

1928.  
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VICTORIA.

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# COUNTRY ROADS BOARD.

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## FOURTEENTH ANNUAL REPORT

FOR YEAR ENDED 30<sup>TH</sup> JUNE, 1927.

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PRESENTED TO BOTH HOUSES OF PARLIAMENT PURSUANT TO ACT No. 2635.

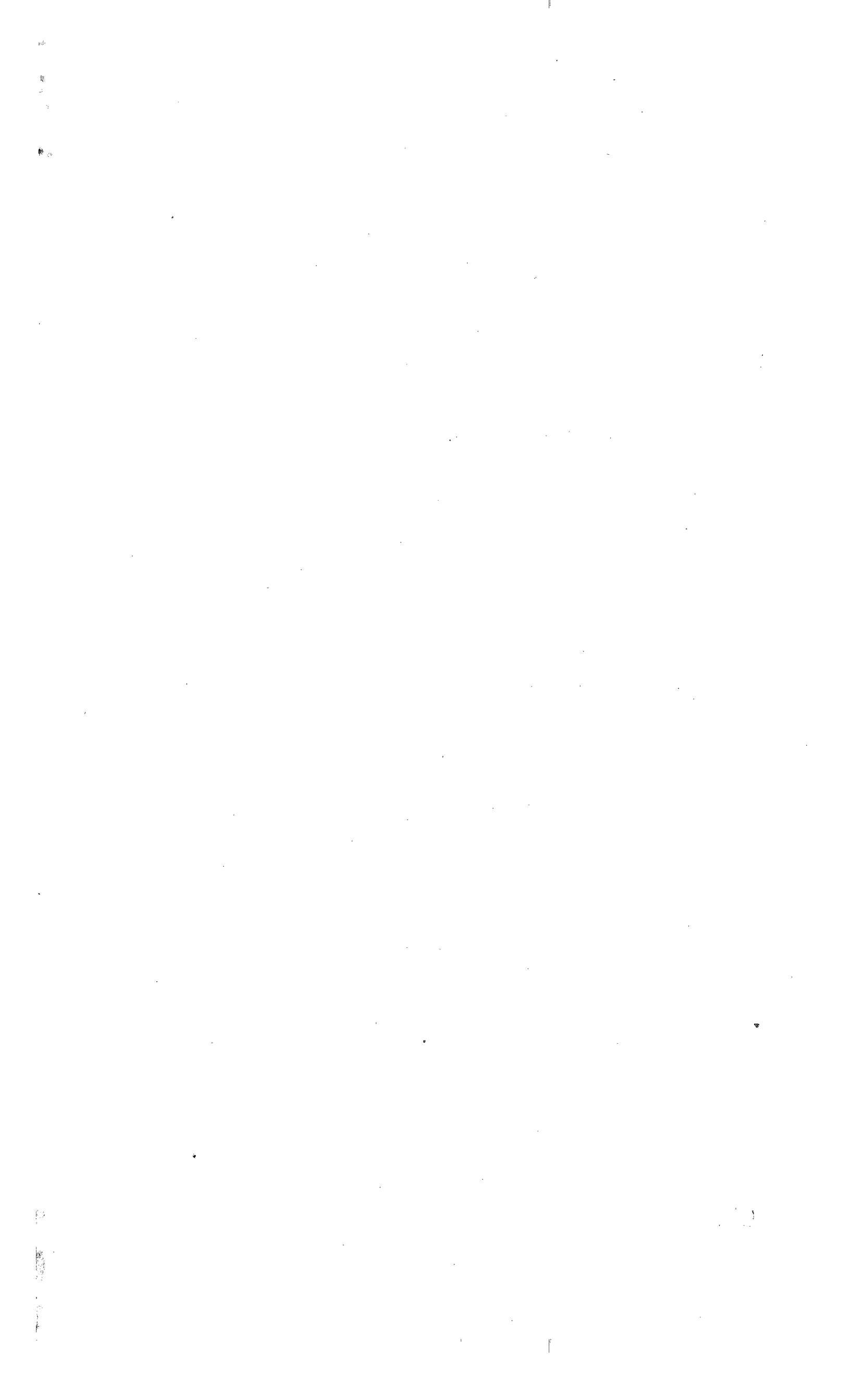
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# COUNTRY ROADS BOARD.

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## FOURTEENTH ANNUAL REPORT.

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Country Roads Board, Melbourne.  
30th April, 1928.

*The Honorable J. P. Jones, M.L.C.,*  
*Minister for Public Works, &c., &c.*

SIR,

I have the honour to submit to you for presentation to Parliament the Fourteenth Annual Report, setting forth proceedings of the Board for the financial year ending 30th June, 1927, in compliance with the provisions of Section 73 of the Country Roads Act (No. 2635).

The *Federal Aid Roads Act* 1926, which formed the subject of an agreement between the Commonwealth and State Governments, came into operation during the year.

Prior to the passing of this Act, assistance for road construction granted by the Commonwealth took the form of special grants for roads classed under Federal legislation as Main Developmental Roads.

With the approval of the Commonwealth Authorities, these funds were expended exclusively upon roads in the more remote and undeveloped areas of the State upon a system of roads classed as "Developmental" under State legislation, the Federal grants being used to supplement loan moneys provided by the State.

The assistance now granted under the *Federal Aid Roads Act* 1926 will be of much wider application than was possible under the Federal grant system, in that in the first place the provision annually is much larger, amounting to £360,000, compared with £90,000 previously, whilst roads in a different category and distinct from the Developmental system—such as the main traffic-bearing arteries which embrace the State highway system—are also subject to assistance under the Act.

When the Highways and Vehicles Act (No. 3379), which vested in the Board the control of six (6) interstate highways, was passed in 1924, the Board immediately entered upon an extensive system of re-conditioning and maintenance with State funds. This work of re-conditioning now comes within the scope of the Federal Aid Roads Act, with the result that more rapid progress is being made with the work.

The amount contributed by the Commonwealth towards this work during the year amounted to £108,499 6s. 5d.

The total expenditure for the year on the six (6) State highways, embracing the construction of new works, re-conditioning, and maintenance amounted to £672,840 7s. 7d., towards which the Commonwealth contribution, as above, amounted to £108,499 6s. 5d.

### MAIN ROAD CONSTRUCTION AND MAINTENANCE.

The expenditure of loan moneys on permanent works on main roads amounted to £350,400 5s. 6d., £173,233 13s. 5d. of which has been debited to municipalities.

In the maintenance of main roads, expenditure amounted to £426,708 15s. 6d., towards which the municipalities are required to contribute one-third. The total expenditure on main roads, exclusive of State highways, therefore reached the large sum of £777,109 1s.

This is the largest sum expended on main roads in any year since the inception of the Board, and is largely due to the increasing demands of traffic and the requirements of municipalities consequent on the reduction to one-third of their contribution.

## DEVELOPMENTAL ROAD CONSTRUCTION.

A number of the roads classed as "Developmental" under State legislation come within the category of "Main roads which open up and develop new country," provided for in Clause 5 of the Federal Aid Roads Agreement, consequently the expenditure for the year by the State of £600,203 6s. 3d. was supplemented by Commonwealth funds, amounting to £70,245 4s. 1d., or a total expenditure of £670,448 10s. 4d.

