
Shepparton Shire	3,909 14 10	74 13 2	1,234 8 4	Whittlesea Shire ..	147 8 11	0 15 3	1,036 2 6
Shepparton Borough	..	..	323 11 2	Wimmera Shire ..	274 5 2	4 18 7	507 15 9
South Barwon Shire	14 4 3	0 3 1	2,262 0 5	Winchelsea Shire	1,227 11 10	16 18 10	971 7 2
South Gippsland	3,302 7 1	25 15 9	606 12 3	Wodonga Shire ..	35 16 3	1 6 7	728 12 6
Shire	..	..	..	Wonthaggi Borough	..	..	392 15 7
St. Arnaud Borough	..	..	523 13 8	Woorayl Shire ..	3,410 5 3	62 2 7	1,836 15 4
Stawell Borough ..	351 11 5	10 5 8	115 18 7	Wycheproof Shire	853 9 2	17 16 8	708 1 3
Stawell Shire ..	4,679 4 8	55 1 2	753 9 9	Yaekandandah	2,989 10 4	47 11 2	507 6 6
Strathfieldsaye Shire	..	..	749 0 0	Shire	..	..	..
Swan Hill Shire ..	2,922 16 5	30 12 4	2,423 13 3	Yarrowonga Shire	1,386 14 5	31 15 7	831 13 10
Talbot Shire ..	..	..	349 14 3	Yea Shire ..	1,555 12 4	26 3 4	1,520 17 11
Tambo Shire ..	12 17 2	0 4 11	688 13 3				
Towong Shire ..	529 6 1	8 8 7	624 0 8				
Traralgon Shire ..	49 16 10	2 1 1	770 11 0				
Carried forward	89,414 13 1	1,539 9 7	130,159 14 7	Total ..	110,978 8 2	1,927 2 8	151,887 9 9

* Liability paid in full.

APPENDIX D.

COUNTRY ROADS BOARD.

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF MAIN ROADS FOR THE YEAR ENDING 30th JUNE, 1930.

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
ALBERTON SHIRE—				
Balook-Traralgon Road .. .. .	.. .. .	..	61 8 3	
Balook-Yarram Road	2 3 6		..	
Boolarra-Welshpool Road	1 5 0		Bd. 167 11 7	
Boolarra-Yarram Road	10 15 0		..	
Carrajung-Gormandale Road .. .. .	.. .. .		1,031 12 7	
Foster-Yarram Road	1,079 12 0		597 7 10	
Sale-Yarram Road	1,182 10 7		1,512 16 6	
Yarram-Boolarra Road .. .. .	.. .. .		2,838 8 3	
Yarram-Port Albert Road .. .. .	.. .. .		1,561 7 2	
Yarram-Won Wron Road .. .. .	.. .. .		584 19 10	
ALBERTON AND MORWELL SHIRES (Joint Works)—				
Boolarra-Yarram Road .. .. .	.. .. .		6 0 0	
Boolarra-Welshpool Road .. .. .	.. .. .		Bd. 55 4 11	
ALBERTON, MORWELL, AND SOUTH GIPPSLAND SHIRES (Joint Works)—				
Gunyah-Ryton Road .. .. .	.. .. .		Bd. 95 19 4	
ALBERTON AND ROSEDALE SHIRES (Joint Works)—				
Carrajung-Gormandale Road	53 8 8		17 10 2	
Sale-Yarram Road .. .. .	.. .. .		Bd. 222 14 5	
		2,329 14 9		8,753 0 10
ALEXANDRA SHIRE—				
Cathkin-Mansfield Road .. .. .	.. .. .		903 11 1	
Healesville-Alexandra Road .. .. .	.. .. .		1,621 3 1	
Upper Goulburn Road	124 7 2		3,440 7 6	
Yarck Road	1,244 0 1		177 16 2	
ALEXANDRA AND YEA SHIRES (Joint Works)—				
Upper Goulburn Road .. .. .	.. .. .		36 8 4	
		1,368 7 3		6,179 6 2
ARAPILES SHIRE—				
Horsham-Hamilton Road	184 16 5		1,332 11 6	
Horsham-Natimuk-Edenhope Road	920 8 0		310 1 6	
ARAPILES, KOWREE, DUNDAS AND WIMMERA SHIRES (Joint Works)—				
Horsham-Hamilton Road	6 2 6		2 3 6	
ARAPILES AND WIMMERA SHIRES (Joint Works)—				
Horsham-Hamilton Road .. .. .	.. .. .		225 9 9	
ARAPILES, WIMMERA, AND HORSHAM BOROUGH (Joint Works)—				
Horsham-Hamilton Road .. .. .	.. .. .		26 13 4	
ARARAT SHIRE—				
Ararat-Elmhurst Road .. .. .	.. .. .	1,111 6 11	1,404 14 0	1,896 19 7
Ararat-Warrnambool Road .. .. .	.. .. .		3,381 17 3	
Ballarat-Hamilton Road	0 8 8		2,594 8 10	
Maroona-Glenthompson Road .. .. .	.. .. .		2,936 14 1	
		0 8 8		10,317 14 2
ARARAT BOROUGH—				
Ballarat-Stawell Road .. .. .	.. .. .		721 3 11	721 3 11
AVOCA SHIRE—				
Ararat Road .. .. .	.. .. .		459 18 0	
Ballarat-St. Arnaud Road .. .. .	.. .. .		1,720 4 4	
Bealiba Road .. .. .	.. .. .		359 17 7	
Landsborough Road .. .. .	.. .. .		79 12 5	
Maryborough Road .. .. .	.. .. .		520 10 10	
				3,140 3 2
AVON SHIRE—				
Dargo Road .. .. .	.. .. .		535 10 0	
Maffra-Sale Road .. .. .	.. .. .		770 16 10	
Prince's Highway .. .. .	.. .. .		1,590 9 6	
				2,896 16 4
Carried forward .. .. .	.. .. .	4,809 17 7	..	33,905 4 2

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	4,809 17 7	..	33,905 4 2
BACCHUS MARSH SHIRE—				
Ballarat Road .. .. .	..	..	1,633 6 4	
Geelong-Bacchus Marsh Road .. .. .	..	..	1,095 11 7	
Gisborne Road .. .. .	..	..	1,593 10 11	4,322 8 10
BAIRNSDALE SHIRE—				
Bulmwaal-Tabberabbera Road	83 5 7		1,086 1 1	
Prince's Highway	19 10 0	102 15 7	972 17 10	2,058 18 11
BALLAN SHIRE—				
Ballarat Road .. .. .	..	..	24 0 1	
Daylesford Road .. .. .	..	..	2,159 18 6	
Gordons-Meredith Road .. .. .	..	..	439 5 4	
Mount Wallace Road .. .. .	..	..	558 1 0	3,181 4 11
BALLARAT SHIRE—				
Ballarat-Creswick Road .. .. .	..	..	0 3 0	
Ballarat-Lexton Road .. .. .	..	..	4,724 9 10	
Maryborough-Ballarat Road .. .. .	..	..	1,718 7 0	
BALLARAT AND BUNGAREE SHIRES AND BALLARAT CITY (Joint Works)—				
Ballarat-Creswick Road	646 13 11	646 13 11	..	6,442 19 10
BANNOCKBURN SHIRE—				
Geelong-Ballarat Road .. .. .	..	..	967 3 6	
Gordons-Meredith Road .. .. .	..	..	93 9 4	
Inverleigh Road .. .. .	..	..	1,866 7 4	
Shelford-Bannockburn Road .. .. .	..	..	157 2 8	3,084 2 10
BARRARBOOL SHIRE—				
Anglesea Road .. .. .	..	..	3,191 3 8	
Hendy Main Road .. .. .	..	..	265 8 8	3,456 12 4
BASS SHIRE—				
Almurta Road .. .. .	..	..	284 12 2	
Almurta-Grantville Road .. .. .	..	..	275 6 10	
Dalyston-Wonthaggi Road .. .. .	..	..	404 12 10	
Inverloch-Wonthaggi Road .. .. .	..	..	698 4 9	
Korumburra-Wonthaggi Road	333 3 2		1,154 12 6	
Main Coast Road	2,238 6 3		774 16 8	
Wonthaggi-Loch Road	1,858 8 10		1,164 12 5	
BASS SHIRE AND WONTHAGGI BOROUGH (Joint Works)—				
Loch-Wonthaggi Road .. .. .	..	4,429 18 3	273 2 2	5,030 0 4
BEECHWORTH SHIRE—				
Beechworth Road .. .. .	..	..	769 15 3	
Bright Road .. .. .	..	..	180 9 11	
Everton-Myrtleford Road	342 2 9		577 7 6	
Stanley Road .. .. .	..	..	132 14 5	
BEECHWORTH AND BRIGHT SHIRES (Joint Works)—				
Bright Road .. .. .	..	..	57 13 6	
BEECHWORTH AND WANGARATTA SHIRES (Joint Works)—				
Beechworth Road .. .. .	..	..	16 16 0	
Wangaratta Road .. .. .	..	..	1 19 8	
BEECHWORTH AND OXLEY SHIRES (Joint Works)—				
Bright Road .. .. .	..	342 2 9	2 18 5	1,739 14 8
BELFAST SHIRE—				
Hamilton Road .. .. .	..	..	3,623 15 4	
Penshurst Road .. .. .	..	..	3,922 0 10	7,545 16 2
BELLARINE SHIRE—				
Geelong-Portarlinton Road .. .. .	..	..	1,810 1 10	
Barwon Heads-Ocean Grove Road .. .. .	..	..	35 0 0	
Geelong-Queenscliff Road .. .. .	..	..	11 10 0	
Geelong-Portarlinton Road .. .. .	..	..	Bd. 4,097 2 9	
Ocean Grove-Barwon Heads Road .. .. .	..	..	Bd. 1,090 16 0	
Geelong-Queenscliff Road .. .. .	..	..	Bd. 4,661 12 6	11,706 3 1
BENALLA SHIRE—				
Benalla-Mansfield Road	77 1 1		1,021 7 4	
Goorambat Road .. .. .	..	..	1,828 13 2	
Goorambat-Thoona Road .. .. .	..	..	536 19 4	
Greta Road .. .. .	..	..	0 17 3	
Lima Road .. .. .	..	..	0 4 4	
Sydney Road .. .. .	..	..	828 14 3	
Tatong-Toombullup Road .. .. .	..	..	311 16 7	
		77 1 1		4,528 12 3
Carried forward .. .. .	..	10,409 9 2	..	87,001 18 4

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	10,409 9 2	..	87,001 18 4
BERWICK SHIRE—				
Gembrook Road .. .. .	..		382 3 3	
Gembrook-Beenak Road .. .. .	..		286 2 4	
Hallam-Emerald Road .. .. .	..		233 11 8	
Nar-nar-goon-Longwarry Road .. .. .	..		974 15 2	
Prince's Highway .. .. .	..		0 11 1	
Woori Yallock-Pakenham-Koo-wee-rup Road	892 3 9		2,687 10 4	
Prince's Highway .. .. .	..		(Bd.) 186 19 11	
		892 3 9		4,751 13 9
BET BET SHIRE—				
Avoca-Bealiba Road	1,344 11 3		1,200 8 9	
Betley Road .. .. .	..		224 5 10	
Dunolly Road .. .. .	..		462 3 10	
BET BET, MALDON, MARONG, AND TULLAROOP SHIRES (Joint Works)—				
Maldon-Eddington Road	519 10 9		..	
		1,864 2 0		1,886 18 5
BIRCHIP SHIRE—				
Beulah-Birchip-Wycheproof Road	2,363 0 0		64 13 5	
Donald-Birchip-Sealake Road	78 14 10		247 19 6	
		2,441 14 10		312 12 11
BLACKBURN AND MITCHAM SHIRE—				
Main Healesville Road .. .. .	..		5,842 14 5	
				5,842 14 5
BORUNG SHIRE—				
Birchip Road	1,141 9 9		1,583 11 1	
Dimboola Road	12 5 5		1,932 18 1	
Hopetoun Road	2,878 17 11		2,100 15 0	
Minyip Road	8 5 4		4,783 13 4	
Rainbow Road	970 10 0		1,897 12 3	
BORUNG AND DUNMUNKLE SHIRES (Joint Works)—				
Minyip Road .. .. .	..		0 1 6	
		5,011 8 5		12,298 11 3
BRAYBROOK SHIRE—				
Ballarat Road .. .. .	..		1,672 2 0	
Prince's Highway .. .. .	..		Bd. 456 16 10	
				2,128 18 10
BRIGHT SHIRE—				
Bright Road .. .. .	..		982 5 5	
Harrietville Road .. .. .	..		1,379 19 6	
Mount Buffalo Road .. .. .	..		Bd. 9,715 3 3	
BRIGHT AND BEECHWORTH SHIRE (Joint Works)—				
Bright Road .. .. .	..		57 13 5	
BRIGHT AND OMEO SHIRES (Joint Works)—				
Bright-Omeo Road .. .. .	..		Bd. 1,000 0 0	
				13,135 1 7
BROADFORD SHIRE—				
Hume Highway	10 9 10		..	
Sydney Road .. .. .	..		Bd. 30 16 3	
BROADFORD AND YEA SHIRES (Joint Works)—				
Upper Goulburn Road .. .. .	..		91 15 9	
		10 9 10		122 12 0
BROADMEADOWS SHIRE—				
Sydney Road .. .. .	..		323 17 8	
BROADMEADOWS AND KEILOR SHIRES (Joint Works)—				
Lancefield Road .. .. .	..		1,116 10 5	
				1,440 8 1
BULLA SHIRE—				
Melbourne-Lancefield Road .. .. .	..		1,425 5 5	
The Gap Road .. .. .	..		444 8 9	
Sunbury Road .. .. .	..		893 18 5	
BULLA AND KEILOR SHIRES (Joint Works)—				
Melbourne-Lancefield Road .. .. .	..		20 3 5	
				2,783 16 0
BULN BULN SHIRE—				
Fumina Road .. .. .	..		94 6 2	
Longwarry-Drouin Road .. .. .	..		707 13 9	
Loch Valley Road .. .. .	..		49 16 2	
Main Neerim "A" Road .. .. .	..		227 18 3	
Main Neerim "B" Road .. .. .	..		1,269 13 7	
Main Neerim "C" Road	174 3 5		328 7 4	
Main South Road .. .. .	..		1,129 5 4	
Neerim East Road .. .. .	..		37 0 3	
Prince's Highway .. .. .	..		264 3 2	
Westernport Road	1,968 18 2		216 13 8	
		2,143 1 7		4,324 17 8
Carried forward .. .. .	..	22,771 9 7	..	136,030 3 3

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	22,771 9 7	..	136,030 3 3
BUNGAREE SHIRE—				
Daylesford-Ballararat Road .. .. .	..		2,239 11 11	
Ballarat-Creswick Road .. .. .	..		0 12 2	
				2,240 4 1
BUNINYONG SHIRE—				
Ballarat-Rokewood Road	155 8 1		385 18 5	
Elaine-Mt. Mercer Road .. .. .	..		41 4 1	
Geelong-Ballararat Road .. .. .	..		1,663 14 6	
		155 8 1		2,090 17 0
CHELSEA CITY—				
Point Nepean Road .. .. .	..		2,166 5 2	
				2,166 5 2
CASTLEMAINE BOROUGH—				
Melbourne-Bendigo Road .. .. .	..		416 6 7	
Melbourne-Bendigo Road .. .. .	..		Bd. 2,397 3 3	
				2,813 9 10
CHARLTON SHIRE—				
Bendigo Road .. .. .	..		2,020 3 2	
Donald Road	835 8 10		1,422 1 9	
St. Arnaud Road	95 10 9		472 7 10	
North-Western Highway	1 1 5		..	
		932 1 0		3,914 12 9
CHILTERN SHIRE—				
Chiltern-Howlong Road .. .. .	..		101 14 0	
Rutherglen-Wodonga Road .. .. .	..		186 12 5	
Sydney Road .. .. .	..		135 5 8	
				423 12 1
CLUNES BOROUGH—				
Maryborough-Ballararat Road .. .. .	..		141 18 0	
				141 18 0
COHUNA SHIRE—				
Cohuna-Leitchville Road .. .. .	..		61 16 1	
Murray River Valley Road .. .. .	..		525 9 6	
				587 5 7
COLAC SHIRE—				
Colac-Ballararat Road .. .. .	..		1,235 0 3	
Colac-Beech Forest Road .. .. .	..		964 15 6	
Cressy-Inverleigh Road	3,092 6 2		1,006 12 4	
Prince's Highway .. .. .	..		335 9 8	
COLAC AND LEIGH SHIRES (Joint Works)—				
Cressy-Rokewood Road .. .. .	..		0 5 6	
Cressy-Lismore Road .. .. .	..		1 17 0	
COLAC, LEIGH, AND HAMPTON SHIRES (Joint Works)—				
Cressy-Lismore Road .. .. .	..		0 5 5	
		3,092 6 2		3,544 5 8
CORIO SHIRE—				
Ballarat Road .. .. .	..		374 3 1	
Fyansford Road .. .. .	..		6 1 9	
Geelong-Bacchus Marsh Road .. .. .	..		3,935 0 4	
Prince's Highway	1,980 13 8		Bd. 88 15 5	
		1,980 13 8		4,404 0 7
CRANBOURNE SHIRE—				
Main Coast Road	1,539 13 2		13,348 0 5	
Westernport Road .. .. .	..		396 13 8	
Koo-wee-rup-Pakenham Road .. .. .	..		678 18 7	
Lang Lang-Nyora Road .. .. .	..		100 1 3	
CRANBOURNE AND DANDENONG SHIRE (Joint Works)—				
Dandenong-Frankston Road .. .. .	..		208 15 8	
		1,539 13 2		14,732 9 7
CRESWICK SHIRE—				
Castlemaine-Ballararat Road .. .. .	..		818 2 8	
Daylesford-Ballararat Road .. .. .	..		650 0 0	
				1,468 2 8
CRESWICK BOROUGH—				
Castlemaine-Ballararat Road .. .. .	..		181 3 1	
				181 3 1
DANDENONG SHIRE—				
Cheltenham Road .. .. .	..		585 6 7	
Prince's Highway .. .. .	..		551 5 1	
DANDENONG AND CRANBOURNE SHIRES (Joint Works)—				
Dandenong-Frankston Road .. .. .	..		208 15 9	
				1,345 7 5
Carried forward .. .. .	..	30,471 11 8	..	176,083 16 9

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	30,471 11 8	..	176,083 16 9
DAYLESFORD BOROUGH—				
Ballan Road .. .. .	..		227 1 5	
Ballarat Road .. .. .	..		94 18 8	
Castlemaine Road .. .. .	..		42 18 10	
Daylesford-Ballarat Road	1,938 14 1		..	
Hepburn-Daylesford Road .. .. .	..		303 4 7	
Malmsbury-Daylesford Road .. .. .	..		165 14 11	
		1,938 14 1		833 18 5
DEAKIN SHIRE—				
Echuca-Cornella Road .. .. .	..		844 4 6	
Echuca-Picola Road .. .. .	..		900 4 11	
Kyabram-Nathalia Road .. .. .	..		158 8 9	
Kyabram-Tongala Road .. .. .	..		582 13 7	
Rochester-Kyabram Road .. .. .	..		1,026 3 9	
DEAKIN AND NUMURKAH SHIRES (Joint Works)—				
Echuca-Picola Road .. .. .	..		65 12 7	
Kyabram-Nathalia Road .. .. .	..		14 7 1	
DEAKIN AND RODNEY SHIRES (Joint Works)—				
Kyabram-Tongala Road .. .. .	..		124 1 9	
Rochester-Kyabram Road .. .. .	..		109 9 5	
				3,825 6 4
DIMBOOLA SHIRE—				
Horsham Road .. .. .	..		480 11 9	
Rainbow Road .. .. .	..		673 3 6	
Warracknabeal Road	2,169 11 7		1,822 1 6	
Rainbow Rises Road .. .. .	..		175 1 0	
Western Highway	52 19 7		..	
DIMBOOLA AND KARKAROC SHIRES (Joint Works)—				
Rainbow Road	563 14 6		158 10 3	
Hopetoun-Rainbow Road .. .. .	..		15 18 4	
		2,786 5 8		3,325 6 4
DONALD SHIRE—				
Donald-Charlton Road	791 19 0		604 11 0	
Donald-Minyip Road .. .. .	..		98 2 10	
Marnoo Road .. .. .	..		94 10 0	
St. Arnaud-Birchip Road	941 0 6		1,109 0 11	
		1,732 19 6		1,906 4 9
DONCASTER AND TEMPLESTOWE SHIRE—				
Doncaster Road .. .. .	..		3,380 5 9	
Heidelberg-Warrandyte Road .. .. .	..		2,420 6 9	
Warrandyte-Ringwood Road	1,042 13 9		859 8 0	
DONCASTER AND TEMPLESTOWE SHIRE, AND RINGWOOD BOROUGH (Joint Works)—				
Ringwood-Warrandyte Road	521 6 11		163 16 8	
DONCASTER AND TEMPLESTOWE AND HEIDELBERG SHIRES (Joint Works)—				
Heidelberg-Warrandyte Road .. .. .	..		0 3 9	
		1,564 0 8		6,824 0 11
DUNDAS SHIRE—				
Hamilton-Dunkeld Road	246 0 0		3,760 5 7	
Hamilton-Horsham Road	445 0 0		3,477 15 0	
Hamilton-Mount Gambier Road .. .. .	..		2,664 10 2	
Hamilton-Port Fairy Road .. .. .	..		3,235 19 7	
Hamilton-Portland Road .. .. .	..		1,396 6 6	
Hamilton-Warrnambool Road .. .. .	..		1,571 7 3	
DUNDAS SHIRE AND HAMILTON TOWN (Joint Works)—				
Hamilton-Warrnambool Road .. .. .	..		2 17 3	
DUNDAS, KOWREE, ARAPLES, AND WIMMERA SHIRES (Joint Works)—				
Horsham-Hamilton Road	6 2 6		2 3 5	
DUNDAS AND PORTLAND SHIRES (Joint Works)—				
Portland-Hamilton Road .. .. .	..		0 2 11	
		697 2 6		16,111 7 8
DUNMUNKLE SHIRE—				
Horsham-Murtoa-Minyip Road	2,086 10 3		..	
Minyip-Donald Road	43 8 1		573 17 4	
Rupanyup-Murtoa Road .. .. .	..		3,363 18 1	
Stawell-Warracknabeal Road	8,276 11 1		2,356 3 4	
DUNMUNKLE AND BORUNG SHIRES (Joint Works)—				
Minyip Road .. .. .	..		0 1 6	
		10,406 9 5		6,294 0 3
EAGLEHAWK BOROUGH—				
Mount Korong Road .. .. .	..		694 1 9	
				694 1 9
Carried forward .. .. .	..	49,597 3 6	..	215,898 3 2

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	49,597 3 6	..	215,898 3 2
EAST LODDON SHIRE—				
Dingee Road	580 19 9		7 4 1	
Mitiamo Road	242 5 0		110 19 4	
Prairie Road	146 3 6		11 10 6	
Prairie-Borong Road .. .. .	..		2 14 5	
EAST LODDON AND KORONG SHIRES (Joint Works)—				
Borong-Hurstwood Road .. .. .	..		179 10 2	
Serpentine Road .. .. .	..		75 4 11	
		969 8 3		387 3 5
ECHUCA BOROUGH—				
Echuca West Road .. .. .	..		982 8 8	
Echuca-Wyuna Road	917 2 8		174 10 4	
Echuca-Cornella Road .. .. .	..		11 0 3	
		917 2 8		1,167 19 3
ELTHAM SHIRE—				
Eltham-Yarra Glen Road .. .. .	..		3,302 7 6	
Hurstbridge-Kinglake Road	465 18 1		1,451 19 0	
Hurstbridge-Kinglake Road .. .. .	..		Bd. 171 3 4	
		465 18 1		4,925 9 10
EUROA SHIRE—				
Avenel-Longwood Road .. .. .	..		0 1 7	
Arcadia Road .. .. .	..		36 12 10	
Euroa-Arcadia Road	309 4 6		578 13 3	
Euroa-Strathbogrie Road	543 17 6		499 11 1	
Euroa-Mansfield Road .. .. .	..		63 6 3	
Murchison-Shepparton Road .. .. .	..		58 1 0	
Sydney Road .. .. .	..		82 13 9	
EUROA, VIOLET TOWN AND SEYMOUR SHIRES (Joint Works)—				
Hume Highway	3,217 4 0		..	
		4,070 6 0		1,318 19 9
FERN TREE GULLY SHIRE—				
Belgrave-Emerald Road .. .. .	..		859 7 4	
Emerald Road .. .. .	..		466 10 6	
Main Fern Tree Gully Road .. .. .	..		2,446 6 5	
Olinda Road	985 13 6		2,751 13 10	
Monbulk Road .. .. .	..		1,599 3 2	
FERN TREE GULLY AND LILYDALE SHIRES (Joint Works)—				
Olinda and Mount Dandenong Road	3 3 6		..	
		988 17 0		8,123 1 3
FLINDERS SHIRE—				
Dromana-Sorrento Road	3,602 0 6		..	
Hastings-Flinders Road	1 0 4		3,644 2 5	
Mornington-Flinders Road	777 0 1		4,420 17 7	
Point Nepean Road .. .. .	..		6,185 11 5	
Stony Point Road	352 10 0		692 8 2	
FLINDERS AND FRANKSTON AND HASTINGS SHIRES (Joint Works)—				
Flinders-Hastings Road	9 12 11		..	
		4,742 3 10		14,942 19 7
FOOTSCRAY CITY—				
Prince's Highway .. .. .	..		Bd. 284 7 6	
				284 7 6
FRANKSTON AND HASTINGS SHIRE—				
Frankston-Flinders Road .. .. .	..		4,669 15 9	
Frankston-Dandenong Road .. .. .	..		1,023 12 3	
Point Nepean Road .. .. .	..		7,046 7 8	
FRANKSTON AND HASTINGS, AND FLINDERS SHIRES (Joint Works)—				
Flinders-Hastings Road	9 12 10		..	
		9 12 10		12,739 15 8
GEELONG CITY AND SOUTH BARWON SHIRE (Joint Works)—				
Barwon Heads Road .. .. .	..		77 13 3	
				77 13 3
GISBORNE SHIRE—				
Calder Highway	1,147 6 11		..	
Bacchus Marsh Road .. .. .	..		333 18 0	
Gisborne Station Road .. .. .	..		171 1 9	
Melbourne-Bendigo Road .. .. .	..		Bd. 551 7 2	
		1,147 6 11		1,056 6 11
GLENELG SHIRE—				
Casterton-Mt. Gambier Road	1,098 16 6		..	
Coleraine-Casterton Road .. .. .	..		1,277 15 3	
Dergholm Road .. .. .	..		1,250 6 0	
Mt. Gambier Road	629 10 0		1,301 19 7	
Wando Road .. .. .	..		731 17 0	
Wando Vale Road	32 10 0		..	
Portland-Casterton Road .. .. .	..		940 17 4	
Carried forward .. .. .	..	62,907 19 1	..	260,921 19 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward	62,907 19 1	..	260,921 19 7
GLENEELG AND PORTLAND SHIRES (Joint Works)— Portland—Casterton Road	1 4 3	..
GLENEELG AND WANNON SHIRES (Joint Works)— Hamilton—Coleraine—Casterton Road	1,760 16 6	6 2 6	5,510 1 11
GLENLION SHIRE— Ballan Road	9 11 11	..	298 10 2	..
Ballarat Road	613 4 10	..
Castlemaine—Daylesford Road	1,030 19 4	..
Daylesford—Heppburn Road	538 10 6	..
Malmsbury—Daylesford Road	317 2 7	..	1,498 5 4	..
GOULBURN SHIRE— Avenel—Longwood Road	326 14 6	26 3 0	3,979 10 2
Goulburn Valley Road	2,293 13 7	..	805 5 4	..
Murchison—Shepparton Road	48 3 2	..	357 4 10	..
Vickers Road	155 1 10	..
GOULBURN AND SEYMOUR SHIRES (Joint Works)— Goulburn Valley Road	2 4 6	..
GOULBURN AND WARANGA SHIRES (Joint Works)— Murchison—Rushworth Road	2,341 16 9	19 16 9	1,365 16 3
GRENVILLE SHIRE— Ballarat—Hamilton Road	4,867 14 7	..
Cressy Road	117 7 11	..
Lismore Road	397 15 10	..
Pitfield Road	221 14 11	5,604 13 3
HAMILTON TOWN— Coleraine Road	1,196 0 7	..
Ararat Road	887 15 0	..
Portland Road	65 19 10	..
Port Fairy Road	5 9 4	..
HAMILTON TOWN AND DUNDAS SHIRE (Joint Works)— Hamilton—Warrnambool Road	2 17 4	2,158 2 1
HAMPDEN SHIRE— Camperdown—Ballarat Road	7,366 18 3	..
Caramut—Lismore Road	765 0 1	..
Lismore—Cressy Road	5,245 18 6	..
Prince's Highway	1,815 15 8	..
Terang—Mortlake Road	682 11 3	..
Prince's Highway	Bd. 731 12 6	..
HAMPDEN, COLAC AND LEIGH SHIRES (Joint Works)— Cressy—Lismore Road	0 5 5	..
HAMPDEN AND RIFON SHIRES (Joint Works)— Ballarat—Hamilton Road	16 16 4	16,624 18 0
HEALESVILLE SHIRE— Healesville—Alexandra Road	714 11 11	..	3,277 17 7	..
Healesville—Woori Yallock Road	45 4 6	..	166 9 4	..
Marysville Road	21 16 0	781 12 5	412 0 6	3,856 7 5
HEIDELBERG SHIRE— Main Heidelberg—Eltham Road	2,227 12 10	..
Greensborough—Hurstbridge Road	2,030 9 3	..
Heidelberg—Warrandyte Road	4 12 6	..
Main Whittlesea Road	381 4 2	..
HEIDELBERG, AND DONCASTER AND TEMPLESTOWE SHIRES (Joint Works)— Heidelberg—Warrandyte Road	0 3 9	4,644 2 6
HEYTESBURY SHIRE— Camperdown—Cobden Road	1,551 10 0	..
Cobden—Port Campbell—Princetown Road	615 14 0	..	2,364 19 11	..
Peterborough—Port Campbell Road	136 19 11	..
Timboon—Port Campbell	4 12 5	620 6 5	Bd. 1,428 18 0	5,482 7 10
HORSHAM BOROUGH— Doon Road	1,426 13 11	..
Dimboola—Horsham Road	1,103 14 10	..
Hamilton Road	388 5 5	..
Natimuk Road	157 15 9	..
Western Highway	12 9 9	..
HORSHAM BOROUGH, WIMMERA AND ARAPILES SHIRES (Joint Works)— Horsham—Hamilton Road	26 13 4	3,115 13 0
Carried forward	68,739 5 8	..	313,263 12 0

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	68,739 5 8	..	313,263 12 0
HUNTLY SHIRE—				
Bendigo-Echuca Road .. .. .	..	..	1,160 4 6	
Heathcote-Elmore Road .. .. .	..	..	400 11 6	
Northern Highway	1,447 7 5		..	
HUNTLY AND WARANGA SHIRES (Joint Works)—				
Heathcote-Elmore Road .. .. .	..	1,447 7 5	76 1 5	1,636 17 5
INGLEWOOD BOROUGH—				
Bendigo-Charlton Road .. .. .	..	..	312 16 10	312 16 10
KARA KARA SHIRE—				
Avoca-St. Arnaud Road	24 5 6		1,817 17 5	
Charlton Road .. .. .	..		300 1 9	
Navarre Road	2,336 11 1		615 14 9	
St. Arnaud-Donald Road	2,072 19 11		5,653 0 9	
KARA KARA AND STAWELL SHIRES (Joint Works)—				
Navarre Road .. .. .	..	4,433 16 6	10 10 0	8,397 4 8
KARKAROOC SHIRE—				
Hopetoun-Rainbow Road	34 6 10		919 12 5	
Hopetoun-Woomelang-Sea Lake Road	1,056 17 10		822 7 2	
Hopetoun-Warracknabeal Road	237 3 6		392 10 9	
Rainbow-Beulah-Birchip Road	886 5 2		1,339 12 7	
KARKAROOC AND DIMBOOLA SHIRES (Joint Works)—				
Rainbow Road	563 14 6		158 10 3	
Hopetoun-Rainbow Road .. .. .	..	2,778 7 10	15 18 3	3,648 11 5
KEILOR SHIRE—				
Melbourne-Bendigo Road .. .. .	..	..	708 10 2	
KEILOR AND BROADMEADOWS SHIRES (Joint Works)—				
Lancefield Road .. .. .	..	..	1,116 10 4	
KEILOR AND BULLA SHIRES (Joint Works)—				
Melbourne-Lancefield Road .. .. .	..	..	20 3 5	1,845 3 11
KILMORE SHIRE—				
Lancefield-Kilmore Road .. .. .	..	..	277 4 6	
Heathcote Road .. .. .	..	..	200 13 9	
Sydney Road .. .. .	..	..	Bd. 156 10 9	
KILMORE AND ROMSEY SHIRES (Joint Works)—				
Lancefield-Kilmore Road .. .. .	..	..	145 0 10	
KILMORE AND PYALONG SHIRES (Joint Works)—				
Heathcote Road .. .. .	..	..	139 11 10	919 1 8
KOROIT BOROUGH—				
Koroit-Warrnambool Road .. .. .	..	..	1,332 18 3	1,332 18 3
KORONG SHIRE—				
Calder Highway	7 4 11		..	
Charlton-Bendigo Road .. .. .	..	..	433 1 8	
Borung-Hurstwood Road .. .. .	..	..	359 0 5	
Serpentine Road .. .. .	..	..	150 9 10	942 11 11
		7 4 11		
KORUMBURRA SHIRE—				
Bena-Poowong Road .. .. .	..	..	690 1 3	
Korumburra-Drouin Road .. .. .	..	..	711 7 9	
Korumburra-Leongatha Road .. .. .	..	..	441 3 6	
Korumburra-Warragul Road .. .. .	..	..	6,179 3 4	
Lang Lang-Nyora Road .. .. .	..	..	279 4 5	
Korumburra-Wonthaggi Road .. .. .	..	..	2,843 7 10	
Loch-Wonthaggi Road .. .. .	..	..	374 7 2	
Nyora-Poowong Road .. .. .	..	..	553 8 11	
Poowong-Ranceby Road .. .. .	..	..	186 1 11	
KORUMBURRA AND WARRAGUL SHIRES (Joint Works)—				
Warragul-Korumburra Road .. .. .	..	..	1 9 11	12,259 16 0
KOWREE SHIRE—				
Booropki Road	82 7 8		399 3 2	
Booropki-Frances Road	983 6 5		2,682 6 4	
Edenhope-Goroke Road .. .. .	..	..	1,107 4 10	
Hamilton-Edenhope-Apsley Road .. .. .	..	..	2,162 7 5	
KOWREE, ARAPILES, DUNDAS AND WIMMERA SHIRES (Joint Works)—				
Horsham-Hamilton Road	9 3 10	1,074 17 11	3 5 2	6,354 6 11
Carried forward .. .. .	..	78,481 0 3	..	350,913 1 0

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	78,481 0 3	..	350,913 1 0
KYNETON SHIRE—				
Daylesford Road .. .. .	..	..	103 15 4	
Melbourne-Bendigo Road .. .. .	..	..	235 15 3	
Redesdale Road .. .. .	..	..	537 12 8	
Trentham Road .. .. .	..	..	1,254 0 10	
Tylden-Woodend Road .. .. .	..	..	81 3 1	
KYNETON AND NEWHAM AND WOODEND SHIRES (Joint Works)—				
Tylden Road .. .. .	..	..	21 12 8	2,233 19 10
LAWLOIT SHIRE—				
Broughton Road	955 4 7	..	1,260 16 7	
Main Yeringa Road	1,302 16 8	..	1,006 4 8	
Nhill-Kaniva-Border Road .. .. .	..	..	340 4 9	
South Lillimur Road .. .. .	..	..	955 9 2	
		2,258 1 3		3,562 15 2
LEIGH SHIRE—				
Ballarat-Rokewood Road .. .. .	..	..	155 13 2	
Cressy-Inverleigh Road .. .. .	..	..	1,508 0 0	
Cressy-Rokewood Road	503 15 0	..	253 18 4	
Inverleigh-Shelford Road .. .. .	..	..	74 6 7	
Rokewood-Shelford Road .. .. .	..	..	235 0 7	
Shelford-Bannockburn Road .. .. .	..	..	44 12 9	
Werneth Road .. .. .	..	..	0 11 8	
LEIGH AND COLAC SHIRES (Joint Works)—				
Cressy-Rokewood Road .. .. .	..	..	0 5 6	
Cressy-Lismore Road .. .. .	..	..	1 17 1	
LEIGH, COLAC, AND HAMPDEN SHIRES (Joint Works)—				
Cressy-Lismore Road .. .. .	..	..	0 5 5	2,274 11 1
		503 15 0		
LEXTON SHIRE—				
Avoca-Ararat Road .. .. .	..	..	270 8 5	
Avoca-Ballarat Road .. .. .	..	..	752 4 9	1,022 13 2
LILYDALE SHIRE—				
Lilydale-Evelyn Road	2,276 13 3	..	13 18 0	
Monbulk Road .. .. .	..	..	724 12 11	
Mount Dandenong Road	49 3 7	..	1,720 1 6	
Main Healesville Road	5,289 7 1	..	944 4 7	
Main Warburton Road	0 6 10	..	769 2 7	
Yarra Glen Road .. .. .	..	..	288 11 8	
Monbulk Road .. .. .	..	..	Bd. 1,967 16 10	
Main Warburton Road .. .. .	..	..	Bd. 135 19 11	
LILYDALE AND FERNTREE GULLY SHIRES (Joint Works)—				
Olinda and Mt. Dandenong Roads	3 3 7	..	..	6,564 8 0
		7,618 14 4		
LOWAN SHIRE—				
Dimboola-Kaniva Road .. .. .	..	..	717 1 1	
Goroke Road .. .. .	..	..	541 10 10	
Lorquon West Road	374 15 7	..	416 19 8	
Yanac Road	1,112 18 7	..	1,772 14 10	
		1,487 14 2		3,448 6 5
MAFFRA SHIRE—				
Briagalong-Dargo Road .. .. .	..	..	37 4 9	
Bushy Park-Valencia Creek Road .. .. .	..	..	30 1 8	
Licola Road	15 18 6	..	1,185 11 0	
Maffra-Sale Road .. .. .	..	..	2,271 14 0	
Tinamba-Boisdale Road .. .. .	..	..	4,407 10 8	
Tinamba-Newry Road .. .. .	..	..	136 4 0	
MAFFRA AND AVON SHIRES (Joint Works)—				
Dargo Road .. .. .	..	..	178 10 0	8,246 16 1
		15 18 6		
MALDON SHIRE—				
Baringhup Road .. .. .	..	..	72 14 11	
Castlemaine-Maldon Road .. .. .	..	..	179 7 7	
Castlemaine-Newstead Road .. .. .	..	..	8 13 5	
Maldon-Eddington Road .. .. .	..	..	900 1 3	
Newstead Road .. .. .	..	..	182 7 0	
MALDON, MARONG, TULLAROOP, AND BET BET SHIRES (Joint Works)—				
Maldon-Eddington Road	1,759 19 0	..	..	1,343 4 2
		1,759 19 0		
Carried forward .. .. .	..	92,125 2 6	..	379,609 14 11