In addition to the above, a number of special appropriations were made by the State, all of which, although of lesser magnitude, are a very material factor in the development of the rural districts.

The following is a summary of the expenditure incurred during the year under the respective financial provisions, all of which are explained in detail in the attached report to the Chairman of the Board by the Chief Engineer, and are also shown in detail in the appendices:—

		1.—STATE HIGHWAYS.									
		£	s.	d.	£	s.	d.	£	s.	d.	
Construction	.. .. .	52,391	9	6							
Maintenance	.. .. .	511,949	1	8	564,341	1	2				
Commonwealth contribution towards construction and re-conditioning					108,499	6	5	672,840	7	7	
		2.—MAIN ROADS.									
Construction	.. .. .	346,467	6	9							
Commonwealth contribution	.. .. .	3,932	18	9							
					350,400	5	6				
Maintenance	.. .. .				426,708	15	6	777,109	1	0	
		3.—DEVELOPMENTAL ROADS.									
Construction	.. .. .	600,203	6	3							
Commonwealth contribution	.. .. .	70,245	4	1							
								670,448	10	4	
4. Unemployment relief	.. .. .							2,356	10	10	
5. Roads to develop tourist resorts	.. .. .							4,919	13	7	
6. Roads for isolated settlers	.. .. .							2,494	19	6	
7. Williamstown Short Road	.. .. .							22,261	8	3	
8. Migration Grant—Childers Settlement Road	.. .. .							6,410	13	1	
9. Thrip (Orchardists') relief	.. .. .							6,097	0	9	
10. Highways and Vehicles Act (No. 3379)	.. .. .							26,162	18	3	
								£2,191,101	3	2	

## APPORTIONMENT OF COSTS.

The cost of permanent works and maintenance to the 30th June, 1926, was apportioned during the year in accordance with the provisions of Section 27 of the *Country Roads Act* 1915, resulting in a sum of £155,742 6s. 4d. being charged to municipal councils on account of permanent works expenditure, and an amount of £144,369 17s. 11d. in respect of expenditure on maintenance.

The number of motor vehicles and motor cycles registered under the Motor Car Act during the year, including renewals, was 110,950 and 23,010 respectively, the net revenue received being £796,912 4s. 2d.

I have the honour to be, Sir,

Your obedient servant,

W. McCORMACK, Chairman.

ARTHUR E. CALLAWAY, Member.

W. L. DALE, Secretary.

Country Roads Board,  
Titles Office, Melbourne.

The Chairman.

SIR,

I have the honour to submit the following Report on the work carried out in this Department, and under the immediate supervision of this office, during the year ended 30th June, 1927.

The following statements of expenditure indicate that the works carried out under the direct supervision of the Board for the year 1926-27 total £798,196, of which amount a total of £462,585 was expended on the highways, as indicated by the attached schedule of highways expenditure. It will be noticed that of the total amount expended £295,612 was spent on maintenance, or approximately 27 per cent. of the whole, and that the mileage of roads so maintained amounts to 684 for highways, and 141 for main roads, or a total of 825 miles.

STATEMENT OF EXPENDITURE ON WORKS CARRIED OUT UNDER THE DIRECT SUPERVISION OF THE BOARD—FINANCIAL YEAR 1926-27.

Permanent Works .. .. .	£78,742
Maintenance (including Reconstruction Works) .. .. .	295,612
Developmental Roads .. .. .	119,741
Special Works—	
Tourist Vote .. .. .	£4,920
Unemployment Relief .. .. .	2,357
Williamstown Short Road .. .. .	22,261
Migration Grant .. .. .	6,410
Thrip (Orchardists') Relief .. .. .	2,815
	38,763
Trust Fund—	
Federal State Grant (No. 1) .. .. .	165
Federal State Grant (No. 2) .. .. .	211
Federal State Grant (No. 3) .. .. .	2,500
Federal State Grant 1925, Act 17 .. .. .	26,463
Federal State Grant 1925, Act 17, Second Schedule .. .. .	22,453
	51,792
Federal Aid Roads Act 1926 .. .. .	213,546
	£798,196

STATE HIGHWAYS.

EXPENDITURE UNDER THE DIRECT SUPERVISION OF THE BOARD—FINANCIAL YEAR 1926-27.

	Maintenance (including Reconstruction).	Federal Aid Roads, 1926.	Permanent.	Developmental.	Total.
	£	£	£	£	£
Princes Highway West—					
Section 1 .. .. .	52,276	31,769	3,793	..	87,838
Section 2 .. .. .	21,459	933	..	..	22,392
Section 3 .. .. .	20	..	..	..	20
Section 4 .. .. .	11	..	..	..	11
Section 5 .. .. .	75	..	..	..	75
Princes Highway East—					
Section 1 .. .. .	38,214	22,699	..	..	60,913
Section 2 .. .. .	20,980	8,534	..	..	29,514
Section 3 .. .. .	6,716	1,388	1,641	27	9,772
Section 4 .. .. .	2,609	..	..	..	2,609
Section 5 .. .. .	7,948	..	..	..	7,948
Section 6 .. .. .	12,564	..	..	..	12,564
Western Highway—					
Section 1 .. .. .	18,178	47,471	21	..	65,670
Section 2 .. .. .	5,297	..	..	..	5,297
Section 3 .. .. .	20	..	..	..	20
Section 4 .. .. .	2	..	..	..	2
North-western Highway—					
Section 1 .. .. .	34,896	23,976	52	..	58,924
Section 2 .. .. .	9,746	1,483	..	..	11,229
Section 3 .. .. .	4,278	5,973	..	..	10,251
Section 4 .. .. .	..	..	..	..	..
Section 5 .. .. .	244	760	..	..	1,004
Section 6 .. .. .	10	..	..	..	10
North-eastern Highway—					
Section 1 .. .. .	14,143	28,960	..	..	43,103
Section 2 .. .. .	11,592	3,307	214	..	15,113
Section 3 .. .. .	15,990	..	..	..	15,990
Omeo Highway—					
Section 1 .. .. .	1,492	423	..	157	2,072
Section 2 .. .. .	48	..	..	..	48
Section 3 .. .. .	169	..	..	..	169
Section 4 .. .. .	25	..	..	..	25
Northern Highway .. .. .	2	..	..	..	2
	279,004	177,676	5,721	184	462,585

The schedule of Main Roads maintained under the direct supervision of the Board, other than Highways, is as follows:—

Warburton to Woods Point..	..	..	40 miles
Woods Point to Jamieson ..	..	..	28 miles
Boolarra to Gunyah ..	..	..	13 miles
Porepunkah to Mount Buffalo ..	..	..	18 miles
Boolarra to Welshpool ..	..	..	38 miles
Walhalla to Aberfeldy ..	..	..	22 miles
Total ..	..	..	159 miles

Under the Tourist Vote, £4,920 was expended on the following roads:—

Promontory-road to National Park ..	..	..	£	s.	d.
Hall's Gap-Wartook (connecting road through Grampians)..	..	..	556	0	0
Great Ocean-road extension between Lorne and Wye River ..	..	..	3,434	0	0
Total ..	..	..	922	0	0
Total ..	..	..	£4,919	0	0

An amount of £2,356 was expended out of the Unemployment Relief Grant as indicated by the following schedule:—

<i>Alberton Shire.</i> —Binginwarri-Welshpool-road, clearing, forming, and gravelling ..	..	..	£	s.	d.
<i>Healesville Shire.</i> —Toolangi-road, earth works ..	..	..	719	0	0
<i>Orbost Shire.</i> —Errinundra-road.—Clearing and earth-works south of Boulder Creek ..	..	..	828	0	0
<i>Orbost Shire.</i> —Deddick River-road.—Widening turn-back to McKellar's Crossing, west of Snowy River ..	..	..	719	0	0
Total ..	..	..	90	0	0
Total ..	..	..	£2,356	0	0

It might be noted that the work done under the latter allotment was entirely day labour, and that compared with contract labour the resultant efficiency was from 66 per cent. to 70 per cent., owing partly to the men not being accustomed to the work, and partly to a considerable amount of work being done entirely by hand labour, plant not being used to the same extent as would be done under the contract system.

The work done on the Williamstown Short Road has formed the subject of a special report detailing the procedure, as it is thought that the technical details would be of considerable interest to engineers generally.

The expenditure under the Migration Grant, £6,410, was entirely within the Childers Settlement, in forming and grading the road between the Estate and Yarragon.

Under the Orchardists' Relief Grant, £2,815 was expended between Castlemaine and Harcourt.

#### STATE HIGHWAYS.

The total length of the highways declared to the end of the year was 1,474 miles, 684 miles being directly controlled by the Board's staff. This length includes practically all the inlying heavy traffic areas within 60–100 miles of Melbourne, and the expenditure during the year on these lengths was as follows:—

Highway.	Total Length Declared.	Length under Direct Control.	Direct Expenditure
Prince's Highway (West) ..	244	46	£ 110,336
Prince's Highway (East) ..	296	261	123,320
Western Highway ..	244	97	70,989
North-western Highway ..	324	117	81,418
North-eastern Highway ..	161	156	74,206
Omeo Highway ..	184	7	2,314
Northern Highway ..	21	..	..
Totals ..	1,474	684	462,583

Progress continues to be made with the reconstruction of the State highways, aided by Federal funds, but, as pointed out in the last Report, the cost of reconstructing roads neglected for forty or fifty years is necessarily high. The annual mileage completed, therefore, is limited, and a continuance of the heavy maintenance work by gangs equipped with rollers, road machines, and similar heavy equipment, has had to be continued. The patrol system has also been extended, and now every mile of State highway controlled directly by the Board is under the care of a patrolman.

In order to give rapid relief to traffic from the intolerable conditions existing, and also to reduce the high cost of keeping the present roads in a safe condition, it is necessary that the reconstruction work be pushed ahead as rapidly as possible. Again, the fast increasing burden of heavy motor traffic is making greater demands on the highways, and this can eventually only be met, in the inner areas particularly, by the construction of high-grade pavements. An attempt to construct these now would make the possible annual mileage very low, and to meet these conflicting conditions a policy of progressive reconstruction is being pursued, following the recommendation made in the Report of the Chairman on his inspection abroad.

As an example, the reconstruction of the highways within the first 60-100 miles of Melbourne might be taken. Here penetration asphaltic macadam is being laid, at the comparatively low cost of approximately £3,000 per mile, utilizing to the full the often high salvage value of the existing foundations. This pavement is quite adequate for present needs, and when traffic conditions warrant, as will be shown by increasing maintenance costs, an asphaltic concrete carpet can be laid, converting it into a high-grade permanent pavement. The total cost of the two stages is little different to the whole construction in one stage, interest on the difference in cost (approximately £6,000 per mile) is saved for some years, and traffic is at all times receiving maximum service with the funds available. A distinct technical advantage also is gained by the initial consolidation of the asphaltic macadam, enabling a much better surface to be obtained when the asphaltic concrete is laid. This general policy is being applied to all conditions. In outlying areas, natural earth and sand-clay formations are being constructed and will later be surfaced with gravel. With increasing traffic, the gravel may be thickened if necessary, and surfaced with a bituminous seal coat; later again, it will serve as an excellent base for an asphaltic macadam wearing surface.

This policy results in the needs of traffic being met adequately at each stage of development without tying up money in excess of requirements. This saves interest and allows full progress to be made with the available funds. It is, of course, essential to the successful development of such a plan that the asset created at each stage should be preserved. The maintenance organization, therefore, takes charge of the works immediately the construction is finished.

In certain of the most heavily trafficked sections needing reconstruction, the immediate provision of high-grade pavements has been necessary.

The asphaltic concrete works described in detail in the last Report have been completed. These were situated at Laverton (5 miles), Mulgrave (3.3 miles), and Keilor (2.4 miles). Additional contracts are now in progress for the construction of 2 miles of reinforced cement concrete road at Corio, and 2 miles of asphaltic concrete near Deer Park. These works are described in detail below.

#### CONTRACT No. 38FP-60, FOR CONSTRUCTING 2 MILES OF REINFORCED CONCRETE ROAD AT NORTH GEELONG.

The work carried out under this contract extends from the junction of the Prince's Highway with the Bacchus Marsh-road to the Separation-street Railway Bridge at North Geelong, a distance of 10,775 lineal feet.

Tenders were called (1) for the construction of a concrete pavement 20 feet wide, flanked on each side by a strip of asphaltic macadam 2 feet wide, or alternatively (2) for widening to 20 feet and strengthening the existing metal base with waterbound macadam, surfacing with 2 inches of asphaltic concrete and kerbing with concrete kerbs to be supplied by the Board.

Owing to the poor quality and small depth of the existing waterbound limestone, as indicated by dipping, the quantity of new material required for the second of the two types of construction was large. Chiefly owing to this factor, the total cost of the first type, that is, the construction of a concrete road, was smaller, and the tender of Victoria Roads Limited for carrying out the work at a cost of £29,714 17s. 9d. was accepted.

Work on the contract was begun on 21st March, 1927; concreting began on the 13th of the following month, and is now practically completed.

The class of construction is shown in the typical cross-section and plan (Figures 1 and 2). The mix specified was 1 part of Portland cement to 5 parts of fine and coarse aggregate, each measured separately, the ratio of fine to coarse aggregate to be fixed by the Engineer. The compressive strength of cylinders, 6 inches in diameter and 9 inches high, made from the materials used on the contract and in the proportions used on the works, was specified as 3,000 lb. per square inch at 28 days, the tests being carried out at the Melbourne University.

The mix used on the works consisted of 1 part of Portland cement, 2 parts of fine aggregate, and 3 parts of coarse aggregate graded from 2 inches down. The fineness modulus of this mix was approximately 5.9. The cylinders made at the University before work began had a compressive strength of 3,085 lb. per square inch (average of two cylinders) at 28 days. Cylinders made in the field during the progress of the work, from concrete dumped in the road bed, had a compressive strength at 28 days (average of two cylinders) varying from 2,370 lb. per square inch to 3,050 lb. per square inch. The low values are considered to be due to the difficulty of making cylinders in the field with ends perpendicular to their axes, and to the fact that the cylinders were cured for six days in wet sand, of which the temperature approached freezing point during the nights in winter.

Daily reports from the Resident Engineer kept the office in close touch with the work, and when filed formed a record of the construction of the pavement. A typical report is reproduced hereunder, and indicates the nature of the information obtained.