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	92,125 2 6	..	379,609 14 11
MANSFIELD SHIRE—				
Euroa-Merton Road .. .. .	..	..	298 15 2	
Mansfield Road	651 0 3		1,828 3 3	
Mansfield-Tolmie Road .. .. .	..		21 11 1	
Mansfield-Woodspoint Road .. .. .	..		723 15 9	
Mansfield-Woodspoint Road .. .. .	..		Bd.3,486 17 5	
		651 0 3		6,359 2 8
MARONG SHIRE—				
Bendigo-Eddington Road	9 11 0		1,825 17 2	
Bendigo-Bridgewater Road .. .. .	..		1,179 12 0	
Bendigo-Serpentine Road .. .. .	..		1,286 14 6	
MARONG, MALDON, TULLAROOP, AND BET BET SHIRES (Joint Works)—				
Maldon-Eddington Road	1,759 19 0		..	
		1,769 10 0		4,292 3 8
MARYBOROUGH BOROUGH—				
Avoca Road .. .. .	..		21 4 6	
Ballarat Road .. .. .	..		30 0 4	
Eddington Road .. .. .	..		45 6 0	
Castlemaine Road .. .. .	..		136 2 0	
				232 12 10
MCIVOR SHIRE—				
Heathcote-Elmore Road .. .. .	..		681 7 9	
Heathcote-Redesdale Road	388 9 6		122 7 8	
Kilmore-Heathcote-Bendigo Road .. .. .	..		3,226 0 2	
		388 9 6		4,029 15 7
MELTON SHIRE—				
Toolern Road .. .. .	..		415 7 10	
The Gap Road .. .. .	..		294 10 3	
				709 18 1
METCALFE SHIRE—				
Kyneton-Redesdale Road .. .. .	..		704 4 11	
				704 4 11
MILDURA TOWN—				
Deakin Avenue .. .. .	..		130 8 0	
Langtree Avenue .. .. .	..		223 12 11	
Punt Road .. .. .	..		34 4 8	
Tenth Avenue .. .. .	..		40 18 1	
				429 3 8
MILDURA SHIRE—				
Deakin Avenue .. .. .	..		343 16 2	
Melbourne Road	619 17 11		254 11 1	
Irymple Road	359 13 7		1,613 0 11	
Wentworth Road	2,016 15 0		2,713 3 2	
		2,996 6 6		4,924 11 4
MINHAMITE SHIRE—				
Hamilton-McArthur-Port Fairy Road .. .. .	..		5,266 18 6	
Warrnambool-Hawkesdale-Penshurst Road .. .. .	..		7,333 4 0	
				12,600 2 6
MIRBOO SHIRE—				
Allambee East-West Tarwin Road .. .. .	..		104 5 6	
Boolarra South Road .. .. .	..		56 7 9	
Mardan Road	908 17 4		63 2 8	
Mirboo South Road	0 7 1		1,049 2 11	
Mirboo-Allambee East Road .. .. .	..		659 11 8	
Mirboo-Leongatha Road .. .. .	..		80 18 10	
		909 4 5		2,013 9 4
MOORABBIN SHIRE—				
Point Nepean Road .. .. .	..		3,455 10 2	
Centre Dandenong Road .. .. .	..		523 8 3	
				3,978 18 5
MORDIALLOC CITY—				
Point Nepean Road .. .. .	..		1,277 5 11	
				1,277 5 11
MORNINGTON SHIRE—				
Point Nepean Road .. .. .	..		837 19 9	
Point Nepean Road .. .. .	..		Bd. 5,546 17 10	
				6,384 17 7
MORTLAKE SHIRE—				
Caramut-Lismore Road .. .. .	..		6,704 3 3	
Mortlake-Ararat Road .. .. .	..		3,800 4 7	
Terang-Mortlake Road .. .. .	..		85 8 4	
Mortlake-Warrnambool Road .. .. .	..		4,012 17 9	
				14,602 13 11
MORWELL SHIRE—				
Boolarra-Foster Road .. .. .	..		Bd. 319 16 2	
Boolarra-Foster Road .. .. .	..		70 15 6	
Boolarra-Welshpool Road	885 11 0		0 10 2	
Boolarra-Welshpool Road .. .. .	..		Bd. 564 4 11	
Jeeralang West Road	1,471 9 2		431 13 2	
Prince's Highway .. .. .	..		1,334 5 0	
Carried forward .. .. .	..	98,839 13 2	..	442,148 15 4

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	98,839 13 2	..	442,148 15 4
MORWELL AND ALBERTON SHIRES (Joint Works)—				
Boolarra-Yarram Road .. .. .	..	..	6 0 0	
Boolarra-Welshpool Road .. .. .	..	..	Bd. 55 4 10	
MORWELL, ALBERTON, AND SOUTH GIPPSLAND SHIRES (Joint Works)—				
Gonyah-Ryton Road .. .. .	..	..	Bd. 95 19 4	
		2,357 0 2		2,878 9 1
MOUNT ROUSE SHIRE—				
Ballarat-Hamilton Road .. .. .	..	..	3,736 6 5	
Hamilton-Dunkeld Road .. .. .	..	..	495 13 8	
Hamilton-Penshurst Road .. .. .	..	..	4,107 4 5	
Penshurst-Caramut Road .. .. .	..	..	2,109 4 0	
				10,448 8 6
MULGRAVE SHIRE—				
Ferntree Gully Road .. .. .	..	..	3,149 3 11	
				3,149 3 11
NARRACAN SHIRE—				
Prince's Highway	349 19 0		274 7 1	
Trafalgar-Thorpdale Road .. .. .	..	..	886 0 9	
Trafalgar-Willowgrove Road .. .. .	..	..	436 8 3	
Walhalla Road	1,619 0 0		708 17 8	
Walhalla Road .. .. .	..	..	Bd. 1,544 13 0	
Yarragon-Leongatha Road .. .. .	..	..	648 6 7	
Yarragon-Shady Creek Road .. .. .	..	..	222 14 9	
Prince's Highway	3,050 6 6		..	
		5,019 5 6		4,721 8 1
NEWHAM AND WOODEND SHIRE—				
Calder Highway	1,808 17 5		..	
Lancefield Road .. .. .	..	..	414 0 8	
Tylden Road	51 8 0		14 0 9	
Melbourne-Bendigo Road .. .. .	..	..	Bd. 14 1 5	
NEWHAM AND WOODEND AND KYNETON SHIRES (Joint Works)—				
Melbourne-Bendigo Road .. .. .	..	..	..
Tylden Road .. .. .	..	..	64 18 1	
		1,860 5 5		507 0 11
NEWSTAD AND MT. ALEXANDER SHIRE—				
Castlemaine-Daylesford Road .. .. .	..	..	1,221 15 1	
Castlemaine-Maryborough Road .. .. .	..	..	474 3 1	
Creswick Road .. .. .	..	..	423 13 8	
Maldon Road .. .. .	..	..	65 2 0	
				2,184 13 10
NUMURKAH SHIRE—				
Echuca-Picola Road .. .. .	..	..	303 5 7	
Murray Valley Road .. .. .	..	..	80 10 11	
Nathalia-Kyabram Road .. .. .	..	..	1,628 4 1	
Nathalia North Road .. .. .	..	..	1,063 16 0	
Nathalia-Picola Road .. .. .	..	..	560 8 2	
Numurkah-Nathalia Road	537 9 0		1,114 6 9	
Numurkah-Tungamah Road	981 8 0		50 12 4	
Shepparton-Numurkah-Cobram Road	123 10 9		283 7 1	
NUMURKAH AND DEAKIN SHIRES (Joint Works)—				
Echuca-Picola Road .. .. .	..	..	82 13 4	
Kyabram-Nathalia Road .. .. .	..	..	14 7 0	
		1,642 7 9		5,181 11 3
OAKLEIGH CITY—				
Ferntree Gully Road .. .. .	..	..	351 14 0	
Prince's Highway .. .. .	..	..	2,496 14 9	
				2,848 8 9
OMEQ SHIRE—				
Benambra Road	2,578 14 1		652 6 3	
Day Avenue .. .. .	..	..	843 3 8	
OMEQ AND BRIGHT SHIRES (Joint Works)—				
Bright-Omeo Road .. .. .	..	..	Bd. 1,000 0 0	
		2,578 14 1		2,495 9 11
ORBOST SHIRE—				
Cann Valley Road	1,155 14 9		1,168 14 2	
Genoa-Gipsy Point Road .. .. .	..	..	565 9 3	
Marlo Road	1,823 19 6		1,051 1 3	
Prince's Highway .. .. .	..	..	171 14 10	
Wangarabelle Road .. .. .	..	..	118 1 7	
		2,979 14 3		3,075 1 1
OTWAY SHIRE—				
Beech Forest-Apollo Bay Road .. .. .	..	..	77 14 11	
Carlisle-Gellibrand Road .. .. .	..	..	118 15 7	
Forrest-Apollo Bay Road .. .. .	..	..	170 15 5	
Glen Aire-Laver's Hill Road .. .. .	..	..	Bd. 91 13 4	
Beech Forest-Apollo Bay Road .. .. .	..	..	Bd. 246 19 5	
Apollo Bay-Wye River Road .. .. .	..	..	Bd. 34 13 0	
Carlisle-Gellibrand Road .. .. .	..	..	Bd. 99 10 9	
Forrest-Apollo Bay Road .. .. .	..	..	Bd. 973 18 4	
				1,814 0 9
Carried forward .. .. .	..	115,277 0 4	..	481,452 11 5

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	115,277 0 4	..	481,452 11 5
OXLEY SHIRE—				
Bright Road	480 6 9		1,043 19 1	
Greta-Glenrowan Road	111 6 3		317 1 11	
Oxley Road .. .. .	..		184 11 11	
OXLEY SHIRE AND WANGARATTA BOROUGH (Joint Works)—				
Oxley Road .. .. .	..		3 11 5	
OXLEY AND BEECHWORTH SHIRES (Joint Works)—				
Bright Road .. .. .	..	591 13 0	2 18 4	1,552 2 8
PHILLIP ISLAND SHIRE—				
Newhaven Road .. .. .	..		544 16 5	
Phillip Island Road .. .. .	..		585 6 0	
Ventnor Road .. .. .	..		553 17 11	1,684 0 4
PORT FAIRY BOROUGH—				
Hamilton Road .. .. .	..		669 9 11	
Prince's Highway (Portland) .. .. .	..		126 5 4	
Prince's Highway (Warrnambool) .. .. .	..		339 12 7	1,135 7 10
PORTLAND SHIRE—				
Heath Road	28 13 6		1 14 4	
Portland-Casterton Road	356 0 0		441 8 9	
Portland-Hamilton Road	240 11 7		3,158 16 8	
PORTLAND AND DUNDAS SHIRES (Joint Works)—				
Portland-Hamilton Road .. .. .	..		0 2 11	
PORTLAND AND GLENELG SHIRES (Joint Works)—				
Portland-Casterton Road .. .. .	..	625 5 1	1 4 4	3,603 7 0
PRESTON CITY—				
Epping Road .. .. .	..		90 17 5	
Whittlesea Road .. .. .	..		3,218 2 7	3,309 0 0
PYALONG SHIRE—				
Kilmore-Heathcote-Bendigo Road .. .. .	..		1,595 19 4	
" " " " " " " " " " " " .. .. .	..		Bd. 2,675 7 11	
PYALONG AND KILMORE SHIRES (Joint Works)—				
Heathcote Road .. .. .	..		139 11 10	4,410 19 1
QUEENSLIFF BOROUGH—				
Geelong Road .. .. .	..		481 18 2	481 18 2
RINGWOOD BOROUGH—				
Main Healesville Road	5 15 9		1,553 16 10	
Mount Dandenong Road .. .. .	..		183 3 3	
Warrandyte Road .. .. .	..		1,020 19 6	
RINGWOOD BOROUGH AND DONCASTER AND TEMPLESTOWE SHIRE (Joint Works)—				
Ringwood-Warrandyte Road	521 6 10	527 2 7	163 16 8	2,921 16 3
RIPON SHIRE—				
Ballarat-Ararat Road .. .. .	..		13 8 2	
Ballarat-Hamilton Road .. .. .	..		4,513 1 5	
Skipton Road .. .. .	..		2,402 11 1	
RIPON AND HAMPDEN SHIRES (Joint Works)—				
Ballarat-Hamilton Road .. .. .	..		16 16 4	6,945 17 0
ROCHESTER SHIRE—				
Bendigo-Echuca Road .. .. .	..		1 12 3	
Murray River Valley Road	883 3 4		..	
Rochester-Barnawm-Prairie Road	3,551 5 4		1,193 11 6	
Timmering Road	10 9 11		788 6 2	
Shepparton-Rochester-Wycheproof-Rainbow Road	343 7 2	4,788 5 9	..	1,983 9 11
RODNEY SHIRE—				
Kyabram-Nathalia Road .. .. .	..		82 4 4	
Kyabram-Tongala Road .. .. .	..		261 19 1	
Mooroopna-Undera Road .. .. .	..		755 9 6	
Shepparton-Tatura Road	7 14 2		2,412 8 3	
Tatura-Murchison Road	3,145 0 6		743 2 3	
Tatura-Bytneside-Kyabram Road .. .. .	..		2,764 9 9	
Shepparton-Rochester-Wycheproof-Rainbow Road	1,864 13 10		..	
RODNEY SHIRE AND SHEPPARTON BOROUGH (Joint Works)—				
Shepparton-Tatura Road .. .. .	..		1,498 19 7	
Shepparton-Mooroopna Road .. .. .	..		7 7 2	
Carried forward .. .. .	..	121,809 6 9	..	509,480 9 8

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	121,809 6 9	..	509,480 9 8
RODNEY AND DEAKIN SHIRES (Joint Works)—				
Shepparton-Tatura Road .. .. .	..	..	..	..
Kyabram-Tongala Road .. .. .	..	..	124 1 9	..
Rochester-Kyabram Road .. .. .	..	..	109 9 5	..
		5,017 8 6		8,759 11 1
ROMSEY SHIRE—				
Lancefield-Kilmore Road .. .. .	..	..	305 3 6	..
Melbourne-Lancefield Road .. .. .	..	..	1,191 8 6	..
Woodend-Lancefield Road	1,165 10 1	..	191 19 3	..
ROMSEY AND KILMORE SHIRES (Joint Works)—				
Lancefield-Kilmore Road .. .. .	..	..	145 0 9	..
		1,165 10 1		1,833 12 0
ROSEDALE SHIRE—				
Prince's Highway .. .. .	..	..	58 13 5	..
Sale-Yarram Road .. .. .	..	..	767 15 7	..
Seaspray Road .. .. .	..	..	434 5 11	..
Traralgon-Gormandale Road .. .. .	..	..	278 9 3	..
Willung Road	383 7 3	..	169 18 6	..
ROSEDALE AND ALBERTON SHIRES (Joint Works)—				
Carrajang-Gormandale Road	9 5 11	..	17 10 2	..
Sale-Yarram Road .. .. .	..	..	222 14 5	..
		392 13 2		1,949 7 3
RUTHERGLEN SHIRE—				
Chiltern-Howlong Road .. .. .	..	..	282 14 9	..
Rutherglen-Wahgunyah Road .. .. .	..	..	920 9 1	..
Springhurst-Rutherglen Road .. .. .	..	..	607 2 7	..
Wodonga Road .. .. .	..	..	548 5 5	..
Yarrawonga Road .. .. .	..	..	683 8 11	..
RUTHERGLEN AND WANGARATTA SHIRES (Joint Works)—				
Yarrawonga Road	34 5 10	..	19 13 8	..
RUTHERGLEN, WANGARATTA, AND YARRAWONGA SHIRES (Joint Works)—				
Yarrawonga-Rutherglen Road .. .. .	..	..	8 9 9	..
		34 5 10		3,070 4 2
SALE TOWN—				
Prince's Highway .. .. .	..	..	359 11 6	..
Sale-Longford Road .. .. .	..	..	1,699 2 3	..
				2,058 13 9
SEBASTOPOL BOROUGH—				
Ballarat-Rokewood Road .. .. .	..	..	Bd. 1,162 15 3	..
				1,162 15 3
SEYMOUR SHIRE—				
Avenel-Longwood Road .. .. .	..	..	36 6 0	..
Goulburn Valley Road .. .. .	..	..	253 4 0	..
Seymour-Yea Road	19 5 0	..	187 1 7	..
Seymour-Yea Road .. .. .	..	..	Bd. 899 16 9	..
Sydney Road .. .. .	..	..	Bd. 151 3 6	..
Upper Goulburn Road	749 3 5	..	1,162 13 5	..
Hume Highway	147 5 2	..	..	..
SEYMOUR AND GOULBURN SHIRES (Joint Works)—				
Goulburn Valley Road .. .. .	..	..	2 4 6	..
		915 13 7		2,692 9 9
SHEPPARTON SHIRE—				
Dookie-Nalinga Road .. .. .	..	..	521 0 4	..
Shepparton-Nalinga Road .. .. .	..	..	4,720 2 5	..
Shepparton-Nagambie Road	923 15 7	..	119 2 3	..
Shepparton-Numurkah Road	2,899 10 8	..	2,193 18 10	..
		3,823 6 3		7,554 3 10
SHEPPARTON BOROUGH—				
Shepparton-Nagambie Road .. .. .	..	..	596 15 5	..
Shepparton-Numurkah Road .. .. .	..	..	564 15 8	..
Shepparton-Nalinga Road .. .. .	..	..	1,056 10 4	..
SHEPPARTON BOROUGH AND RODNEY SHIRE (Joint Works)—				
Shepparton-Mooroopna Road .. .. .	..	..	7 7 3	..
Shepparton-Tatura Road .. .. .	..	..	1,498 19 8	..
				3,724 8 4
SOUTH BARWON SHIRE—				
Barwon Heads Road .. .. .	..	..	2,234 7 2	..
Prince's Highway .. .. .	..	..	634 0 3	..
SOUTH BARWON SHIRE AND GEELONG CITY (Joint Works)—				
Barwon Heads Road .. .. .	..	..	77 13 3	..
				2,946 0 8
Carried forward .. .. .	..	133,158 4 2	..	545,231 15 9

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	133,158 4 2	..	545,231 15 9
SOUTH GIPPSLAND SHIRE—				
Boolarra-Foster Road	2,066 2 8		454 7 7	
Boolarra-Welshpool Road .. .. .	..		141 16 2	
Boolarra-Welshpool Road .. .. .	..		Bd. 368 15 0	
Falls Road	73 11 7		31 6 2	
Foster-Yarram Road	2,302 13 0		794 18 0	
Main South Gippsland Road .. .. .	..		366 8 6	
Sale-Foster Road	2,034 8 7		..	
Stony Creek-Dollar Road	1,411 4 0		39 11 8	
Toora-Gunyah Road .. .. .	..		528 18 8	
Turton's Creek Road .. .. .	..		352 15 1	
SOUTH GIPPSLAND, ALBERTON, AND MORWELL SHIRES (Joint Works)—				
Gunyah-Ryton Road .. .. .	..		Bd. 95 19 3	
SOUTH GIPPSLAND AND WOORAYL SHIRES (Joint Works)—				
Boolarra-Foster Road .. .. .	..	7,887 19 10	Bd. 145 6 6	3,320 2 7
ST. ARNAUD BOROUGH—				
Avoca-St. Arnaud Road .. .. .	..		94 13 4	
Charlton Road .. .. .	..		8 1 9	
Navarre Road .. .. .	..		42 13 5	
St. Arnaud-Donald Road .. .. .	..		386 2 6	531 11 0
STAWELL BOROUGH—				
Ararat-Stawell Road .. .. .	..		271 1 10	
Glenorchy Road	22 18 0		572 13 2	843 15 0
		22 18 0		
STAWELL SHIRE—				
Landsborough Road .. .. .	..		40 12 3	
Marnoo Road	1,662 15 2		705 16 7	
Navarre Road	1,137 12 6		1,329 0 0	
Stawell-Grampians Road .. .. .	..		582 4 11	
Stawell-Glenorchy-Horsham Road	876 1 6		1,171 10 8	
Stawell-Warracknabeal Road	389 8 10		59 16 2	
STAWELL AND KARA KARA SHIRES (Joint Works)—				
Navarre Road .. .. .	..		10 10 0	
STAWELL AND WIMMERA SHIRES (Joint Works)—				
Horsham-Wal Wal Road .. .. .	..	4,065 18 0	97 12 6	3,997 3 1
STRATHFIELDSAYE SHIRE—				
Heathcote-Bendigo Road .. .. .	..		797 13 0	
Mandurang Road .. .. .	..		618 5 0	
Strathfieldsaye Road .. .. .	..		607 6 0	2,023 4 0
SWAN HILL SHIRE—				
Euston Road	3,388 6 4		5,328 11 11	
Ouyen-Piangil Road	707 8 11		..	
Nyah-Ouyen Road	46 8 0		227 4 3	
Sea Lake-Ultima	915 11 10		119 4 4	
Swan Hill Road	94 14 0		1,152 11 1	
Tooleybuc Road .. .. .	..		1 14 3	
Ultima Road .. .. .	..		906 11 1	7,735 16 11
		5,152 9 1		
TALBOT SHIRE—				
Maryborough-Avoca Road .. .. .	..		164 15 11	
Maryborough-Ballarat Road .. .. .	..		810 7 1	975 3 0
TAMBO SHIRE—				
Bairnsdale-Bruthen Road .. .. .	..		241 12 4	
Bruthen-Omeo Road .. .. .	..		87 16 9	
Mossiface Road .. .. .	..		90 8 3	
Nowa Nowa-Buchan-Gelantipy Road	98 19 0		2,178 3 0	
Prince's Highway .. .. .	..		648 0 5	3,246 0 9
		98 19 0		
TOWONG SHIRE—				
Murray Valley Road .. .. .	..		2,007 7 0	
Omeo Road .. .. .	..		122 5 6	2,129 12 6
TRARALGON SHIRE—				
Callignee Road .. .. .	..		408 18 6	
Prince's Highway .. .. .	..		214 5 4	
Traralgon-Gormandale Road .. .. .	..		707 14 3	
Traralgon-Jeeralang Road .. .. .	..		917 2 1	2,248 0 2
				2,248 0 2
Carried forward .. .. .	..	150,386 8 1	..	572,282 4 9

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward	150,386 8 1	..	..	572,282 4 9
TULLAROOP SHIRE—				
Avoca Road .. .. .	..		778 4 3	
Ballarat Road .. .. .	..		123 18 0	
Castlemaine-Maryborough Road .. .. .	..		208 19 10	
Eddington Road .. .. .	..		997 2 1	
Natte Yallock Road	45 13 0		81 13 4	
Castlemaine-Maryborough Road .. .. .	..		Bd. 1,188 17 10	
TULLAROOP, MALDON, MARONG, AND BET BET SHIRES (Joint Works)—				
Maldon-Eddington Road	2,019 14 4		..	
		2,065 7 4		3,378 15 4
TUNGAMAH SHIRE—				
Cobram South Road .. .. .	..		199 3 7	
Cobram-Strathmerton Road	889 5 2		247 9 5	
Numurkah-Tungamah-Wilby Road	1,909 11 9		941 5 2	
St. James Road	1,982 15 0		403 12 3	
Yarrawonga-Cobram Road	618 9 0		425 13 6	
		5,400 0 11		2,217 3 11
UPPER MURRAY SHIRE—				
Corryong Road	88 0 0		738 18 0	
Tintalra Road	250 5 10		175 0 4	
		338 5 10		913 18 4
UPPER YARRA SHIRE—				
Don Road .. .. .	..		637 14 1	
Warburton Road .. .. .	..		1,150 18 5	
Warburton Road .. .. .	..		Bd. 1,350 16 6	
Woodspoint Road .. .. .	..		1,423 2 6	
				14,562 11 6
VIOLET TOWN SHIRE—				
Hume Highway	98 14 1		..	
Shepparton Road	728 0 0		51 5 4	
Sydney Road .. .. .	..		17 8 2	
Violet Town-Dookie Road	629 17 9		196 8 0	
VIOLET TOWN, SEYMOUR AND EUROA SHIRES (Joint Works)—				
Hume Highway	1,152 4 10		..	
		2,608 16 8		265 1 6
WALPEUP SHIRE—				
Ouyen-Pinnaroo Road	3,028 4 1		4,571 17 1	
Mildura Road .. .. .	..		542 5 5	
		3,028 4 1		5,114 2 6
WANGARATTA BOROUGH—				
Sydney Road .. .. .	..		0 6 3	
WANGARATTA BOROUGH AND WANGARATTA SHIRE (Joint Works)—				
Sydney Road .. .. .	..		0 5 0	
WANGARATTA BOROUGH, AND OXLEY SHIRE (Joint Works)—				
Oxley Road .. .. .	..		3 11 5	
				4 2 8
WANGARATTA SHIRE—				
Beechworth Road .. .. .	..		761 14 11	
Rutherglen Road .. .. .	..		203 12 8	
Wangaratta-Myrtleford Road .. .. .	..		292 0 10	
Yarrawonga Road	100 0 0		119 8 10	
WANGARATTA AND RUTHERGLEN SHIRES (Joint Works)—				
Yarrawonga	34 5 10		19 13 7	
WANGARATTA AND BEECHWORTH SHIRES (Joint Works)—				
Beechworth Road .. .. .	..		18 15 8	
WANGARATTA BOROUGH AND WANGARATTA SHIRE (Joint Works)—				
Sydney Road .. .. .	..		0 5 0	
WANGARATTA, YARRAWONGA, AND RUTHERGLEN SHIRES (Joint Works)—				
Yarrawonga-Rutherglen Road .. .. .	..		8 9 8	
		134 5 10		1,424 1 2
WANNON SHIRE—				
Coleraine-Harrow-Apsley Road .. .. .	..		2,266 18 4	
Hamilton-Coleraine-Casterton Road .. .. .	..		1,644 18 10	
Wannon Bridge Road .. .. .	..		1,217 7 4	
WANNON AND GLENELG SHIRES (Joint Works)—				
Hamilton-Coleraine-Casterton Road .. .. .	..		6 2 6	
				5,135 7 0
Carried forward .. .. .	..	163,961 8 9	..	605,297 8 8

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	163,961 8 9	..	605,297 8 8
WARANGA SHIRE—				
Colbinabbin—Moora Road .. .. .	..	..	2,855 11 6	
Elmore—Colbinabbin Road .. .. .	..	..	2,754 3 5	
Heathcote—Elmore Road	2,139 14 4	..	2,830 0 1	
Murchison—Rushworth Road	917 12 0	..	1,006 8 11	
Tatura Road .. .. .	..	..	22 11 11	
WARANGA AND GOULBURN SHIRES (Joint Works)—				
Murchison—Rushworth Road .. .. .	..	..	19 16 9	
WARANGA AND HUNTLY SHIRES (Joint Works)—				
Heathcote—Elmore Road .. .. .	..	..	76 1 6	
		3,057 6 4		9,564 14 1
WARRAGUL SHIRE—				
Bloomfield Road .. .. .	..	..	145 6 2	
Brandy Creek Road .. .. .	..	..	3,472 2 1	
Darnum—Allambee Road .. .. .	..	..	587 14 3	
Prince's Highway .. .. .	..	..	193 15 11	
Warragul—Korumburra Road	23 7 0	..	1,225 5 0	
Warragul—Leongatha Road .. .. .	..	..	422 0 2	
WARRAGUL AND KORUMBURRA SHIRES (Joint Works)—				
Warragul—Korumburra Road .. .. .	..	..	1 9 11	
		23 7 0		6,047 13 6
WARRENAMBOOL SHIRE—				
Allansford—Nirranda Road .. .. .	..	..	5,949 19 2	
Caramut—Lismore Road .. .. .	..	..	3,716 19 0	
Garvoc—Lang Road .. .. .	..	..	1,911 3 6	
Mortlake Road .. .. .	..	..	6,683 1 0	
Framlingham Road .. .. .	..	..	1,908 3 3	
Peterborough Road	602 0 0	..	600 3 9	
		602 0 0		20,769 9 8
WERRIBEE SHIRE—				
Geelong—Bacchus Marsh Road .. .. .	..	..	304 0 0	
Prince's Highway .. .. .	..	..	230 4 2	
				534 4 2
WHITTLESEA SHIRE—				
Epping Road .. .. .	..	..	1,352 6 2	
Main Whittlesea Road	4,016 14 9	..	2,656 17 3	
Wallan Road .. .. .	..	..	168 9 7	
Whittlesea—Kingslake Road .. .. .	..	..	461 17 2	
		4,016 14 9		4,639 10 2
WIMMERA SHIRE—				
Dooen Road .. .. .	..	..	1,604 11 1	
Horsham—Wal Wal Road	74 9 7	..	85 11 6	
Natimuk Road	362 0 0	..	179 19 5	
WIMMERA, KOWREE, ARAPILES, AND DUNDAS SHIRES (Joint Works)—				
Horsham—Hamilton Road	3 1 2	..	1 1 8	
WIMMERA AND STAWELL SHIRES (Joint Works)—				
Horsham—Wal Wal Road .. .. .	..	..	97 12 6	
WIMMERA AND ARAPILES SHIRES AND HORSHAM BOROUGH (Joint Works)—				
Horsham—Hamilton Road .. .. .	..	..	26 13 4	
WIMMERA AND ARAPILES SHIRES (Joint Works)—				
Horsham—Hamilton Road .. .. .	..	..	225 9 10	
		439 10 9		2,220 19 4
WINCHELSEA SHIRE—				
Birregurra—Forrest Road.. .. .	1,727 16 3	..	1,477 14 5	
Lorne Road	1,284 14 3	..	439 3 7	
Lorne Road .. .. .	..	..	Bd. 3,076 8 7	
Prince's Highway .. .. .	..	..	Bd. 383 13 1	
Prince's Highway .. .. .	..	..	Shire 89 1 11	
		3,012 10 6		5,466 1 7
WODONGA SHIRE—				
Murray Valley Road .. .. .	..	..	3 6 11	
Sydney Road .. .. .	..	..	183 9 9	
Kiewa—Wodonga Road .. .. .	..	..	21 1 5	
Tallangatta Road .. .. .	..	..	239 14 7	
Wodonga—Yackandandah Road .. .. .	..	..	286 7 7	
				734 0 3
WONTHAGGI BOROUGH—				
Loch—Wonthaggi Road .. .. .	..	..	1,265 12 3	
Wonthaggi—Inverloch Road .. .. .	..	..	1,305 4 2	
Wonthaggi—Korumburra Road .. .. .	..	..	71 17 7	
WONTHAGGI BOROUGH AND BASS SHIRE (Joint Works)—				
Loch—Wonthaggi Road .. .. .	..	..	273 2 1	
				2,915 16 1
Carried forward .. .. .	..	175,112 18 1	..	658,189 17 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	175,112 18 1	..	658,189 17 6
WOORAYL SHIRE—				
Farmer's Road .. .. .	..		3,420 3 3	
Inverloch-Leongatha Road	797 9 6		1,227 16 5	
Inverloch-Wonthaggi Road .. .. .	..		141 9 10	
Leongatha-Yarragon Road	983 5 0		1,945 14 10	
Lower Tarwin Road .. .. .	..		781 14 3	
Main South Gippsland Road	153 16 3		2,853 13 1	
Mardan Road .. .. .	..		859 4 10	
Meeniyah-Stony Creek	64 13 4		..	
Turton's Creek Road .. .. .	..		49 12 3	
Warragul-Leongatha Road .. .. .	..		98 6 1	
Wild Dog Valley Road	475 10 10		273 1 5	
WOORAYL AND SOUTH GIPPSLAND SHIRES (Joint Works)—				
Boolarra-Foster Road .. .. .	..	2,474 14 11	145 6 6	11,796 2 9
WYCHEPROOF SHIRE—				
Calder Highway	26 7 3		..	
Birchip-Sealake Road	337 16 10		391 3 1	
Birchip-Wycheproof Road	1,080 19 9		166 10 7	
Sealake-Ultima Road	43 15 5		453 3 8	
Woomelang-Sealake Road .. .. .	..		90 2 3	
Wycheproof-Sealake Road .. .. .	..	1,488 19 3	1,195 12 6	2,296 12 1
YACKANDANDAH SHIRE—				
Dederang Road	562 7 11		428 4 6	
Gundowring Road	713 12 10		498 16 9	
Kiewa-Wodonga Road	442 5 8		527 10 2	
Yackandandah-Wodonga Road	148 8 5	1,866 14 10	532 0 6	1,986 11 11
YARRAWONGA SHIRE—				
Tungamah-Wilby Road	35 3 5		14 16 9	
Yarrawonga-Cobram Road	18 10 0		876 1 2	
Wangaratta-Yarrawonga Road .. .. .	..		1,888 0 8	
Yarrawonga-Wangaratta Road	1,057 10 11		..	
Yarrawonga-Rutherglen Road .. .. .	..		732 11 4	
YARRAWONGA, WANGARATTA, AND RUTHERGLEN SHIRES (Joint Works)—				
Yarrawonga-Rutherglen Road .. .. .	..	1,111 4 4	8 9 9	3,519 19 8
YEA SHIRE—				
Upper Goulburn Road	422 11 2		1,631 12 1	
Yea-Glenburn Road	180 2 10		895 6 8	
YEA AND ALEXANDRA SHIRES (Joint Works)—				
Upper Goulburn Road .. .. .	..		36 8 4	
YEA AND BROADFORD SHIRES (Joint Works)—				
Upper Goulburn Road .. .. .	..	602 14 0	91 15 10	2,655 2 11
	..	182,657 5 5	..	680,444 6 10
STATE HIGHWAYS.				
Prince's Highway West .. .. .	..		173,861 5 7	
Prince's Highway East .. .. .	..		114,821 18 2	
Western Highway	291 16 11		96,708 5 8	
Calder Highway .. .. .	..		71,760 16 8	
Northern Highway .. .. .	..		20,832 12 1	
Hume Highway	1,380 10 8		75,988 11 3	
Omeo Highway	823 11 3		36,484 16 1	
		2,495 18 10		590,458 5 6
Suspense		185,153 4 3		1,270,902 12 4
Adjustment—		31 10 6		..
WINCHELSEA SHIRE—				
Prince's Highway .. .. .	..		..	267 3 1
Total .. .. .	..	185,121 13 9	..	1,270,635 9 3

APPENDIX E.

COUNTRY ROADS BOARD.

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS FOR YEAR ENDED 30TH JUNE, 1930.

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3662 (3255).	
	Amount.	Total.		Amount.	Total.
	£	s. d.		£	s. d.
ALBERTON SHIRE—			Brought forward ..		31,455 17 6
Albert River Road ..	711	10 0			
Balook-Traralgon Road ..	6,557	13 10	BENALLA SHIRE—		
Binginwarri-Albert River Road ..	35	12 0	Toombullup Road ..	174	11 10
Carrajung-Gormandale Road ..	4,003	8 3	Molyullah-Tatong Road ..	397	15 6
Gelliondale Road ..	595	8 4			572 7 4
Lay's Road ..	3,003	2 0	BERWICK SHIRE—		
Madalya Road ..	418	6 8	Emerald Road ..	..	..
Whitelaw's Track Road ..	1,984	1 0	Beaconsfield-Emerald Shire ..	1,217	2 11
ALBERTON AND ROSEDALE SHIRES			Cockatoo-Gembrook Road ..	1,042	14 3
(Joint Works)—			Garfield-Catani Road ..	650	10 11
Merriman's Creek Road ..	52	17 6	Tynong-Tonimbuk ..	255	18 8
ALBERTON AND SOUTH GIPPSLAND			Upper Beaconsfield - Upper		
SHIRES (Joint Works)—			Pakenham Road ..	49	5 0
Devil's Pinch Road ..	0	1 0	BERWICK AND FERNTREE GULLY		
ALBERTON AND TRARALGON			SHIRES (Joint Works)—		
SHIRES (Joint Works)—			Emerald Road ..	271	10 5
Ridge Road ..	0	10 0			3,487 2 2
ALEXANDRA SHIRE—			BIRCHIP SHIRE—		
Terip Terip Road ..	778	5 0	Curyo West Road ..	895	0 0
ARAPILES SHIRE—			Berriwillock Road ..	68	7 0
Arapiles-Grassy Flat Road ..	311	5 0	Kinnabulla West Road ..	61	12 0
Miga Lake-Gymbowen Road ..	225	0 0	Watchupga Road ..	35	0 0
ARARAT SHIRE ..	..	..			1,059 19 0
AVOCA SHIRE—			BORUNG SHIRE—		
Maryborough-Natte Yallock			Aubrey Road ..	28	3 0
Road ..	151	7 11	Boolite-Sheep Hills Road ..	264	5 3
BACCHUS MARSH SHIRE—			Brim West Road ..	514	0 5
Parwan ..	824	0 0	Donald-Warracknabeal Road ..	603	5 6
BAIRNSDALE SHIRE—			Galaquil West Road ..	834	14 6
Bulumwaal-Tabberabbera			Lah West Road ..	1,050	9 5
Road ..	1,327	18 3	BORUNG AND KARKAROOC SHIRES		
Bairnsdale-Bengworden Road ..	357	13 2	(Joint Works)—		
Calulu-Boggy Creek Road ..	95	8 9	Galaquil West Road ..	151	8 6
Fernbank-Stockdale Road ..	106	18 9			3,446 6 7
Hodge's Estate Road ..	1,437	14 7	BRIGHT SHIRE—		
		3,325 13 6	Kiewa Valley Road ..	2,776	1 4
BALLAN SHIRE—			Buffalo River Road ..	2	16 2
Blakeville Road ..	111	19 11	Happy Valley Road ..	1,969	12 8
BALLAN AND BUNINYONG SHIRES			Myrtleford-Yackandandah		
(Joint Works)—			Road ..	453	7 9
Egerton-Bungal Road ..	1,856	5 3			5,201 17 11
BANNOCKBURN SHIRE ..	..	..	BROADMEADOWS AND BULLA		
BASS SHIRE—			SHIRES (Joint Works)—		
Dalyston-Glen Forbes Road ..	2,735	17 1	Konagadera Road ..	1,130	10 7
Glen Alvie Road ..	1,683	17 0			1,130 10 7
Wonthaggi-Loch Road ..	1,892	7 5	BULLA SHIRE—		
Korumburra-Wonthaggi Road ..	31	15 4	Riddell Road ..	2,461	5 4
		6,343 16 10	BULLA AND BROADMEADOWS		
BEECHWORTH SHIRE—			SHIRES (Joint Works)—		
Beechworth-Hillsborough Road ..	95	12 4	Konagadera Road ..	1,130	10 7
Myrtleford-Yackandandah					3,591 15 11
Road ..	50	19 1	BULN BULN SHIRE—		
Stanley Road ..	0	10 0	Apteds Road ..	374	18 0
BEECHWORTH AND YACKANDAN-			Jindivick-Neerim South Road ..	553	16 4
DAH SHIRES (Joint Works)—			Neerim South-Neerim East		
Beechworth Yackandandah			Road ..	2,239	14 9
Road ..	1	15 4	Poowong Road ..	1,556	15 2
Myrtleford-Yackandandah			Rokeby-North Jindivick Road ..	1,577	5 6
Road ..	16	16 9	Torwood-Topiram Road ..	1,203	0 7
		165 13 6	BULN BULN AND WARRAGUL		
Carried forward ..	..	31,455 17 6	SHIRES (Joint Works)—		
			Lardner's Track Road ..	578	5 1
					8,083 15 5
			BUNGAREE SHIRE—		
			Bolwarrah Road ..	100	8 3
					100 8 3
			BUNINYONG AND BALLAN SHIRES		
			(Joint Works)—		
			Egerton-Bungal Road ..	1,856	5 3
					1,856 5 3
			Carried forward ..	..	59,986 5 11

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—continued.