The maintenance of the pavement will be in the hands of the Board. It is intended that the crack which has developed, as predetermined, on the line of the longitudinal centre joint, shall be treated with a mixture of benzol and bitumen, and covered with coarse sand, and shall only be cut out and filled with an asphaltic mixture in places where breaking away of the concrete edges of the crack occurs. The cost of maintenance will be kept with a view to having definite data for future estimates.

#### TYPICAL REPORT, 1927.

##### DAILY REPORT—CONCRETE ROAD CONSTRUCTION.

Date—11th May, 1927.  
Contract No. 38FP-60.  
Road—P.H. Main Road, Corio.

##### Progress.

	Chainage to Chainage.	Width.	Length.
	Feet.	Feet.	Feet.
Contract quantity .. .. .	— 400 to 10,375	20	10,775
Previously completed .. .. .	— 400 to 2,635	20	3,035
Day's work .. .. .	2,635 to 2,756	20	121
Total to date .. .. .	— 400 to 2,756	20	3,156

##### Mix.

Material.	Origin.	Dry Volume per Mix.		Allowance for Bulking.	Fineness Modulus.	Number of Grading Sheet.
			Cub. ft.	Per cent.	Per cent.	
Stone .. .. .	Albion .. .. .	3	12	10	7.83	6
Sand .. .. .	Lara .. .. .	2	8		3.23	
Cement .. .. .	Geelong .. .. .	1	4	F.M. for mix 5.9		
Water .. .. .	Geelong Supply .. .. .	..	..			

Volume per batch in road—16.52.

Cement factor—0.275.

Batches for day—100.

##### Slump Tests.

Time of mixing—4.30 p.m.

Slump—1½ inches.

##### Test Cylinders.

Cylinder number .. .. .	33	34	35	36
F.M. of mix .. .. .	..	5.9	..	..
Time of mixing .. .. .	..	4.45 p.m.	..	..
Slump of batch .. .. .	..	1½ inches	..	..
Road chainage of batch .. .. .	..	2,740 feet	..	..
Date of despatch .. .. .	..	..	..	..
Date of breaking .. .. .	18th May	8th June	8th June	18th May
Strength, lb. per square inch .. .. .	2,250	3,250	2,460	1,850

Weather—Dull.

Remarks—4 feet of mesh placed in bottom of slab over water pipe.

Joint very close to excavation.



CONTRACT No. 23FH-9, FOR WIDENING, RE-SHEETING, AND SURFACING TWO MILES OF THE WESTERN HIGHWAY AT DEER PARK.

The work carried out under this contract extends from the Albion Railway crossing to the bridge over the Kororoit Creek at Deer Park, a distance of 11,315 feet.

Owing to the presence of a cobblestone base, approximately 16 feet wide, on the whole length of the works, it was decided to call tenders for widening to 20 feet and re-sheeting the existing road with waterbound macadam, setting a concrete kerb on each side of the widened base and surfacing with 2 inches of asphaltic concrete. The tender of the Neuchatel Asphalte Co. for carrying out the works at a cost of £22,079 13s. 3d. was accepted.

Work on the contract was begun on 5th April, 1927; laying of the surfacing was begun on 11th May, 1927, and is still in progress.

The method of reconstruction is indicated in the typical cross-section shown in Figure 3. The specification for the surfacing called for 2-in. asphaltic concrete, with premixed seal coat of the type described in the Thirteenth Annual Report. The specification submitted by the Neuchatel Asphalte Co., with an alternative tender, for a 2-in. stone-filled asphalt surfacing, with no seal coat, was however, accepted.

The French coefficient of wear specified for the 2½-in. metal for the waterbound base, and the coarse aggregate for the asphaltic surfacing, was reduced to 8, with a view to using local stone. Refined Trinidad native bitumen fluxed to produce a mixture having a penetration of 40-50 (100g. 5 secs., 77° F.) was used as the asphaltic cement in the surfacing.

Details of the mix on 18th July, 1927, which is typical of the material used in the greater part of the surfacing, are given below:—

1. Grading of total *dry* mix, including filler—

Circular Openings in Size of Screen, or Designation of Sieve.	Percentage of Total Dry Mix passing Screen or Sieve.
1 inch .. .. .	100
¾ inch .. .. .	96
½ inch .. .. .	87
¼ inch .. .. .	65
10 .. .. .	55
200 .. .. .	7

2. Grading of sand (all material, including filler, passing a 10-mesh sieve and held on a 200-mesh sieve)—

Sieve Numbers.		Percentage passing Sieve A held on Sieve B.
A.	B.	
10	40	32
40	80	46
80	200	22

3. *Nature of Filler.*—Basalt dust, of which 80 per cent. passed a 200-mesh sieve.

4. *Percentage of Filler Used.*—Amount of filler used, expressed as percentage of total *dry* mix, was 7.9.

5. *Percentage of Asphaltic Cement.*—Amount of asphaltic cement used, expressed as percentage of *total* mix, was 11.

The surfacing, a sheet asphalt with a stone filling comparable to plums in mass concrete, necessitated a more stable sand and bitumen matrix than a coarse graded asphaltic concrete, in which interlock between the larger particles helps towards stability. The sand grading obtained agrees closely with Clifford Richardson's ideal mix for traffic midway between "medium" and "light." This is considered satisfactory.

#### DISTRICTS.

Another District Engineer was appointed during the year, taking charge of both the Stawell and Warrnambool districts for the present. This makes the fourth District Office to be established. The work of the District Engineers consists to a large extent of general supervision of work carried out by the various shire councils on the Board's behalf, and of consultation with the Shire Engineers. The greater part of the work carried out directly by them for the Board

is in the maintenance and reconstruction of State highways, and of special works not supervised by the Shire Engineers. The following details indicate generally the activities controlled by District Engineers, with some general reference to the condition of the highways in the various districts :—

#### SALE DISTRICT.

The Sale District reaches from Warragul to the far east of the State, and 208 miles of the Princes Highway East in this district is controlled directly by the District Engineer. This highway is in better condition than any of the others under the jurisdiction of the Board. Gravel and granitic sand of fair to good quality are readily obtainable practically throughout, and this has made possible a much more speedy restoration than can be attempted in the other highways, where material has often to be brought long distances. Again, these materials lend themselves to economical maintenance by means of graders and drags even when thin and narrow.

The main reconstruction carried out during the year was the re-sheeting with asphaltic macadam of 7 miles of old metal near Traralgon. Re-sheeting with gravel and granitic sand was carried out on the worst sections throughout the district, the work being generally in a large number of isolated places, and a commencement was made with the re-alignment of certain sections. The road in the Haunted Hills, and east of Orbost, requires widening and re-aligning practically throughout to increase visibility, but the worst section of the Haunted Hills was completed during the year, together with about 5 miles of road west of the Cann River. A continuation of this work, to make the road safer for the greatly increased tourist traffic, is one of the most important improvements required on this road, although the generally thin character of the existing gravel and sand surfacing will require considerable strengthening in the near future.

An experimental stretch of bituminous sealed gravel was laid down near Sale, a priming coat of fuel oil and bitumen, followed by a coat of bitumen, being used. The result has been very good, a stable mat of fair thickness having been formed, and it is anticipated that its life will be much greater than that given by previous methods, with lower maintenance costs. An extension of this type of seal coat is proposed if the experimental length successfully withstands the next summer, as seems very probable.