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3662 (3255).	
	Amount.	Total.		Amount.	Total.
	£ s. d.	£ s. d.		£ s. d.	£ s. d.
Brought forward ..	..	59,986 5 11	Brought forward ..	..	99,271 11 4
CHARLTON SHIRE—			ELTHAM SHIRE—		
Buckrabanyule North Road ..	549 15 7		Cottle's Bridge Road ..	5 16 0	
Glenloth Road ..	45 13 6		Cottle's Bridge—Strathewan Road ..	3,454 4 10	
Lake Marmal Road ..	920 15 6		Tarrawarra Road ..	58 18 3	3,518 19 1
Teddywaddy Road ..	59 9 10		EUROA SHIRE—		
Wooroonook Road ..	1,173 6 11		Fern Hills Road ..	683 10 0	
Yeungroon Road ..	1,701 17 4		Merton—Strathbogie Road ..	2,135 12 8	
CHARLTON AND KORONG SHIRES (Joint Works)—			Strathbogie Road ..	606 3 0	3,425 5 8
Buckrabanyule South Road ..	71 7 7	4,522 6 3	FERNTREE GULLY SHIRE—		
COHUNA SHIRE—			Belgrave—Narrewarren Road ..	827 19 4	
Murray River Valley Road (F.A.R. Exp.) ..	4,818 18 3		Emerald Road ..	574 17 6	
Murray River Valley Road ..	1 13 0		Emerald—Macclesfield Road ..	169 18 6	
Cohuna—Leitchville Road ..	2,112 7 8		Lysterfield Road ..	301 16 4	
Cohuna—Mead Road ..	680 0 0		Emerald—Monbulk Road ..	55 18 5	2,202 0 6
Cohuna—McMillan's Road ..	323 14 7		FERNTREE GULLY AND BERWICK SHIRES (Joint Works)—		
Gannawarra Road ..	1,489 18 7		Emerald Road ..	271 10 5	2,202 0 6
Gunbower Island Road ..	24 5 1		FLINDERS SHIRE—		
Keely's Lane Road ..	58 15 2		Main Creek Road ..	4 12 0	
COHUNA AND ROCHESTER SHIRES (Joint Works)—			Boneo Road ..	1,529 0 7	
Gunbower Island Road ..	115 7 1	9,624 19 5	Bittern—Dromana Road ..	1,154 4 10	
CRANBOURNE SHIRE—			Brown's Road ..	610 14 3	
Koo-wee-rup—Modella Road ..	829 19 11		Red Hill Road ..	256 0 0	3,554 11 8
Mank's Road ..	55 0 5		FRANKSTON AND HASTINGS SHIRE—		
Pearcedale Road ..	3,121 18 9	4,006 19 1	Hodgin's Road ..	400 0 0	
COLAC SHIRE—			Quarry Road ..	261 18 8	661 18 8
Colac—Forrest Road ..	1,617 3 5		GLENELG SHIRE—		
COLAC AND OTWAY SHIRES (Joint Works)—			Dergholm—Elderslie Road ..	479 14 6	
Colac—Forrest Road ..	0 4 8	1,617 8 1	Glenorchy Estate Road ..	414 18 5	
CORIO SHIRE—			Merino Struan—Tahara Road ..	1,337 0 8	2,231 13 7
Geelong Bacchus Marsh Road ..	4,544 9 5		GLENLYON SHIRE—		
Little River—Ripley Road ..	39 7 2		Daylesford—Trentham Road ..	2,176 5 9	
Stacy Road ..	1 19 6	4,585 16 1	Porcupine Ridge Road ..	441 2 9	
DEAKIN SHIRE—			South Bullarto Road ..	355 0 0	2,972 8 6
Echuca—Wyuna Road ..	1,352 11 10		GORDON SHIRE—		
Echuca East Road ..	4 2 6		Pyramid Hill—Mologa Road ..	54 17 7	
Girgarre East Road ..	428 4 1		Boort—Durham Ox Road ..	514 8 3	
Girgarre West Road ..	542 9 0		Boort—Marmal Road ..	31 17 0	
Girgarre South Road ..	34 17 5		Barrarport West Road ..	930 9 3	1,531 12 1
Kyabram West Road ..	256 4 0		GOULBURN SHIRE—		
Strathallan East Road ..	853 7 2		Longwood—Ruffly Road ..	1,734 14 8	1,734 14 8
Tongala East Road ..	414 0 0		GRENVILLE SHIRE—		
Tongala West Road ..	376 9 6		Gillett's Road ..	920 2 10	
DEAKIN AND RODNEY SHIRES (Joint Works)—			Lismore—Pittong Road (F.A.R. Exp.) ..	525 8 0	1,445 10 10
Kyabram—Stanhope Road ..	137 16 3	4,400 1 9	HAMPDEN SHIRE—		
DIMBOOLA SHIRE—			Vite Vite Road (F.A.R. Exp.) ..	213 8 7	
Albacutya Road ..	170 14 7		Ayresford Road ..	1 13 9	
Glenlee—Jeparit Road ..	588 3 10		Berrybank—Werneth Road ..	23 16 6	
Pepper's Plains Road ..	590 0 0	1,348 18 5	Cundare—Duverney Road ..	225 10 6	
DONALD SHIRE—			Foxhow Road ..	628 0 2	1,092 9 6
Jeffcott Road ..	848 16 4		HEALESVILLE SHIRE—		
Corack East Road ..	710 0 0		Myer's Creek Road ..	814 19 2	
Litchfield Road ..	1,027 0 0		Buxton—Marysville Road ..	414 0 0	1,228 19 2
Rich Avon Road ..	472 13 4		HEYTESBURY SHIRE—		
Sheep Hills Road ..	174 17 11		Timboon—Scott's Creek Road (F.A.R. Exp.) ..	888 19 7	
Watchem—Warracknabeal Road ..	905 0 0	4,138 7 7	Corriejong Road ..	12 18 6	
DUNDAS SHIRE—			Devil's Gully Road ..	3,189 12 11	
Kanawalla—Noske Road ..	23 12 4	23 12 4	Glenfyne West Road ..	1,678 18 9	
DUNMUNKLE SHIRE—			Kennedy's Creek Road ..	120 10 0	
Marnoo—Donald Road (F.A.R. Exp.) ..	1,230 5 2		South Ecklin Road ..	1,868 14 0	
Banyena Road ..	29 19 10		Timboon—Scott's Creek Road ..	14 8 3	
Minyip—Kewell Road ..	1,632 0 5		Timboon—Terang Road ..	765 6 9	
Murtoa North Road ..	2,124 11 0	5,016 16 5	HEYTESBURY AND WARRNAMBOOL SHIRES (Joint Shires)—		
Carried forward ..	..	99,271 11 4	Ayresford Road ..	558 14 10	9,098 3 7
			Carried forward ..	..	133,969 18 10

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3662 (3255).	
	Amount.	Total.		Amount.	Total.
	£ s. d.	£ s. d.		£ s. d.	£ s. d.
Brought forward ..	..	133,969 18 10	Brought forward ..	..	163,777 6 10
HUNTLY SHIRE—			LAWLOIT SHIRE—		
Diggora Road ..	954 14 2		Cove Estate Settlement Road ..	45 6 9	
Drummartin Road ..	1,027 3 4		Little Desert Road ..	90 0 0	
Holmberg Road ..	79 11 11	2,061 9 5	Lillimur South Road ..	27 3 0	
			Miram West Road ..	2,082 6 3	
KARA KARA SHIRE—			Miram ..	12 4 10	
Coonooer Road ..	571 16 0		Serviceton North Road ..	30 1 8	
Marnoo-St. Arnaud Road ..	1,951 1 4		Serviceton South Road ..	9 18 6	2,297 1 0
Swanwater Road ..	1,005 18 9				
Sandy Creek Road ..	32 6 5	3,561 2 6	LILLYDALE SHIRE—		
			The Chalet Road ..	592 16 0	
KARKAROO SHIRE—			Monbulk-Seville Road ..	1,346 4 9	
Burroin East Road ..	7 11 3		York Road ..	173 18 0	
Cambacanya Road ..	50 17 10		Tarrawarra Road ..	2 5 0	2,115 3 9
Hopetoun-Lascelles Road ..	493 15 1				
Hopetoun-Yaapect Road ..	1,108 16 10		LOWAN SHIRE—		
Rosebery West Road ..	110 4 9		Netherby Road ..	780 4 6	
Rosebery East Road ..	493 16 7		Woorak Road ..	24 10 4	
Wathe Siding Road ..	262 5 8		Winiam Road ..	757 0 0	
Yaapect-Nypo Road ..	177 4 8		Yanac South Road ..	639 0 3	2,200 15 1
Yarto Road ..	61 12 6				
Yarto-Patchewollock Road ..	140 5 11		MAFFRA SHIRE—		
			Bundalaguah Road ..	1,177 19 5	
KARKAROO AND BORUNG SHIRES			Licola Road ..	2 5 0	
(Joint Works)—			Maffra-Newry Road ..	70 0 0	
Galaquil West Road ..	151 8 6	3,057 19 7	Mewburn Park Road ..	578 7 8	1,828 12 1
KERANG SHIRE—			MANSFIELD SHIRE—		
Murray River Valley Road			Benalla-Mansfield Road ..	1,802 16 11	
(F.A.R. Exp.) ..	2,908 10 7		Tolmie Road ..	419 8 9	2,222 5 8
Murrabit Road ..	1,237 16 7				
Murrabit-Nyal Road ..	1,093 7 9		MARONG SHIRE—		
Murrabit West Road ..	1,161 18 4		Bendigo-Serpentine Road ..	812 8 11	
			Kamarooka Road ..	27 7 11	
KERANG AND SWAN HILL SHIRES			Yarraberb Road ..	548 12 2	
(Joint Works)—			Newbridge-Shelbourne Road ..	260 9 1	1,648 18 1
Winlaton Road ..	1,824 9 1	8,226 2 4			
			MELTON SHIRE—		
KORONG SHIRE—			Coburns Road ..	33 13 8	
Emu-Logan Road ..	0 4 2		Exford Road ..	1,018 6 4	
Inglewood North Road ..	560 17 7		Mount Kororoit Road ..	244 2 0	1,296 2 0
Kinypaniel ..	590 1 11				
Kurting-Rheola Road ..	0 9 7		MILDURA SHIRE—		
Mysia West Road ..	50 0 0		Murray River Valley Road		
Mysia East Road ..	46 11 6		(F.A.R. Exp.) ..	683 15 10	
Nine Mile Road ..	0 0 7		Benetook South Road ..	0 8 4	
Wychitella North Road ..	380 14 0		Colignan Road ..	645 11 11	
Wedderburn-Springhill Road ..	420 19 11		Brownport Road ..	336 5 3	
Woolshed Road ..	0 5 3		Karrawinna South Road ..	58 16 0	
			Karween North Road ..	108 11 6	
KORONG AND CHARLTON SHIRES			Karween South Road ..	146 10 5	
(Joint Works)—			Meringur South Road ..	33 2 3	
Buckrabanyule South Road ..	71 7 7	2,121 12 1	Merrinee North Road ..	0 10 8	
			Merrinee South Road ..	200 0 0	
KORUMBURRA SHIRE—			Redcliffs East Road ..	699 4 4	
Bena-Kongwak Road ..	2,634 14 0		Redcliffs South-east Road ..	714 10 4	
Briggs Road ..	17 2 6		Redcliffs West Road ..	240 8 6	
Henry's Road ..	71 1 6		Werrimull North Road ..	1 19 0	
Korumburra South Road ..	324 1 0		Werrimull South Road ..	91 0 0	
Loch-Nyora Road ..	2,023 12 6				
New Territory Road ..	340 0 0		MIGRATION ROADS—		
Poowong-Olsen Road ..	400 0 0		Bambill North Road ..	5 18 8	
Sheepways Road ..	0 2 6		Bambill South Road ..	1 6 2	
Witherden Road ..	944 6 0		Benetook North Road ..	136 17 0	
			Benetook South Road ..	51 17 4	
KORUMBURRA AND WOORAYL			Karrawinna North Road ..	255 0 7	
SHIRES (Joint Works)—			Karrawinna South Road ..	458 10 10	
Wild Dog Valley Road ..	91 4 6	6,846 4 6	Meringur North Road ..	551 17 0	
			Meringur South Road ..	790 3 10	
KOWREE SHIRE—			Merrinee North Road ..	272 3 9	
Little Desert Road (F.A.R.			Pirlta North Road ..	56 15 9	
Exp.) ..	622 18 5		Pirlta South Road ..	20 15 2	
Benayeo Road ..	317 17 10		Werrimull North Road ..	183 5 5	
Edenhope-Natimuk Road ..	547 19 11		Yarrara North Road ..	4 12 7	
Elderslie Road ..	732 5 8		Yarrara South Road ..	295 11 5	
Little Desert Road ..	27 13 4		Werrimull South Road ..	36 15 6	
Miga Lake-Gymbowen Road ..	324 13 11		Merrinee South Road ..	336 0 1	7,418 5 5
Minimay Road ..	1,313 15 6	3,887 4 7			
			MINHAMITE SHIRE—		
KYNETON SHIRE—			Condah-McArthur Road ..	671 6 5	
Lagoon Road ..	45 13 0	45 13 0	Heywood Road ..	58 9 3	
			Nardoo-Moyne River Bridge		
Carried forward ..	..	163,777 6 10	Road ..	87 1 4	
			Nardoo Road ..	246 11 6	
			Orford-St. Helens Road ..	994 12 7	2,058 1 1
			Carried forward ..	..	186,862 11 0

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3662 (3255).	
	Amount.	Total.		Amount.	Total.
	£ s. d.	£ s. d.		£ s. d.	£ s. d.
Brought forward ..	..	186,862 11 0	Brought forward ..	..	220,171 18 0
MIRBOO SHIRE—			OXLEY SHIRE—		
Boolarra-Mirboo Road (F.A.R. Exp.) ..	1,339 5 9		Buffalo River Road ..	110 6 10	
Mount Vernon-Boorool Road (F.A.R. Exp.) ..	197 2 10		Carbour-Meadow Creek Road ..	4 18 0	
Darlimurla-Thorpdale Road ..	49 7 4		Boggy Creek Road ..	463 14 11	
Mirboo North-Thorpdale Road ..	507 1 3		Fifteen Mile Road ..	1,467 15 3	
Nichol's Road ..	2,398 16 5	4,491 13 7	King Valley Road ..	783 13 8	
MORTLAKE SHIRE—			Rose River Road ..	220 9 6	3,050 18 2
Vite Vite Road (F.A.R. Exp.) ..	687 12 11	687 12 11	PORTLAND SHIRE—		
MORWELL SHIRE—			Grubbed Road ..	2,285 8 8	
Boolarra-Mirboo Road (F.A.R. Exp.) ..	929 14 10		Grubbed-Connexion Road ..	49 10 0	
Boolarra-Morwell Road ..	1,324 10 6		Gorae Road ..	42 16 0	
Boolarra-Welshpool Road ..	11 0 4		Winnap-Drik Drik Road ..	107 15 0	2,485 9 8
Hazelwood Estate Road ..	42 13 6		PYALONG SHIRE—		
Middle Creek Road ..	32 18 4		Lancefield-Tooboora Road ..	549 11 4	549 11 4
Moe-Yallourn Road ..	874 5 6		RIPON SHIRE—		
Thorpdale East Road ..	1,898 15 0		Trawalla West Road ..	1,762 18 5	1,762 18 5
Tyer's Road ..	46 0 0		ROCHESTER SHIRE—		
MORWELL AND TRARALGON SHIRES (Joint Works)—			Murray River Valley Road (F.A.R. Exp.) ..	57 17 7	
Walker's Road ..	122 14 2	5,282 12 2	Corop Road ..	609 9 3	
MCIVOR SHIRE—			Echuca West Road ..	304 17 0	
Mt. Camel Estate Road (F.A.R. Exp.) ..	1,040 3 11		Kotta East Road ..	1,597 1 9	
Mt. Camel Estate-Lady's Pass Bridge (F.A.R. Exp.) ..	688 14 4		ROCHESTER AND COHUNA SHIRES (Joint Works)—		
Baynton Road ..	2,000 0 0	3,728 18 3	Gunbower Island Road ..	115 7 1	2,684 12 8
NARRACAN SHIRE—			RODNEY SHIRE—		
Childers-Thorpdale Road ..	178 17 6		Mooroopna-Undera Road ..	1,247 13 11	
Darlimurla-Thorpdale Road ..	50 6 11		Tatura-Toolamba Road ..	476 14 6	
Coalville-Narracan Road ..	695 19 5		RODNEY AND DEAKIN SHIRES (Joint Works)—		
Erica Road ..	2 3 7		Kyabram-Stanhope Road ..	137 16 3	1,862 4 8
Mirboo-Yarragon Road ..	43 0 4		ROMSEY SHIRE—		
Moe-Yallourn Road ..	1,096 15 11		Baynton Road ..	515 13 9	515 13 9
Platina Road ..	1,075 7 0		ROSEDALE SHIRE—		
Thorpdale-Yarragon Road ..	1,760 3 4		Calligee South Road ..	18 9 10	
Trafalgar-Willow Grove Road ..	48 6 1		ROSEDALE AND ALBERTON SHIRES (Joint Works)—		
Thorpdale East Road ..	179 6 6		Merriman's Creek Road ..	52 17 6	71 7 4
Trafalgar South Road ..	322 7 8	5,452 14 3	RUTHERGLEN SHIRE—		
NEWHAM AND WOODEND SHIRE—			Black Swamp Road ..	290 19 0	290 19 0
Campaspe Road ..	453 0 4		SEYMOUR SHIRE—		
Macedon-Village Settlement Road ..	1,031 17 5	1,484 17 9	Highland's Road ..	52 15 0	
NEWSTEAD AND MT. ALEXANDER SHIRES—			Hughes Creek Road ..	868 19 9	921 14 9
Glengower-Joyce's Creek Road ..	479 11 4	479 11 4	SHEPPARTON SHIRE—		
NUMURKAH SHIRE—			Congupna Road ..	453 3 4	
Waaia North Road ..	1,041 17 0		Graham's Vale Road ..	1,033 11 6	
Waaia South Road ..	61 15 0		Nathalia Road ..	265 8 11	
Wunghnu East Road ..	724 11 9	1,828 3 9	Pine Lodge North Road ..	1,063 13 4	2,815 17 1
OMEQ SHIRE—			SOUTH GIPPSLAND SHIRE—		
Beloka Road ..	285 14 0		Binginwarri South Road ..	189 16 2	
Benambra-Corryong Road ..	292 5 7		Chadwicke's Road ..	597 19 2	
Brookville Road ..	1,393 1 4		Dollar-Stony Creek Connexion Road ..	17 12 5	
Glen Wills Road ..	7 0 0		Foster-Dollar Road ..	1,568 11 10	
Bindi Road ..	1,387 7 8		Foster-Boolarra Road ..	2 8 9	
Little River Road ..	1,472 5 7		Foster-Mount Best Road ..	41 2 2	
Mt. Leinster Road ..	31 17 6		Foster-Stony Creek Road ..	753 10 7	
Reedy Creek Road ..	498 3 3		Franklin Road ..	1,080 7 6	
Sandy Creek Road ..	120 17 4		Harding-Lawson Road ..	85 11 6	
Swift's Creek East Road ..	1,425 12 9	6,914 5 0	McCartin's Road ..	92 10 2	
ORBOST SHIRE—			O'Grady's Ridge Road ..	487 10 0	
Bete Bolong-Waygara Road ..	1,045 5 8		Waratah Road ..	599 19 4	
Grove's Road ..	30 0 0		Whitelaw's Track Road ..	791 2 7	
Lower Bemm Road ..	957 14 2		Woorara West Road ..	36 0 0	
Mallacoota West Road ..	334 19 7		Woomera Creek Road ..	0 2 2	
Tamboon Road ..	410 12 9		Yanakie Road ..	106 8 5	
Wangarabelle Road ..	6 13 9		SOUTH GIPPSLAND AND ALBERTON SHIRES (Joint Works)—		
West Cann Road ..	173 7 5	2,958 13 4	Devil's Pinch Road ..	0 1 0	
OTWAY AND COLAC SHIRES (Joint Works)—			Carried forward ..	..	237,183 4 10
Colac-Forrest Road ..	0 4 8	0 4 8			
Carried forward ..	..	220,171 18 0			

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3661 (3255).	
	Amount.	Total.		Amount.	Total.
	£ s. d.	£ s. d.		£ s. d.	£ s. d.
Brought forward ..	..	237,183 4 10	Brought forward ..	..	281,068 2 1
SOUTH GIPPSLAND AND WOORAYL SHIRES (Joint Works)—			WALPEUP SHIRE—		
Dollar—Dumbalk Road ..	1,560 15 0	8,011 8 9	Panitya North Road (F.A.R. Exp.) ..	1,056 17 6	
STAWELL SHIRE—			Boinka North Road ..	279 7 4	
Marnoo—Donald Road (F.A.R. Exp.) ..	677 8 9		Boinka South Road ..	249 8 8	
Marnoo—St. Arnaud Road ..	159 8 8		Boorongie North Road ..	104 12 2	
Pomonal Road ..	227 7 3	1,064 4 8	Boulka—Timberoo Road ..	586 19 11	
SWAN HILL SHIRE—			Carina South Road ..	612 15 9	
Fish Point Road ..	135 19 0		Carina North Road ..	878 0 7	
Kooloonong East Road ..	73 11 1		Cowangie Road ..	1,234 14 5	
Manangatang—Euston Road ..	3,872 5 11		Danyo North Road ..	817 10 4	
Piangil—Prooingā Road ..	78 4 4		Galah—Timberoo Road ..	845 0 9	
Tyntynder Settlement Road ..	71 10 10		Kattyong Road ..	540 2 3	
Woorinen Settlement Road ..	16 6 5		Linga North Road ..	94 14 10	
SWAN HILL AND KERANG SHIRES (Joint Works)—			Murrayville North Road ..	993 7 10	
Winlaton Road ..	1,824 9 1		Murrayville South Road ..	529 18 5	
SWAN HILL AND WYCHEPROOF SHIRES (Joint Works)—			Nyang South Road ..	382 19 0	
Nyarrin Road ..	259 13 2	6,331 19 10	Ouyen—Kulwin Road ..	415 0 8	
TAMBO SHIRE—			Ouyen—Tempy Road ..	486 0 0	
Bills Road ..	1 15 6		Panitya South Road ..	509 15 9	
Buchan South Road ..	132 19 6		Tiega North Road ..	378 7 7	
Nowa Nowa—Buchan Road ..	154 16 1		Tyalla—Koonda Road ..	102 8 9	
Metung Road ..	282 1 1		Underbool—Gnarr Road ..	410 0 4	
Nyremilang Road ..	10 17 11	582 10 1	Underbool—Mamengarook Road ..	1,612 17 1	
TOWONG SHIRE—			Underbool South Road ..	796 18 11	
Murray River—Tallangatta Road (F.A.R. Exp.) ..	2,227 3 5		Walpeup—South-west Road ..	995 10 3	
George's Creek ..	559 10 11		Walpeup—Patchewollock Road ..	517 19 9	
Shelley—Jingellic Road ..	3,942 0 9		Walpeup—North Meridian Road ..	408 16 3	15,840 5 1
Tallangatta Creek Road ..	57 11 8		WANGARATTA SHIRE—		
Yabba Road ..	1,061 13 10	7,848 0 7	Boorhaman—Springhurst Road ..	1,256 4 8	
TRARALGON SHIRE—			Peechelba Station Road ..	592 7 0	
Callignee Factory Road ..	136 2 1		WANGARATTA AND YARRAWONGA SHIRES (Joint Works)—		
Traralgon Creek Road ..	12 0 0		Peechelba Bridge ..	32 10 0	1,881 1 8
Traralgon—Jeeralang Road ..	1,812 1 11		WANNON SHIRE—		
TRARALGON AND ALBERTON SHIRES (Joint Works)—			Melville Forest Road ..	27 3 10	27 3 10
Ridge Road ..	0 10 0		WARANGA SHIRE—		
TRARALGON AND MORWELL SHIRES (Joint Works)—			Mount Camel Estate Road (F.A.R. Exp.) ..	451 5 8	
Walker's Road ..	122 14 2	2,083 8 2	Mount Camel—Corop Road ..	2,301 11 5	2,752 17 1
TUNGAMAH SHIRE—			WARRAGUL SHIRE—		
Boweya Road ..	1,195 18 4		Bona Vista—Nilma Road ..	460 16 2	
Cobram—Katamatite Road ..	181 2 4		Ellinbank Road ..	60 4 6	
Yabba North Road ..	3,033 11 6		Ferndale Road ..	154 8 8	
Yabba South Road ..	243 5 0		Lardner—Tetoora Road ..	55 15 7	
Katandra Road ..	690 13 6		Mountain View—McDonald's Track Road ..	177 5 4	
Youanmite—Wunghnu Road ..	1,430 7 4		Nilma—Shady Creek Road ..	1,424 0 9	
Yarroweyah—Tocumwal Road ..	1,811 8 0	8,586 6 0	Telegraph Road ..	1,564 7 7	
UPPER MURRAY SHIRE—			WARRAGUL AND BULN BULN SHIRES (Joint Works)—		
Murray River Valley Road (F.A.R. Exp.) ..	378 15 6		Lardner's Track Road ..	578 5 1	4,475 3 8
Corryong—Benambra Road ..	536 14 4		WARRNAMBOOL SHIRE—		
Kancobin Road ..	92 9 0		Childer's Cove Road ..	1,774 13 2	
Thowgla Road ..	44 13 10	1,052 12 8	Naringle Road ..	1,035 0 0	
UPPER YARRA SHIRE—			Nullawarre—Timboon Road ..	79 9 2	
Woodspoint Road ..	212 17 10		Panmure Road ..	379 16 0	
Woori Yallock—Cockatoo Road ..	3,524 6 3	3,737 4 1	WARRNAMBOOL AND HEYTESBURY SHIRES (Joint Works)—		
VIOLET TOWN SHIRE—			Ayresford Road ..	558 14 11	3,827 13 3
Fern Hills Road ..	1,194 19 0		WERRIBEE SHIRE—		
Harry's Creek Road ..	3,392 3 5	4,587 2 5	Exford Road ..	1,126 9 5	1,126 9 5
Carried forward ..	..	281,068 2 1	WHITTLESEA SHIRE—		
12206.—6			Chadd's Creek Road ..	234 16 6	
			Eden Park Road ..	3,878 11 11	4,113 8 5
			WINCHELSEA SHIRE—		
			Bitreguita—Forrest Road ..	841 10 1	
			Boonah Road ..	1,047 7 8	
			Conn's Lane Road ..	449 17 6	
			Lorne Road ..	277 12 1	
			Winchelsea—Inverleigh Road ..	1,108 2 4	3,724 9 8
			Carried forward ..	..	318,836 14 2

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3662 (3255).	
	Amount.	Total.		Amount.	Total.
	£ s. d.	£ s. d.		£ s. d.	£ s. d.
Brought forward ..	..	318,836 14 2	SPECIAL PROVISION.		
WODONGA SHIRE—			ALBERTON SHIRE—		
Beechworth-Wodonga Road ..	1,326 7 4		Binginwarri-Welshpool Road ..	162 1 0	
Kiewa-Wodonga Road ..	108 10 0	1,434 17 4	Binginwarri-Albert River Road ..	1,545 12 4	
WOORAYL SHIRE—			Carrajung-Balook Road ..	4,846 6 4	
Boorool Road ..	31 4 6		Madalya Road ..	2,399 8 10	
Buffalo-Waratah Road ..	0 2 2		Dingo Creek Road ..	0 4 3	
Canavan's Road ..	167 3 10		ALBERTON AND TRARALGON		
Coulter's Road ..	628 5 3		SHIRES (Joint Works)—		
Dollar-Dumbalk Road ..	5 10 0		Ridge Road ..	630 18 1	
Inverloch-Lower Tarwin Road ..	857 17 1		ALBERTON AND ROSEDALE SHIRES		
Leongatha-Mirboo Road ..	1,840 5 8		(Joint Works)—		
Mardan Road ..	2,608 5 5		Merriman's Creek Road ..	147 6 5	
Mardan-Dumbalk Road ..	303 10 0		Callignee South Road ..	279 18 6	
Meeniyan-Nerrena Road ..	545 11 5		Gilmour's Outlet Road ..	21 5 0	
Nerrena Road ..	559 4 8		ALBERTON AND MORWELL SHIRES		
WOORAYL AND SOUTH GIPPSLAND			(Joint Works)—		
SHIRES (Joint Works)—			Boolarra-Welshpool Road ..	0 1 0	
Dollar-Dumbalk Road ..	1,560 15 1		Gunyah-Ryton Road ..	0 2 1	
WOORAYL AND KORUMBURRA			ALBERTON, MORWELL, AND		
SHIRES (Joint Works)—			TRARALGON SHIRES (Joint		
Wild Dog Valley Road ..	91 4 7	9,198 19 8	Works)—		
WYCHEPROOF SHIRE—			Ridge-road-Whitelaw's Track	4,054 17 11	14,088 1 9
Berriwillock-Woomelang Road ..	769 5 0		Road ..	..
Culgoa-Lalbert Road ..	327 11 11		AVOCA SHIRE—		
Dumosa Road ..	9 1 10		Maryborough-Natte Yallock	0 0 9	0 0 9
Glenloth Road ..	199 8 11		Road ..	..
Meridian Road ..	705 16 6		BENALLA AND OXLEY SHIRES		
Nullawil-Winston Road ..	322 1 8		(Joint Works)—		
Nyarrin Road ..	177 7 2		Toombullup Road (F.A.R. Exp.)	2,958 11 3	2,958 11 3
Sealake-Myall Road ..	30 8 9		BRIGHT SHIRE—		
Sealake-Tyrell Downs Road ..	281 3 10		Happy Valley Road ..	1 4 0	
WYCHEPROOF AND SWAN HILL			BRIGHT AND YACKANDANDAH		
SHIRES (Joint Works)—			SHIRES (Joint Works)—		
Nyarrin Road ..	259 13 2	3,081 18 9	Happy Valley and Running	0 2 9	
YACKANDANDAH SHIRE—			Creek Road ..	722 3 0	723 9 9
Kergunyah Road ..	93 3 7		Running Creek Road ..	..
Dederang-Tawanga Road ..	335 2 6		BULN BULN SHIRE—		
Kiewa-Wodonga Road ..	73 14 11		Fumina Road (F.A.R. Exp.) ..	1,270 2 6	
Running Creek Road ..	377 3 6		Duggan North Road ..	2,230 2 2	
Woololonga Gap Road ..	419 0 6		Fumina Road ..	393 1 4	
Myrtleford-Yackandandah	17 11 8		Mountain View Road ..	1,210 11 2	
Road ..	..		Turner's Road ..	371 14 7	5,475 11 9
Sandy Creek Road ..	4 10 0		ELTHAM SHIRE—		
YACKANDANDAH AND BEECH-			Kinglake Road (F.A.R. Exp.)	1,669 10 1	
WORTH SHIRES (Joint			Kinglake-Glenburn Road ..	33 2 3	
Works)—			ELTHAM AND YEA SHIRES (Joint		
Beechworth-Yackandandah	1 15 4	1,338 18 9	Works)—		
Road ..	..		Yarra Glen-Glenburn (F.A.R.	1,377 3 9	3,079 16 1
Myrtleford-Yackandandah	16 16 9		Exp.) ..	..
Road ..	..		HEALESVILLE SHIRE—		
YARRAWONGA AND WANGARATTA			Toolangi-Kinglake Road ..	4 10 0	
SHIRES (Joint Works)—			Healesville-Toolangi Road ..	432 2 1	436 12 1
Peechelba Bridge ..	32 10 0	32 10 0	HEYTESBURY SHIRE—		
YEA SHIRE—			Peterborough-Port Campbell	846 14 5	
Flowerdale Road ..	1,509 12 0		Road (F.A.R. Exp.) ..	..
Highlands Road ..	35 18 10		Timboon-Nirranda (F.A.R.	1,051 5 2	
Killingsworth Road ..	178 19 10		Exp.) ..	..
Molesworth-Dropmore Road ..	70 4 4	1,794 15 0	Cooriejong Road ..	483 16 0	
Total ..	..	335,718 13 8	Eastern Creek Road ..	761 19 9	
			Timboon-Scott's Creek Road ..	160 0 0	
			Timboon-Digney's Bridge Road ..	198 17 6	
			Peterborough-Port Campbell	56 11 7	
			Road ..	..
			Timboon-Cowley's Creek Road ..	1,025 4 6	
			Port Campbell-Princetown	1,791 6 0	6,375 14 11
			Road ..	..
			KORUMBURRA SHIRE—		
			Trida-Strezlecki Road ..	599 7 9	
			KORUMBURRA AND WARRAGUL		
			SHIRES (Joint Works)—		
			Trida-Strezlecki Road ..	15 15 9	615 3 6
			Carried forward ..	..	369,471 15 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Act No. 3662 (3255).		Municipality and Road.	Act No. 3662 (3255).	
	Amount.	Total.		Amount.	Total.
	£ s. d.	£ s. d.		£ s. d.	£ s. d.
Brought forward ..	..	369,471 15 6	Brought forward ..	..	399,779 14 6
KERANG SHIRE—			OTWAY SHIRE—<i>continued.</i>		
Murray River Valley Road (F.A.R. Exp.) ..	187 6 4	187 6 4	Colac-Forrest Road ..	2,269 14 8	
MANSFIELD SHIRE—			Colac-Beech Forest Road ..	3,529 5 0	
Tolmie Road ..	1,899 13 6	1,899 13 6	Dehnert's Road ..	667 15 6	
MIRBOO SHIRE—			Ferguson-Charley's Creek Road ..	304 14 5	
Mount Vernon-Boorool Road (F.A.R. Exp.) ..	620 6 8		Forrest-Apollo Bay Road ..	302 0 0	
Clear Creek Road ..	409 14 5		Gellibrand East Road ..	1,746 0 9	
Mirboo-Yarragon Road ..	5,667 6 1	6,697 7 2	Laver's Hill-Glenaire Road ..	19 15 9	
MORWELL SHIRE—			Hordern Vale Road ..	1,552 12 1	
Morwell River Road (F.A.R. Exp.) ..	1,503 17 10		Lardner's Track Road ..	1,180 1 0	
Morwell River Road, Eastern Branch (F.A.R. Exp.) ..	216 15 7		Laver's Hill - Chapplevale - Devondale Road ..	2,104 7 4	
MORWELL AND TRARALGON SHIRES (Joint Works)—			Princetown-Port Campbell Road ..	520 18 5	
Jeeralang West Road (F.A.R. Exp.) ..	97 1 5		Sunnyside Road ..	150 19 2	
Walker's Road ..	269 2 5	2,086 17 3	Wild Dog Valley Road ..	2 12 6	31,656 8 1
NARRACAN SHIRE—			OXLEY SHIRE—		
Walhalla-Matlock Road (F.A.R. Exp.) ..	1,014 2 3		Rose River Road ..	827 13 4	
Allambee-Childers Road ..	709 6 3		Whitfield-Tolmie Road (F.A.R. Exp.) ..	1,731 19 5	
Allambee-Thorpdale Road ..	1,690 4 3		Whitfield-Tolmie Road ..	472 11 1	3,032 3 10
Sunny Creek Road ..	598 8 10		ROSEDALE SHIRE—		
Yarragon-Leongatha Road ..	438 6 0		Callignee South Road ..	295 10 8	
Yarragon-Mirboo Road ..	7,188 14 7		Merriman's Creek Road ..	407 16 11	703 7 7
NARRACAN, WARRAGUL, AND MORWELL SHIRES (Joint Works)—			SOUTH GIPPSLAND SHIRE—		
Allambee Estate Road (F.A.R. Exp.) ..	389 8 1	12,028 10 3	Binginwarri South Road ..	88 5 11	
OMEIO SHIRE—			McCartin's Road ..	25 15 10	
Beloka Road ..	260 0 0	260 0 0	Toora-Gunyah Road ..	1,954 10 1	
ORBOST SHIRE—			Toora-Wonyip Road ..	2,250 11 0	
Orbost-Buchan Road (F.A.R. Exp.) ..	1,176 3 11		Woomera Creek Road ..	100 16 1	4,419 18 11
Orbost-Delegate Road (F.A.R. Exp.) ..	5,946 8 8		TAMBO SHIRE—		
Combiobar Road ..	16 0 0		Basin Road ..	1,429 1 5	
Errinundra Valley Road ..	0 0 6		Deddick Road ..	2,737 16 9	
Deddick River Road ..	9 10 0		Bonang-Gelantipy Road (F.A.R. Exp.) ..	6 8 7	
Wangarabelle Road ..	0 1 5	7,148 4 6	Buchan-Gelantipy Road ..	1,078 3 0	
OTWAY SHIRE—			TAMBO AND OMEIO SHIRES (Joint Works)—		
Apollo Bay-Wye River Road (F.A.R. Exp.) ..	Bd.803 15 6		Buchan-Ensary Road ..	1 0 0	5,252 9 9
Apollo Bay-Wye River Road (F.A.R. Exp.) ..	Cl.317 14 9		TRARALGON SHIRE—		
Forrest-Apollo Bay Road (F.A.R. Exp.) ..	1,518 8 1		Walker's Road ..	840 3 9	
Hordern Vale-Apollo Bay Road (F.A.R. Exp.) ..	4,063 5 7		Traralgon Creek Road ..	72 5 9	
Laver's Hill-Glenaire Road (F.A.R. Exp.) ..	563 15 4		Callignee Road ..	0 5 4	912 14 10
Laver's Hill-Princetown Road (F.A.R. Exp.) ..	4,367 0 1		WARRAGUL SHIRE—		
Mt. Sabine-Laver's Hill Road (F.A.R. Exp.) ..	4,261 19 1		Mountain View Road ..	408 6 6	
Amiet's Track Road ..	32 13 2		Ferndale-Strezlecki Road ..	66 10 0	
Apollo Bay-Wye River Road ..	287 0 0		Seaview-Korumburra Road ..	55 4 6	530 1 0
Barham Valley Road ..	1,086 11 10		WINCHELSEA SHIRE—		
Birregurra-Forrest Road ..	3 8 1		Cape Patten Road ..	296 18 10	
Carried forward ..	..	399,779 14 6	Wymbooliel Road ..	65 10 0	362 8 10
			WOORAYL SHIRE—		
			Central Road ..	232 4 11	
			Leongatha-Mirboo Road ..	2,231 1 5	
			Leongatha-Yarragon Road ..	2,012 7 5	
			West Tarwin Valley Road ..	1,007 19 10	5,483 13 7
			YACKANDANDAH SHIRE—		
			Running Creek Road ..	157 5 4	157 5 4
			Total ..		452,290 6 3
			Suspense ..		627 8 5
					452,917 14 8

APPENDIX F.

COUNTRY ROADS BOARD.

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS UNDER THE PROVISIONS OF THE COUNTRY ROADS ACT DURING THE YEAR ENDED 30TH JUNE, 1930.

Name of Municipality and Road.	Locality of Work.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES.					
ALBERTON SHIRE— Foster-Yarram Road ..	Between Shire boundary and Gelliondale ..	..	..	2	..
ARAPILES SHIRE— Horsham - Natimuk - Edenhope Road ..	Parish of Tooan	1'87	1'87	1'02	1'02
BASS SHIRE— Main Coast Road	Near San Remo .. .. .	..	..	..	1
	Near Grantville .. .. .	..	'81	'5	'5
Wonthaggi-Loch Road	Near Blackwood .. .. .	..	..	..	'67
	Near Ryanston and Blackwood .. ..	..	1	..	..
Korumburra-Wonthaggi Road ..	Bituminous treatment of floodway ..	..	'2	'2	'2
	Concrete culvert at Powlett River ..	..	'08	..	..
BIRCHIP SHIRE— Beulah - Birchip - Wycheproof Road ..	Two miles east of Birchip .. ..	..	..	1'33	1'33
BLACKBURN AND MITCHAM SHIRE— Main Healesville Road	Between Blackburn and Mitcham	3'51	3'51	..	..
BORUNG SHIRE— Birchip Road	Gravelling 5 miles from Warracknabeal ..	1	1	1	1
Hopetoun Road	Joining metalling 1 mile from Warracknabeal	'13	'13	'13	'13
	Metalling 1 mile north of Brim	1'11	1'11	(Day labour)	(Day labour)
	Metalling 1 mile south of Lah	'95	'95	'95	'95
Minyip Road	Joining metalling 3 miles from Warracknabeal	'4	'4	'4	'4
BUNINYONG SHIRE— Ballarat-Rokewood Road	Double pipe culvert 1 mile south of Napoleons	..	..	(Day labour)	(Day labour)
CHARLTON SHIRE— Donald Road	Gravelling at Lake Wooroonook	'47	'47	'47	'47
CRANBOTERNE SHIRE— Westernport Road	At Heath Hill	'05	'05	'05	'05
DONALD SHIRE— St. Arnaud-Birchip Road	One mile south of Watchem	'94	'94	'94	'94
	Near Buloke	'47	'47	'47	'47
Donald-Charlton Road	At Dixon's Hill	'89	'89	'89	'89
DUNDAS SHIRE— Hamilton-Horsham Road	Forming and gravelling between 4½ and 5, 6½ and 7½, 20 and 22½, 30½ and 30¾ miles from Hamilton, Parishes of North Hamilton, Jerrywarook, Mooralla and Woolpooper	2'62	'47	1'81	1'81
DUNMUNKLE SHIRE— Stawell-Warracknabeal Road ..	Foundation course 4½ miles north of Rupanynp	1'97	1'97	1'97	1'97
	Foundation course 6½ miles north of Rupanynp	1'06	1'06	1'06	1'06
	Foundation course 7½ miles north of Rupanynp	1'2	1'2	1'2	1'2
Horsham-Murtoa Road	Westerly from Murtoa Township	2'88	2'88	2'88	2'88
EAST LODDON SHIRE— Prairie Road	Parish of Talambe	'72	'72	'72	'72
Dingee Road	Parish of Talambe	'65	'65	'65	'65
Mitiamo Road	Parish of Hayanmi	'29	'29	'29	'29
ECHUCA BOROUGH— Echuca-Wyuna Road	Top course gravelling between Allotments 20, 21 and 30 and 32A, 32B, Parish of Echuca North	'66	'66	'66	'66
FERNTREE GULLY SHIRE— Olinda Road	Between Upper Ferntree Gully and Tremont	..	1'16	1'16	1'16
FLINDERS SHIRE— Mornington-Flinders Road	Near King's	'76	'76	'76	'76
	At Avenue Corner	'1	'1	'1	'1
Point Nepean Road	Between Rye and Sorrento	'8	'8	'8	'8
	Bridge over Boneo Drain	'05	'05	'05	'05
Hastings-Flinders Road	Near Flinders	'12	'12	..	..
GLENLYON SHIRE— Malmesbury-Daylesford Road ..	Drummond-Malmesbury section	'17	'17	'17	'17
Balla Road	Reinforced concrete bridge over Stony Creek	'01	'01	'01	'01
GOULBURN SHIRE— Goulburn Valley Road	North of Nagambie, near Adams' Lane ..	1	1	1	1
HEALESVILLE SHIRE— Healesville-Woorl Yallock Road ..	Between Allotment 5A and Dalry P.R., Parish of Gracedale	'4	'4	'23	'23
HEYESBURY SHIRE— Cobden-Port Campbell-Princetown Road	From Scott's Creek Post Office to Scott's Creek Bridge	'41	'41	'41	'41
HUNTLY SHIRE— Elmore-Heathcote Road	In the Township of Elmore	'25	'25	'2	'2
Bendigo-Echuca Road	In the Township of Elmore	'25	'25	'12	'12
	In the Township of Epsom	'25	'25	'25	'25
KARA KARA SHIRE— St. Arnaud-Donald Road	Between Sutherlands and Cope Cope	5'96	5'96	1'81	1'81
Avoca-St. Arnaud Road	Between Stuart Mill and Redbank	4'61	4'61	..	..
	Between Carapooee West and Stuart Mill ..	3'94	3'94	3'1	3'1
KARKAROO SHIRE— Hopetoun-Warracknabeal Road ..	Parish of Galaquill .. .. .	..	..	..	'49
Rainbow - Beulah - Birchip Road	Parish of Kurdgweechee .. .. .	..	..	..	1'67
Hopetoun - Woomelang - Sea Lake Road	North of Woomelang .. .. .	..	..	..	1'55
	Between Allotments 9A and 10, Parish of Nyallo	..	1'06	..	..
KOWREE SHIRE— Edenhope-Goroke Road	Near Karnak	'24	'24	'24	'24
Boorooopki-Francis Road	Near Boorooopki and Neurpur	'83	2'94	2'94	2'94
LAWLOIT SHIRE— Broughton Road	Metalling between Kaniva and Sandsmere	..	'43	'43	'43
Yearinga Road	Metalling between Kaniva and Yearinga ..	..	'86	'86	'86
	Carried forward	43'99	49'55	37'24	40'62

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Road.	Locality of Work.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES— <i>continued.</i>					
	Brought forward	43·99	49·55	37·24	40·62
LEIGH SHIRE— Cressy-Rokewood Road ..	Culvert and approaches, Salt Creek Flat, near Rokewood Railway Station	·15	·15	·15	·15
LOWAN SHIRE— Yanac Road	Between Allotments 20A and 26, 51, Parish of Yanac, and between Allotments 157, 238 and 158, 230, Parish of Tarranginnie	·77	·77	·77	·77
	Lorquon West Road ..	·15	·15	·15	·15
MANSFIELD SHIRE— Mansfield Road	6 miles west of Mansfield	·29	·29	·29	·29
MILDURA SHIRE— Wentworth Road	Mena-road, Main-avenue and Wentworth-road, Merbein to Abbotsford Bridge	8·03	8·03	8·03	8·03
	Irymple Road	·87	·87	·87	·87
MIRBOO SHIRE— Mardan Road	Gingam Avenue from 15th Street to railway..	·9	·9	·9	·9
	Metalling through Allotments 31 and 34, Parish of Mardan	·9	·9	·9	·9
MORWELL SHIRE— Boolarra-Welshpool Road ..	From Boolarra-Foster Road to Morwell River Bridge in Parish of Mirboo, near Boolarra	..	..	·53	·53
	Jeeralang West Road ..	1·01	1·01	1·01	1·01
	Sanding deviation through Ellis's and others..	·23	·23	·23	·23
	Sanding at South Hazelwood	·23	·23	·23	·23
NARRACAN SHIRE— Walhalla Road	Construction of bridge over the Thomson River	·07	·07	·07	·07
NUMURKAH SHIRE— Nathalia-Kyabram Road ..	Gravelling north from Racecourse	1·24	1·24	1·24	1·24
	Gravelling from Kotupna School to bridge ..	2·19	2·19	2·19	2·19
	Gravelling north from Ball's Weir	1·5	1·5	1·5	1·5
	Echuca-Picola Road	2·38	2·12	2·12	2·12
	Clearing from Boal's to Stewart's Bridges ..	2	2	2	..
	Forming from Madowla Park to Gallaway's..	2	2	2	..
OMEO SHIRE— Benambra Road	North of Omeo-Smoky Gully section	·57	·57	·57	·57
ORBOST SHIRE— Marlo Road	Gravelling from Western corner of Allotment 15, Parish of Orbost East, southerly towards Marlo	2·79	2·19	·75	·75
	Cann Valley Road	1·7	3·45	3·45	3·45
	Wangarabell Road	·23	·23	·07	·07
	Re-alignment and reconstruction of bridges at Big Flat and Wangarabell Creeks	·23	·23	·07	·07
OXLEY SHIRE— Bright Road	Rocky Point section	·5	·5	·5	..
	Sections at Smyth's and Dalton's	·5	·5	·26	·26
PORTLAND SHIRE— Portland-Casterton Road ..	3 sections between Drumborg and Hotspur ..	·3	·3	·3	·3
	Heath Road	·63	·63	·63	·63
	2 sections near Portland Freezing Works ..	·63	·63	·63	·63
ROCHESTER SHIRE— Rochester - Bamawm - Prairie Road	Between Allotments 176 and 177, Parish of Bamawm	1	1	1	1
RODNEY SHIRE— Tatura - Byneside - Kyabram Road	Metalling west of Byneside .. ..	..	..	1·98	1·98
	Tatura-Murchison Road	·94	·94	·26	·26
SHEPPARTON SHIRE— Shepparton-Numurkah Road	North of Tallygaroopna	1·23	1·23	1·23	1·23
SOUTH GIPPSLAND SHIRE— Foster-Yarram Road	Agnes River towards Welshpool	2·22	2·22	2·22	2·22
	Toora-Gunyah Road	·2	·2	·2	·2
	Main South Gippsland Road	1·69	1·69	1·69	1·69
STAWELL SHIRE— Navarre Road	Sheeting at Stony Creek	1·69	1·69	1·69	1·69
	Marnoo Road	2·24	2·24	2·24	2·24
	Landsborough Road	1·11	1·11	1·11	1·11
	Stawell - Glenorchy - Horsham Road	·43	·43	·43	·43
	.. ..	·8	·8	·8	·8
STRATHFIELDSAYE SHIRE— Strathfieldsaye Road	Pieper's Hill .. ..	..	·15	·15	·15
	Adjoining Bendigo City .. ..	..	·41	·41	·41
	In Strathfieldsaye Township .. ..	..	·52	·52	·52
	Near Bull's Head-Grassy Flat .. ..	..	·07	·07	·07
	Opposite Railway Station, Axedale .. ..	..	·21	·21	·21
	Extending westerly from Junntown State School	..	·61	·61	·61
SWAN HILL SHIRE— Murray Valley Road	4 miles north of Piaugli .. ..	..	..	..	4·56
	Nyah-Onyen Road .. ..	..	..	..	1
	Easton Road .. ..	..	1·5	..	..
	Murray Valley Road .. ..	..	2·21	..	..
TOWONG SHIRE— Murray Valley Road	Bethanga Bridge to Thologolong (Hume Weir deviations)	23·96	5·55	5·55	5·55
TUNGAMAH SHIRE— Numurkah-Tungamah-Wilby Road	Allotments 20, 18, 3, 4, 17, Parish of Pelluebla..	1·01	1·01	1·01	1·01
	Cobram-Strathmerton road ..	·98	·98	·72	·72
	Yarrowonga-Cobram Road ..	·34	·34	·34	·34
	Allotments 29A and 29 and 70B and 26, Parish of Cobram	·57	·57	·57	·57
	Township of Katamatite	·57	·57	·57	·57
UPPER MURRAY SHIRE— Tintalra Road	Forming, grading and gravelling west of Allotments 4A, 4B, 4C, 6A, 6B, Section XI, Parish of Cudgewa	·94	·94	·94	·94
WALFRUP SHIRE— Ouyen-Pinnaroo Road	Metalling near Underbool .. ..	..	..	..	·25
	Between Walpen and Underbool .. ..	..	..	..	·35
	Resheeting near Ouyen	1·17	1·17	1·17	1·17
	Metalling between Cowangie and Murrayville	·73	·73	·73	·73
	Metalling in Tutye Township	·37	·37	·37	·37
	Metalling, Allotments 23-27, Parish of Ouyen..	·61	·61	..	..
	Metalling, Allotments 13-14, Parish of Tiega ..	·51	·51	..	..
	Metalling, Allotments 5-7, Parish of Tiega ..	·34	·34	..	..
	Forming, Allotments 2-29, Parish of Nyang ..	·36	·36	..	..
	Forming, Allotments 11-11A, Parish of Walpen, and 2-6, Parish of Tiega	·64	·64	..	..
	Carried forward	117·38	111·1	92·35	99·39

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Road.	Locality of Work.	Lengths of Roads.			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>					
	Brought forward	117·38	111·1	92·35	99·39
WANGARATTA SHIRE— Yarrowonga Road	Forming and gravelling adjoining Allotments 38C and 37A, Parish of Killawarra	..	..	..	·81
WANNON SHIRE— Hamilton—Coleraine—Caster- ton Road	Between 3 and 4 mile pegs	·82	·82	·82	·82
Wannon Bridge Road	Several short sections	1·01	1·01	1·01	1·01
Coleraine—Harrow—Apsley Road	J. McDonald's to McVeas, between 9 and 10 mile pegs	·8	·8	·8	·8
WARANGA SHIRE— Heathcote—Elmore Road	Between Heathcote and Toolleen .. .. .	..	..	·5	·5
.. .. .	Between Toolleen and Myola	·35	·35	..	..
Murchison—Rushworth Road	Between Rushworth and Moora .. .. .	..	..	·37	·37
WARRAGUL SHIRE— Princes Highway	Opposite Allotments 17 and 18, Section XI, Town of Warragul	·08	·08	·08	·08
Bloomfield Road	Opposite Allotments 41 and 41A, Parish of Drouin East	·36	·36	·36	·36
WARRNAMBOOL SHIRE— Framlingham Road	2½ miles from Panmure	·61	·61	·61	·61
Peterborough Road	7 miles from Peterborough	·63	·63	·63	·63
.. .. .	Near Peterborough	4	4	..	..
WIMMERA SHIRE— Horsham—Hamilton Road (Jt. with Horsham Borough and Arapiles Shire)	Construction at Borough boundary	·18	·18	·18	·18
Natimuk Road	Construction from between Allotments 241/243 to between Allotments 240 and 242, Parish of Vectis East	1·17	1·17	·93	·93
.. .. .	Constructing 2 pipe culverts .. .. .	..	..	..	..
Horsham—Wal Wal Road	Construction between Allotments 4 and 30 and 6 and 35, Parish of Drung	1·04	1·04	..	..
.. .. .	Constructing two pipe culverts .. .. .	..	..	..	..
WINCHELSEA SHIRE— Birregurra—Forrest Road	In Parishes of Murroon and Whoorel, chainages 20800—22000 and 24600—28000	·87	·87	·87	·87
Lorne Road	In Parish of Bamba, chainage 17500—20500 .. .. .	..	·57	·57	·57
WOORAYL SHIRE— Leongatha—Yarragon Road	Through the "Tangle", near junction with Leongatha—Mirboo Road	·66	·66	·66	·66
WYCHEPROOF SHIRE— Birchip—Wycheproof Road	West of Wycheproof	·55	·55	·55	·55
YACKANDANDAH SHIRE— Gundowring Road	Earthworks and gravelling, Parish of Guudow- ring	2	2	..	..
Dederang Road	Culvert and approaches near Briggs'	·08	·08	·08	·08
Kiewa—Wodonga Road	Earthworks and gravelling deviation near Kiewa	·21	·21	·21	·21
YARRAWONGA SHIRE— Wangaratta—Yarrowonga Road	Forming and metalling 3 miles from Town of Yarrowonga	1·92	1·92	1·92	1·92
Yarrowonga—Rutherglen Road	Forming and gravelling at Bundalong South	1·14	1·14	..	..
.. .. .	Forming and gravelling	·88	·88	·88	·88
YEA SHIRE— Yea—Glenburn Road	Near Devlin's Bridge	·55	·55	·55	·55
Upper Goulburn Road	Concrete bridge and approaches over Yea River	·12	·12	·12	·12
	Total	137·41	131·7	105·05	112·9
UNDER DIRECT SUPERVISION OF THE BOARD.					
BERWICK AND FERNTREE GULLY SHIRES— Emerald Road	Reforming and grading near Avonsleigh House	1·83	1·83	1·83	1·83
Woori Yallock—Pakenham—Koo- wee-rup Road	Reforming and sanding	2·5	2·5	2·5	2·5
BET BET SHIRE— Avoca—Bealiba Road	Construction of R.C. bridge near Archdale .. .. .	..	·01	·01	·01
CASTLEMAINE BOROUGH Melbourne—Bendigo Road	Surfacing with bitumen	1·25	1·25	(Day labour)	(Day labour)
CHARLTON SHIRE— St. Arnaud Road	Forming, grading, and gravelling from Coonoer Township northerly	4·73	4·73	..	..
CRANBOURNE SHIRE— Main Coast Road	Construction of two-span timber bridge, 50 feet long, over Yallock Creek .. .. .	..	·01	·01	·01
ELTHAM SHIRE— Yarra Glen—Glenburn Road	Forming and gravelling	1·26	1·26	1·26	1·26
GORDON SHIRE— Serpentine—South Kerang Road	Reforming and gravelling north of Durham Ox	3·97	3·97	..	..
HEYTESBURY SHIRE— Cobden—Port Campbell—Prince- town Road	Clearing, forming, and metalling	3·5	2·22	2·22	2·22
Timboon—Port Campbell Road	Bituminous spraying	4	4	(Day labour)	(Day labour)
LILYDALE SHIRE— Monbulk Road	Widening and reforming	·31	·31	(Day labour)	(Day labour)
MANSFIELD SHIRE— Mansfield—Wood's Point Road	Construction of timber bridge over Goulburn River in Township of Wood's Point .. .. .	..	·01	·01	·01
MARYBOROUGH BOROUGH— Castlemaine—Maryborough Road	Reforming, widening, and resheeting from Maryborough railway crossing to east boundary of Maryborough Borough	1·63	1·33	(Day labour)	(Day labour)
NEWHAM AND WOODEND SHIRES— Melbourne—Bendigo Road	Sealing with Trinidad bitumen in the Township of Woodend	1	1	(Day labour)	(Day labour)
ORBOST SHIRE— Orbost—Delegate Road	Grubbing, clearing, and forming, and three timber bridges	3·4	3·4	3·4	3·4
OTWAY SHIRE— Colac—Beech Forest Road	Reforming and gravelling	2·75	2·75	2·75	2·75
PYALONG SHIRE— Kilmore—Heathcote—Bendigo Road	Construction of R.C. bridge over Mollison's Creek .. .. .	..	·01	·01	·01
SHEPPARTON AND BENALLA SHIRES— Shepparton—Nalinga Road (Benalla—Shepparton Road)	Forming and gravelling	8·5	8·5	8·5	8·5
SWAN HILL SHIRE— Piangil Station Road	Reforming, gravelling, and metalling .. .. .	..	1·47	1·47	1·47
TULLAROOP SHIRE— Castlemaine—Maryborough Road	Mixed-in-place bituminous gravelling at Caris- brook	11·5	10·5	(Day labour)	(Day labour)
UPPER YARRA SHIRE— Wood's Point Road	Construction of timber bridge over McMahon's Creek .. .. .	..	·01	·01	·01
WOORAYL SHIRE— Wild Dog Valley Road	Grubbing, clearing, forming, and draining	3·26	3·26	(Day labour)	(Day labour)
	Total	55·39	54·33	23·98	23·98

APPENDIX G.

COUNTRY ROADS BOARD.

PARTICULARS OF SURVEYS, ETC., ON HIGHWAYS DECLARED STATE HIGHWAYS UNDER THE PROVISIONS OF SECTION 38 OF THE COUNTRY ROADS ACT 1928, DURING THE YEAR ENDED 30th JUNE, 1930.

Name of Municipality and Highway.	Locality of Works.	Lengths of Highways—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES.					
BACCHUS MARSH SHIRE— Western Highway—Section 1 ..	Anthony's Cutting	·66	·66	·66	·66
" " " " ..	Bacchus Marsh	·87	·87	·87	·87
BULN BULN SHIRE— Princes Highway East—Section 1 ..	Opposite Allotment 47, Parish of Drouin East ..	·99	·99	·99	·99
KORONG SHIRE— Calder Highway—Section 3 ..	Bituminous gravelling between Wedderburn and Glenalbyn ..	3·3	3·3	3·3 (Day labour)	3·3 (Day labour)
LAWLOTT SHIRE— Western Highway—Section 5 ..	Gravelling between Kaniva and Lawloit ..	..	3·08	3·08	3·08
LOWAN SHIRE— Western Highway—Section 5 ..	Between Allotments 70-72 and 71-76, Parish of Kinimakatka ..	1·13	1·13	1·13	1·13
" " " " ..	Between Allotments 72 and 76-74 and B, Parish of Kinimakatka ..	·96	·96	..	..
MILDURA SHIRE— Calder Highway—Section 6 ..	Irymple to Red Cliffs	3·11	3·11	3·11	3·11
" " " " ..	Red Cliffs to Yatpool boundary	1·9	1·9	·98	·98
" " " " ..	From turn-off at Hattah deviation, 38-mile peg towards Hattah ..	·98	·98	·98	·98
" " " " ..	At 43-mile peg on Hattah deviation	·7	·7	·7	·7
OMEQ SHIRE— Omeo Highway—Section 1 ..	Between " Double Bridges " and " St. Patricks " ..	·5	·5	..	..
" " " " ..	Between " Piano Box " and " Jew's Pinch " (Wild Dog) ..	·4	·4	..	..
" " " " Section 2 ..	South of Tambo Crossing (" Bark Sheds ") ..	1·2	1·2	..	..
" " " " Section 3 ..	North of Tambo Crossing (Tucker Box) ..	·4	·4	..	..
" " " " ..	North of Omeo (Wilson's Creek section) ..	·4	·4	..	..
STAWELL SHIRE— Western Highway—Section 3 ..	Armstrongs to Great Western	5·55	5·55	5·55	5·55
" " " " ..	Stawell to Great Western	3·95	3·95	..	..
TOWONG SHIRE— Omeo Highway—Section 3 ..	Forming at Granite Flat	3·06	3·06	3·06	3·06
" " " " ..	Sanding and gravelling at Granite Flat ..	3·48	3·48	3·48	3·48
" " " " ..	Forming and gravelling Larsen's Cutting ..	1·16	1·16	1·16	1·16
" " " " Section 4 ..	Forming and gravelling curves north of Eskdale ..	·37	·37	·37	·37
" " " " ..	Forming and gravelling Swann's Cutting ..	·68	·68	·68	·68
" " " " ..	Forming and gravelling at Tallandoon Store ..	·87	·87	·87	·87
" " " " ..	Bolga to Wodonga Shire boundary (Hume Weir deviations) ..	9·02	9·02	..	..
WALPEUP SHIRE— Calder Highway—Section 5 ..	Various sections in Parishes of Boulka and Woornack ..	..	..	·53	·53
" " " " Section 6 ..	Near Mittyack	·13	·13	·13	·13
" " " " ..	Kiamal to Trinita .. .. .	..	1·8	1·8	1·8
" " " " ..	Near Trinita	·51	·51	·51	·51
WARRAGUL SHIRE— Princes Highway East—Section 1 ..	Westerly from west Town boundary ..	·3	·3	·3	·3
WODONGA SHIRE— Omeo Highway—Section 4 ..	Bonegilla School to Yackandandah Shire boundary (Hume Weir deviations) ..	3·93	3·93	2·64	..
WYCHEPROOF SHIRE— Calder Highway—Section 4 ..	Between Wycheproof and Sea Lake	15·08	15·08	15·08	15·08
" " " " Section 5 ..	Between Sea Lake and Mittyack	5·7	5·7	5·7	5·7
	Total	71·29	76·17	57·66	55·02
UNDER DIRECT SUPERVISION OF THE BOARD.					
AVON SHIRE— Prince's Highway East—Section 3 ..	Reforming, widening, and gravelling between Montgomery and Stratford ..	1·77	1·77	1·77	1·77
" " " " " " ..	Reforming, regrading, and gravelling between Stratford and Providence Ponds ..	4·32	4·32	4·32	4·32
AVON AND BAIRNSDALE— Prince's Highway East—Section 3 ..	Forming and gravelling between Providence Ponds and Bairnsdale ..	13·85	13·85	13·85	13·85
BACCHUS MARSH SHIRE— Western Highway—Section 1 ..	Widening, reshaping with bituminous macadam and sealing from Deep Creek westerly ..	·71	·71	(Day labour)	(Day labour)
" " " " " " ..	Widening and reconstruction of pavement and formation ..	·07	·07	(Day labour)	(Day labour)
BAIRNSDALE SHIRE— Prince's Highway East—Section 4 ..	Construction of R.C. bridge with approaches near Bairnsdale ..	..	·01	·01	·01
" " " " " " ..	Widening east of Mitchell bridge	·25	·25	..	..
BALLAN SHIRE— Western Highway—Section 1 ..	R.C. bridge over Pyke's Creek .. .. .	..	·01	·01	·01
BALLARAT SHIRE— Western Highway—Section 2 ..	Forming and grading at Burrumbeet Creek ..	·14	·14	·14	·14
" " " " " " ..	Resheeting, widening, and surfacing with bituminous macadam between Ballarat and Burrumbeet ..	4·05	4·05	4·05	4·05
	Carried forward	25·16	25·18	24·15	4·15

PARTICULARS OF SURVEYS, ETC., ON HIGHWAYS DECLARED STATE HIGHWAYS, ETC.—*continued.*

Name of Municipality and Name of Highway.	Locality of Works.	Lengths of Highways—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD—<i>continued.</i>					
BALLARAT, RIPON, AND LEXTON SHIRES— Western Highway—Section 2 ..	Brought forward	25·16	25·18	24·15	24·15
" " " " ..	Reforming, widening, and surfacing with bituminous macadam from Burrumbeet Park towards Middle Creek	23·35	11	11	11
" " " " ..	Reforming, widening, and surfacing with bituminous macadam from Burrumbeet Park towards Middle Creek	..	11	(Day labour)	(Day labour)
BELFAST SHIRE— Prince's Highway West—Section 4 ..	Widening, reforming, and gravelling over Eumeralla Flats	4·21	4·21	4·21	4·21
" " " " ..	Construction of R.C. culverts and lengthening existing culverts between Port Fairy and Portland	..	·01	·01	·01
BENALLA SHIRE— Hume Highway—Section 2 ..	Construction of R.C. culvert with approaches 2 miles north of Glenrowan	..	·01	·01	·01
" " " " ..	Construction of two-cell culvert near Baddaginnie	..	·01	(Day labour)	(Day labour)
BERWICK SHIRE— Prince's Highway East—Section 1 ..	Resealing with bitumen between Narre Warren and Hallam	1·43	1·43	(Day labour)	(Day labour)
" " " " ..	Construction of single-span bridge over Narre Warren Creek	..	..	..	..
" " " " ..	Forming, grading, and surfacing with bituminous macadam between Nar-nar-noon and Tynong	..	1·96	1·96	1·96
" " " " ..	Reforming, grading, and sanding between Bunyip River and Tynong	..	6·94	6·94	6·94
" " " " ..	Construction of two R.C. culverts between Hallam and Narre Warren	..	..	..	..
" " " " ..	Widening and surfacing with asphaltic macadam	·44	·44	·44	·44
BERWICK AND BULN BULN SHIRES— Prince's Highway East—Section 1 ..	Construction of timber bridge over Bunyip River	..	·01	·01	·01
BUNGAREE SHIRE— Western Highway—Section 1 ..	Reconstruction of pavement formation ..	·18	·18	(Day labour)	(Day labour)
CHARLTON SHIRE— Calder Highway—Section 3 ..	Forming, gravelling, &c. .. ..	..	..	..	..
COLAC SHIRE— Prince's Highway West—Section 2 ..	Record survey	19	..	..	..
CORIO SHIRE— Prince's Highway West—Section 1 ..	Sub-grade drainage between Bacchns Marsh Road and Separation Street Bridge	3·36	3·36	3·36	3·36
CORIO AND WERRIBEE SHIRES— Prince's Highway West—Section 1 ..	Lengthening and widening about 40 minor structures to 40 feet between kerbs	..	..	..	..
" " " " ..	Construction of two R.C. culverts .. ..	..	·01	·01	·01
DIMBOOLA SHIRE— Western Highway—Section 4 ..	Waterproofing and gravel surfacing of clay formation between Wail and Dimboola	·48	·48	(Day labour)	(Day labour)
DIMBOOLA AND WIMMERA SHIRES— Western Highway—Section 4 ..	Forming, grading, and gravelling between Wail and Dimboola	5·35	5·35	5·35	5·35
" " " " ..	Surfacing clay formation with bitumen between Pimpinio and Dimboola	7·51	7·51	(Day labour)	(Day labour)
HAMPDEN SHIRE— Prince's Highway West—Section 3 ..	Reforming, widening, and surfacing with bituminous macadam between Camperdown and Terang	4	4	4	4
" " " " Sections 2 and 3 ..	Record survey	23	..	..	..
HEYTESBURY SHIRE— Prince's Highway West—Section 2 ..	Widening, reforming, and surfacing through Stony Rises	3·1	3·1	3·1	3·1
" " " " ..	Record survey	8	..	..	..
HEYTESBURY AND HAMPDEN SHIRES— Prince's Highway West—Section 2 ..	Widening, reforming, and surfacing with bituminous macadam between Stoneyford and Camperdown	14·26	14·26	(Day labour)	(Day labour)
HUNTLY SHIRE— Northern Highway—Section 1 ..	Forming, trimming, and gravelling between Goornong and Elmore	5·47	5·47	5·47	5·47
" " " " ..	Forming, grading, and gravelling at Bagshot railway crossing	..	·26	·26	·26
" " " " ..	Reforming, widening, and resheeting from 1 mile north of Epsom to south boundary of Huntly Township	1·45	1·45	..	..
HUNTLY AND ROCHESTER SHIRES— Northern Highway—Section 1 ..	Forming, trimming, and gravelling between Elmore and Rochester	6·55	6·55	6·55	6·55
KILMORE SHIRE— Hume Highway—Section 1 ..	Superelevating of the curve south of railway crossing at Kilmore	·05	·05	(Day labour)	(Day labour)
" " " " ..	Reforming, resheeting, and surfacing with asphaltic macadam through Township of Kilmore	1·55	1·55	1·55	1·55
KILMORE, BROADMEADOWS, AND BROADFORD SHIRES— Hume Highway—Section 1 ..	Spraying between Campbellfield and Bylands ..	17·25	17·25	(Day labour)	(Day labour)
" " " " ..	Spraying between Kilmore and Broadford ..	7·35	7·35	(Day labour)	(Day labour)
MALDON AND METCALFE SHIRES— Calder Highway—Section 2 ..	Construction of R.C. box culvert	·01	·01	(Day labour)	(Day labour)
MARONG SHIRE— Calder Highway—Section 2 ..	Reforming, widening, and gravelling ..	14·91	14·91	(Day labour)	(Day labour)
METCALFE SHIRE— Calder Highway—Section 2 ..	Forming, resheeting, penetrating, and sealing with bitumen, Castlemaine to Harcourt	3·72	3·72	(Day labour)	(Day labour)
MORWELL SHIRE— Prince's Highway East—Section 2 ..	Clearing, forming, and sanding near Morwell ..	1·55	1·55	1·55	1·55
" " " " ..	Reforming and resheeting with granitic sand between Yallourn turnoff and Traralgon boundary	6·8	6·8	6·8	6·8
" " " " ..	Construction of R.C. bridge over Waterholes Creek	..	·01	·01	·01
" " " " ..	Reforming, widening, and reshaping with granitic sand and resheeting with gravel near Morwell	1·83	1·83	1·83	1·83
MORWELL AND NARRACAN SHIRES— Prince's Highway East—Section 2 ..	Forming and grading through Haunted Hills ..	1·23	1·23	1·23	1·23
NARRACAN SHIRE— Prince's Highway East—Section 2 ..	Widening, forming, surfacing with bituminous macadam between Trafalgar and Moe	2·91	2·91	2·91	2·91
" " " " ..	Construction of two R.C. culverts near Township of Yarragon	..	..	..	..
	Carried forward	215·36	173·35	103·71	103·71

PARTICULARS OF SURVEYS, ETC., ON HIGHWAYS DECLARED STATE HIGHWAYS, ETC.—*continued.*

Name of Municipality and Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>					
	Brought forward	215·36	173·35	103·71	103·71
OMEO SHIRE— Omeo Highway—Section 2 ..	Bridge and approaches at Haunted Stream ..	·28	·28	·28	·28
ORBOST SHIRE— Prince's Highway East—Section 4 ..	Clearing, forming, and gravelling at Johnson's Gully	·55	·55	·55	·55
" " " " Section 5 ..	Re-alignment and widening between the Brodribb and the Cann rivers	7·55	7·55	(Day labour)	(Day labour)
" " " " Section 6 ..	Re-alignment and widening between the Cann and the Wingan rivers	8·76	8·35	(Day labour)	(Day labour)
PORTLAND SHIRE— Prince's Highway West—Section 5 ..	Forming and gravelling between Heywood and Dartmoor	·44	·44	·44	·44
" " " " " " ..	Clearing, forming, and limestone metalling between Dartmoor and South Australian Border	4·47	4·47	4·47	4·47
" " " " " " ..	Clearing, forming, and grading	4·27	4·27	4·27	4·27
RIPON AND LEXTON SHIRES— Western Highway—Section 2 ..	Widening, reshaping, and surfacing with gravel between Burrumbeet and Trawalla	2·94	2·94	2·94	2·94
ROSEDALE AND TRARALGON SHIRES— Prince's Highway East—Section 2 ..	Construction of timber bridge and approaches over Flynn's Creek	·14	·14	·14	·14
SEYMOUR SHIRE— Hume Highway—Section 1 ..	Bituminous treatment westerly from bridge over Goulburn River	·23	·23	(Day labour)	(Day labour)
" " " " " " ..	Forming, reforming, grading and gravelling, surfacing with asphaltic macadam between Tallarook level crossing and Goulburn River Bridge	..	5	(Day labour)	(Day labour)
" " " " " " Section 2 ..	Construction of R.C. bridge over Eight-mile Creek	..	·01	·01	·01
" " " " " " ..	Forming, grading, and gravelling	1·03	1·03	1·03	1·03
" " " " " " ..	Construction of three R.C. culverts and gravelled approaches between Seymour and Mangalore	..	·01	·01	·01
" " " " " " ..	Forming, grading, and gravelling	·86	·86	·86	·86
" " " " " " ..	Construction of two R.C. box culverts with gravelled approaches near Mangalore	..	·01	·01	·01
" " " " " " ..	Construction of three R.C. box culverts with gravelled approaches between Mangalore and Avenel	..	·01	·01	·01
" " " " " " ..	Forming, grading, and gravelling	1·42	1·42	1·42	1·42
SEYMOUR, EUROA, AND VIOLET TOWN— Hume Highway—Section 2 ..	Forming and gravelling in Townships of Avenel, Euroa, and Violet Town	3·09	3·09	3·09	3·09
STAWELL SHIRE— Western Highway—Section 3 ..	Construction of culvert 1 mile Horsham side of Deep Lead	..	..	..	..
TAMBO SHIRE— Prince's Highway East—Section 4 ..	Grubbing, forming, and gravelling	·39	·39	·39	·39
" " " " " " " " ..	Re-alignment at Salt Creek and Merrangbanr ..	·88	·88	·88	·88
Omeo Highway—Section 1 ..	Forming and gravelling and bridge at Ramrod Creek	·1	·1	·1	·1
TRARALGON SHIRE— Prince's Highway East—Section 2 ..	Reforming and gravelling east of Traralgon ..	1·54	1·54	1·54	1·54
" " " " " " " " ..	Sealing with bitumen and gravel	1·55	1·55	(Day labour)	(Day labour)
WANGARATTA SHIRE— Hume Highway—Section 3 ..	Grubbing and forming at Springhurst .. ..	..	·31	·31	·31
" " " " " " " " ..	Trimming, shouldering, and reshecting with gravel	1·89	1·89	1·89	1·89
" " " " " " " " ..	Clearing and forming	1·52	1·52	1·52	1·52
WARRNAMBOOL SHIRE— Prince's Highway West—Section 3 ..	Record survey	19	..	..	..
WERRIBEE SHIRE— Prince's Highway West—Section 1 ..	Reshecting between concrete shoulders east of Werribee	1·71	1·71	(Day labour)	(Day labour)
" " " " " " " " ..	Construction, bituminous concrete, &c., Guiding Star deviation	·56	·56	·56	·56
" " " " " " " " ..	Construction of concrete shoulders and reforming near Werribee	..	1·71	1·71	1·71
" " " " " " " " ..	Widening from 16 to 20 feet in bituminous macadam and laying concrete kerbs between Skeleton Creek and railway	..	1·28	(Day labour)	(Day labour)
" " " " " " " " ..	Reconstruction of edges between 17- and 18-mile posts	1	1	(Day labour)	(Day labour)
WERRIBEE AND CORIO SHIRES— Prince's Highway West—Section 1 ..	Widening and resurfacing with asphaltic macadam and concrete kerbs between Little River turnoff and Little River bridge	1·05	1·05	(Day labour)	(Day labour)
WIMMERA SHIRE— Western Highway—Section 4 ..	Surfacing sand clay with bitumen between Horsham and Pimpinio	6	6	(Day labour)	(Day labour)
" " " " " " " " ..	Reconstruction between Horsham and Pimpinio	1	1	(Day labour)	(Day labour)
" " " " " " " " ..	Forming and grading between Pimpinio and Wall	..	4·95	4·95	4·95
" " " " " " " " ..	Construction of three timber bridges over Mount William Creek and floodways near Dadswell's	..	·01	·01	·01
" " " " " " " " ..	Trimming and gravelling near Dadswell's bridge	·25	·25	·25	·25
" " " " " " " " ..	Forming, reforming, and gravelling between Dadswell's and Horsham	6·85	6·85	6·85	6·85
" " " " " " " " ..	Clearing, forming and draining (Dadswell's bridge section)	..	·77	·77	·77
WINCHELSA SHIRE— Prince's Highway West—Section 1 ..	Widening and reshaping and sheeting	2·4	2·4	2·4	2·4
" " " " " " " " Section 2 ..	Rescaling west of Winchelsea	6·25	6·25	(Day labour)	(Day labour)
" " " " " " " " " " ..	Record survey	11	..	..	..
WODONGA SHIRE— Hume Highway—Section 3 ..	Survey	3	..	..	..
WYCHEPROOF SHIRE— Calder Highway—Section 3 ..	Clearing, forming, and gravelling	6·15	6·15	6·15	6·15
	Total	325·48	264·13	152·64	152·64

APPENDIX H.
COUNTRY ROADS BOARD.