Besides the works on the highway, the District Engineer supervised the construction of a timber truss bridge on concrete piers over the Murrindal River, beyond Buchan, the construction of a section of the Omeo Highway on the Mountain Ash deviation, and new construction on the Orbost-Delegate road at Granite Creek.

#### BENDIGO DISTRICT.

While the North-western Highway in the Bendigo District reaches from Castlemaine to Mildura, the length controlled directly by the Board through its District Engineer is only from Castlemaine to Bridgewater, and the section in the Charlton Shire from Woosang to Teddywaddy.

North-Western Highway, Woosang Flats.



PLATE No. 1.—Typical View of Old Formation.

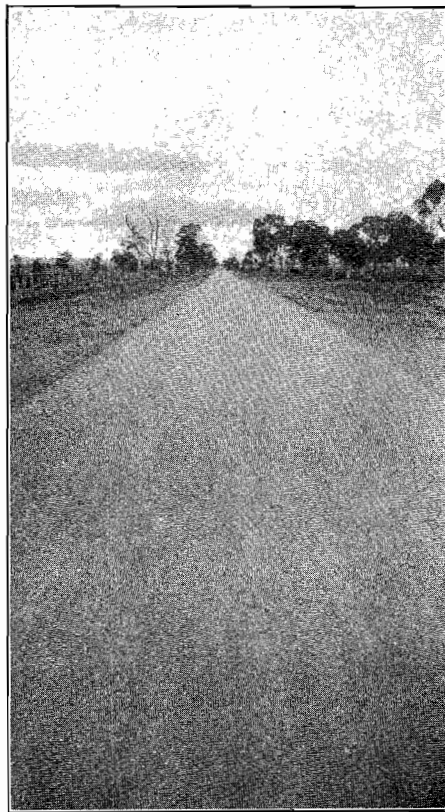


PLATE No. 2.—Reconstructed Portion (Asphaltic Macadam).

The most important works on these sections were the reconstruction of the rough section of road from the Big Hill, beyond Ravenswood, to Kangaroo Flat, and the construction, also in asphaltic macadam, over the boggy clay flats at Woosang.

The road from Castlemaine to Bendigo is now in good order, but from Bendigo to Bridgewater the old macadam is generally rough, although considerable improvement has been made by regular patrol work. All this length requires re-sheeting when funds are available, the most urgent work being the construction of an unsurfaced length at Derby, which is very bad in wet weather. It is proposed to gravel this next year, and, with the completion in the coming year of contracts in progress in the Korong Shire, north-west of Wedderburn, and of the Woosang Flats, a hard-surfaced road will be available from Melbourne beyond Charlton to Teddywaddy.

North from Teddywaddy, through Sea Lake and Ouyen to Mildura, further contracts are in progress for forming and sheeting with gravel in the Wimmera country and with limestone in the Mallee—the latter forming the much greater length. Maintenance work on these sections consists chiefly in scooping sand from unmade sections and keeping the old limestone surfacings or old formations free from defects.

On the Bendigo-Echuca road, portions of which have been declared a State Highway, and which will eventually all be declared the Northern Highway, an important contract has been let for the deviation from Huntly to Goornong, a length of approximately 9 miles. This work is directly supervised by the District Engineer.

The problem of availability of road-making materials presents its greatest difficulties in this district, and it is the constant aim of the Board's staff to correlate experience of different engineers in the use of available materials. Deposits of Mallee limestone in the far north-west, the gravel deposits near Korong Vale, and the metamorphic sandstone quarry in the Barrakee Hills, near Charlton, have provided satisfactory materials for last year's work in the outer areas, but greater development of all such resources will be necessary if progress is to be continued.

#### BENALLA DISTRICT.

The whole of the North-eastern Highway from Seymour to the Murray River is under the control of the District Engineer, except for the last 8 miles, which are in the Wodonga Shire.

The whole length was subjected to intensive patrol maintenance, scarifier graders and drags being used freely, with the result that considerable improvement was effected. This had the result (noted on other roads, but most marked on this highway) of greatly increasing the traffic. This, in turn, considerably increases the difficulties of adequate maintenance.

Several bad sections of the highway have been re-sheeted with metal or gravel, but the main works of reconstruction were centred on the Avenel-Longwood section. This is a long-disused part of the old Sydney-road, which was excellently located both as regards alignment and soundness of the ground. Many drainage structures have been built during the year, including some large concrete box culverts and one concrete bridge, and about 9 miles have been re-formed and sheeted with granitic sand or ironstone gravel. A contract is now in progress, and this will complete the section early in the next financial year.

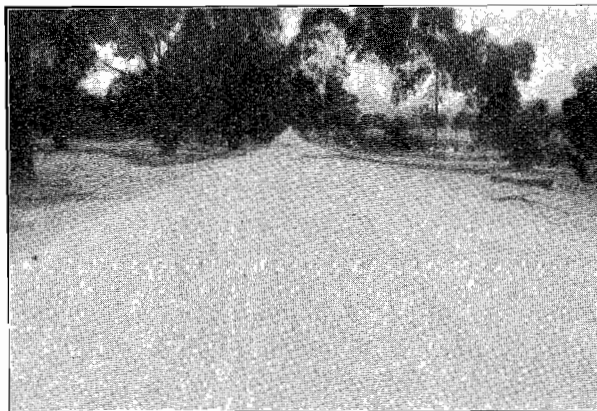


PLATE NO. 3.—North-Eastern Highway. Section between Glenrowan and Wangaratta, re-conditioned and surfaced with Granitic Sand.

A grade separation work is now being done between Barnawartha and Wodonga, where the old level crossing is to be abolished. The Railway Department have completed the construction of the subway, and a contract has been commenced for forming and gravelling the road to pass through it. More work of this nature will need to be undertaken in the future, particularly on the Springhurst-Wodonga section of this highway. The condition of the old bridges and culverts from Seymour onwards is particularly poor, and the maintenance of these structures until they can be replaced by permanent ones is a matter of considerable expense. Some have been rebuilt in timber by the District Staff, and contracts are being prepared for replacement by concrete structures. As funds permit it is intended to gradually replace all the old timber structures.

In addition to works on the highways, several other important works have been supervised by the District Engineer. One of the most important was the construction by contract of the road and bridges over the Ovens River and flats to link up the Peechelba Settlement with the new Bowser-Peechelba railway. Work has also been commenced, by day labour, to widen and re-align the Mount Buffalo-road. Fifty men are still at work on the lower section of the road, with the object of soon abolishing the first control gate. A continuation of the work will avoid the necessity for these gates, which are an expense in upkeep and a hindrance to traffic, but are at present necessary for safety.

#### STAWELL AND WARNAMBOOL DISTRICTS.

The Stawell and Warrnambool districts are still supervised by the one District Engineer, as the lengths of road controlled directly by the Board are not great.

On the Princes Highway West about 7 miles of metalling has been done directly by the Board, between Dartmoor and the South Australian border. This covers practically all the section of road likely to seriously hinder traffic, while on the Heywood side of Dartmoor gravelling of the bad sections by the Portland Shire Council is now practically completed. The construction of a further 7 miles will now complete the constructed road from Melbourne to the border. A considerable amount of reconstruction of old, badly-worn road therefore remains the major problem on this highway.