MAIN ROADS.

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED AND ROADS MAINTAINED DURING THE YEAR ENDED 30TH JUNE, 1930.

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES.				
ALBERTON SHIRE—				
Balook-Traralgon Road	General maintenance, J. McKenzie's to Balook	..	3·7
Carrajung-Gormandale Road	Metal sheeting at Carrajung .. .. .	..	9
" " " "	General maintenance, Yarram to Won Wron and Carrajung to Gormandale .. .. .	..	17·5
Foster-Yarram Road	General maintenance, Shire boundary to Alberton .. .. .	..	8
Sale-Yarram Road	Gravel sheeting near Martingale's .. .. .	..	4
Yarram-Boolarra Road	General maintenance, Yarram to Monkey Creek .. .. .	..	27·6
Yarram-Port Albert Road	Metal sheeting and tar sealing from Mason's Corner to Waverley Corner .. .. .	..	2·8
Yarram-Won Wron Road	General maintenance, Yarram to Madalya .. .. .	..	17
" " " "	Metal sheeting and tar sealing in Alberton Township .. .. .	..	1
" " " "	General maintenance, Yarram to Port Albert .. .. .	..	8·9
" " " "	Metal sheeting, near Yarram Cemetery .. .. .	..	25
" " " "	General maintenance, Cemetery to junction with Carrajung-Gormandale Road .. .. .	..	4
ALEXANDRA SHIRE—				
Cathkin-Mansfield Road	General maintenance .. .. .	..	15
Healesville-Alexandra Road	Reinforced concrete box culvert at Alexandra Township Boundary .. .. .	..	1·25
Upper Goulburn Road	General maintenance .. .. .	..	16·75
" " " "	Scarifying, rolling, and sheeting, with granitic sand .. .. .	..	11
" " " "	General maintenance .. .. .	..	14
Yarek Road	General maintenance .. .. .	..	3·75
ARAPILES SHIRE—				
Horsham-Hamilton Road	At Cherrypool .. .. .	..	·06
ARARAT BOROUGH—				
Ballarat-Stawell Road	Colas sealing .. .. .	..	3
ARARAT SHIRE—				
Ararat-Elmhurst Road	General maintenance .. .. .	..	23
Ararat-Warrnambool Road	Bitumen surfacing between Ararat and Maroona .. .. .	..	9·3
Ballarat-Hamilton Road	General maintenance .. .. .	..	35
Maroona-Glen Thompson Road	Bitumen surfacing near Streatham .. .. .	..	4·8
" " " "	General maintenance .. .. .	..	23
" " " "	Bitumen Surfacing between Kiora Lake and Glenthompson .. .. .	..	6·5
" " " "	General maintenance .. .. .	..	24
AYOCA SHIRE—				
Ararat Road	Patrol maintenance .. .. .	..	8
Ballarat-St. Arnaud Road	Patrol maintenance .. .. .	..	25
Bealiba Road	Patrol maintenance .. .. .	..	10
Landsborough Road	Patrol maintenance .. .. .	..	2
Maryborough Road	Patrol maintenance .. .. .	..	4
AVON SHIRE—				
Dargo Road	General maintenance .. .. .	..	31
Princes Highway	Reforming and gravelling and bitumen surfacing .. .. .	..	1·35
Maffra-Sale Road	Reconditioning gravel surface and formation and laying bitumen .. .. .	..	·76
BACCHUS MARSH SHIRE—				
Ballarat Road	Resheeting, spraying and patrol maintenance .. .. .	..	1·21
Geelong-Bacchus Marsh Road	Resheeting with gravel, spraying and patrol maintenance .. .. .	..	7·8
Gisborne Road	Resheeting with gravel, spraying and patrol maintenance .. .. .	..	9·9
BAIRNSDALE SHIRE—				
Bullumwaal-Tabberabbera Road	Sealing reconstruction, McCulloch Street .. .. .	..	·2
Princes Highway	Sheeting and bitumen sealing .. .. .	..	·5
BALLAN SHIRE—				
Daylesford Road	Bitumen surfacing in sections .. .. .	..	3·23
" " " "	Resheeting .. .. .	..	·37
" " " "	General maintenance .. .. .	..	13
Gordon-Meredith "A" Road	Resheeting .. .. .	..	·37
" " " "	Scarifying .. .. .	..	·25
" " " "	General maintenance .. .. .	..	4
Gordon-Meredith "B" Road	Gravelling .. .. .	..	4·38
" " " "	General maintenance .. .. .	..	1·5
Mount Wallace Road	Resheeting .. .. .	..	·88
" " " "	General maintenance .. .. .	..	11
BALLARAT SHIRE—				
Ballarat-Lexton Road	Reconstruction in semi-penetration macadam .. .. .	..	1·2
Maryborough-Ballarat Road	Surfacing with tar and bitumen .. .. .	..	3·25
BANNOCKBURN SHIRE—				
Geelong-Ballarat Road	Reconstruction in semi-penetration macadam .. .. .	..	1·8
Gordon-Meredith Road	Gravel sheeting between Batesford and Gheringhap, and Meredith and Lethbridge .. .. .	..	5·6
Inverleigh Road	Gravel sheeting .. .. .	..	·5
" " " "	Semi-penetration macadam near Fyansford and bitumen surfacing, near Murghebolue .. .. .	..	1·4
BARRABOOL SHIRE—				
Anglesea Road	General maintenance .. .. .	..	17
Hendy Main Road	General maintenance .. .. .	..	14
BASS SHIRE—				
Almurta Road	Resheeting with gravel and drainage .. .. .	..	5·5
Almurta-Grantville Road	Resheeting with gravel and drainage .. .. .	..	3·25
Inverloch-Wonthaggi Road	Resheeting with gravel and drainage .. .. .	..	3·75
Korumburra-Wonthaggi Road	Bituminous treatment of floodway .. .. .	..	·21
Resheeting with gravel, repairs to bridges and drainage	8	..	..
Main Coast Road	Bridge at Hurdy Gurdy Creek .. .. .	..	18·75
" " " "	Reforming and gravelling at San Remo .. .. .	..	1
Wonthaggi-Loch Road	Gravelling and reforming near Blackwood .. .. .	..	·67
Wonthaggi-Loch Road (joint works with Wonthaggi Borough)	Scarifying, resheeting, repairs to bridges and drainage .. .. .	..	10
" " " "	Scarifying, reshaping and resheeting with gravel .. .. .	..	·75
Carried forward			1·88	513·91

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—continued.				
		Brought forward	1·88	513·91
BEECHWORTH SHIRE—				
Beechworth Road		General maintenance from Wangaratta Shire boundary to Yackandandah Shire boundary		23
Bright Road		General maintenance, Rocky Point Bridge to Barwidgee Creek, Myrtleford		6
Everton-Myrtleford Road		General maintenance, Everton to Gapsted (Rocky Point)		13
Stanley Road		General maintenance, Stanley to Barwidgee Gap		4
BELFAST SHIRE—				
Hamilton Road		Metalling and sheeting		3
"		Single coat bitumen spraying		6·8
"		Double coat bitumen spraying		3·95
Penshurst Road		Metalling and sheeting		3·5
"		Double coat bitumen spraying		2·08
BELLARINE SHIRE—				
Geelong-Portarlington Road		Resheeting with gravel and general maintenance from Geelong boundary to Moolap State School		2·5
" " "		Resheeting with gravel and general maintenance from Drysdale Post Office to Portarlington		6
" " "		General maintenance from Curlewis Crossing to Drysdale Post Office		3·5
BENALLA SHIRE—				
Benalla-Mansfield Road		General maintenance		22
Goorambat Road		General maintenance		12
Goorambat-Thoona Road		General maintenance		13
Greta Road		General maintenance		1·5
Lima Road		General maintenance		4
Sydney Road		Bitumen surfacing and general maintenance ..		2
BERWICK SHIRE—				
Gembrook Road		General maintenance		5·5
Gembrook-Beenak Road		General maintenance		2
Hallam-Emerald Road		General maintenance		4·7
Nar Nar Goon-Longwarry Road		General maintenance		10·25
" " "		Bituminous surfacing		1
Woori Yallock-Pakenham - Koo-weerup Road		Sheeting north of Pakenham		1·11
Woori Yallock-Pakenham - Koo-weerup Road		Bituminous surfacing north of Pakenham ..		1·11
" " "		General maintenance		22
BET BET SHIRE—				
Avoca-Bealiba Road		Sheeting and flanking in detached sections between Bealiba and Archdale		2·6
" " "		General maintenance		14
Betley Road		Gravelling near Burnt Creek		·44
"		Gravelling near Eddington Road		·19
"		General maintenance		6
Dunolly Road		Sheeting with gravel and flanking in detached sections		2·75
" " "		General maintenance		5
BIRCHIP SHIRE—				
Beulah-Birchip-Wycheproof Road	2 miles east of Birchip	General maintenance	1·33	1·85
Donald-Birchip-Sea Lake Road		General maintenance		2·25
BLACKBURN AND MITCHAM SHIRE—				
Main Healesville Road		Reconstruction and widening in four sections ..		1·47
BORUNG SHIRE—				
Birchip Road	Gravelling 5 miles from Warracknabeal	Gravelling 4½ miles from Warracknabeal ..	1	·76
"		Bitumen surfacing from Warracknabeal		1·01
"		General maintenance		14
Dimboola Road		Gravelling 4 miles from Warracknabeal		1
"		Bitumen surfacing from Warracknabeal		1·43
"		General maintenance		7·5
Hopetoun Road	Joining existing lengths of metalling 1 mile from Warracknabeal	Gravelling at Batchica	·13	·82
"	Metalling 1 mile north of Brim	Bitumen surfacing from Warracknabeal ..	1·11	·75
"	Gravelling at Brim	Resheeting at Warracknabeal	1·61	·32
"		General maintenance		18
Minyip Road		Gravelling and metalling in three sections ..		1·52
"		Gravelling in 4 sections between Warracknabeal and Sheep Hills		1·58
"		Bitumen surfacing from Warracknabeal		3·5
"		General maintenance		13
Rainbow Road	Gravelling 2½ miles from Warracknabeal	Gravelling 3½ miles from Warracknabeal ..	1	1·04
"		Bitumen surfacing from Warracknabeal		1·11
"		General maintenance		18
BRAYBROOK SHIRE—				
Ballarat Road		Bitumen surfacing between Footscray tram terminus and Albion railway gates		3·33
BRIGHT SHIRE—				
Bright Road		General maintenance		20
Harriestville Road		Scarifying and resheeting		16
BROADMEADOWS SHIRE—				
Lancefield Road		Essendon City boundary to Aerodrome		1·15
BULLA SHIRE—				
Gap Road		General maintenance from Sunbury Township to Calder Highway		1·5
Melbourne-Lancefield Road		General maintenance from Tullamarine to Clarkefield		15
Sunbury Road		Bituminous surfacing from junction with Melbourne-Lancefield Road to Sunbury Township		2·75
BULN BULN SHIRE—				
Bloomfield Road		General maintenance		1
Longwarry-Drouin Road		General maintenance		6
Main Neerim "A" Road		General maintenance		8
Main Neerim "B" Road		General maintenance		12·25
Main Neerim "C" Road		General maintenance		5·5
Main South Road		General maintenance		16
Neerim East Road		General maintenance		4
Prince's Highway		General maintenance		1
Western Port Road		General maintenance		8
BUNGAREE SHIRE—				
Daylesford-Ballarat Road		Reconstruction in semi-penetration macadam in various sections from boundary to boundary		2·3
		Carried forward	8·06	938·58

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<i>UNDER MUNICIPALITIES—continued.</i>				
		Brought forward	8·06	938·58
BUNINYONG SHIRE—				
Ballarat-Rokewood Road .. ..	.. ..	Patrol maintenance .. ..	..	14
Elaine-Mt. Mercer Road .. ..	.. ..	Patrol maintenance, Elaine to Cargerie .. ..	..	5
Geelong-Ballarad Road .. ..	.. ..	Seal coat of bitumen at Elaine .. ..	..	·45
" " " .. ..	.. ..	Reconditioning and bitumen spraying from 1 mile west of Elaine towards Ballarat .. ..	..	2'12
" " " .. ..	.. ..	Reconditioning and bitumen spraying at Canadian South from Ballarat City boundary .. ..	..	·75
" " " .. ..	.. ..	Patrol maintenance, Ballarat to Clarendon .. ..	..	12'5
CASTLEMAINE BOROUGH—				
Melbourne-Bendigo Road .. ..	.. ..	Resealing in three sections .. ..	..	2'52
CHARLTON SHIRE—				
Donald Road	Gravelling at Lake Wooroonook	Small sections of resheeting and general maintenance	·47	12'75
St. Arnaud Road .. ..	.. ..	General maintenance .. ..	..	15
CHELSEA CITY—				
Point Nepean Road .. ..	.. ..	Seal coat spraying and general maintenance .. ..	..	5'66
CHILTERN SHIRE—				
Chiltern-Howlong Road .. ..	.. ..	General maintenance .. ..	..	7'12
Rutherglen-Wodonga Road .. ..	.. ..	General maintenance .. ..	..	6'7
Sydney Road .. ..	.. ..	General maintenance, scarifying, and resheeting .. ..	..	2'21
CLUNES BOROUGH—				
Maryborough-Ballarad Road .. ..	.. ..	Gravelling sections throughout .. ..	..	1
COHUNA SHIRE—				
Cohuna-Letchville Road .. ..	.. ..	General maintenance .. ..	..	2'84
Murray River Valley Road .. ..	.. ..	General maintenance .. ..	..	7'09
COLAC SHIRE—				
Colac-Ballarad Road .. ..	.. ..	Reforming and resheeting with metal north of Beeac Township .. ..	..	1'53
" " " .. ..	.. ..	Seal coating with bitumen northwards from Prince's Highway .. ..	..	1'79
" " " .. ..	.. ..	Reforming and resheeting with metal southwards from Meredith Park Road .. ..	..	·38
Colac-Beech Forest Road	Reforming and gravelling at Kawarren	General maintenance	1'67	21'15
" " " .. ..	.. ..	Resheeting with metal at Elliminyt .. ..	..	·75
" " " .. ..	.. ..	Resheeting with gravel south from Gerangamete Road .. ..	..	1'02
" " " .. ..	.. ..	General maintenance .. ..	..	11'25
Cressy-Inverleigh Road .. ..	.. ..	General maintenance .. ..	..	11
Prince's Highway .. ..	.. ..	General maintenance .. ..	..	2'48
CORIO SHIRE—				
Ballarat Road .. ..	.. ..	General maintenance .. ..	..	4'5
Fyansford Road .. ..	.. ..	General maintenance .. ..	..	·8
Geelong-Bacchus Marsh Road .. ..	.. ..	General maintenance .. ..	..	14'17
CRANBOURNE SHIRE—				
Koo-wee-rup-Pakenham Road .. ..	.. ..	General maintenance .. ..	..	5'5
Lang Lang-Nyora Road .. ..	.. ..	General maintenance .. ..	..	4'7
Main Coast Road .. ..	.. ..	General maintenance .. ..	..	38
Westernport Road .. ..	.. ..	General maintenance .. ..	..	9
CRESWICK BOROUGH—				
Castlemaine-Ballarad Road .. ..	.. ..	General maintenance .. ..	..	3'5
CRESWICK SHIRE—				
Castlemaine-Ballarad Road .. ..	.. ..	General maintenance .. ..	..	21
Daylesford-Ballarad Road .. ..	.. ..	General maintenance .. ..	..	12'38
DANDENONG SHIRE—				
Cheltenham Road .. ..	.. ..	Between Prince's Highway and Moorabbin Shire boundary .. ..	..	6
Dandenong-Frankston Road .. ..	.. ..	Between Prince's Highway and Frankston Shire boundary .. ..	..	6
Prince's Highway .. ..	.. ..	Town section .. ..	..	2
DAYLESFORD BOROUGH—				
Ballan Road .. ..	.. ..	General maintenance .. ..	..	·8
Ballarat Road .. ..	.. ..	General maintenance .. ..	..	1
Castlemaine Road .. ..	.. ..	General maintenance .. ..	..	·62
Daylesford-Hepburn Road .. ..	.. ..	Bitumen surfacing and general maintenance .. ..	..	1
DEAKIN SHIRE—				
Echuca-Cornelia Road .. ..	.. ..	Reforming and gravelling through Cornelia Creek Estate .. ..	..	·37
Echuca-Picola Road .. ..	.. ..	Reforming and gravelling north of Village Settlement .. ..	..	·64
Kyabram-Tongala Road .. ..	.. ..	Bitumen semi-penetration south of Tongala .. ..	..	·5
Rochester-Kyabram Road .. ..	.. ..	Reforming and gravelling east and west of Mount Scoble .. ..	..	2'06
Kyabram-Tongala Road (joint works with Rodney Shire) .. ..	.. ..	Bitumen semi-penetration north of Kyabram Corner .. ..	..	·5
Rochester-Kyabram Road (joint works with Rodney Shire) .. ..	.. ..	Bitumen semi-penetration south of Kyabram Corner .. ..	..	·5
DIMBOOLA SHIRE—				
Rainbow Road	About 5 miles south of Rainbow	About 4 miles south of Rainbow	1'06	·32
Warracknabeal Road	About 6 miles north-east of Dimboola	North and south from Tarranyurk .. ..	..	·65
		About 2 miles north-east of Dimboola	1'18	1'13
DONALD SHIRE—				
Donald-Charlton Road	At Dixon's Hill	General maintenance	·89	14
Donald-Minyip Road .. ..	.. ..	General maintenance .. ..	..	2'5
Marnoo Road .. ..	.. ..	General maintenance .. ..	..	5
St. Arnaud-Birchip Road	1 mile south of Watchem	General maintenance	·94	28'5
" " "	Near Buloke .. ..	.. ..	·47	..
DONCASTER AND TEMPLESTOWE SHIRE—				
Doncaster Road .. ..	.. ..	General maintenance .. ..	..	6'2
Heidelberg-Warrandyte Road .. ..	.. ..	General maintenance .. ..	..	9'75
Warrandyte-Ringwood Road .. ..	.. ..	General maintenance .. ..	..	3
DUNDAS SHIRE—				
Hamilton-Dunkeld Road .. ..	.. ..	General maintenance .. ..	..	·84
Hamilton-Horsham Road	Between 30½ and 30¾ miles from Hamilton in Parish of Woolpooper	General maintenance	·47	2'15
Hamilton-Mt. Gambier Road .. ..	.. ..	General maintenance .. ..	..	1'08
Hamilton-Port Fairy Road .. ..	.. ..	General maintenance .. ..	..	3'36
Hamilton-Portland Road .. ..	.. ..	General maintenance .. ..	..	·37
		Carried forward	15'21	1,302'23

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Work Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	15·21	1302·23
DUNMUNKLE SHIRE—				
Horsham—Murtoa Road	Westerly from Murtoa Township	Resealing bitumen-sprayed road starting at Minyip Township	2·88	1·8
Minyip—Donald Road .. ..	.. ..	Bituminous spraying of gravelled road easterly from Murtoa Township	..	2·9
Rupanyup—Murtoa Road .. ..	.. ..	Resealing bitumen-sprayed road westerly from Rupanyup Township	..	6·5
Stawell—Warracknabeal Road	Foundation course 4½ miles north of Rupanyup	Bituminous spraying of gravelled road starting 1½ miles north of Rupanyup	1·97	·8
.. ..	Foundation course 6½ miles north of Rupanyup	Resealing bitumen-sprayed road east and west of Minyip Township	1·06	2·5
.. ..	Foundation course 7½ miles north of Rupanyup	Resealing bitumen-sprayed road north and south of Rupanyup Township	1·2	2·2
EAGLEHAWK BOROUGH—				
Mount Kerong Road .. ..	.. ..	Reconstruction westerly from Lester Street ..	..	1·13
.. ..	.. ..	General maintenance to Marong Shire boundary	..	1·5
.. ..	.. ..	Bituminous macadam patching and seal coat of bitumen from Parsonage Avenue westerly towards Kirkwood Street	..	2·2
EAST LODDON SHIRE—				
Dingee Road .. ..	.. ..	West of Dingee in Parish of Talambe ..	..	·56
Mitiamo Road .. ..	.. ..	General maintenance westerly from Mitiamo ..	..	2
Prairie Road .. ..	.. ..	East and west of Prairie in Parish of Talambe..	..	·2
ECHUCA BOROUGH—				
Echuca West Road .. ..	.. ..	Semi-penetration bitumen surfacing west from High Street junction, thence south to Boundary Road	..	1·42
Echuca—Wyuna Road	Top course gravelling between allotments 20, 21, and 30, 32A, 32B, Parish of Echuca North	Semi-penetration bitumen surfacing east from railway crossing to junction with Echuca-Cornelia Road	·66	·4
ELTHAM SHIRE—				
Eltham—Yarra Glen Road .. ..	.. ..	General maintenance between Eltham and Yarra Glen	..	20
Hurstbridge—Kingslake Road .. ..	.. ..	General maintenance between Wattle Glen and Kingslake	..	15
EUROA SHIRE—				
Arcadia Road .. ..	.. ..	General maintenance .. ..	..	5
Euroa—Arcadia Road	Forming and sanding between Miepoll and Arcadia Road	Patrol maintenance	4·45	16
Euroa—Mansfield Road .. ..	.. ..	General maintenance .. ..	..	15
Euroa—Strathbogie Road	Forming, sidecutting, &c., Anker's deviation, Strathbogie North	Patrol maintenance	·98	16
Murchison—Shepparton Road .. ..	.. ..	General maintenance .. ..	..	5
FERNTREE GULLY SHIRE—				
Belgrave—Emerald Road .. ..	.. ..	General maintenance, Belgrave to Emerald ..	..	6·73
Emerald Road .. ..	.. ..	General maintenance, Emerald to Avonsleigh ..	..	1·63
Main Ferntree Gully Road .. ..	.. ..	General maintenance, Wheeler's Hill to Belgrave	..	10·81
Monbulk Road .. ..	.. ..	General maintenance, Belgrave to Monbulk ..	..	5
Olinda Road	Upper Ferntree Gully to Tremont	General maintenance, Upper Ferntree Gully to Olinda	1·16	6·25
FLINDERS SHIRE—				
Hastings—Flinders Road .. ..	.. ..	East of Bittern .. ..	..	·8
Mornington—Flinders Road	Near Kings	At Dowling's Corner	·76	1
.. ..	At Avenue Corner .. ..	.. ..	1	..
Point Nepean Road	Between Rye and Sorrento	North from McLear's Hill	2·54	1·13
.. ..	Bridge over Boneo drain	At Dromana	·05	·18
.. ..	.. ..	At Lighthouse, Rosebud .. ..	..	·22
Stony Point Road .. ..	.. ..	To Naval Base gates .. ..	..	2·75
FRANKSTON AND HASTINGS SHIRE—				
Dandenong—Frankston Road .. ..	.. ..	General maintenance in Parish of Frankston ..	..	3
Frankston—Flinders Road .. ..	.. ..	Widening in Parish of Frankston .. ..	..	1·19
.. ..	.. ..	Sealing with bitumen in Parishes of Frankston and Tyabb	..	6·33
.. ..	.. ..	General maintenance .. ..	..	13
Point Nepean Road .. ..	.. ..	Widening in Parish of Frankston .. ..	..	5·04
.. ..	.. ..	General maintenance .. ..	..	7·8
GISBORNE SHIRE—				
Bacchus Marsh Road .. ..	.. ..	General maintenance .. ..	..	9·42
Gisborne Station Road .. ..	.. ..	General maintenance .. ..	..	1·18
GLENELG SHIRE—				
Dergholm Road	Gravelling near Dergholm	.. ..	·32	..
Mount Gambier Road	Metalling near Strathdownie turn-off	Metal sheeting and semi-penetration at 15th mile post	·26	·3
.. ..	Metalling near Carmichaels	Semi-penetration near Carmichaels	1·43	1·43
.. ..	Metalling and timber bridge near Cain's deviation	Scarifying, reforming, and rolling old road ..	·17	10
GLENLYON SHIRE—				
Ballarat Road	Reinforced concrete bridge over Stony Creek	Gravelling and general maintenance	·01	4·5
Ballarat Road .. ..	.. ..	General maintenance .. ..	..	3·5
Castlemaine—Daylesford Road .. ..	.. ..	Gravelling, culverts and general maintenance ..	..	13
Daylesford—Heppburn Road .. ..	.. ..	Bitumen surfacing and general maintenance ..	..	1
Malmsbury—Daylesford Road	Forming and gravelling	Gravelling and general maintenance	·17	15·12
GOULBURN SHIRE—				
Goulburn Valley Road	1 mile north of Nagambie near Adam's Lane	General maintenance	1	12·5
Murchison—Shepparton Road .. ..	.. ..	Near Dargalong and Moorlim .. ..	..	3·5
GRENVILLE SHIRE—				
Ballarat—Hamilton Road .. ..	.. ..	Bitumen penetration .. ..	..	3·89
.. ..	.. ..	General maintenance .. ..	..	24·5
Cressy Road .. ..	.. ..	General maintenance .. ..	..	9·5
Lismore Road .. ..	.. ..	General maintenance .. ..	..	10·4
Pitfield Road .. ..	.. ..	General maintenance .. ..	..	12·7
HAMILTON TOWN—				
Ararat Road .. ..	.. ..	Bitumen penetration, Sections 1 and 11, Parish of South Hamilton	..	·34
Coleraine Road .. ..	.. ..	Remetalling and bitumen surfacing Sections A and C, Parish of North Hamilton	..	·36
Portland Road .. ..	.. ..	Regravelling Section B, Parish of South Hamilton	..	·5
HAMPDEN SHIRE—				
Camperdown—Ballarat Road .. ..	.. ..	Reshaping and bitumen spraying .. ..	..	9
.. ..	.. ..	General maintenance .. ..	..	54
Caramut—Lismore Road .. ..	.. ..	General maintenance .. ..	..	16
Lismore—Cressy Road .. ..	.. ..	Reshaping and bitumen spraying .. ..	..	6·5
.. ..	.. ..	Reconstruction and semi-penetration	..	·62
.. ..	.. ..	General maintenance .. ..	..	18
Prince's Highway .. ..	.. ..	Reconstruction and bituminous macadam	..	1
.. ..	.. ..	General maintenance .. ..	..	2·75
Terang—Mortlake Road .. ..	.. ..	General maintenance .. ..	..	7
		Carried forward	36·38	1743·81

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	36·38	1743·81
HEALESVILLE SHIRE—				
Healesville—Alexandra Road .. ..	.. ..	General maintenance, Yarra Flats to Buxton ..	..	21
Healesville—Woori Yallock Road .. ..	.. ..	General maintenance, Healesville—Alexandra Road to Badger Creek and between Dalry P.R. and Allotment 12A, Parish of Gracedale	..	6
Marysville Road .. ..	.. ..	General maintenance between St. Fillans and Marysville	..	6
HEIDELBERG SHIRE—				
Main Heidelberg—Eltham Road .. ..	.. ..	Widening roadway with bitumen penetrated metal between Darebin Creek and Shire boundary	..	2·33
" .. ..	.. ..	Bitumen surfacing between Merri Creek Bridge and Banksia Street and between Heidelberg Township and Shire boundary	..	4·04
Greensborough—Hurstbridge Road .. ..	.. ..	Widening roadway between Eltham Road and Greensborough	..	·2
" .. ..	.. ..	Surfacing with bitumen between Sutherland Home and Diamond Creek	..	1·82
Main Whittlesea Road .. ..	.. ..	Surfacing with bitumen .. ..	..	1·19
Heidelberg—Warrandyte Road .. ..	.. ..	General maintenance .. ..	..	·47
HEYTESBURY SHIRE—				
Camperdown—Cobden Road .. ..	.. ..	Bitumen surfacing, Bostock's to Cobden ..	..	2·86
Cobden—Port Campbell—Princetown Road .. ..	.. ..	General maintenance .. ..	..	1·98
" .. ..	.. ..	Bitumen surfacing south-east from Cobden ..	41	1·51
" .. ..	.. ..	General maintenance .. ..	..	27·49
HORSHAM BOROUGH—				
Dimboola—Horsham Road .. ..	.. ..	Penetration .. ..	..	·5
Dooen Road .. ..	.. ..	Sealing .. ..	..	·81
" .. ..	.. ..	Sealing .. ..	..	·75
Hamilton Road .. ..	.. ..	Penetration .. ..	..	·5
" .. ..	.. ..	Sealing .. ..	..	·75
" .. ..	.. ..	Reconditioning bridge and penetrating decking and approaches	..	·04
Natinuk Road .. ..	.. ..	Sealing .. ..	..	·5
HUNTLY SHIRE—				
Bendigo—Echuca Road	In the Township of Elmore ..	In the Township of Elmore	12	·5
"	In the Township of Epsom ..	In the Township of Epsom	25	1
" .. ..	.. ..	In the Township of Huntly .. ..	..	1·5
INGLEWOOD BOROUGH—				
Bendigo—Charlton Road .. ..	.. ..	Double-coat bitumen surfacing .. ..	..	·24
" .. ..	.. ..	Single-coat bitumen surfacing .. ..	..	·4
" .. ..	.. ..	General maintenance .. ..	..	1·5
KARA KARA SHIRE—				
Avoca—St. Arnaud Road .. ..	.. ..	General maintenance between St. Arnaud and Redbank	..	23
Charlton Road .. ..	.. ..	General maintenance .. ..	..	10
Navarre Road .. ..	.. ..	General maintenance between St. Arnaud and Navarre	4·02	24
St. Arnaud—Donald Road .. ..	.. ..	Bitumen surfacing between St. Arnaud North and Shire boundary at Cope Cope	1·82	9·43
KARKAROO SHIRE—				
Hopetoun—Warracknabeal Road	Metalling at Galaquil .. ..	.. ..	49	..
Hopetoun—Woomelang—Sea Lake Road	Metalling north of Woomelang ..	.. ..	1·55	..
Rainbow—Beulah—Birchip Road	Metalling in Parish of Kurdgweechee	Metallug in Parish of Kurdgweechee ..	1·67	·66
KEILOR SHIRE—				
Melbourne—Bendigo Road .. ..	.. ..	At North Essendon .. ..	..	·11
KERANG SHIRE—				
Koondrook Road .. ..	.. ..	Between Koondrook and Barham .. ..	..	·1
KILMORE SHIRE—				
Heathcote Road .. ..	.. ..	Resheeting between Bower's Culvert and McIssac's Bridge	..	·7
" .. ..	.. ..	Patrol maintenance .. ..	..	3·56
" .. ..	.. ..	Resheeting in sections .. ..	..	·62
" .. ..	.. ..	Patrol maintenance .. ..	..	1·29
" .. ..	.. ..	Resheeting Boundary Flat .. ..	..	1·16
" .. ..	.. ..	Patrol maintenance .. ..	..	2·99
" .. ..	.. ..	Resheeting in sections .. ..	..	·55
" .. ..	.. ..	Patrol maintenance .. ..	..	2·28
KOROIT BOROUGH—				
Koroit—Warrnambool Road .. ..	.. ..	Single-coat bitumen spraying .. ..	..	4·72
" .. ..	.. ..	Double-coat bitumen spraying .. ..	..	·78
" .. ..	.. ..	Drainage works and general maintenance ..	..	4
KORONG SHIRE—				
Borong—Hurstwood Road .. ..	.. ..	Sanding in detached sections .. ..	..	4·2
Charlton—Bendigo Road .. ..	.. ..	Double-coat bitumen surfacing .. ..	..	·57
" .. ..	.. ..	Single-coat bitumen surfacing .. ..	..	·57
Serpentine Road .. ..	.. ..	Gravelling near Allotments 9 and 9A, Parish of Salisbury West	..	·49
KORUMBURRA SHIRE—				
Bena—Poowong Road .. ..	.. ..	General maintenance .. ..	..	6·01
Korumburra—Drouth Road .. ..	.. ..	General maintenance .. ..	..	4·04
Korumburra—Leongatha Road .. ..	.. ..	General maintenance .. ..	..	4·84
Korumburra—Warragul Road .. ..	.. ..	General maintenance .. ..	..	13
Korumburra—Wonthaggi Road .. ..	.. ..	General maintenance .. ..	..	13·25
Lang Lang—Nyora Road .. ..	.. ..	General maintenance .. ..	..	5
Loch—Wonthaggi Road .. ..	.. ..	General maintenance .. ..	..	4·64
Nyora—Poowong Road .. ..	.. ..	General maintenance .. ..	..	5·99
Poowong—Ranceby Road .. ..	.. ..	General maintenance .. ..	..	4·15
KOWREE SHIRE—				
Booroopki Road .. ..	.. ..	General maintenance .. ..	..	40·5
Booroopki—Francis Road	Near Booroopki and Neuarpur ..	Near Booroopki and Neuarpur	1·65	4·32
" .. ..	.. ..	General maintenance .. ..	..	16
Edenhope—Goroke Road .. ..	.. ..	Near Edenhope .. ..	..	·5
" .. ..	.. ..	General maintenance .. ..	..	16
Hamilton—Edenhope—Apsley Road .. ..	.. ..	Near Edenhope and Kadnook .. ..	..	1·57
" .. ..	.. ..	General maintenance .. ..	..	40·5
KYNETON SHIRE—				
Daylesford Road .. ..	.. ..	General maintenance .. ..	..	·75
Melbourne—Bendigo Road .. ..	.. ..	Resurfacing with bitumen .. ..	..	·75
Redesdale Road .. ..	.. ..	Resheeting .. ..	..	·25
" .. ..	.. ..	General maintenance .. ..	..	7
Trentham Road .. ..	.. ..	Resheeting .. ..	..	·5
Tylden—Woodend Road .. ..	.. ..	General maintenance .. ..	..	4
		Carried forward	48·36	2119·63

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	48·36	2119·63
LAWLOIT SHIRE—				
Broughton Road	Metalling between Kaniva and Sandsmere	Resheeting	·43	·4
"	"	Bitumen surfacing		·23
"	"	General maintenance		9·48
Nhill-Kaniva-Border Road	"	Bitumen surfacing, &c.		·5
South Lillimur Road	"	Resheeting		·6
"	"	General maintenance		6
Yearlinga Road	Metalling between Kaniva and Yearlinga	Resheeting	·86	·72
"	"	General maintenance		8·6
LEIGH SHIRE—				
Ballarat-Rokewood Road	"	General maintenance		8
Cressy-Inverleigh Road	"	Reconditioning through Parish of Dorog		6
"	"	Bitumen spraying westerly from Bannockburn Shire boundary		3·25
Cressy-Rokewood Road	Culvert and approaches, Salt Creek Flat	General maintenance	·15	11
Inverleigh-Shelford Road	"	Reconditioning 4 miles south of Shelford		1
Rokewood-Shelford Road	"	General maintenance		17
Shelford-Bannockburn Road	"	General maintenance, Teesdale to Shelford		4
LEXTON SHIRE—				
Avoca-Ararat Road	"	Gravelling between Amphitheatre and Elmhurst		1·5
Avoca-Ballarat Road	"	Metalling and gravelling between Lexton and Waudra		2·5
LILLYDALE SHIRE—				
Evelyn-Lillydale Road	Between Lillydale and Evelyn	"	·83	
Main Healesville Road	Between Lillydale and Coldstream	"	1·72	
LOWAN SHIRE—				
Dimboola-Kaniva Road	"	Victoria and Nelson Streets, Nhill		·18
Goroke Road	"	Between Allotments 28 and 24, Parish of Winiam		·22
"	"	Between Allotments 17 and Water Reserve 20, Parish of Winiam		·23
Lorquon West Road	Between Allotments 133 and 134A, Parish of Woorak	Between Allotments 108/111 and 18A, 18, Parish of Lorquon	·15	·53
Yanac Road	Between Allotments 20A and 26/51, Parish of Yanac	Between Allotments 99/207 and 206, Parish of Tarranginnie	·26	·23
"	Between Allotments 157/238 and 158/230, Parish of Tarranginnie	Between Allotments 125 and 126A, Parish of Tarranginnie	·51	·24
MAFFRA SHIRE—				
Briagolong-Dargo Road	"	General maintenance, Briagolong Township to Shire boundary		2
Bushy Park-Valencia Creek Road	"	General maintenance, Bushy Park Station to O'Connor's		7
Licola Road	"	General maintenance, Heyfield to Licola		40
Maffra-Sale Road	"	General maintenance, Maffra to Myrtlebank		7
Tinamba-Boisdale Road	"	General maintenance, from Tinamba to Boisdale		14
Tinamba-Newry Road	"	General maintenance, Tinamba Store to Stokes Corner		3
MALDON SHIRE—				
Baringhup Road	"	General maintenance, Maldon to Tullaroop Shire boundary		10
Castlemaine-Maldon Road	"	General maintenance, Maldou to Castlemaine Borough boundary		10
Castlemaine-Newstead Road	"	General maintenance from junction with Castlemaine-Maldon Road to Newstead and Mount Alexander Shire boundary		·75
Maldon-Eddington Road	"	General maintenance, Maldon to old Eddington Bridge		16
Newstead Road	"	General maintenance, Maldon to Newstead and Mount Alexander Shire boundary		6
MANSFIELD SHIRE—				
Euroa-Merton Road	"	General maintenance		4·4
Mansfield Road	6 miles west of Mansfield	General maintenance	·29	42·7
Mansfield-Tolmie Road	"	General maintenance		5·75
Mansfield-Wood's Point Road	"	General maintenance		18·5
MARONG SHIRE—				
Bendigo-Bridgewater Road	"	Resheeting and penetrating and sealing with bitumen		1·24
Bendigo-Eddington Road	"	Constructing two flood crossings through Bradford Creek near Laanecoorie		·02
"	"	Reshaping and sealing near Laanecoorie		·94
"	"	Constructing flood crossing at West Shelbourne		·05
"	"	Constructing flood crossing at East Shelbourne		·08
Bendigo-Serpentine Road	"	Resheeting and sealing at Myer's Flat and Woodvale		·57
MARYBOROUGH BOROUGH—				
Avoca Road	"	General maintenance		1
Ballarat Road	"	General maintenance		·75
Castlemaine Road	"	General maintenance		1·25
Eddington Road	"	General maintenance		1
MELTON SHIRE—				
The Gap Road	"	Sheeting with metal, gravelling, and patrol maintenance		·8
Toolern Road	"	Sheeting with metal, gravelling, and patrol maintenance		6
METCALFE SHIRE—				
Kyneton-Redesdale Road	"	General maintenance between Langley and Redesdale		10
MILDURA SHIRE—				
Deakin Avenue	"	Between 14th and 15th Streets		·6
Irymple Road	Between 15th Street and railway on Ginquam Avenue	Between Deakin Avenue and Ginquam Avenue	·87	4
Wentworth Road	Construction in 17th Street, Mena Road, Main Avenue, and from 3rd Street to Abbotsford Bridge	Deakin Avenue between 15th Street and 17th Street, and 17th Street between Deakin Avenue and Riverside Avenue	8·03	2·5
MILDURA TOWN—				
Deakin Avenue	"	14th Street to 10th Street		1
Langtree Avenue	"	10th Street to Punt Road		·42
Punt Road	"	Langtree Avenue to Magnolia Avenue		·48
Tenth Street	"	Deakin Avenue to Langtree Avenue		·08
MINHAMITE SHIRE—				
Hamilton - Macarthur - Port Fairy Road	"	Semi-penetration in small sections		1·16
Warrnambool-Hawkesdale-Penshurst Road	"	Semi-penetration in small sections		3·55
		Carried forward	62·46	2426·63

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	62·46	2426·63
MIRBOO SHIRE—				
Allambee East—West Tarwin Road	Patrol maintenance .. .. .	.. .. .	4·2
Boolarra South—Mirboo Road	Patrol maintenance .. .. .	.. .. .	4
Leongatha—Mirboo Road	General maintenance .. .. .	.. .. .	4
Mardan Road	General maintenance .. .. .	.. .. .	5
Mirboo—Allambee East Road	Bitumen painting from Allambee Junction .. .. .	.. .. .	1·2
..	Patrol maintenance .. .. .	.. .. .	6
Mirboo South Road	Bitumen painting, Mardan Road to Allotment 46, Parish of Mirboo .. .. .	.. .. .	1
..	Patrol maintenance .. .. .	.. .. .	9·5
..	Metalling approaches, T. Brown's subway .. .. .	.. .. .	·07
MOORABBIN SHIRE—				
Centre Dandenong Road	Scarifying and metalling, penetrating and sealing with bitumen eastwards from Cavanagh Street .. .. .	.. .. .	·13
..	General maintenance .. .. .	.. .. .	2·75
Point Nepean Road	Scarifying and metalling, penetrating and sealing with bitumen from Cheltenham Township to Turner Street .. .. .	.. .. .	·85
..	Scarifying and metalling, penetrating and sealing with bitumen from Wickham Road to Moorabbin Station .. .. .	.. .. .	·75
..	General maintenance .. .. .	.. .. .	1·5
MORDIALLOC CITY—				
Point Nepean Road	Widening and placing kerbs north-westerly from Parker's Road .. .. .	.. .. .	·51
..	General maintenance .. .. .	.. .. .	3
MORNINGTON SHIRE—				
Point Nepean Road	Widening to 20 feet from Main Street, Mornington .. .. .	.. .. .	4
MORTLAKE SHIRE—				
Caramut—Lispore Road	Resheeting with metal and double-coat bitumen surfacing .. .. .	.. .. .	10
Mortlake—Ararat Road	Resheeting with metal and double-coat bitumen surfacing .. .. .	.. .. .	1·79
Mortlake—Warrnambool Road	Resheeting with metal and double-coat bitumen surfacing .. .. .	.. .. .	3·53
MORWELL SHIRE—				
Boolarra—Foster Road	General maintenance .. .. .	.. .. .	5
Boolarra—Welshpool Road	Sanding from Boolarra—Foster Road to Morwell River Bridge, including deviation near river .. .. .	.. .. .	·53
Jeeralang West Road	Deviation through Ellis's and others, including sanding .. .. .	.. .. .	1·36
..	General maintenance .. .. .	.. .. .	17
..	Bitumen penetration on flood crossing .. .. .	.. .. .	·14
..	Two-coat bitumen surfacing on gravel .. .. .	.. .. .	·7
Prince's Highway
MOUNT ROUSE SHIRE—				
Ballarat—Hamilton Road	Bitumen surfacing between Dunkeld and Glenthompson .. .. .	.. .. .	1
..	Gravel sheeting between Glenthompson and Wickliffe .. .. .	.. .. .	1·13
Hamilton—Penshurst Road	Bitumen semi-penetration between 8½ miles north and 2½ miles south of Penshurst .. .. .	.. .. .	2·04
..	Bitumen surfacing between 6½ miles north and 1½ miles south of Penshurst .. .. .	.. .. .	1·09
Penshurst—Caramut Road	Bitumen surfacing between 0 miles and 3 miles 31 chains from Penshurst .. .. .	.. .. .	1·24
..	R.C. culvert at 14-mile post .. .. .	.. .. .	.. .. .
MULGRAVE SHIRE—				
Ferntree Gully Road	Widening to 20 feet .. .. .	.. .. .	1
..	General maintenance .. .. .	.. .. .	6
MCIVOR SHIRE—				
Heathcote—Elmore Road	Gravel sheeting .. .. .	.. .. .	1·05
Heathcote—Redesdale Road	Construction of timber bridge over Mia Mia Creek .. .. .	.. .. .	·11
..	Forming, boxing, grading, and gravelling .. .. .	.. .. .	.. .. .
Kilmore—Heathcote—Bendigo Road	Gravel sheeting .. .. .	.. .. .	3·5
..	Forming, boxing, grading, and gravelling .. .. .	.. .. .	·12
..	Metal sheeting .. .. .	.. .. .	·5
..	Construction R.C. concrete culverts .. .. .	.. .. .	.. .. .
..	Repairing and renewing timber bridges .. .. .	.. .. .	.. .. .
NARRACAN SHIRE—				
Trafalgar—Thorpdale Road	General maintenance from Trafalgar southerly .. .. .	.. .. .	8
Trafalgar—Willowgrove Road	General maintenance from Trafalgar .. .. .	.. .. .	4
Walhalla Road	General maintenance from Moe, thence northerly .. .. .	.. .. .	18
Yarragon—Leongatha Road	General maintenance from Yarragon, thence southerly .. .. .	.. .. .	10
Yarragon—Shady Creek Road	General maintenance from Yarragon .. .. .	.. .. .	3
NEWHAM AND WOODEND SHIRE—				
Lancefield Road	General maintenance .. .. .	.. .. .	9
Tylden Road	Gravelling, &c. .. .. .	.. .. .	.. .. .
..	Gravelling, &c. .. .. .	.. .. .	.. .. .
..	Gravelling, &c. .. .. .	.. .. .	.. .. .
(joint works with Kyneton Shire)
NEWSTEAD AND MOUNT ALEXANDER SHIRE—				
Castlemaine—Daylesford Road	Reconditioning with gravel .. .. .	.. .. .	·5
..	Tar paving .. .. .	.. .. .	2·5
Castlemaine—Maryborough Road	Reconditioning with gravel .. .. .	.. .. .	1·5
..	Tar paving .. .. .	.. .. .	1
Creswick Road	Reconditioning with gravel .. .. .	.. .. .	·6
..	Tar paving .. .. .	.. .. .	·25
Maldon Road	Reconditioning with gravel .. .. .	.. .. .	·12
NUMURKAH SHIRE—				
Nathalia North Road	Gravelling north of existing gravel .. .. .	.. .. .	·47
Nathalia—Kyabram Road	Gravelling from Skeleton Creek to Camelback bridge .. .. .	.. .. .	1·09
..	Gravelling easterly from Kotupna School .. .. .	.. .. .	·95
Numurkah—Nathalia Road	Gravelling near Nathalia Water-tower .. .. .	.. .. .	·47
Numurkah—Tungamah Road	Gravelling, &c., westerly from Gordon's Road .. .. .	.. .. .	·87
OAKLEIGH CITY—				
Ferntree Gully Road	Surface painting and general maintenance .. .. .	.. .. .	·48
Prince's Highway—Section 1	Reconstruction in asphaltic penetration macadam .. .. .	.. .. .	·27
..	Resurfacing asphaltic concrete experimental section .. .. .	.. .. .	·07
..	Maintenance of concrete experimental section .. .. .	.. .. .	·23
..	Maintenance .. .. .	.. .. .	·09
..	Reconstruction in asphaltic penetration macadam .. .. .	.. .. .	·25
..	Maintenance (patching only) .. .. .	.. .. .	·21
OMEIO SHIRE—				
Benambra Road	North of Omeo (deviation at Wilson's Creek) .. .. .	.. .. .	1·4
..	Omeo Highway to Township of Benambra .. .. .	.. .. .	11
..	Carried forward	66·62	2606·08

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	66·62	2606·08
ORBOST SHIRE— Cann Valley Road	Widening and re-aligning between Redman's cutting and Holmes's, and near Flat Rock Creek	General maintenance between Prince's Highway and New South Wales Border	3·45	29
Genoa-Gipsy Point Road	General maintenance, including widening and improvement of curves	..	7
Marlo Road	Gravelling from western corner of Allotment 15, Parish of Orbost East, southerly towards Marlo	Reforming earth section as required, bitumen surfacing metalled section, and general maintenance	·75	9
Prince's Highway	Surface treatment with bitumen and general maintenance	..	1·32
Wangarabelle Road	Construction of temporary bridges to carry traffic pending reconstruction of damaged bridges at Big Flat and Wangarabelle Creeks and general maintenance	..	12·5
OXLEY SHIRE— Bright Road	Dalton's Oxley Flats	General maintenance, Wangaratta to Rocky Point	·26	25
Greta-Glenrowan Road	Maloney's Creek section Wilson's Approaches Fifteen-mile Creek bridge	.. .. .	·2	..
Oxley Road	General maintenance, Glenrowan to Greta ..	·1	8
PHILLIP ISLAND SHIRE— Newhaven Road	General maintenance, Wangaratta to Oxley ..	..	7
Phillip Island Road	General maintenance .. .. .	..	7·75
Ventnor Road	General maintenance .. .. .	..	2·4
PORT FAIRY BOROUGH— Hamilton Road	General maintenance .. .. .	..	4·5
Portland Road	Bitumen sealing .. .. .	..	1·4
Prince's Highway	Drainage works and general maintenance ..	..	1·56
PORTLAND SHIRE— Heath Road	Drainage works and general maintenance ..	..	2·6
Portland-Casterton Road	Two sections near Portland Freezing Works	.. .. .	·63	..
PRESTON CITY— Epping Road	Three sections between Drumborg and Hotspur	.. .. .	·3	..
Whittlesea Road	General maintenance .. .. .	..	1·42
..	Resheeting, bitumen penetration and sealing between pegs 7092 and 14359	..	1·38
..	General maintenance balance of road ..	..	1·34
PYALONG SHIRE— Kilmore-Heathcote-Bendigo Road	Resheeting, reforming, installation of pipe culvert from Percival's Bridge to Tooborac	..	8·06
..	Construction 4 x 2 concrete culvert, Fullard's	..	·05
..	Straightening road and construction 4-ft. diameter culvert near Fullard's	..	·24
..	Patrol maintenance .. .. .	..	11·34
QUEENSCLEIFE BOROUGH— Geelong Road	Bitumen surfacing and general maintenance ..	..	3·25
RINGWOOD BOROUGH— Main Healesville Road	General maintenance .. .. .	..	3·25
Mount Dandenong Road	General maintenance .. .. .	..	1·5
Ringwood-Warrandyte Road	General maintenance .. .. .	..	2·5
RIPON SHIRE— Ballarat-Ararat Road	General maintenance .. .. .	..	1·35
Ballarat-Hamilton Road	Bitumen, painting .. .. .	..	2·59
..	Bluestone resheeting .. .. .	..	2·66
..	Patrol maintenance, balance of road ..	..	11·05
..	Bitumen painting .. .. .	..	2·55
..	Bluestone resheeting .. .. .	..	1·19
..	Patrol maintenance, balance of road ..	..	14·26
..	Bitumen penetration, bridge decking ..	..	·03
ROCHESTER SHIRE— Rochester - Bamawm - Prairie Road ..	Between Allotments 176 and 177, Parish of Bamawm	General maintenance between Lockington and Northern Highway	1	9
RODNEY SHIRE— Kyabram-Nathalia Road	Patrol maintenance .. .. .	..	1
Kyabram-Tongala Road	Bitumen spraying .. .. .	..	·44
..	Patrol maintenance .. .. .	..	1
Mooroopna-Undera Road	Bitumen spraying .. .. .	..	2·5
..	Patrol maintenance .. .. .	..	8
Shepparton-Tatura Road	Bitumen spraying .. .. .	..	3·27
..	Patrol maintenance .. .. .	..	10
Tatura - Byrneside - Kyabram Road ..	Metalling west of Byrneside	Bitumen spraying	1·98	4·35
..	Patrol maintenance .. .. .	..	18
Tatura-Murchison Road	Metalling south of Tatura	Bitumen spraying	·26	1·59
..	Patrol maintenance .. .. .	..	13
Shepparton-Tatura Road (joint works with Shepparton Borough)	Patrol maintenance .. .. .	..	2
ROMSEY SHIRE— Lancefield-Kilmore Road	General maintenance .. .. .	..	9·71
Melbourne-Lancefield Road	General maintenance .. .. .	..	15·85
Woodend-Lancefield Road	General maintenance .. .. .	..	5·62
Lancefield-Kilmore (joint works with Kilmore Shire)	General maintenance .. .. .	..	2·25
ROSEDALE SHIRE Carraung-Gormandale Road	Placing running boards on bridge and patrol maintenance	..	·75
Prince's Highway	Patrol maintenance .. .. .	..	·91
Sale-Yarram Road	Placing running boards on bridges, repairs to decking, and patrol maintenance	..	13·8
Seaspray Road	Patrol maintenance .. .. .	..	14·9
Traralgon-Gormandale Road	Patrol maintenance .. .. .	..	9·06
Willung Road	Forming and gravelling	Patrol maintenance	·74	7·26
RUTHERGLEN SHIRE— Chiltern-Howlong Road	General maintenance, Howlong Flats ..	..	1·25
Yarrowonga Road	General maintenance .. .. .	..	14·4
Rutherglen-Wahgunyah Road	Improvement to turns, Drummond-street, Rutherglen	..	·19
..	General maintenance .. .. .	..	6·35
..	Bitumen penetration .. .. .	..	·31
Springhurst-Rutherglen Road	Bitumen penetration .. .. .	..	·13
..	General maintenance .. .. .	..	7·3
RUTHERGLEN SHIRE— Wodonga Road	Bitumen penetration .. .. .	..	·29
..	General maintenance .. .. .	..	10·35
		Carried forward	76·29	2989·95

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works. Miles.	Maintenance. Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	76.29	2089.95
SALE TOWN—				
Prince's Highway		General maintenance, Wurruk Bridge to Sale Post Office		1
Sale-Longford Road		General maintenance, Sale Post Office to Swing Bridge		2.5
SEBASTAPOL BOROUGH—				
Ballarat-Rokewood Road		Resealing .. .. .		.84
"		Scarifying, reshaping, rolling, and bitumen semi-penetration .. .. .		.56
SEYMOUR SHIRE—				
Avenel-Longwood Road		General maintenance		2.2
Upper Goulburn Road		General maintenance		8.5
"		Widening, grading, filling, with pipe culverts along Goulburn River .. .. .		.98
SHEPPARTON BOROUGH—				
Shepparton-Nagambie Road		Bitumen painting southerly from Sabraon Street Reconstruction from 1,300 feet south of Sabraon Street to railway line .. .. .		.25
"		Reconstruction from railway line to Archer street .. .. .		.22
Shepparton-Nalinga Road		Reconstruction from railway line to Archer street .. .. .		.3
Shepparton-Numurkah Road		Bitumen painting northwards from 80 feet south of Lightfoot Street .. .. .		.25
"		Reconstruction from 1,220 feet north of Lightfoot Street to the Balaclava Road .. .. .		.16
Shepparton-Tatura Road		Bitumen painting from Goulburn River to Cutting Bridge .. .. .		.21
SHEPPARTON SHIRE—				
Dookie-Nalinga Road		General maintenance		6
Shepparton-Nagambie Road		General maintenance		12
Shepparton-Nalinga Road		Bitumen penetration east from Shepparton Borough boundary		2.25
"		General maintenance		15
Shepparton-Numurkah Road	Metalling north of Tallygaroopna	Resheeting near Tallygaroopna	1.23	1.15
"		Sanding south from Shire boundary		1.22
"		General maintenance		12
SOUTH BARWON SHIRE—				
Barwon Heads Road		From 7-mile finger-post towards Barwon Heads Scarifying, resheeting, and semi-penetration with bitumen from 7-mile finger-post towards Geelong		2.25
"		Spraying between 1 mile from Geelong and 7-mile post .. .. .		.56
"		General maintenance		1.99
Prince's Highway		Spraying from intersection with Barwon Heads Road to Settlement Road		12.5
"		General maintenance .. .. .		.76
SOUTH GIPPSLAND SHIRE—				
Boolarra-Foster Road	Bitumen penetration Station Road section	Patrol maintenance .. .. .	.76	1
Boolarra-Welshpool Road		Patrol maintenance		12
Falls Road		Patrol maintenance		3
Foster-Yarram Road	Blackwood Creek to Agnes Nine Mile Creek to Hedley	Patrol maintenance	2.44	5
"		Patrol maintenance	1.2	18
Main South Gippsland Road		Patrol maintenance		14
Stony Creek-Dollar Road		Patrol maintenance		8
Toora-Gunyah Road		Patrol maintenance		10
Turton's Creek Road		Culvert at Berry's and general maintenance		5
ST. ARNAUD BOROUGH—				
Avoca-St. Arnaud Road		General maintenance		1.3
Charlton Road		General maintenance		1.5
Navarre Road		General maintenance		1
St. Arnaud-Donald Road		General maintenance		2
STAWELL BOROUGH—				
Ararat-Stawell Road		Retarring		1.2
Glenorchy Road		Bitumen surfacing .. .. .		.95
"		Metalling and tarring .. .. .		.17
STAWELL SHIRE—				
Landsborough Road		Patrol maintenance		2
Marnoo Road	North of Marnoo	Callawadda to Marnoo	1.88	.24
Navarre Road	Green's Creek to Navarre	Green's Creek to Navarre	2.15	2.87
"		Patrol maintenance		18
Stawell-Glenorchy-Horsham Road	North of Glenorchy	Patrol maintenance	1.72	.8
"		Patrol maintenance		17
Stawell-Grampians Road		Patrol maintenance, Stawell to Hall's Gap		14
STRATHFIELDSAYE SHIRE—				
Bendigo-Heathcote Road		Harrowing and reshaping		1.25
"		Forming and gravelling, McIvor Hill .. .. .		.2
Mandurang Road		General maintenance		13
"		Reshaping		2.5
Strathfieldsaye Road		General maintenance		7
"		Harrowing and reshaping .. .. .		.56
"		General maintenance		8
SWAN HILL SHIRE—				
Euston Road	2 miles north of Swan Hill	Through Nyah Settlement	1	3.18
"		5 miles north of Swan Hill .. .. .		.4
Murray Valley Road	½ mile north of Wood Wood		2.94	
"	At Nyah		1.14	
"	At Boundary Bend .. .. .		.37	
"	North of Piangil		1.7	
Ultima Road		In Swan Hill .. .. .		.4
Ultima-Sealake Road	At Lalbert Creek		1.08	
TALBOT SHIRE—				
Maryborough-Avoca Road		Gravelling near Bung Bong bridge .. .. .		.5
"		Redecking bridge at Bung Bong		
Maryborough-Ballararat Road		Gravelling between Talbot Shire boundary and Clunes Borough boundary		2
"		Repairing Liverpool Arms bridge		
TAMBO SHIRE—				
Bairnsdale-Bruthen Road		General maintenance between Bruthen to Omeo Road and Omeo Highway		2
Bruthen-Omeo Road		General maintenance between Omeo Highway and Bairnsdale to Bruthen Road		1
Mossiface Road		General maintenance between Mossiface Wharf and Omeo Highway		2
Nowa Nowa-Buchan-Gelantipy Road		General maintenance between Nowa Nowa and Buchan		15
Prince's Highway		General maintenance		3
TOWONG SHIRE—				
Murray Valley Road	Approach to Bethanga Bridge	Mitta River to Burrowye Creek .. .. .	.38	45
Omeo Road		Omeo Highway to Tallangatta Railway Station		1.5
		Carried forward	96.28	3323.12

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	96·28	3323·12
TRARALGON SHIRE—		General maintenance		2·5
Callignee Road		Widening formation		16
Prince's Highway		Double-coat bitumen surfacing		26
"		General maintenance		1·5
Traralgon—Gormandale Road		Resheeting with gravel		12
"		Double-coat bitumen surfacing		8
Traralgon—Jeeralang Road		General maintenance		6
"		Double-coat bitumen surfacing		2
"		Seal coat of bitumen		42
"		General maintenance		4
TULLAROOP SHIRE—				
Avoca Road		Reconditioning		9
Ballarat Road		General maintenance		4
Castlemaine—Maryborough Road		General maintenance		16
Eddington Road		Reconditioning		15
Natte Yallock Road		General maintenance		8
TUNGAMAH SHIRE—				
Cobram South Road		General maintenance		4·36
Cobram—Strathmerton	Between Allotments 1, 2, 3, 5, 6, Parish of Yarrowonga	General maintenance	78	6·32
Nunurkah — Tungamah — Wilby Road	Between Allotments 20, 18, 34, 17, Parish of Pelluebla	General maintenance	1·01	30·7
St. James Road		General maintenance		8·98
Yarrowonga—Cobram Road	Between Allotments 29A and 70B, and 26, Parish of Cobram	General maintenance	34	14·16
UPPER MURRAY SHIRE—				
Corryong Road		Tarring through Corryong		13
"		General maintenance		16·52
Tintalra Road		General maintenance		14·5
UPPER YARRA SHIRE—				
Don Road		Reconstruction of floodway, bituminous macadam		12
"		General maintenance balance of road		1·03
Warburton Road		General maintenance		12
VIOLET TOWN SHIRE—				
Shepparton Road	Forming, gravelling, &c., approximately 2 miles from Violet Town	Patrol maintenance	1·59	4
Violet Town—Dookie Road		Patrol maintenance		18
WALPEUP SHIRE—				
Mildura Road		Bituminous seal coat		61
Ouyen—Pinnaroo Road	Metalling through Underbool	Resheeting	1·2	1·45
"	Metalling Murrayville to Carina		1·04	
"	Metalling near Ouyen	Resheeting near Ouyen		62
"	Metalling near Walpeup	Bituminous macadam, Ouyen		19
"	Metalling Cowangie to Tutye	Bituminous macadam, Walpeup		56
"	Metalling near Underbool	Bituminous macadam, Murrayville		25
"	Metalling, Walpeup to Underbool			35
WANGARATTA BOROUGH—				
Beechworth Road		General maintenance, including repairs to bridges		1
Sydney Road		General maintenance, including repairs to bridges		5·5
WANGARATTA SHIRE—				
Beechworth Road		General maintenance		12
Rutherglen Road		General maintenance		1
Wangaratta—Myrtleford Road		General maintenance		3
Yarrowonga Road		General maintenance		8
WANNON SHIRE—				
Coleraine—Harrow—Apsley Road		Gravelling near Harrow		82
"		General maintenance		34
Hamilton—Coleraine—Casterton Road		General maintenance		16
Wannon Bridge Road		General maintenance		6
WARANGA SHIRE—				
Colbinabbin—Moora Road		Resheeting at Moora		1·19
"		General maintenance		6·81
Elmore—Colbinabbin Road		Reconstruction at Colbinabbin West		55
"		General maintenance		10·45
Heathcote—Elmore Road	Between Toolleen and Heathcote	Reconstruction at Elmore	5	59
"		General maintenance		18·41
WARANGA SHIRE—				
Murchison—Rushworth Road	Between Rushworth and Moora	General maintenance	37	16
Tatura Road		General maintenance		96
WARRAGUL SHIRE—				
Bloomfield Road		General maintenance, Prince's Highway to Shire boundary		7·5
Brandy Creek Road		General maintenance, Prince's Highway East to Shire boundary		8
Darnum—Allambee Road		General maintenance, Prince's Highway southwards to Shire boundary		4·5
Warragul—Leongatha Road		General maintenance, junction of Warragul—Korumburra Road to Shire boundary		8·25
Warragul—Korumburra Road		General maintenance, Warragul Town boundary to Shire boundary at Strezlecki		12
Prince's Highway		General maintenance, eastern to western boundary of Warragul township		1
WARRNAMBOOL SHIRE—				
Caramut—Lismore Road		Sheeting and spraying		7
Garvoc—Laang Road		Sheeting and spraying		75
Framlingham Road		Sheeting and spraying		5
Mortlake Road		Sheeting and spraying		2·5
Peterborough Road	Gravel construction	Sheeting	63	25
WERRIBEE SHIRE—				
Geelong—Bacchus Marsh Road		Sheeting with metal and gravel, patrol maintenance		2·37
Prince's Highway		Bitumen surfacing		99
WHITTLESEA SHIRE—				
Epping Road		General maintenance, Preston boundary to Woodstock		10·5
Main Whittlesea Road		General maintenance, Janefield to Whittlesea Railway Station		14
Wallan Road		General maintenance, Whittlesea Station to Shire boundary		5·75
Whittlesea—Kingslake Road		General maintenance, Whittlesea station to top of Scrubby Creek deviation		10·56
		Carried forward	105·71	3760·12

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—<i>continued.</i>				
		Brought forward	105·71	3760·12
WIMMERA SHIRE—				
Dooen Road		Resheeting and bitumen penetration from Horsham Borough boundary	..	·95
Horsham—Wal Wal		Shouldering metal .. .. .	..	2·02
Natimuk Road		Forming, gravelling, and fencing at Middle Bridge	..	·01
Horsham—Hamilton Road (joint works with Arapiles Shire)		General maintenance .. .. .	..	12
Horsham—Wal Wal Road (joint works with Stawell Shire)		General maintenance	0·35	1·7
	Construction between Allotments 241 and 243, Parish of Vectis East	Constructing 2-pipe culvert .. .. .	..	..
		Shouldering and resheeting east of Allotments 24 and 24A, Parish of Bungalally	..	·75
		Dragging metal .. .. .	..	3
		Redecking bridge over Wimmera River and constructing approaches	..	·08
WINCHELSEA SHIRE—				
Birregurra—Forrest Road	Gravelling in two sections in Parishes of Whoorel and Murrone	Resheeting in Parish of Whoorel	·87	·8
Lorne Road	Gravelling in Parish of Bamba	General maintenance balance of road	..	9·2
		General maintenance, including repairs to Erskine River Bridge	·57	17·8
WODONGA SHIRE—				
Murray Valley Road		Ebden Railway Station to Mitta River	..	1·1
Sydney Road		In Wodonga Township .. .. .	..	2·03
WODONGA SHIRE—				
Tallangatta Road		In Wodonga Township .. .. .	..	·88
Wodonga—Yackandandah Road		Wodonga to Baranduda .. .. .	..	3·25
WONTHAGGI BOROUGH—				
Loch—Wonthaggi Road		General maintenance .. .. .	..	·87
Wonthaggi—Inverloch Road		General maintenance .. .. .	..	2
Wonthaggi—Korumburra Road		General maintenance .. .. .	..	·75
WOORAYL SHIRE—				
Farmers Road		General maintenance .. .. .	..	13·5
Inverloch—Leongatha Road		General maintenance .. .. .	..	16·5
Leongatha—Yarragon Road	Through the "Tangle"	General maintenance	·66	13
Lower Tarwin Road		General maintenance .. .. .	..	12·5
Main South Gippsland Road		General maintenance .. .. .	..	17·5
Mardan Road		General maintenance .. .. .	..	12
Turton's Creek Road		General maintenance .. .. .	..	5
Warragul—Leongatha Road		General maintenance .. .. .	..	7
Wild Dog Valley Road		General maintenance .. .. .	..	9
WYCHEPROOF SHIRE—				
Birchip—Sea Lake Road	Approaching Sea Lake Township	Nine miles south of Sea Lake	·46	·6
Birchip—Wycheproof Road	Approximately 3 miles west of Wycheproof	Approximately 4½ miles west of Wycheproof ..	·55	·47
Sea Lake—Ultima		In Sea Lake Township .. .. .	..	·15
YACKANDANDAH SHIRE—				
Dederang Road	Culvert and approaches	General maintenance	·08	20
Gundowring Road		General maintenance .. .. .	..	22
Kiewa—Wodonga Road	Earthworks and gravelling deviation near Kiewa	General maintenance and resheeting	21	6
Yackandandah—Wodonga Road		General maintenance .. .. .	..	15·75
YARRAWONGA SHIRE—				
Tungamah—Wilby Road		General maintenance .. .. .	..	·75
Wangaratta—Yarrowonga Road		Double-coat bitumen surfacing .. .. .	..	..
Wangaratta—Cobram Road		General maintenance .. .. .	..	20
Wangaratta—Rutherglen Road		Double-coat bitumen surfacing .. .. .	..	2
		Patrol maintenance .. .. .	..	10
		General maintenance .. .. .	..	1
YEA SHIRE—				
Upper Goulburn Road	Yea River concrete bridge and approaches	West of Kerrisdale to Seymour boundary ..	·12	·5
" " " "		Terangavill Hill .. .. .	..	·52
" " " "		East of second concrete bridge near Yea ..	..	·48
" " " "		In Yea Township .. .. .	..	2
" " " "		Gardiner's Hill .. .. .	..	·25
Yea—Glenburn Road	Near Devlin's bridge	Kitty's Creek bridge and approaches	·55	·1
		Total	130·92	4027·08

UNDER DIRECT SUPERVISION OF THE BOARD.

ARARAT SHIRE—				
Ballarat—Hamilton Road	Completion of new steel concrete superstructure bridge over Fiery Creek	.. .. .	..	..
BALLARAT AND BUNGAREE SHIRES—				
Ballarat—Creswick Road		Supply and delivery of 4,150 cubic yards of gravel	..	..
BANNOCKBURN AND BUNINYONG SHIRES—				
Geelong—Ballarat Road		Supply and delivery of 5,000 cubic yards of gravel	..	..
BELLARINE SHIRE—				
Geelong—Queenscliff Road		Supply of 950 cubic yards of gravel .. .. .	..	..
" " " "		Supply and delivery of 2,500 cubic yards of gravel between Wellington Hotel and Queenscliff boundary	..	..
" " " "		Supply of 137 cubic yards of 2½ metal, 510 cubic yards of screenings, and 165 cubic yards of topplings	..	..
Geelong—Portarlington Road		Supply and delivery of 1,650 cubic yards of metal, 890 cubic yards of screenings, and 275 cubic yards of topplings	..	..
BERWICK AND FERNTREE GULLY SHIRES—				
Emerald Road	Reforming and grading near Avonleigh House	.. .. .	1·83	..
BET BET SHIRE—				
Avoca—Bealiba Road	Erection of reinforced concrete bridge near Archdale	.. .. .	·01	..
		Carried forward	1·84	..

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>				
		Brought forward	1·84	..
CASTLEMAINE BOROUGH— Melbourne-Bendigo Road	Supply and delivery of 1,650 cubic yards of gravel in Castlemaine Borough	..	..
..	Supply and delivery of 200 cubic yards of gravel	..	..
..	Re-forming, re-shaping, and re-sheeting with gravel and sealing	..	1·05 (day labour)
CRANBOURNE SHIRE— Main Coast Road	Construction of timber bridge 50 feet long over Yallock Creek	..	..
..	Construction of single cell reinforced concrete culvert with approaches	..	..
DAYLESFORD BOROUGH— Ballarat Road	Construction of reinforced concrete bridge and approaches over Wombat Creek near Daylesford	..	..
..	Foundation work, Leggatt's Bridge	..	..
ELTHAM SHIRE— Yarra Glen-Glenburn Road	Forming and grading .. .. .	..	..
..	Grubbing, clearing, forming, grading, and gravelling	1·26 ·73	..
GISBORNE SHIRE— Melbourne-Bendigo Road	Reconstruct in asphaltic macadam	..	..
HEYTESBURY SHIRE— Cobden-Port Campbell-Princetown Road	Clearing, forming, and metalling	..	..
Timboon-Port Campbell Road	Bituminous spraying .. .. .	..	..
LILYDALE SHIRE— Main Healesville Road	Grubbing, clearing, forming, metalling and penetrating with bitumen between "The Grange" and the Yarra River	..	..
..	Widening, re-forming, and shaping from near Mt. Evelyn Station easterly	..	..
MONBULK ROAD	1·09 (day labour)	..
MANSFIELD SHIRE— Mansfield-Wood's Point Road	Construction of single span timber bridge over Goulburn River in Township of Wood's Point	..	..
NEWHAM AND WOODEND SHIRE— Melbourne-Bendigo Road	Widening, forming, and surfacing with bituminous macadam through the Township of Woodend	..	..
..	Sealing with Trinidad of bituminous macadam in the Township of Woodend	..	1·1 (day labour)
ORBOST SHIRE— Orbost-Delegate Road	Grubbing, clearing, forming, and three timber bridges near Goongerah Creek and Brodribb River	..	..
..	Timber bridge over Martin's Creek	..	..
..	Clearing, grading, forming, and draining	..	..
..	2·67 (day labour)	..
OTWAY SHIRE— Colac-Beech Forest Road	Grubbing, clearing, forming, and trimming	..	..
..	Grubbing, clearing, forming, and grading	..	..
..	Grubbing, clearing, forming .. .. .	..	..
..	Forming, grading, side-cutting, rolling, and reinforced concrete culverts	..	..
..	Re-forming and grading	..	..
..	2·75	..
PYALONG SHIRE— Kilmore - Heathcote - Bendigo Road	Construction of reinforced concrete bridge over Mollison's Creek (five-span) near Township of Pyalong	..	..
SEBASTOPOL BOROUGH— Ballarat-Rokewood Road	Supply and delivery of 500 cubic yards of metal and 200 cubic yards of gravel	..	..
SEYMOUR SHIRE— Seymour-Yea Road	Supply and delivery of 1,200 cubic yards of gravel	..	..
..	Construction of two-span timber bridge over Fall's Creek	..	..
..	·02	..
SWAN HILL SHIRE— Piangil Station Road	Forming, re-forming, gravelling, and metalling	..	..
..	1·47	..
TULLAROOP SHIRE— Castlemaine-Maryborough Road	Supply and delivery of 10,000 cubic yards of gravel between Carisbrook and Joyce's Creek	..	..
..
TULLAROOP AND MALDON SHIRES— Eddington Road	Erection of post and wire fencing	..	..
..
UPPER YARRA SHIRE— Main Warburton Road	Supply and delivery of 5,000 cubic yards of metal, 1,050 cubic yards of screenings, and 300 cubic yards of toppings between Launching Place and Upper Yarra	..	..
..	Construction of timber bridge over McMahon's Creek	..	..
..	·02
WOOD'S POINT ROAD
WOORAYL SHIRE— Wild Dog Valley Road	Grubbing, clearing, forming, grading, and draining	..	..
..
..	·83 (day labour)	..
		Total	42·86	2·21

APPENDIX J.

COUNTRY ROADS BOARD.