The Western Highway from Ballarat to the Ararat Shire boundary, about 35 miles, is controlled directly by the Board, the remainder being under council control. On the Board's section heavy maintenance with rollers has enabled considerable improvement to be effected to the very bad road from Ballarat to Burrumbeet;  $3\frac{1}{2}$  miles of reconstruction in waterbound and asphaltic macadam has been completed, and general repairs carried out on the whole length.

The sections under council control have been reconstructed in short lengths, but the major work has been the grading up and dragging of the old gravel roads and clay formations.

The District Engineer has also controlled work on the Grampians Tourist Road, the present objective being the making of a passable summer track from Hall's Gap along the Wannon Valley to the Mirranatwa Gap, thence by the Victoria Valley to Dunkeld.

#### BRIDGES.

During the year under review much of the bridge work throughout the State has been supervised directly by the Board's staff. In this manner twenty inspections for new projects have been made, and ten inspections where maintenance was recommended; 40 sites have been surveyed, plans and specifications for the work—to the value of £53,000—have been prepared, payments on contracts have totalled £76,000, and 28 structures, with a total length of 2,977 feet, have been opened to traffic.

The most important of the new bridges brought into use is that over the Barwon River at Geelong. Designs for this bridge were commenced during 1921, and in January, 1924, the tender of Armstrong, Whitworth Pty. Ltd. was accepted at £73,900. The bridge was opened on 18th August, 1926, by His Excellency Lord Somers, Governor of Victoria. There are four spans of approximately 104 feet. This number of spans was adopted in order to salvage and make use of the foundations of the pier of the old two-span bridge. For the new superstructure steel-plate girders were used, as this was found to be the most economical form of construction and most suited to the site, which is an alluvial flat subject to great floods that rise within 2 feet of the girders. The new abutments and piers are of mass concrete, and the foundations of the old abutments and pier were utilized, being strengthened with additional piles where necessary. The two new piers rest on ironbark piles. The substructure supports four lines of steel girders, which are arranged in a series of cantilever and suspended spans so as to obtain the greatest possible economy in the steelwork. The reinforced concrete deck rests directly on the top flanges of the girders. It is surfaced with wood blocks.

The bridge is 33 feet wide between kerbs, and has a footway 5 feet wide on each side. The contract included excavation of the river channel so as to provide material for widening the approaches from 40 feet to 60 feet, at the same time straightening the river for rowing. The bridge carries a single line of electric tramway located on the centre line.

An indication of the extent of the contract is given by the following principal quantities:—

Timber piles	..	..	..	..	5,876 feet.
Concrete	..	..	..	..	4,486 cubic yards.
Reinforcing steel	..	..	..	..	56 tons.
Structural steel	..	..	..	..	395 tons.
Earthwork	..	..	..	..	16,000 cubic yards.
Metalling	..	..	..	..	1,272 cubic yards.
Wood blocking	..	..	..	..	1,410 square yards.



Views of the completed structure are shown in Plates Nos. 4, 5, and 6.

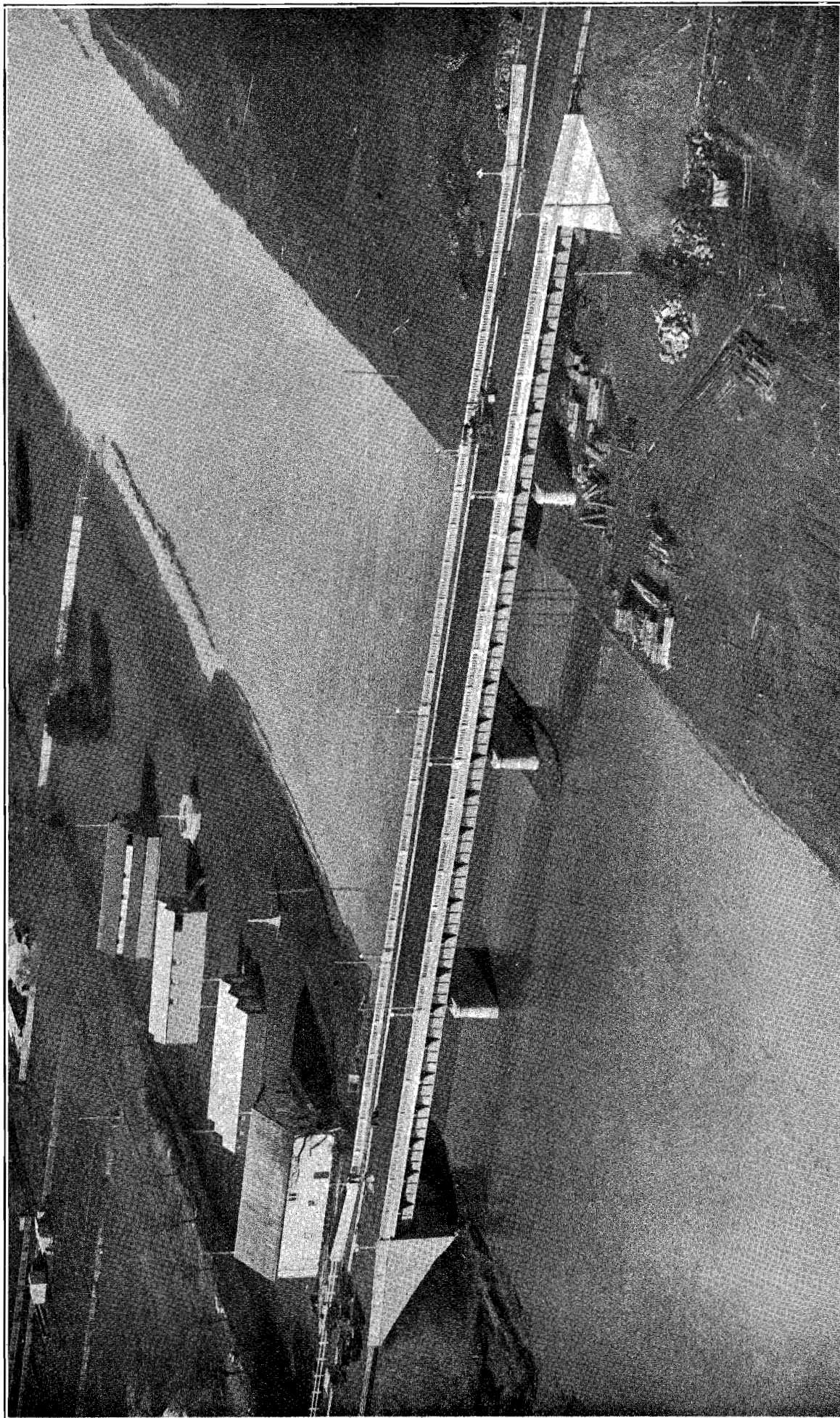


PLATE No. 4.