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED ON DEVELOPMENTAL ROADS DURING THE YEAR ENDED 30TH JUNE, 1930.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads—			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER MUNICIPALITIES.						
ALBERTON SHIRE—						
Albert River Road	Gravelling near Stacey's Bridge and McAuley's	7	..	..	..	..
Traralgon-Balook Road	Gravelling between Morris's and Balook	4.2	..	..	..	..
Binginwarri-Albert River Road	Earthworks through Turnbull's and Cantwell's	1.5	..	..	..	..
Binginwarri-Welshpool Road	Gravelling near W. N. Ross's	2.23	..	1.5	..	..
Blackwarri-Yarram Road .. .. .	.. .. .	..	3	3	..	..
Carraung-Balook Road	Forming and gravelling Balook to Blackwarri	4.5	..	4.5	4.5	4.5
Carraung-Gormandale Road	Forming and gravelling at Won Wron and Boodyarn	2.7	8	8	8	8
Gelliondale Road	Gravelling McGalliard's deviation	1	1	1	1	1
Lay's Road	Metalling from Lay's to Wilson's	4	4	4	4	4
Madalya Road	Earthworks from Wine's to Anderson's	2	..	..	..	..
Whitelaw's Track	Gravelling from Alford's to Thomas's	2	..	2	2	2
ALEXANDRA SHIRE—						
Terip Terip Road	At top of range Allotments 1 and 2, Parish of Dropmore	33	..	..	..	..
ARAPILES SHIRE—						
Miga Lake-Gymbowen Road	At Miga Lake .. .. .	..	5	5	5	5
Arapiles-Grassy Flat Road	At Arapiles .. .. .	..	75	75	75	75
BACCHUS MARSH SHIRE—						
Parwan Road	East from Parwan Railway Station	42	1.06	1.06	1.06	1.06
BAIRNSDALE SHIRE—						
Bullunwaal-Tabberabbera Road	Near Wentworth River	69	69	69	69	69
Bairnsdale-Bengworden Road	At Bengworden .. .. .	..	29	29	29	29
BASS SHIRE—						
Dalyston-Glen Forbes Road	At Ryanston	3.24	..	3.24	3.24	3.24
Glen Alvie Road	At Glen Alvie	94	..	77	2	2
Wonthaggi-Loch Road	At Glen Alvie	1.42	..	..	..	..
BEECHWORTH SHIRE—						
Hillsborough Road	Deviation Ferguson's on Mount Stanley section	3	..	..	..	..
BENALLA SHIRE—						
Mollyullah-Tatong Road .. .. .	.. .. .	1.25	1.25	1.25	1.25	1.25
BERWICK SHIRE—						
Beaconsfield-Emerald Road	Metalling southerly from the Shire boundary at Emerald	35	35	35	35	35
Happy Valley Road	Forming at Dewhurst	36	36	36	36	36
BIRCHIP SHIRE—						
Curyo West Road	West from Curyo Railway Station	45	..	..	..	..
BORUNG SHIRE—						
Galaquil West Road	Limestone, 1½ miles west of Galaquil	63	..	..	..	..
Brim West Road	Limestone, 2¼ miles west of Galaquil	73	..	..	..	..
Brim East Road	Limestone gravel ½ mile west of Brim	2	1.42	1.42	1.42	1.42
Lah West Road	Metalling 1 mile from Brim .. .. .	..	91	91	91	91
Donald-Warracknabeal Road	Gravelling 1 mile west of Lah	85	..	..	..	..
Boofite-Sheephills Road	Gravelling 3¼ miles from Warracknabeal	75	..	..	..	..
Bright Shire—	Gravelling 4 miles from Warracknabeal	54	54	54	54	54
Kiewa Valley Road	Gravelling 1½ miles east of Sheep Hills	92	..	..	..	..
Happy Valley Road	Earthworks and metalling section between peg 37,900 and 47,300	8	95	95	95	95
Myrtleford-Yackandandah Road	Gravelling 30,500 to 40,110, 45,000 to 50,000	2.82	3.6	1.82	1.82	1.82
Myrtleford-Yackandandah Road	Earthworks and sanding in Mudgegonga Township	1	1	1	1	1
BULN BULN SHIRE—						
Duggan North Road	Through Allotments 112 and 115, Parish of Fumina	1.72	1.72	1.72	1.72	1.72
Turner's Road	Opposite Allotment 93, Parish of Fumina	19	19	19	19	19
Fumina Road	Through Allotments 206A and 67, Parish of Fumina	1.1	1.1	1.1	1.1	1.1
Mountain View Road	Through Allotments 2 and 18, Parish of Poowong East	37	37	37	37	37
Apteds Road	Through Allotments 41 and 41A, Parish of Neerbin	18	18	18	18	18
Poowong Road	Opposite Allotment 53A, Parish of Longwarry	62	62	62	62	62
Rokeby-North Jindivick Road	Opposite Allotment 6, Parish of Jindivick	95	95	95	95	95
CHARLTON SHIRE—						
Yeungroon Road	Metalling south from existing metal	82	82	82	82	82
Teddywaddy Road	Gravelling final section west from Teddywaddy	59	1.55	1.55	1.55	1.55
Lake Marmal Road	Gravelling 7 miles from Charlton	59	59	59	59	59
COHUNA SHIRE—						
Murray River Valley Road	Between Cohuna and Gunbower	8.6	..	8.6	8.6	8.6
Cohuna-Leitchville Road	Between Cohuna and Leitchville	4.23	..	..	..	..
Cohuna-McMillans Road	At McMillans	1.02	..	..	..	..
Cohuna-Mead Road	Southerly from Kerang Road towards Mead	73	73	73	73	73
Gannawarra Road	2 miles from Cohuna	1	1	1	1	1
COLAC SHIRE—						
Colac-Forrest Road	Between Boundary Creek and Shire boundary	2.24	1.68	1.68	1.68	1.68
CORIO SHIRE—						
Geelong-Bacchus Marsh Road	At Woolloomanatta	2.69	..	..	..	..
CRANBOURNE SHIRE—						
Pearcedale Road	From junction of Frankston Road to Donnolly's	1.87	1.87	1.87	1.87	1.87
.. .. .	From Donnolly's to Allotment 62, Parish of Langwarrin	87	87	87	87	87
DEAKIN SHIRE—						
Echuca-Wyuna Road	Reforming and gravelling between Allotments 55A1 and 63A1 and 63C and D, Parish of Echuca North	81	81	81	81	81
Tongala East Road	Reforming and gravelling between Allotments 80, 81, and 88, 89A and B, Parish of Tongala	61	61	61	61	61
Tongala West Road	Reforming and gravelling between Allotments 18, 18A, and 19, Parish of Koyuga	39	39	39	39	39
Kyabram West Road	Reforming and gravelling between Allotments 142A and 151A and C, Parish of Kyabram	28	28	28	28	28
DIMBOOLA SHIRE—						
Glenlee-Jeparit Road	Between Jeparit and Glenlee	6	77	77	6	6
Pepper Plains Road	East from Dimboola-Jeparit railway	37	37	37	37	37
DONALD SHIRE—						
Watchem-Warracknabeal Road	Two miles from Watchem	76	76	76	76	76
Corack East-Donald Road	½ mile south of Corack Township	87	87	87	87	87
Jeffcott Road	Opposite Coat's	63	63	63	63	63
Rich Avon Road	Opposite Rich Avon Estate and Broughton's	94	1.94	1.94	1.94	1.94
Sheep Hills Road	3 miles from Donald	98	98	98	98	98
Litchfield Road	1 mile from Carron	76	76	76	76	76
Carried forward .. .. .	.. .. .	77.81	42.28	60.2	53.41	52.41

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads—			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER MUNICIPALITIES—<i>continued</i>						
	Brought forward	77·81	42·28	60·2	53·41	52·41
DUNDAS SHIRE—						
Kanawalla-Noske Road	Opposite Allotment 2A, Section 21, and Allotment 3B, Section 20, Parish of Kanawalla, about 1½ miles north of Kanawalla Railway Station	·44	..	..	..	..
DUNMUNKLE SHIRE—						
Murtoa North Road	Starting 3 miles north of Murtoa	1·32	1·32	1·32	1·32	1·32
Minyip-Kewell Road	Starting 1½ miles west of Minyip	1·17	1·17	1·17	1·17	1·17
Banyena Road	From Banyena Siding northwards	2·07	2·07	2·07	2·07	2·07
ELTHAM SHIRE—						
Cottles Bridge-Strathewan Road ..	Along Allotments 1 and 4, Parish of Queenstown; along Allotment 1, Parish of Kinglake	·64	·64	·64	·64	·64
FERNTREE GULLY SHIRE—						
Emerald-Macclesfield Road	Between Avonsleigh and Macclesfield	·43	·43	·43	·43	·43
FLINDERS SHIRE—						
Bittern-Dromana Road	At Moat's Corner	·46	·46	·46	·46	·46
"	Warnecke's Deviation	·31	·31	·31	·31	·31
Brown's Road	Bridge and approaches	·04	·04	·04	·04	·04
"	West of Boneo Road	·76	·76	·76	·76	·76
Red Hill Road	Near McSwain's	·16	·16	·16	·16	·16
Boneo Road	At Rosebud	1·02	1·02	1·02	1·02	1·02
FRANKSTON AND HASTINGS SHIRE—						
Hodgins Road	Near Shire boundary in Parish of Bittern	·31	·31	·31	·31	·31
GLENELG SHIRE—						
Glenorchy Estate Road	Gravelling near McPhee's	·14	..	..	..	..
Meriuo-Sruan-Tahara Road	Metalling near Paschendale Hall	·32	..	..	..	..
"	Gravelling Harvey's Flat	·26	..	..	..	..
Dergholm-Elderslie Road .. .. .	" .. .. .	..	·24	..	..	..
GLENLYON SHIRE—						
Daylesford-Trentham Road	Bullarto to Musk	·99	..	..	..	..
Porcupine Ridge Road	The Ridge	·35	1·17	1·17	·93	·93
South Bullarto Road	East and West ends	·35	·7	·7	·7	·7
GORDON SHIRE—						
Barraport West Road	West from the Barraport Railway Station	·73	..	..	..	..
GRENVILLE SHIRE—						
Gillett's Road	From Cressy Road east	1·1	1·93	1·93	1·1	1·1
Pittong Road	"	2·38	11·36	9	2·38	2·38
HAMPDEN SHIRE—						
Cundare-Duverney Road	Forming and metalling west of Allotments 46A and 46B, Parish of Poliah South	..	..	·6	·6	·6
Foxhow Road	Forming and gravelling south-west of Allotments 107 and 108, Parish of Corangamite	·6	1·51	1·51	·6	·6
Vite Vite Road	Forming and metalling through Allotments 10A, 11B, 11A, 14A, 13B and 20A, Parish of Carramballue South	..	1·5	1·5	1·5	1·5
HEALESVILLE SHIRE—						
Healesville-Toolangi Road	Between Toolangi Hotel and Post Office	·4	·81	·81	·81	·81
Myers Creek Road	Between Allotments 73C, Parish of Tarrawarra, and 21, 22, Parish of Monda	·23	·23	·23	·23	·23
Buxton-Marysville Road	Between Allotments 12A and 16, 24 and 26, Parish of Buxton	·25	·25	·25	·25	·25
HEYTESBURY SHIRE—						
Ayersford Road	Metalling south from Ayersford Bridge	·45	..	·45	·45	·45
Timboon-Cowleys Creek Road	Gravelling by Morgan's through State Forest	·8	..	..	..	..
"	Imestone metalling LeCouteur's outlet	·34	..	·34	·34	·34
South Ecklin " Road	Metalling by Stone's towards Lake Elingamite	·56	..	·6	·6	·6
"	Metalling by Scanlon's towards Lake Elingamite	·6	..	·6	·6	·6
Glenfine West Road	Forming and metalling by Findlow's	·68	..	..	..	..
"	Forming and metalling by Love's Bridge	·34	·34	·34	·34	·34
Devil's Gully Road	Reforming and metalling through Cole's	1·27	1·27	1·27	1·27	1·27
Timboon-Digney's Bridge Road	Reforming and metalling by Browne's	·01	..	·01	·01	·01
Timboon-Scott's Creek Road	Reforming and gravelling by Ley's Bridge	·01	..	·01	·01	·01
HUNTLY SHIRE—						
Diggora Road	East of Hunter Railway Station	·53	1·11	1·11	·53	·53
Drummartin Road	West of Picaniny Creek	·4	1·5	1·5	·4	·4
KARA KARA SHIRE—						
Marnoo-St. Arnaud Road	At Marnoo East	·45	·96	·96	·45	·45
Swanwater Road	North of Swanwater Railway Station .. .. .	..	·45	..	..	..
Sandy Creek Road	In Parish of Carapooee West .. .. .	..	1·15	..	..	..
KARKAROO SHIRE—						
Yarto-Patchewoolock Road	Metalling at Willa	1·62	..	..	..	..
Hopetonn-Yaapeet Road	Metalling between 2 and 7 mile pegs	1·84	..	..	..	..
"	Metalling between Allotments 5 and 6, Parish of Gaalanungah	..	..	..	1·14	..
Yaapeet-Nyppo Road	Metalling between Allotments 31, 7, and 29, 10, Parish of Nyppo	·83	..	..	..	..
"	Metalling between Allotments 7 and 29, Parish of Nyppo ..	·19	..	..	..	·19
KERANG SHIRE—						
Murrabit Road	Parish of Kerang	·7	7	..	..	..
Winlaton Road	Parish of Boga	1·3	..	..	..	..
Murray River Valley Road	Parishes of Boga and Danlagook .. .. .	..	2·75	2·75	2·75	2·75
KORONG SHIRE—						
Wedderburn-Spring Hill Road	Metalling and gravelling in three detached sections ..	1·2	..	..	..	..
Mysia West Road	Gravelling near Allotments 123, 124, Parish of Mysia ..	·48	..	..	..	..
Kinypaniel Road	Gravelling near Allotments 110, 127, Parish of Kinypaniel..	·56	..	..	..	..
Inglewood North Road	Forming and grading through Eucalyptus Reserve	2·28	..	..	..	..
Buckrabanyde South Road	Gravelling near Allotments 31 and 32, Parish of Woosang..	·11	..	..	..	..
KORUMBURRA SHIRE—						
New Territory Road	North from Korumburra-Warragul Road	·23	·23	·23	·23	·23
Poowong-Olsen Road	Near Pheasant Creek	·22	·22	·22	·22	·22
Korumburra South Road	Hopkins to Whitesides	1·2	1·2	1·2	1·2	1·2
Bena-Kongwak Road	Various sections	1·1	·23	·23	·23	·23
Sheepways Road	Near Brown's	·56	·56	·56	·56	·56
KOWREE SHIRE—						
Benayeo Road	Near Benayeo	·33	·33	·33	·33	·33
Edenhope-Natimuk	Near Maryvale	·39	..	..	..	..
Elderslie Road	Near Meecreek and Mundarra	1·3	·12	·12	·12	·12
Little Desert Road	Near Boorooptki and Lemon Springs	2·16	1·49	1·49	1·49	1·49
Minimay Road	Near Minimay	1·01	·89	·89	·89	·89
Miga Lake-Gymbowen Road	Near Gymbowen	·34	·34	·34	·34	·34
KYNETON SHIRE—						
Baynton Road .. .. .	" .. .. .	..	·57	·57	·5	..
LAWLOIT SHIRE—						
Miram West Road	Gravelling west of Miram	1·42	..	1·42	1·42	1·42
Little Desert Road	Gravelling between Kaniva and Little Desert .. .. .	..	..	1·66	1·66	1·66
LILLYDALE SHIRE—						
Chalet Road	At Olinda	·55	..	..	..	..
Monbulk-Seville Road	At Seville	·56	..	..	..	..
	Carried forward	123·96	93·38	105·19	88·68	86·23

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Con- structed.	Lengths of Roads—				
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.	
		miles.	miles.	miles.	miles.	miles.	
UNDER MUNICIPALITIES—continued							
	Brought forward..	123·96	93·38	195·19	88·68	86·23	
LOWAN SHIRE—							
Netherby Road ..	Between Allotments 42 and 25-26A, Parish of Lorquon ..	·45	·45	·45	·45	·45	
Winlam Road ..	Between Allotments 64 and 48-49, 84 and 58-53, Parish of Winlam ..	1·74	1·74	1·74	1·74	1·74	
Yanac South Road ..	Between Allotments 27 and 92-47, Parish of Yanac ..	·45	·45	·45	·45	·45	
MAFFRA SHIRE—							
Mewburn Park Road ..	Opposite Coleman's and Phelan's ..	·45	..	..	..	..
Bundalaguah Road ..	Opposite Gray's and Dingwell's ..	1·4	..	·5	..	..
MANSFIELD SHIRE—							
Benalla-Mansfield Road ..	Sections 8 and 9 at Crown Allotments 48B and 50 and Forests deviation, Parish of Nillahcootie ..	2·39	2·39	2·39	1·2	1·2	
Mansfield-Tolmie Road ..	From 18 to 19 miles from Mansfield ..	1	1	1	1	1	
" " " " ..	At 10 miles from Mansfield ..	..	2	..	..	..
MARONG SHIRE—							
Yarrarberb Road ..	Easterly from north-east corner of Yarrarberb Homestead block ..	·19	·19	·19	·19	·19	
Newbridge-Sheibourne Road ..	Westerly from Sheibourne Railway Station ..	·15	·15	·15	·15	·15	
					(Day labour)		
MELTON SHIRE—							
Exford Road ..	From present construction towards Werribee River ..	·4	·3	·3	..	..
MILDURA SHIRE—							
Red Cliffs East Road ..	Between Red Cliffs Township and pumping station ..	·7	1·25	1·25	·7	·7	
Red Cliffs South-East Road ..	Between Red Cliffs Township and Block 147 ..	·7	1	1	·7	·7	
Red Cliffs West Road ..	Between Red Cliffs Township and Cardross ..	·93	1·5	1·5	·93	·93	
MINNAMITE SHIRE—							
Orford-St. Helen's Road ..	South of Allotment 131, Parish of St. Helen's ..	1·02	..	..	..	..
Condah-Macarthur Road ..	South of Allotments 3 of Section XI, and 4 of Section XII, and south of Allotment 4 of Section XIV, Parish of Weerangourt ..	1·18	..	..	..	..
" " " " ..	South of Allotment 3 of Section XXXIII, Parish of Kangertong ..	·4	1·3	1·3	·72	·72	
Nardoo Road ..		..	·64	·64	..	..
MIRBOO SHIRE—							
Nicholls Road ..	Metalling from Campbell's to W. J. Trease ..	1·2	1·2	1·2	1·2	1·2	
" " " " ..	Metalling to Shire boundary ..	..	1·25	1·25	..	..
Mirboo North-Thorpdale ..	Sanding two sections ..	1·1	1·1	1·1	1·1	1·1	
Mirboo-Boolarra Road ..	Sanding from Kennedy's to Murray's ..	2·6	2·6	2·6	2·6	2·6	
" " " " ..	Metalling from Kennedy's to Clark's ..	1·9	1·9	1·9	1·9	1·9	
" " " " ..	Sanding from Clark's to Murray's ..	·7	·7	·7	·7	·7	
" " " " ..	Earthworks and sanding from Township to Kennedy's ..	..	2	2	..	..
Boorool Road ..	Metalling in sections ..	·25	·25	·25	·25	·25	
Clear Creek Road ..	Sanding through Allotment 100C, Parish of Mirboo ..	·3	·3	·3	·3	·3	
Mirboo-Yarragon Road ..	Metalling from Jervis's to Shire boundary ..	3·2	3·2	3·2	3·2	3·2	
MORELAKES SHIRE—							
Vite Vite Road ..	Metal formation from Pura Pura towards Mt. Emu Creek ..	1·33	..	1·33	1·33	1·33	
" " " " ..	Metal formation north-westerly from Pura Pura ..	..	..	·67	·67	·67
MORWELL SHIRE—							
Tyers Road ..	Sanding and gravelling between Tyers and Latrobe Rivers, Parish of Tanjil East ..	..	1·96	1·01	1·01	1·01
Boolarra-Morwell Road ..	Sanding from 8,900 to 16,000 ft., Parish of Yinnar, and 3,300 ft. on Guthrie's Hill, Parish of Budgerec ..	1·34	1·57	1·57	1·57	1·57	
Thorpdale East Road ..	Forming ..	1·88	3·7	·95	·95	·95	
" " " " ..	Sanding from 26,300 to 26,800 ft. ..	..	..	·38	·38	·38
Middle Creek Road ..	Deviation and timber bridge at Vagg's Creek ..	..	·05	·05	·05	·05
MCIIVOR SHIRE—							
Baynton Road ..	Between Kyneton Shire boundary and Glenhope P.R. section ..	1·41	..	..	..	..
NARRACAN SHIRE—							
Trafalgar-Willowgrove Road ..	Opposite Allotments 17 and 7A, Parish of Neerim ..	1·08	1·08	1·08	1·08	1·08	
Childers-Thorpdale Road ..	In Thorpdale Township ..	·22	·22	·22	·22	·22	
Allambee-Thorpdale Road ..	Through Allotments 145, 51, 74 and 75, Parish of Allambee ..	·61	·61	·61	·61	·61	
Coalville-Narracan Road ..	Through Allotments 104 and 106, Parish of Narracan ..	·91	·91	·91	·91	·91	
Mirboo-Yarragon Road ..	Through Allotments 16, 17C and 29, Parish of Allambee East ..	1·3	1·3	1·3	1·3	1·3	
Thorpdale-Yarragon Road ..	Through Allotment 13, Parish of Moe ..	·7	·7	·7	·7	·7	
Sunny Creek Road ..	Opposite Allotment 41, Parish of Moe ..	·53	·53	·53	·53	·53	
Shady Creek Road ..	Opposite Allotments 96 and 98, Parish of Darnum ..	..	·62	·62	·62	·62
NEWHAM AND WOODEND SHIRE—							
Macedon Village Settlement Road ..	Forming through Allotments 7, 8, and 9, Section A, Parish of Newham ..	·41	..	..	..	..
Campaspe Road ..		..	..	·47	·47	·47
NEWSTEAD AND MOUNT ALEXANDER SHIRE—							
Glenbower-Joyces Creek Road ..	Dodson's Corner, opposite Allotment 9, Glenbower Estate ..	·38	·38	·38	·38	·38	
NUMURKAH SHIRE—							
Waal North Road ..	Gravelling between Cowan's and Tweddle's ..	1·14	..	..	..	..
" " " " ..	Gravelling between Cowan's and Breusing's ..	..	·38	·38	·38	·38
Wunghnu East Road ..	Gravelling, &c., near Watter's ..	..	·43	·43	·43	·43
OMELO SHIRE—							
Beloka Road ..	Extension of "Sargent's" Track ..	·3	·3	·3	·3	·3	
Bindi Road ..	Gravelling north of Tambo River ..	·57	..	..	..	..
Brookville Road ..	Extension of "Shelton's Gap" deviation ..	..	·68	·68	·68	·68
Reedy Creek Road ..	Within Ensay Township ..	·48	·48	·48	·48	·48	
Sandy Creek Road ..	Through "Angns's", near "new lue" ..	·33	·33	·33	·33	·33	
ORBOST SHIRE—							
Comblenbar Road ..		..	..	·59	·59	·59
Bete Bolong-Waygara Road ..	Through Russell's and Brew's ..	1·07	1·07	1·07	1·07	1·07	
Mallacoota West Road ..	Sanding near Cape Horn Creek ..	·69	1·89	1·89	·69	·69	
Groves Road ..		..	·16	·16	..	..
Lower Bemn Road ..	Sanding between 1 and 3 mile posts ..	1·1	1·1	1·1	1·1	1·1	
Tamboon Road ..	Sanding between School and Rothe's ..	·49	·49	·49	·49	·49	
Lower Tonghi Road ..		..	·66	..	..	..
OXLEY SHIRE—							
Carboor-Meadow Creek Road ..	Section between Holley's and Russell's ..	..	·7	·7	·4	..
Fifteen Mile Creek Road ..	Mason's deviation ..	1·8	..	..	..	..
" " " " ..	Bridge over Middle Creek ..	·01	·01	·01	·01	·01	
King Valley Road ..	Section between Edi Cutting and Hyem ..	·5	·9	·9	·9	·9	
Rose River Road ..	Section between Red Gate and Nella's ..	1·1	..	..	..	..
PORTLAND SHIRE—							
Heath Road ..	Connection with Grubbed Road ..	1·03	1·03	1·03	1·03	1·03	
Grubbed Road ..	Through State Forest, end of existing metal ..	4	4	4	4	4	
PYALONG SHIRE—							
Lancefield-Tooborac Road ..	From Lehaire's Bridge through Reserve, and along Patterson's deviation ..	..	1·56	..	..	..
RIPON SHIRE—							
Trawalla West Road ..	At Mount Emu ..	1·32	..	..	..	..
	Carried forward ..	176·79	152·04	159·87	133·82	130·97	

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—continued.

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			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER MUNICIPALITIES—continued.						
	Brought forward	179.79	152.04	159.87	133.82	130.97
ROCHESTER SHIRE—						
Echuca West Road	Between Allotments 62 and 65, Parish of Millewa ..	7.22	.28	.28	.28	.28
Corop Road	Between Allotments 167 and 166, 168A, Parish of Nanneella ..	2.2	.54	.54	.54	.54
Cotta East Road	Between Allotment 9, Parish of Torumbarry, and Allotments 9, 12, and 13, Parish of Bamawm ..	1.95	.58	.58	.58	.58
RODNEY SHIRE—						
Tatura-Toolamba Road ..	Metalling between Allotments 36 and 68, Parish of Toolamba ..	.24	.9	.9	.24	.24
Mooroopna-Undera Road ..	Gravelling between Allotments 10, 9, 17, and 20, Parish of Undera, and Allotments 13 and 13A, Parish of Coombona ..	1.09	1.09	1.09	1.09	1.09
ROMSEY SHIRE—						
Baynton Road	North of Allotment 27, Parish of Lancefield .. ..	.3	.11	.11	.11	.11
RUTHERGLEN SHIRE—						
Black Swamp Road	Forming and sanding near Shea's .. ..	.15	..	..	..	..
SHEPPARTON SHIRE—						
Pine Lodge North Road ..	South from Shire boundary .. ..	.57	.57	.57	.57	.57
Grahamvale Road	Near High School farm .. ..	.3	.3	.3	.3	.3
SOUTH GIPPSLAND SHIRE—						
Waratah Road	Gravelling near Fish Creek .. ..	.77	.77	.77	.77	.77
Franklin River Road	Forming Holdensens towards Fish Creek at Henwood's ..	1.27	.1	.1	.1	.1
McCartin's Road	Extension to McMaster's Saddle .. ..	.49	..	..	..	..
Binginwarri South Road ..	Gravelling to Shire boundary and near Hunt's ..	.65	.28	.28	.28	.28
Dollar-Foster Road	Metalling near Mount Rumbug and Amey's Track ..	1.1	..	..	..	..
Whitelaw's Track Road ..	Forming Falls Road to Bright's, and gravelling sections ..	3.24	1.31	1.31	1.31	1.31
Port Franklin Road	Gravelling Foster-Yarram Road to Bennison Railway Station ..	..	2.04	2.04	2.04	2.04
Toora-Gunyah Road	Blinding to Mount Fatigue deviation .. ..	..	1.8	1.8	1.8	1.8
" " " " " " " "	Gravelling Mount Best to Blunderns	1.15	1.15	1.15	1.15 (Day labour)	1.15
O'Grady's Ridge Road	Gravelling from Main South Gippsland Road .. ..	..	.79	.79	.79	.79
Toora-Wonyip Road	Gravelling the Tin Mine to Bjorkstein's	2.28	2.28	2.28	2.28	2.28
STAWELL SHIRE—						
Pomonal Road	Stawell to Pomonal .. ..	..	4	4	..	..
Marnoo-Rupanyup Road ..	North-west of Marnoo	2.2	2.2	2.2	2.2	2.2
Marnoo-St. Arnaud Road ..	East of Marnoo .. ..	.34	.34	.34	.34	.34
SWAN HILL SHIRE—						
Kooloonong East Road ..	East from Kooloonong .. ..	.83	..	..	..	..
Manangatang-Euston Road ..	At Koimbo Station .. ..	.33	..	..	..	..
TAMBO SHIRE—						
Buchan-Gelantipy Road ..	Reforming and gravelling 3 miles north from Buchan-township ..	1	1	1	1	1
TOWONG SHIRE—						
Yabba Road	Formation, fencing, and culvert, &c., at Tallandoon ..	.35	.35	.35	.35	.35
Shelley-Jungellic Road ..	Boxing and metalling 9 miles from Shelley	1.21	1.21	1.21	1.21	1.21
TRARALGON SHIRE—						
Traralgon-Jeeralong Road ..	Widening from 9,280 ft. to 14,500 ft. .. ..	.99	..	..	..	..
TUNGAMAH SHIRE—						
Yarroweyah-Toomwal Road ..	Between Allotments 6, 7, and 12, 13, Parish of Yarroweyah ..	1.81	1.81	1.81	1.81	1.81
Wungnhu-Younamite Road ..	Between Allotments 45, 6, 22 and 23, Parish of Younamite ..	.9	.9	.9	.9	.9
Katandra Road	Between Allotments 15, 17B, and 17C, Parish of Yabba ..	.4	.4	.4	.4	.4
Yabba South Road	Opposite Allotment 51, Parish of Yabba .. ..	.3	.3	.3	.3	.3
Yabba North Road	Between Allotments 19A, 20, 1 and 27, and Allotments 55, 55A, 53, 3 and 48, 49B, Parish of Yabba ..	1.86	1.86	1.86	1.86	1.86
Boweya Road	Township of Karrabumet, and Allotments 28B, 28C, 21E, Parish of Karrabumet ..	.7	.7	.7	.7	.7
UPPER MURRAY SHIRE—						
Murray Valley Road	Forming and grading south of Allotments 1 and 1D, Section 1A, Parish of Tintaldra ..	.55	.55	.55	.55	.55
" " " " " " " "	Forming, grading, and sanding between Allotments 9, 10, and 22, Section 1A, Parish of Tintaldra ..	..	.72	.72	.72	.72
Beetoomba Road	Forming, grading and gravelling east of Allotments 7A and 6 of A, and 23 of A, Parish of Berrigama ..	..	.54	.54	.54	.54
UPPER YARRA SHIRE—						
Woori Yallock-Cockatoo Road ..	From Shepherd's Creek, Allotment 11, Parish of Nangana, to Allotment 47, Parish of Woori Yallock ..	3.13	..	..	..	..
Woods Point Road	At Braham's Creek, Allotment 42A, Parish of Warburton, and at Quarry near Melbourne and Metropolitan Board of Works bridge ..	.19	..	..	..	..
Woori Yallock-Cockatoo Road ..	From Allotment 2, Parish of Woori Yallock, to Township of Yellingbo ..	1.69	1.72	1.72	1.69	1.69
" " " " " " " "	From Allotment 18, Parish of Woori Yallock, to Allotment 47, Parish of Woori Yallock ..	..	1.2	1.2	..	..
VIOLET TOWN SHIRE—						
Harry's Creek Road .. ..	.. ..	..	2.27	2.27	..	..
WALPEUP SHIRE—						
Ouyen-Tempy Road	Forming and metalling between Bronzewing and Gypsum ..	1.55	..	..	..	..
" " " " " " " "	Forming and metalling between Allotment 37, Parish of Boulka, and railway ..	..	1.05	1.05	1.05	1.05
Boorongie North Road ..	Metalling through Allotments 15, 27, Parish of Boorongie ..	.25	.25	.25	.25	.25
Boulka-Timberoo Road ..	Metalling in Parish of Boulka .. ..	.52	..	..	.52	.52
Ouyen-Kulwin Road	Opposite Allotment 43, Parish of Boulka, and railway ..	..	.23	.23	..	.62
" " " " " " " "	Metalling through Allotments 29, 30, 29A, 31, Parish of Boorongie, and Allotments 6 and 9, Parish of Wagant ..	.64	..	..	..	.26
" " " " " " " "	Metalling through Allotments 36 and 36A, 31-37, Parish of Wagant ..	..	.26	.26	.26	.26
Tiega North East Road ..	Metalling in Parish of Tiega .. ..	.4	..	..	..	..
" " " " " " " "	Metalling through Allotments 26, 27, Parish of Tiega ..	..	.13	.13	..	..
Galah-Timberoo Road ..	Metalling in Parishes of Tiega and Timberoo ..	.75	..	..	..	.68
Walpeup-Patchewollock Road ..	Forming and metalling through Allotments 32, 34, Parish of Walpeup ..	.69	..	..	..	..
Walpeup-South-West Road ..	Forming and metalling through Allotments 53-54, Parish of Walpeup ..	.97	..	..	..	..
" " " " " " " "	Metalling through Allotments 64, 65, 66, 63, 68, 67, Parish of Walpeup ..	.71	.91	.91	.71	.7
Walpeup-North Meridian Road ..	Forming and metalling through Allotments 1 and 2, Parish of Nyang, Allotment 1, Parish of Paignie, and Allotment 12, Parish of Kattoyong ..	.9	..	..	..	..
" " " " " " " "	Metalling through Allotments 13 and 28, Parish of Kattoyong, and Allotments 14 and 15, Parish of Paignie ..	..	.71	.71	..	..
Kattoyong Road	Metalling through Allotments 24 and 25, Parish of Nyang ..	.51	..	..	..	.13
" " " " " " " "	Metalling through Allotments 1 and 11, Parish of Nyang ..	.13	.4	.4	.13	.13
" " " " " " " "	Metalling through Allotment 1, Parish of Gnarr, and Allotment 6, Parish of Kattoyong ..	..	.87	.87	..	..
Nyang South Road	Metalling in Parishes of Nyang and Daalko .. ..	.56	..	..	.34	.34
" " " " " " " "	Metalling through Allotments 49 and 50, Parish of Nyang ..	..	.76	.76	.34	.34
" " " " " " " "	Metalling through Allotments 34, 50-35, 49, Parish of Nyang, and Allotments 37-37A, Parish of Daalko ..	..	.19	.95	..	..
Underbool-Gnarr Road ..	Metalling in Parishes of Underbool and Gnarr ..	.55	..	..	..	..
	Carried forward	229.67	194.76	203.35	165.36	164.33

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads—			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER MUNICIPALITIES—<i>continued.</i>						
	Brought forward	229·67	194·76	203·35	165·36	164·33
WALPEUP SHIRE— <i>continued.</i>						
Underbool-Mamengarook Road ..	Metalling through Allotments 5 and 8, Parish of Underbool, and Allotments 1A and 7A, Parish of Mamengarook	1·02	..	..	..	..
Underbool-South Road	Metalling through Allotments 5, 6, 7, 8, Parish of Underbool, Allotment 59, Parish of Nyang, and Allotments 18 and 22, Parish of Daalko	·71 ·84	·71 ·21	·71 ·21	·71 ..	·71 ·42
Linga North Road	Metalling through Allotments 13, 16, 20, Parish of Underbool	..	·21	·35	..	..
Boinka North Road	Forming and metalling through Allotments 19, 29, 27, Parish of Boinka	·39	..	..	..	·39
Boinka South Road	Metalling through Allotments 17 and 18, Parish of Boinka	..	·4	·4	·15	·15
Tyalla-Koonda Road	Metalling through Allotments 19 and 30, Parish of Worooa	..	·24	·24	·13	·13
Cowangie Road	Forming and metalling in sections in Parish of Tyalla	1·23	..	..	..	..
"	Metalling in Parish of Tyalla	·67	..	..	..	..
"	Metalling through Allotments 6 and 7, Parish of Tutye	..	1	1	·5	·5
"	Metalling through Allotments 36 and 38, Parish of Tutye, and Allotments 13, 14, Parish of Bunurouk	·54	..	..	..	..
"	Metalling through Allotments 3-44, Parish of Tutye	..	·13	·13	·13	·13
Danyo North Road	Metalling through Allotments 13-16, Parish of Bunurouk	1·23	..	..	..	1·23
"	Forming and metalling through Allotments 25 and 40, Parish of Danyo	..	·6	·6	..	..
"	Forming and metalling in Parishes of Danyo, Duddo, and Walpa	..	·75	·75	..	..
Murrayville North Road ..	Metalling in Parishes of Duddo and Goongee	·74	..	..	..	..
"	Metalling through Allotments 45, 29, 30, 34, 29, Parish of Duddo	·79	·9	·9	·67	·67
Murrayville-South Road ..	Forming and metalling in Parish of Gunamalary ..	·68	..	..	..	..
"	Through Allotments 5 and 13, Parish of Gunamalary ..	·19	·19	·19	·19	·19
Carina North Road	Metalling in Parishes of Mulcra and Carina	·64	..	..	..	..
"	Metalling through Allotments 11, 15, 34 and 35, Parish of Mulcra	·34	·34	·34	·34	·34
Carina South Road	Forming and metalling in Parishes of Carina and Ngallo ..	·33	..	..	..	..
"	Metalling through Allotments 30-42, Parish of Ngallo ..	·23	·23	·23	·23	·23
"	Metalling through Allotments 27, 30, 41-42, Parish of Ngallo	..	·61	·61	..	..
Paninya South Road	Forming and metalling in Parish of Carina	1·14	..	..	·77	·77
Paninya North Road	Metalling in Parishes of Mulcra, Manya, and Berook ..	2·01	..	..	..	..
"	Metalling through Allotments 11, 12 and 13, Parish of Manya	·74	..	..	·74	·74
"	Metalling through Allotments 54, 49 and 50, Parish of Mulcra	·66	·66	·66	·66	·66
"	Metalling through Allotments 1, 3 and 64, Parish of Mulcra	..	..	..	·25	·25
"	Metalling through Allotments 3, 27, 60, 61, 28, 49, 55 and 58, Parish of Mulcra	..	2·57	2·57	·79	·79
WANGARATTA SYIRE—						
Peechelba Station Road ..	Forming and gravelling adjoining Allotments 54A, 54A1, and 55A, Parish of Boorhaman	·75	..	..	..	..
WANNON SHIRE—						
Melville Forest Road .. ..	.. .. .	..	1·87	1·87	1·87	1·87
WARANGA SHIRE—						
Mount Camel-Corop Road ..	Between Colbinabbin and Corop	·63	·93	·93	·63	·63
Mount Camel-Estate Road ..	Between Mount Camel and Colbinabbin	·66	1·62	1·62	1·62	1·62
WARRAGUL SHIRE—						
Ferndale Road	Southwards from Allotment 63A, Parish of Allambee	·5	·5	·5	·5	·5
Nilma-Shady Creek Road ..	Northwards from Allotment 12B to Allotment 9, Parish of Darnum	·6	·6	·6	·6	·6
Bona Vista-Nilma Road	From Allotment 9 to Allotment 7, Bona Vista Estate, Parish of Warragul	·5	·5	·5	·5	·5
Old Telegraph Road	Eastwards from Brandy Creek Main Road	·99	·99	·99	·99	·99
Mountain View Road	Westwards from Warragul-Korumburra Main Road ..	·43	·43	·43	·43	·43
WARRNAMBOOL SHIRE—						
Pannure Road	2½ miles from Pannure	·57	·4	·4	..	..
Naringal Road	1 mile east of Naringal	·83	..	..	..	..
Childer's Cove Road	Near Childer's Cove	·61	1·5	1·5	..	..
WERRIBEE SHIRE—						
Exford Road	From end of construction towards Parwan	·6	..	..	..	..
WINCHELSEA SHIRE—						
Boonah Road	Gravelling near Bamba Cemetery	·29	..	..	..	..
Inverleigh-Winchelsea Road ..	Bottom coat metalling, in Parish of Carrung-e-Murnong ..	1·13	..	1·13	1·13	1·13
Birregurra-Forrest Road	Clearing, forming and gravelling at Barwon Downs ..	·37	..	·37	·37	·37
Cann's Lane Road	Gravelling final section near Ripplevale School between chainages 14200-15600	·27	..	·27	·27	·27
WODONGA SHIRE—						
Beechworth-Wodonga Road ..	At Lenina West Post Office	·55	·55	·55	·55	·55
Kiewa-Wodonga Road	At junction with Yackandandah Road	·04	..	..	..	..
WOORAYL SHIRE—						
Leongatha-Mirboo Road	Near the Shire boundary	1·04	1·04	1·04	1·04	1·04
Meeniyah-Nerrena Road	Through C. Whelan's	·68	·68	·68	·68	·68
Central Road	Past Nicholas's and York's	2	2	2	2	2
Nerrena Road	Near Slater's	·35	·35	·35	·35	·35
WYCHEPROOF SHIRE—						
Glenloth Road	East of Glenloth Township	·09	·09	·09	·09	·09
Nullawil-Winston Road	4 miles west of Nullawil	·45	·45	..	..	..
Culgoa-Lalbert Road	5 miles west of Lalbert	·18	·08	·08	·08	·08
Meridian Road	4 miles south of Berriwillock	·79	..	..	..	..
Sea Lake-Tyrrell Downs Road ..	North of Sea Lake	·37	..	..	..	..
Nyarrin Road	East from Nyarrin	·61	..	..	..	..
Berriwillock-Woomelang Road ..	West from Berriwillock	1·11	1·5	1·5	1·5	1·5
YACKANDANDAH SHIRE—						
Kergunyah Road	Bridge and approaches over Bight Creek .. .. .	..	·07	·07	·07	·07
Dederang-Tawonga Road	Culvert and approaches	·01	·01	·01	·01	·01
Running Creek Road	Culvert and approaches	·24	·24	·24	·24	·24
YEA SHIRE—						
Flowerdale Road	Between Collins Hotel and Lammy's Hut	2·68	2·68	2·68	2·68	2·68
	Total	266·2	223·59	233·64	189·82	190·88
UNDER DIRECT SUPERVISION OF THE BOARD.						
ALBERTON SHIRE—						
Dingo Creek Road	Widening existing formation from 10 feet to 15 feet ..	4	4	4	(Day labour)	
ALBERTON, MORWELL, AND TRARALGON SHIRES—						
Ridge Road	Reforming and gravelling	2·13	2·13	2·13	2·13	2·13
BALLAN SHIRE—						
Blakeville Road	Reforming, grading, and metalling	·45	·45	·45	·45	·45
BALLAN AND BUNGAREE SHIRES—						
Egerton-Bungala Road	Forming, grading, and metalling	2·29	2·29	2·29	2·29	2·29
Spargo Creek-Bolwarrah Road ..	Clearing, forming, and loaming	3·2	4	4	4	4
	Carried forward	12·07	12·87	12·87	8·87	8·87

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads—			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER DIRECT SUPERVISION OF THE BOARD—continued.						
Brought forward		12'07	12.87	12'87	8'87	8'87
BENALLA SHIRE—						
Toombullup Road	Reforming, gravelling, and sanding	5'3	5'75	5'75	5'75	5'75
" " " " " " " " " "	Reforming and metalling	1'59	1'59	..	..	..
BERWICK SHIRE—						
Garfield-Catani Road	Construction of bridge at Vervale .. .. .	.01	..	.01	.01	.01
Cockatoo-Gembrook Road	Reforming, grading and sanding	1'96	3'12	3'12	3'12	3'12
" " " " " " " " " "	Clearing and forming	1'5	1'5	..	..	..
BERWICK AND "FERN TREE" GULLY SHIRES						
Emerald Road	Construction of timber bridge over Cockatoo Creek .. .. .	.01	..	.01	.01	.01
BRIGHT AND YACKANDANDAH SHIRES—						
Running Creek Road	Clearing and forming	1'76	1'76	..	..	..
BULN BULN SHIRE—						
Duggan North Road	Clearing and grading near Icy Creek Bridge .. .. .	.06	.06	.06	..	..
ELTHAM SHIRE—						
Kinglake Road	Reforming and metalling	1'19	3	1'19	1'19	1'19
Whittlesea-Kinglake Road	Reforming and metalling .. .. .	.92	.92	..	..	..
Yarra Glen-Glenburn Road	Clearing, forming, and gravelling .. .. .	.73	.73	..	..	..
Kinglake-Kinglake East Road	Clearing and forming .. .. .	..	4'25	4'25	4'25	4'25
Toolangi-Kinglake Road	Clearing and forming	6	6	6	6	6
HEYTESBURY SHIRE—						
Cooriejong Road	Grubbing and grading .. .. .	.86	1'92	1'92	1'92	1'92
Eastern Creek Road	Grubbing and clearing	3'68	3'68	3'68	3'68	3'68
" " " " " " " " " "	Forming and grading	1'43	1'93	1'93	1'93	1'93
Kennedy's Creek Road	Clearing and forming .. .. .	..	1'75	..	..	..
Peterborough-Port Campbell Road	Gravelling	3	3	..	..	..
" " " " " " " " " "	Grubbing, clearing, and forming	7'27	7'27	4'55	4'55	4'55
Timboon-Curdies Vale Road	Forming, grading, and timber bridge	2'47	2'47	2'47	2'47	2'47
Timboon-Nirranda Road	Reforming and loaming	2	2	..	..	..
" " " " " " " " " "	Reforming and metalling .. .. .	.53	.53	..	..	..
" " " " " " " " " "	Grubbing, clearing, and forming	1'08	1'08	..	..	..
HEYTESBURY AND WARRNAMBOOL SHIRES—						
Peterborough-Port Campbell Road	Tarring and painting bridge over Curdies Inlet, and constructing guard fence and approaches .. .. .	.01	..	.01	.01	.01
Timboon-Scott's Creek Road	Bitumen spraying from Cobden-Port Campbell Road to Timboon Railway Station	5'4	5'4	5'4	(Day labour)	(Day labour)
MCIVOR SHIRE—						
Mount Camel Estate Road	Clearing and forming .. .. .	.26	.26	..	..	..
" " " " " " " " " "	Reforming, gravelling, and metalling .. .. .	.54	.54	..	..	..
" " " " " " " " " "	Erection of timber bridge at Lady Pass Creek .. .. .	.01	.01	.01	(Day labour)	(Day labour)
" " " " " " " " " "	Reforming and gravelling at Lady Pass Creek .. .. .	.11	.11	.11	.11	.11
" " " " " " " " " "	Reforming and gravelling .. .. .	.92	3'5	.92	.92	.92
MELTON SHIRE—						
Mount Kororoit Road	Reforming and metalling .. .. .	.32	.32	..	..	..
MILDURA SHIRE—						
Benetook South Road	Clearing, forming, and marling	2'49	..	1'15	1'15	1'15
Karawinna North Road	Forming and marling	1'4	..	..	..	..
Karawinna South Road	Forming and metalling	1'92	..	1'04	1'04	1'04
Meringur North Road	Forming and limestone	2'9	..	..	..	..
Meringur South Road	Forming and marling	3'82	..	5	5	5
Merrinee North Road	Forming and marling	1'45	..	..	..	..
Merrinee South Road	Grubbing, forming, and marling	3'4	..	1'83	1'83	1'83
Murray River Valley Road	Forming and surfacing with limestone gravel	5'26	..	..	..	..
" " " " " " " " " "	Forming and grading	2	..	2	2	2
" " " " " " " " " "	Grubbing, forming, and surfacing with limestone	2'36	2'36	2'36	2'36	2'36
Werrimull North Road	Forming and rubbling	1'85	..	..	..	..
Werrimull South Road	Forming and rubbling	2'58	..	..	..	..
Yarrara North Road	Grubbing, clearing, and marling .. .. .	.27	.27	..	..	..
Yarrara South Road	Grubbing, forming, and marling	2'77	2'77	..	..	..
MORWELL SHIRE—						
Morwell River Road	Reforming and sanding	4'57	6	4'57	4'57	4'57
" " " " " " " " " "	Reforming and sanding	2'96	..	2'96	2'96	2'96
" " " " " " " " " "	Clearing and forming	1'43	..	..	..	..
" " " " " " " " " " (East Branch)	Grubbing, clearing, forming, and draining	3	..	3	(Day labour)	(Day labour)
" " " " " " " " " "	Grubbing, clearing, forming, and draining .. .. .	.85	..	.85	(Day labour)	(Day labour)
NARRACAN SHIRE—						
Aberfeldy-Matlock Road	Clearing, forming, and grading	1'03	..	1'03	1'03	1'03
Alambe Estate Road	Clearing and forming	2'25	2'25	2'25	(Day labour)	(Day labour)
Childer's Settlement Road	Reforming and sanding	1'66	2	1'66	1'66	1'66
" " " " " " " " " "	Reforming and metalling .. .. .	.31	..	.31	.31	.31
Walhalla-Matlock Road	Clearing and forming	2'72	4'37	4'37	4'37	4'37
OSEO SHIRE—						
Benambra-Corryong Road	Clearing and forming at Gibbo River	4'88	4'88	4'88	(Day labour)	(Day labour)
ORBOST SHIRE—						
Dedick River Road	Grubbing, clearing, forming, and draining	1'13	1'13	1'13	(Day labour)	(Day labour)
" " " " " " " " " "	Grubbing, clearing, forming, and draining	5'15	5'15	5'15	(Day labour)	(Day labour)
Orbost-Delegate Road	Re-alignment, forming, and widening	1'4	1'4	1'4	(Day labour)	(Day labour)
OTWAY SHIRE—						
Cape Patten Road	Grubbing, clearing, and grading south of Wye River .. .. .	.32	.63	.32	.32	.32
" " " " " " " " " "	Bridge and approaches at Whalebone Creek .. .. .	.14	.14	.14	.14	.14
Colac-Beech Forest Road	Clearing and forming .. .. .	.54	..	..	..	..
Ferguson-Charley's Creek Road	Clearing	1'12	1'12	1'12	(Day labour)	(Day labour)
Laver's Hill-Chapplevale-Devondale Road	Reforming and gravelling	3	..	3	3	3
Glen Aire-Laver's Hill Road	Reforming and sanding	2'71	5	5	5	5
Hordern Vale-Apollo Bay Road	Forming and grading	3'15	..	4'13	4'13	4'13
" " " " " " " " " "	Grubbing and clearing	4'66	7'25	4'66	4'66	4'66
" " " " " " " " " "	Grubbing, clearing, and forming	2'59	..	2'59	2'59	2'59
Kennedy's Creek Road	Clearing and forming .. .. .	..	2'08	..	..	..
Lardner's Track Road	Reforming and metalling .. .. .	.64	..	.64	.64	.64
Laver's Hill-Princetown Road	Reforming and metalling	3'04	3'04	3'04	3'04	3'04
Mount Sabine-Laver's Hill Road	Reforming and metalling	3'71	..	3'71	3'71	3'71
Wild Dog Valley Road	Supply and delivery of 500 cubic yards of gravel near Smythesdale .. .. .	..	..	..	..	..
OXLEY SHIRE—						
Buffalo River Road	Construction of two timber bridges, and approaches at Cooper's Creek .. .. .	.05	..	.05	.05	.05
Toimie-Whitfield Road	Reforming and grading	1'55	9'4	8'54	4'03	4'03
" " " " " " " " " "	Reforming, grading, and metalling .. .. .	.86	..	.86	.86	.86
SEYMOUR AND YEA SHIRES—						
Highlands Road	Grubbing, clearing, forming, and draining	4'45	7'66	4'45	(Day labour)	(Day labour)
SWAN HILL SHIRE—						
Manangatang-Euston Road	Forming and limestone metalling	5'19	..	3'62	3'62	3'62
Murray River Valley Road	Timber bridge and approaches over inlet to Lake Powell .. .. .	.01	..	.01	.01	.01
Ouyen-Piangil Road	Forming, reforming, and metalling	1'01	..	1'01	1'01	1'01
Carried forward		163'64	143'52	144'80	109'88	109'88

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads—			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER DIRECT SUPERVISION OF THE BOARD—<i>continued.</i>						
	Brought forward	163·64	143·52	144·89	109·88	109·88
TAMBO SHIRE—						
Buchan-Ensary Road	Clearing and forming	5·85	38·8	5·85	(Day labour)	1·5
Deddick River Road	Clearing and forming	1·5	..	1·5	1·5	1·5
TOWONG SHIRE—						
Murray River Valley Road (Bethanga connection)	Clearing, forming, and gravelling	2·65	..	3·28	3·28	3·28
" " " "	Construction of reinforced concrete bridge and culvert at Bethanga	'01	..	'01	'01	'01
Murray River Valley Road	Clearing and forming	2·81	..	2·81	2·81	2·81
" " " "	Gravelling	2·91	..	2·91	2·91	2·91
" " " "	Clearing, forming, and grading between Hawkesview and Bethanga Bridge	2·74	..	5·19	5·19	5·19
TRARALGON SHIRE—						
Whitelaw's Track Road	Forming, grading, and gravelling	1·65	..	1·65	1·65	1·65
TUNGAMAH AND SHEPPARTON SHIRES—						
Congupna and Katandra Roads	Forming and grading	'59	..	'59	'59	'59
WERRIBEE SHIRE—						
Point Cook Road	Supply and delivery of 595 cubic yards of gravel .. .. .	..	..	..	..	..
" " " "	Resealing	5·23	5·23	5·23	(Day labour)	..
WINCHELSEA AND OTWAY SHIRES—						
Cape Patten Road	Construction of three-span bridge over Wye River	'01	'01	'01	'01	'01
" " " "	Bridge and approaches at Separation Creek	'59	'61	'61	'61	'61
YEA SHIRE—						
Kinglake-Glenburn Road	Reforming and metalling .. .. .	..	1·5	1·5	..	..
Yarra Glen-Glenburn Road	Grubbing, clearing, forming, and gravelling	'4	'74	'74	'74	'74
	Total	190·58	190·41	169·64	129·18	129·18

APPENDIX K.

COUNTRY ROADS BOARD.

STATE HIGHWAYS.

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED AND HIGHWAYS MAINTAINED DURING THE YEAR ENDED 30TH JUNE, 1930.

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES.				
ARARAT SHIRE— Western Highway—Sections 2 and 3	Bitumen surfacing top of Mount Mistake ..	..	2
..	Bitumen surfacing at Borough boundary ..	..	15
..	General maintenance .. .. .	..	18' 8
BACCHUS MARSH SHIRE— Western Highway—Section 1	Anthony's Cutting .. .. .	..	66
..	Bacchus Marsh .. .. .	..	87
BULN BULN SHIRE— Princes Highway East—Section 1	From Bunyip River to western boundary of Drouin Township, and from eastern boundary of Township, to Shire boundary	..	8' 75
KORONG SHIRE— Calder Highway—Section 2	Bituminous gravelling between Bridgewater and Inglewood ..	1' 5	37
..	Double coat bitumen surfacing between Bridge-water and Bulabul Creek ..	..	3
..	General maintenance, including scarifying, &c. ..	..	3' 9
..	Double coat bitumen surfacing in detached sections ..	26	..
..	Gravelling at Woosang .. .. .	..	1' 4
..	Gravelling at Kurting-Glenalbyn .. .. .	..	2' 89
..	General maintenance, including scarifying, &c. ..	..	28
LAWLOIT SHIRE— Western Highway—Section 5	Forming and gravelling between Kaniva and Lawloit ..	..	4' 71
..	Re-sheeting gravel between Kaniva and Lawloit ..	..	75
..	Bitumen surfacing east of Kaniva .. .. .	..	1' 5
..	General maintenance .. .. .	..	28' 25
LOWAN SHIRE— Western Highway—Section 5	Between Allotments 70-72 and 71-76, Parish of Kinimakatka ..	..	1' 13
..	Between Allotments 72 and 76-74 and B, Parish of Kinimakatka ..	..	96
MILDURA SHIRE— Calder Highway	From Irymple to Red Cliffs	3' 11	9' 5
..	From Red Cliffs towards Yatpool boundary ..	..	95
..	From turn off, Hattah deviation, 38-mile peg towards Hattah ..	..	98
..	At 43-mile peg on the Hattah deviation ..	..	7
OMEQ SHIRE— Omeo Highway—Section 1	North of Bruthen (Heard's Gully) ..	..	57
..	North of Bruthen (Piano Box) ..	1	..
..	North of Ensay (O'Brien's Hill) ..	..	56
..	56
STAWELL SHIRE— Western Highway—Section 3	Great Western to Armstrong	5' 55	5' 55
..	16
..	Patrol maintenance, Stawell to Ararat ..	..	4' 5
TAMBO SHIRE— Omeo Highway—Section 1	Forming, grubbing, and clearing near Ramrod Creek (known as Heard's Gully) ..	..	5
..	General maintenance between Nicholson River and Ramrod Creek ..	..	8
TOWONG SHIRE— Omeo Highway—Sections 3 and 4	Between Tallandoon and Granite Flat ..	9' 62	65
WALPEUP SHIRE— Calder Highway—Section 5	Various sections in Parishes of Boulka and Woornack ..	..	53
..	Near Mittyack .. .. .	..	13
..	Kiamal to Trinita .. .. .	..	1' 8
..	General maintenance	1' 8	13
WARRAGUL SHIRE— Princes Highway East—Section 1	2' 04
..	From Western Shire boundary to Warragul Town boundary ..	..	6' 01
..	From eastern town boundary to eastern Shire boundary ..	..	..
WODONGA SHIRE— Omeo Highway—Section 4	11
..	Wodonga Township to Yackandandah Shire boundary ..	..	..
WYCHEPROOF SHIRE— Calder Highway—Section 4	Between Wycheproof and Nullawil ..	5' 18	1' 51
..	Between Sea Lake and Nandaly ..	8' 77	..
..	Total	43' 24	407' 67
UNDER DIRECT SUPERVISION OF THE BOARD.				
AVON SHIRE— Prince's Highway East—Section 3	Reforming, widening, and gravelling between Montgomery and Stratford ..	1' 77	..
..	Supply and delivery of 220 cubic yards of gravel between Stratford and Providence Ponds ..	..	..
..	Supply and delivery of 300 cubic yards of gravel between Montgomery and Stratford ..	..	..
..	Supply and delivery of 400 cubic yards of Avon River gravel between Sale and Montgomery ..	..	..
..	Reforming, regrading, gravelling between Stratford and Providence Ponds ..	4' 32	..
..	Forming, grading, and gravelling between Stratford and Munro Turnoff ..	76	..
..	Carried forward	6' 85	..

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD—<i>continued.</i>				
		Brought forward	6·85	..
BACCHUS MARSH SHIRE— Western Highway—Section 1 ..	.. .. .	Widening, reshaping with bituminous macadam over sealing from Deep Creek westerly	..	*71
" " " " ..	.. .. .	Widening and reconstruction of pavement and formation	..	*07 (day labour)
BACCHUS MARSH AND MELTON SHIRES— Western Highway—Section 1 ..	.. .. .	Supply of 195 cubic yards of screenings between Bacchus Marsh and Myrning	..	..
BACCHUS MARSH AND BALLAN SHIRES— Western Highway—Section 1 ..	.. .. .	Sealing in various sections between Bacchus Marsh and Ballan	..	2·8 (day labour)
" " " " ..	.. .. .	Supply and delivery of gravel between Bacchus Marsh and Myrning and at Djerriwarrh Creek	..	..
BAIRNSDALE SHIRE— Prince's Highway East ..	Reinforced concrete bridge with approaches near Bairnsdale	.. .. .	*01	..
BALLAN SHIRE— Western Highway—Section 1 ..	Reinforced concrete bridge over Pyke's Creek Reservoir	Construction of foundations of a concrete-bridge over Pyke's Creek Reservoir	..	(day labour)
" " " " ..	.. .. .	Widening and reconstruction of pavement and formation	..	*05 (day labour)
BALLAN, BACCHUS MARSH, AND BUNGAREE SHIRES— Western Highway—Section 1 ..	.. .. .	Supply and delivery of 170 cubic yards of metal, 40 cubic yards of screenings, and 12 cubic yards of toppings	..	..
BALLAN, BUNGAREE, AND BUNIN- YONG SHIRES— Western Highway—Section 1 ..	.. .. .	Resealing in three sections between Wallace and Ballarat	..	3·49 (day labour)
BALLARAT SHIRE— Western Highway—Section 2 ..	Forming and grading at Burrum- beet Creek	Sealing with bitumen previously constructed asphaltic macadam between Cardigan School and Burrumbeet Park	*14	2·66 (day labour)
" " " " ..	Construction of reinforced con- crete bridge at Burrumbeet Creek	.. .. .	..	..
" " " " ..	Resheeting, widening, and sur- facing with bituminous maca- dam between Ballarat and Burrumbeet	.. .. .	4·05	..
BALLARAT, RIPON, AND LEXTON SHIRES— Western Highway	Reforming, widening, and sur- facing with bituminous maca- dam from Burrumbeet Park towards Trawalla	.. .. .	8·03	..
BARRABOOL SHIRE— Prince's Highway West—Section 1	.. .. .	Resealing with bitumen and gravel .. ..	..	7·5 (day labour)
BELFAST SHIRE— Prince's Highway West—Section 4	Widening, reforming, and gravel- ling	.. .. .	4·21	..
" " " " ..	Construction of reinforced con- crete culverts and lengthening existing culverts between Port Fairy and Portland	.. .. .	..	..
BENALLA SHIRE— Hume Highway—Section 2 ..	Construction of reinforced con- crete culvert, earth filling and granite approaches 2 miles north of Glenrowan	Construction of two-cell culvert near Badda- ginnie	..	(day labour)
BERWICK SHIRE— Prince's Highway East—Section 1	Construction of reinforced con- crete bridge over Narre Warren Creek	Supply and delivery of 325 cubic yards of metal and 65 cubic yards of screenings between Beaconsfield and Nar-nar-noon	..	..
" " " " ..	Forming, grading, and surfacing with bituminous macadam be- tween Nar-nar-noon and Tynong	Resealing with bitumen between Narre Warren and Hallam	1·19	1·43
" " " " ..	Supply and delivery of 250 cubic yards of screenings between Bunyip Turnoff and Bunyip River	Supply and delivery of 840 cubic yards of screenings	..	..
" " " " ..	Reforming, grading, and sanding between Bunyip River and Tynong	Supply and delivery of 270 cubic yards of screenings	6·94	..
" " " " ..	Construction of two reinforced concrete culverts between Hal- lam and Narre Warren	Sealing and resealing at Hallam, Narre Warren, Beaconsfield, and Officer	..	5·1 (day labour)
" " " " ..	Supply and delivery of 280 cubic yards of screenings between Garfield and Bunyip	Bituminous surface treatment of granitic sand between Garfield and the Bunyip River, Bunyip River eastwards, and at Targo over- flow	*44	4·3 (day labour)
" " " " ..	Widening and surfacing with asphaltic macadam	.. .. .	..	..
" " " " ..	Supply and delivery of 275 cubic yards of screenings	Sealing with bituminous macadam between Nar-nar-noon and Tynong	..	2 (day labour)
" " " " ..	.. .. .	Construction in waterbound and penetration macadam of approaches to new bridge at Narre Warren	..	*13 (day labour)
BERWICK AND BULN BULN SHIRES— Prince's Highway East—Section 1	Construction of timber bridge over Bunyip River	.. .. .	*04	..
BRAYBROOK AND MELTON SHIRES— Western Highway—Section 1 ..	.. .. .	Supply and delivery of 525 cubic yards of screenings between chainages 104,500 feet and 124,000 feet	..	..
" " " " ..	.. .. .	Supply and delivery of 525 cubic yards of screenings between Deer Park and Rockbank	..	..
BROADMEADOWS SHIRE— Hume Highway—Section 1 ..	Supply and delivery of 720 cubic yards of gravel at Wallan and Beveridge	Supply and delivery of 1,080 cubic yards of gravel between Summerton and Craigieburn	..	..
		Carried forward	31·9	30·24

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<i>UNDER DIRECT SUPERVISION OF THE BOARD—continued.</i>				
		Brought forward	31·9	30·24
BROADMEADOWS AND KILMORE SHIRES— Hume Highway—Section 1 ..	.. .. .	Supply and delivery of 550 cubic yards of screenings between Wallan and Kilmore	..	..
" " " " ..	.. .. .	Supply and delivery of 1,300 cubic yards of screenings between 18-mile post and 26½-mile post	..	..
BULLA, KEILOR, MELTON, GISBORNE, KYNETON, NEWHAM AND WOODEND SHIRES— Calder Highway—Section 1 ..	.. .. .	Spraying with bituminous macadam between Holden and Malmesbury	..	36·72 (day labour)
BULN BULN SHIRE— Prince's Highway East—Section 1	Construction of reinforced concrete culvert	.. .. .	..	..
BUNGAREE SHIRE— Western Highway—Section 1 ..	.. .. .	Reconstruction of pavement formation	..	*18 (day labour)
BUNGAREE AND BUNINYONG SHIRES— Western Highway—Section 1 ..	.. .. .	Supply and delivery of 350 cubic yards of gravel between Bungaree and Ballarat	..	..
" " " " ..	.. .. .	Supply and delivery of 536 cubic yards of metal at Bungaree and Llandiello	..	..
CHARLTON SHIRE— Calder Highway—Section 3 ..	Forming and gravelling	.. .. .	*1	..
CHILTERN SHIRE— Hume Highway—Section 3 ..	.. .. .	Supply and delivery of 1,000 cubic yards of gravel on roadside between Chiltern and Barnawartha subway	..	..
CHILTERN AND RUTHERGLEN SHIRES— Hume Highway—Section 3 ..	.. .. .	Supply and delivery of 500 cubic yards of gravel near Chiltern	..	..
CHILTERN AND WODONGA SHIRES— Hume Highway—Section 3 ..	Supply and delivery of 8,500 cubic yards of gravel between Wodonga and Barnawartha	.. .. .	..	..
CORIO SHIRE— Prince's Highway West—Section 1	Subgrade drainage between Bacchus Marsh Road and Separation Street bridge	.. .. .	3·36	..
DANDENONG SHIRE— Prince's Highway East—Section 1	Pre-mixed bitumen seal coat between Spring Vale and Dandenong—45,133 cubic yards	.. .. .	..	..
DIMBOOLA SHIRE— Western Highway—Section 4 ..	.. .. .	Waterproofing and gravel surfacing of clay formation between Wail and Dimboola	..	*48 (day labour)
DIMBOOLA AND WIMMERA SHIRES— Western Highway—Section 4 ..	Forming, grading, and gravelling between Wail and Dimboola	Surfacing clay formation with bitumen between Pimpinio and Dimboola	5·35	7·51 (day labour)
EUROA SHIRE— Hume Highway—Section 2 ..	Supply and delivery of 1,000 cubic yards of gravel	Construction of end walls and extending double 3-ft. diameter reinforced concrete pipe by 12 feet	..	..
GISBORNE AND NEWHAM AND WOODEND SHIRES— Calder Highway—Section 1 ..	.. .. .	Supply and delivery of 1,134 cubic yards of gravel	..	..
GOULBURN SHIRE— Hume Highway—Section 2 ..	Supply and delivery of 1,550 cubic yards of screenings stacked between chainages 379,000 feet and 440,500 feet	.. .. .	..	..
HAMPDEN SHIRE— Prince's Highway West—Section 3	Reforming, widening, and surfacing with bituminous macadam between Camperdown and Terang	.. .. .	10	..
" " " " ..	Unloading and carting 15,254 cubic yards of metal and screenings to Prince's Highway West between Boorean and Garvoc	.. .. .	..	..
" " " " ..	Supply and delivery of 5,750 cubic yards of metal and 1,150 cubic yards of screenings	.. .. .	..	..
HAMPDEN AND HEYTESBURY SHIRES— Prince's Highway West—Section 2	Supply and delivery of 7,400 cubic yards of metal, Class A; 7,400 cubic yards of metal, Class B; 3,300 cubic yards of coarse screenings, and 1,900 cubic yards of fine screenings	Widening, reforming, and surfacing with bituminous macadam between Stoneyford and Camperdown	..	14·26 (day labour)
" " " " ..	Unloading 20,000 cubic yards of metal and screenings from railway trucks and carting to Prince's Highway West between Stoneyford and Camperdown	.. .. .	..	..
" " " " ..	Supply and delivery of 9,034 cubic yards of metal, 1,942 cubic yards of coarse screenings, and 1,278 cubic yards of fine screenings	.. .. .	..	..
" " " " ..	Supply and delivery of 17,826 cubic yards of metal and 3,565 cubic yards of screenings	.. .. .	..	..
HEYTESBURY SHIRE— Prince's Highway West—Section 2	Loading and carting 8,700 cubic yards of scoria to Stoneyford	.. .. .	..	..
" " " " ..	Widening, reforming, and surfacing through Stony Rises	.. .. .	3·1	..
HUNTLY SHIRE— Northern Highway—Section 1 ..	Supply of 13,255 cubic yards of gravel	.. .. .	..	..
" " " " ..	Forming, boxing, trimming, and gravelling between Goornong and Elmore	.. .. .	5·47	..
" " " " ..	Reforming, grading, and gravelling at Bagshot Railway Crossing	.. .. .	*20	..
		Carried forward	59·54	89·39

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD—<i>continued.</i>				
		Brought forward	59·54	89·39
HUNTLY AND ROCHESTER SHIRES— Northern Highway—Section 1 ..	.. .. .	Forming, boxing, trimming, and gravelling between Elmore and Rochester	..	6·55
KEILOR AND MELTON SHIRES— Calder Highway—Section 1 ..	Supply and delivery of 781 cubic yards of screenings between Holden and The Gap	.. .. .	..	..
KILMORE SHIRE— Hume Highway—Section 1 ..	Reforming, resheeting, and surfacing with asphaltic macadam (penetration method), Township of Kilmore	Superelevating of the curve south of railway crossing at Kilmore	1·55	·05 (day labour)
KILMORE, BROADMEADOWS, AND BROADFORD SHIRES— Hume Highway—Section 1 ..	.. .. .	Spraying (sealing and resealing) between Campbellfield and Bylands and between Kilmore and Broadford	..	19·1 (day labour)
KYNETON SHIRE— Calder Highway—Section 1 ..	.. .. .	Supply and delivery of 1,100 cubic yards of gravel	..	..
" " " " ..	.. .. .	Supply and delivery of 956 cubic yards of blue-stone screenings between Woodend and Carlsruhe	..	..
MELTON AND GISBORNE SHIRES— Calder Highway—Section 1 ..	.. .. .	Supply and delivery of 1,316 cubic yards of screenings	..	..
METCALFE SHIRE— Calder Highway—Section 1 ..	.. .. .	Supply and delivery of 1,750 cubic yards of crushed granite, 440 cubic yards of screenings, and 1,800 cubic yards of granite waste	..	..
METCALFE AND MALDON SHIRES— Calder Highway—Section 2 ..	.. .. .	Construction of 3 feet by 4 feet reinforced concrete box culvert	..	(day labour)
MORWELL SHIRE— Prince's Highway East—Section 2	Clearing, forming, grading, and sanding near Morwell	.. .. .	1·55	..
" " " " ..	Reforming, reshaping, and resheeting with granitic sand between Yallourn turnoff and Morwell Township	.. .. .	3	..
" " " " ..	Construction of three-span reinforced concrete bridge over Waterholes Creek	.. .. .	..	..
" " " " ..	Reforming, widening, and reshaping with granitic sand and resheeting with gravel east of Morwell	.. .. .	1·83	..
MORWELL AND NARRACAN SHIRES— Prince's Highway East—Section 2	Forming and grading through Haunted Hills	.. .. .	1·23	..
NARRACAN SHIRE— Prince's Highway East—Section 2	Construction in the Townships of Moe, Yarragon, and Trafalgar	Supply and delivery of 585 cubic yards of screenings between Moe River and Yarragon and Township of Trafalgar	4	..
" " " " ..	Widening, forming, and surfacing with bituminous macadam between Trafalgar and Moe	.. .. .	2·91	..
" " " " ..	Construction of two reinforced concrete culverts near Township of Yarragon	.. .. .	..	..
ORBOST SHIRE— Prince's Highway East—Section 4	Clearing, forming, and gravelling near Nowa Nowa at Johnson's Gully	.. .. .	·55	..
PORTLAND SHIRE— Prince's Highway West—Section 5	Supply of 5,200 cubic yards of limestone metal and 720 cubic yards of screenings near South Australian border	.. .. .	..	..
" " " " ..	Forming and gravelling between Heywood and Dartmoor	.. .. .	·44	..
" " " " ..	Clearing, forming, and limestone metalling between Dartmoor and South Australian border	.. .. .	4·47	..
" " " " ..	Clearing, forming, grading, &c. ..	.. .. .	4·27	..
RIPON SHIRE— Western Highway—Section 2 ..	Supply and delivery of 540 cubic yards of gravel between Trawalla and Beaufort, also supply of 25,000 cubic yards on trucks	Supply and delivery of 200 cubic yards of quartz gravel west of Beaufort	..	..
" " " " ..	.. .. .	Supply of 1,000 cubic yards of gravel between Beaufort and Middle Creek	..	..
" " " " ..	.. .. .	Supply and delivery of 4,880 cubic yards of metal and 1,220 cubic yards of screenings	..	..
" " " " ..	.. .. .	Supply and delivery of 550 cubic yards of gravel	..	..
RIPON AND LEXTON SHIRES— Western Highway—Section 2 ..	Widening, reshaping, and surfacing with gravel between Trawalla and Burrumbeet	.. .. .	2·94	..
ROSEDALE SHIRE— Prince's Highway East—Section 2	.. .. .	Concrete culvert over Nambrook Creek	..	..
ROSEDALE AND TRARALGON SHIRES— Prince's Highway East—Section 2	Construction of timber bridge and approaches over Flynn's Creek	.. .. .	·08	..
SEYMOUR SHIRE— Hume Highway—Section 1 ..	.. .. .	Bituminous treatment westerly from bridge over Goulburn River	..	·23 (day labour)
" " " " ..	.. .. .	Quarrying, erushing, and carting 7,000 cubic yards of metal and 2,550 cubic yards of screenings from Broadford Quarries to between Tallarook and Seymour	..	..
" " " " ..	.. .. .	Forming, reforming, grading, gravelling, and surfacing with asphaltic macadam (sealed) between Tallarook level crossing and Goulburn River bridge	..	(day labour)
" " Section 2 ..	Clearing, forming, grading, and gravelling	.. .. .	·29	..
" " " " ..	Construction of reinforced concrete bridge over Eight-mile Creek and gravelling approaches	.. .. .	·01	..
" " " " ..	Forming, grading, and gravelling	.. .. .	1·03	..
" " " " ..	Supply and delivery of 900 cubic yards of gravel between Seymour and Avenel	.. .. .	..	..
		Carried forward	89·69	115·32

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD—<i>continued.</i>				
SEYMOUR SHIRE— <i>continued.</i> Hume Highway—Section 2 ..	Construction of three reinforced concrete culverts with gravelled approaches between Seymour and Mangalore	Brought forward	89·69 '01	115·32 ..
" " " ..	Forming, reforming, grading, and gravelling	.. .. .	'86	..
" " " ..	Supply and delivery of 12,350 cubic yards of gravel between Tallarook and Goulburn River bridge	.. .. .	..	..
" " " ..	Construction of two reinforced concrete box culverts, together with gravelled approaches near Mangalore	.. .. .	'01	..
" " " ..	Construction of three reinforced concrete box culverts with gravelled approaches between Mangalore and Avenel	.. .. .	'01	..
SEYMOUR, EUROA, AND VIOLET TOWN SHIRES— Hume Highway—Section 2 ..	Forming, grading, and gravelling	.. .. .	1'42	..
STAWELL SHIRE— Western Highway—Section 3 ..	Forming and gravelling in Townships of Avenel, Euroa, and Violet Town	.. .. .	3'09	..
STAWELL AND WIMMERA SHIRES— Western Highway—Section 3 ..	Construction of culvert near Deep Lead	.. .. .	..	..
STAWELL AND WIMMERA SHIRES— Western Highway—Section 3 ..	Forming, reforming, trimming, and gravelling between Deep Lead and Dadswells	.. .. .	9'89	..
TAMBO SHIRE— Prince's Highway East—Section 4 ..	Dismantling and removal of old bridge over Tambo River at Swan Reach	.. .. .	..	..
" " " " ..	Grubbing, clearing, forming, grading, and gravelling	.. .. .	'39	..
TRARALGON SHIRE— Prince's Highway East—Section 2 ..	.. .. .	Reforming and gravelling east of Traralgon ..	..	1'54
" " " " ..	.. .. .	Sealing with bitumen and gravel ..	..	1'54 (day labour)
WANGARATTA SHIRE— Hume Highway—Section 3 ..	Clearing, forming, and grading ..	750 cubic yards of quartz tailings ..	1'2	..
" " " " ..	Grubbing, clearing, and forming at Springhurst	Clearing, forming, and grading ..	'31	1'52
" " " " ..	Trimming, shouldering, and re-sheeting with gravel	.. .. .	1'89	..
WARRNAMBOOL SHIRE— Prince's Highway West—Section 3 ..	Supply and delivery of 1,950 cubic yards of scoria between Garvoc and Panmure	Supply and carting of 1,450 cubic yards of scoria between Cudgee and Warrnambool	..	..
" " " " ..	.. .. .	Supply and delivery of 1,000 cubic yards of scoria between Warrnambool and Port Fairy	..	..
WERRIBEE SHIRE— Prince's Highway West—Section 1 ..	Construction of bituminous concrete, &c., Guiding Star deviation	Resheeting between concrete shoulders east of Werribee Township	'56	1'71 (day labour)
" " " " ..	Construction of concrete shoulders and reforming near Township of Werribee	Reconstruction of edges between 17 and 18 mile posts east of Werribee	1'71	1 (day labour)
" " " " ..	.. .. .	Crushing spalls and providing crushed rock for shoulders between Kororoit Creek and Skeleton Creek	..	6'6 (day labour)
" " " " ..	.. .. .	Widening from 16 to 20 feet in bituminous macadam (penetration) and laying concrete kerbs between Skeleton Creek and 755 feet east of railway crossing	..	1'28
WERRIBEE AND CORIO SHIRES— Prince's Highway West—Section 1 ..	Supply and delivery of 3,000 cubic yards of metal and 900 cubic yards of screenings	Widening and resurfacing with asphaltic macadam (penetration) with concrete kerbs between Little River turnoff and 500 feet past Little River Bridge	..	1'05 (day labour)
" " " " ..	.. .. .	Lengthening and widening about 40 minor structures to 40 feet between kerbs	..	(day labour)
" " " " ..	.. .. .	Construction of two reinforced concrete culverts	..	(day labour)
WIMMERA SHIRE— Western Highway—Section 3 ..	Construction of three timber bridges over Mt. William Creek and floodways near Dadswell's Post Office	.. .. .	..	..
" " " " ..	Boxing, trimming, and grading near Dadswell's Bridge	.. .. .	'25	..
" " " " Section 4 ..	Reconstruction between Horsham and Pimpinio	Surfacing sand clay with bitumen between Horsham and Pimpinio	1	6 (day labour)
" " " " ..	Forming and grading between Pimpinio and Wail	.. .. .	4'95	..
WINCHELSEA SHIRE— Prince's Highway West—Section 1 ..	Widening, reshaping, and re-sheeting	.. .. .	2'4	..
" " " " " ..	Supply of 720 cubic yards of screenings stacked on roadside	.. .. .	..	..
" " " " Section 2 ..	.. .. .	Resealing west of Winchelsea	..	6'25 (day labour)
" " " " " ..	.. .. .	Supply and delivery of 920 cubic yards of gravel west of Winchelsea	..	..
WODONGA SHIRE— Hume Highway—Section 3 ..	Supply and delivery of 1,400 cubic yards of metal and 420 cubic yards of screenings between Wodonga and Albury	.. .. .	..	..
WYCHEPROOF SHIRE— Calder Highway—Section 3 ..	Clearing, forming, and gravelling	.. .. .	6'15	..
Total			125·79	143·81

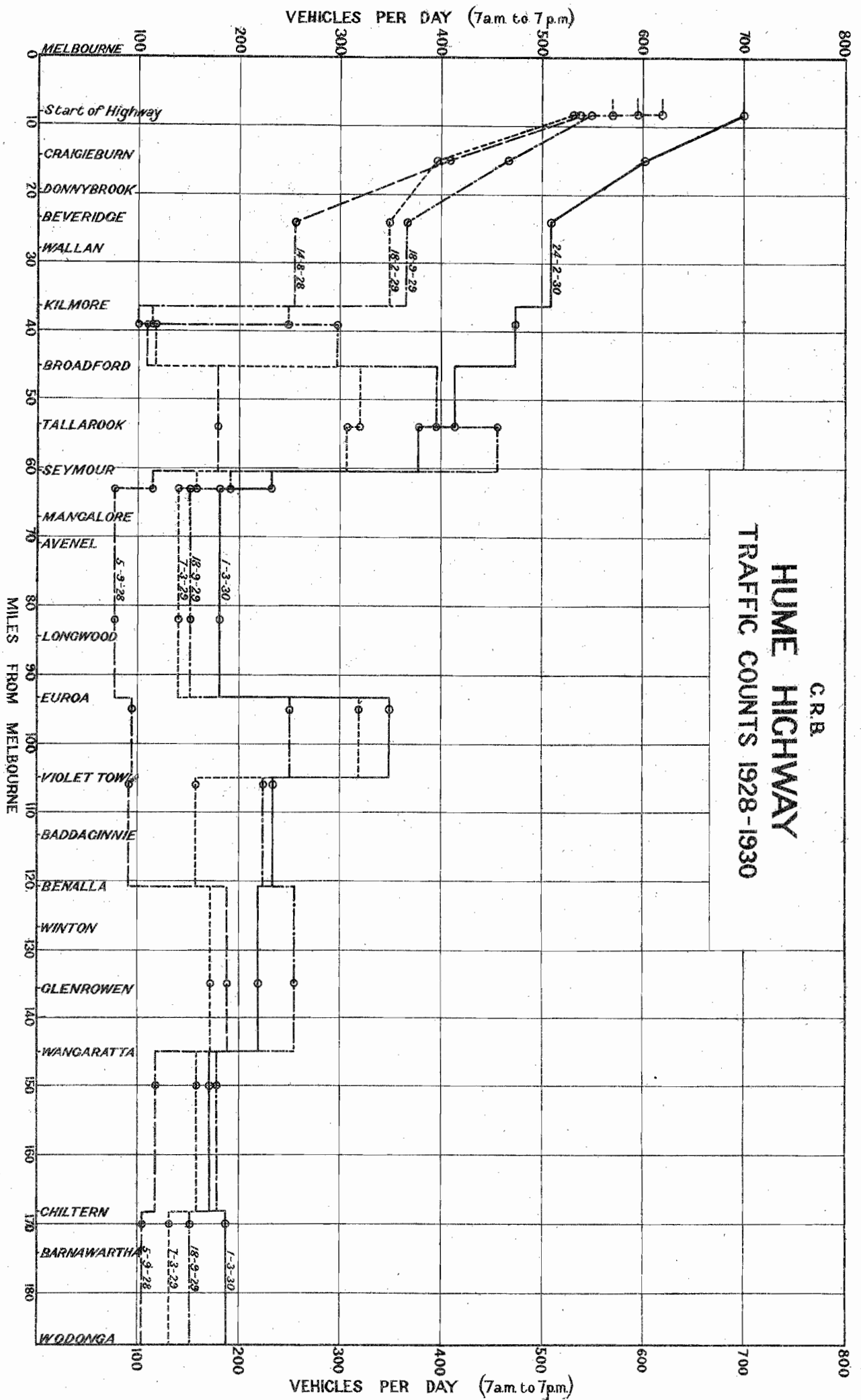


FIGURE No. 2.