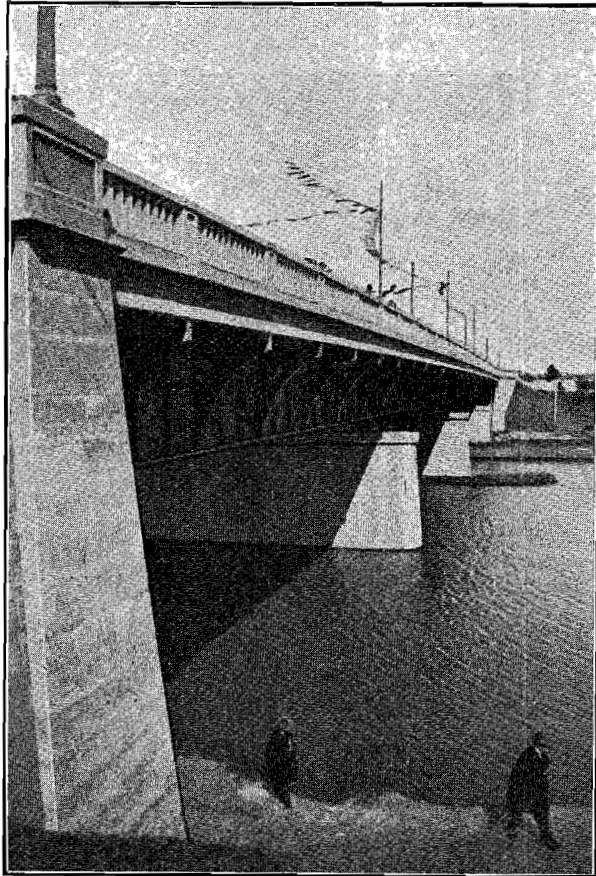


PLATE No. 5.

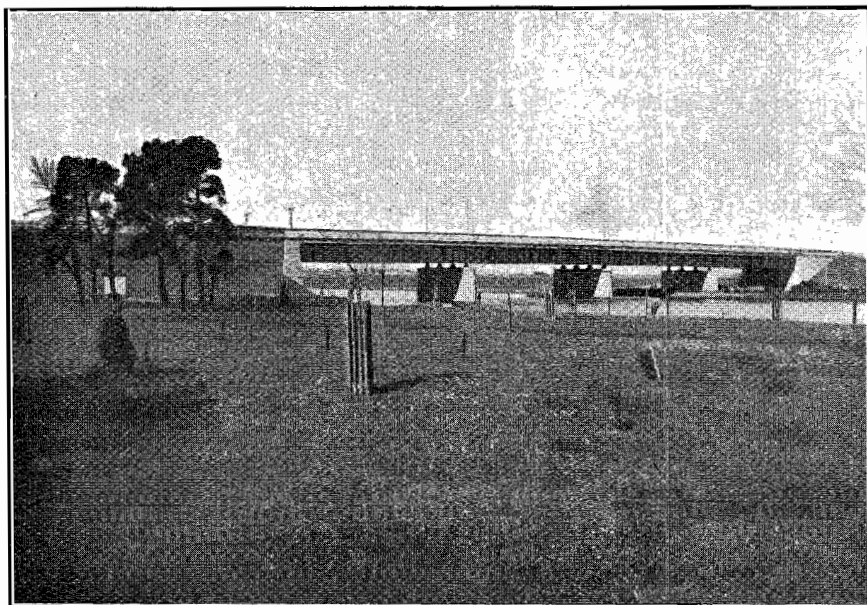


PLATE No. 6.

Two important reinforced concrete bridges near Melbourne were completed during the year, one at Eltham and one at Woori Yallock. The old bridge over Diamond Creek at Eltham picnic ground was a high-level timber structure, erected over 50 years ago. In 1923 a flood carried away the Melbourne abutment and adjacent span. The tourist and other traffic on the road has recently increased greatly, both in volume and weight, and the old timbers had become unduly light, apart from their extreme decrepitude. Repair of the bridge would have been quite uneconomical, and a new bridge was necessary. Very complete investigations were made to ensure that the design should give the fullest degree of economy. The foundation adopted consists of timber piles driven to rock, the heads being protected with concrete down to lowest ground-water level. The superstructure contains two spans of 40 feet with two of 30 feet on each end, designed as continuous over-piers, with cast-iron rocker bearings at the abutments. The roadway width is 18 feet, with a footway 5 feet wide on one side. There are ornamental



concrete parapets, and these, together with the light appearance of the structural members, make up an architectural composition that fits the setting adequately. (See Plate No. 7.) The contract price was £5,738.



PLATE No. 7.—Bridge over Diamond Creek, Eltham.

At Woori Yallock Creek, on the main Warburton Road, the old crossing consisted of two decayed timber bridges, one over the main stream and one over a billabong, together with a section of road constructed as a floodway. An investigation of probable run-off was made, and consideration given to known flood levels, and as a result it was decided that the floodway should be retained at least for some years until traffic increases sufficiently to warrant raising the whole road above floods. The structure is therefore rebuilt in two portions as formerly, each now consisting of three spans of 30 feet. The portions are separated by a bank 90 feet long, and the abutments at each end of this bank are designed so that they may be joined by three central spans whenever it is desired to do away with the floodway. The bridge has foundations similar to those of the Eltham bridge, and each group of three spans is designed as continuous, with copper and steel expansion bearing-plates at the abutments. A very good finish was obtained in the parapets of this bridge (See Plate No. 8). The Board's practice in finishing concrete bridges is to require a smooth strip on all exposed faces, and to insist that any defects be remedied not by plastering of the whole surface (a method which does not make a permanently sound or clean finish) but by rubbing off irregularities with an artificial smoothing stone which, at the same time, works finely-ground particles into the surface and so restores uniformity of line and texture. The best results are secured, however, by careful preparation and manipulation of forms and careful placing of concrete. The cost of the Woori Yallock Creek bridge and approaches was £5,808.



PLATE No. 8.—Portion of Bridge over Woori Yallock Creek.

In Warracknabeal, on the Dimboola road, an old timber bridge over Yarriambiack Creek had fallen into such disrepair as to render maintenance very costly. The bridge was also a typical example of location square to a stream which the right-of-way crosses obliquely, so that severe curves were involved at each approach. This "S" curve location was allowable when traffic was all slow-moving and sparse, but with modern faster and denser traffic sharply-curved approaches become dangerous. The new crossing joins the lines of the road on either side with as little curving as possible, the bridge being built askew to the stream. The creek is one of the branches into which the Wimmera River divides, and the velocity of flood waters is not high, so that it was unnecessary to make the clear opening of the new bridge longer than previously existed. The bridge has three reinforced concrete spans of 27 feet, with roadway width of 22 feet and a footway 6 feet wide (See Plate No. 9). The contract amount was £3,700, including approaches.

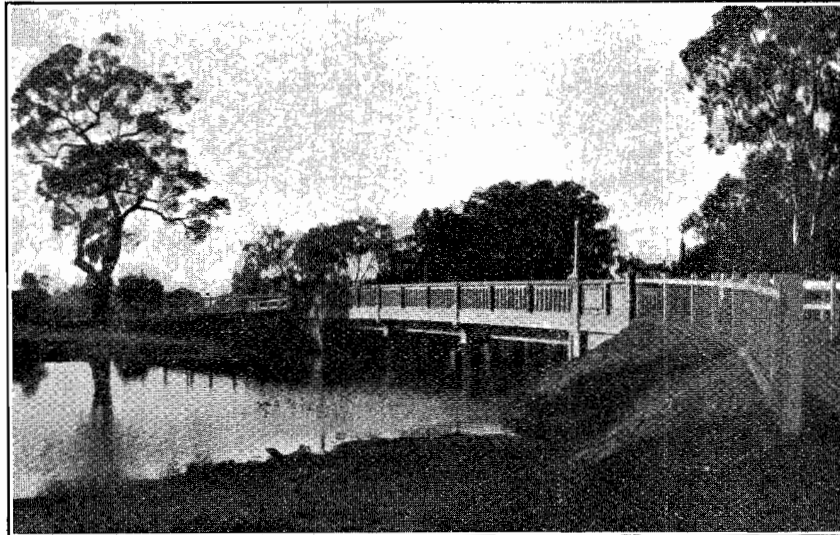


PLATE No. 9.—Bridge over Yarriambiack Creek, Warracknabeal.

The longest timber bridge yet erected by the Board was completed during this year, namely, that at the mouth of the Barwon River. The Bellarine peninsula contains several popular holiday townships, and the linking of these with Barwon Heads and so with the tourist area further along the Great Ocean-road constitutes an important part of the project for providing a continuous scenic highway along the south-western coast of Victoria.

The alignment selected for the crossing is just above the lowest of the small boat jetties and the adjacent park, in direct extension of the main street of Barwon Heads township, affording easy road approaches at each end, with a good view of the coast, and at the same time at a sufficient distance from the sea to make the structure free from the most severe wave action, whilst giving a considerable anchorage area for boats on the downstream side. The bridge contains 33 spans of 30 feet and one of 19 feet at the centre of the low-tide channel, where a lift span may be introduced at some future date. The width between kerbs is 16 feet, and there is a footway 5 feet wide on the downstream side, where there are also four refuge bays for use by fishermen.

To make the bridge as permanent as possible, New South Wales turpentine was used for piles and ironbark for braces and cross-heads of the piers, these timbers being relatively durable

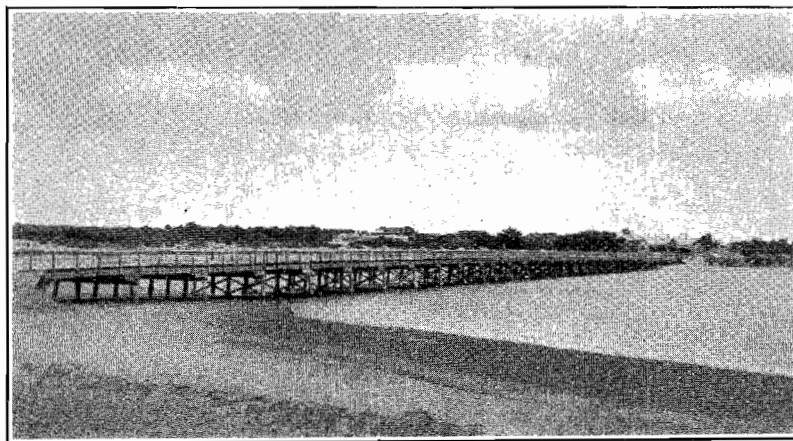


PLATE No. 10.—Bridge at Barwon Heads.



in sea water. For the superstructure messmate from Colac District was used. Funds were provided partly by local subscriptions and donations from local authorities and partly by loan funds under Permanent Works. The contract was let to Mr. S. Patience for £11,540. The quality of materials and workmanship put into the structure was exceptionally good (see Plate No. 10). The pile-driving plant of outrigger type is shown in Plate No. 11. Unfortunately, towards the completion of this bridge, the contractor lost his life whilst at work.

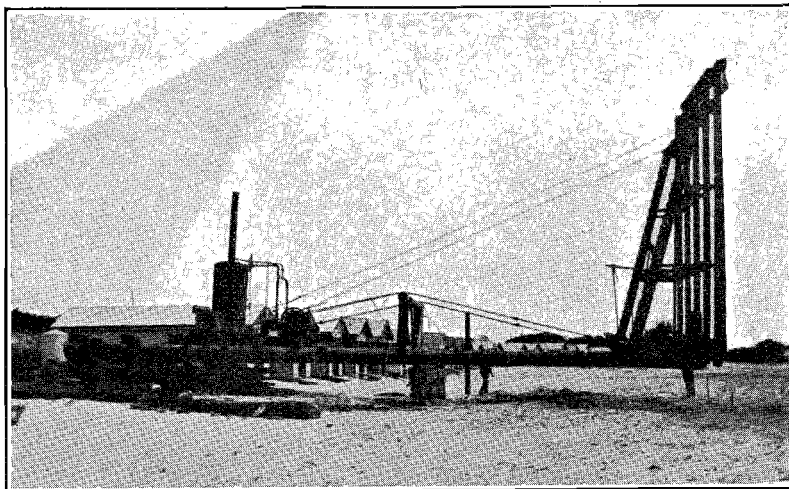


PLATE NO. 11.—Pile Driving Plant used at Barwon Heads.

Another link in the road system of the State was formed by the completion of a crossing over the Yarra River at Woori Yallock, on the developmental road from Healesville. The bridge over the main stream consists of a timber truss span of 60 feet, with two approach spans of 20 feet and one of 25 feet. Over the adjacent billabongs there are three stringer span bridges, with a total length of 170 feet. The bridges are 15 feet wide between kerbs. The roadway between the bridges is raised above flood-level, but over the flats on the left bank additional flood relief is afforded by a floodway 600 feet long. This is constructed with normally crowned cross-section, the top course being penetration asphaltic macadam and the downstream shoulder being protected with rough stone pitching. The floodway affords waterway only during heavy floods, when the maximum depth of water may be 2 feet. The total length of approaches in the contract was 3,000 feet. The contract price was £6,100. Plate No. 12 shows the bridge over the main stream.

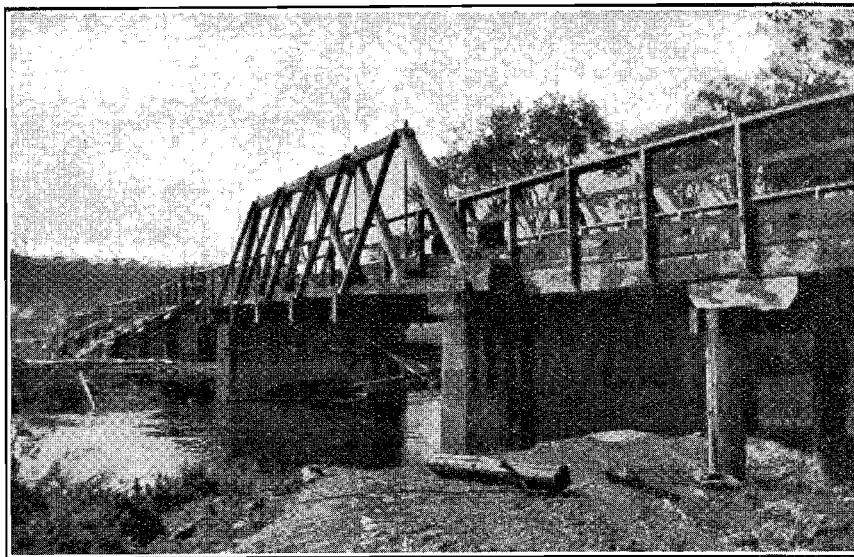


PLATE NO. 12.—Bridge over Yarra River, Woori Yallock.

The completion during the year of the formation of the Apollo Bay—Wye River road required the construction of a bridge over the Kennet River, so as to open the road to traffic from this point to the Wye River. The bridge contains a truss span of 60 feet and three approach spans, total length 125 feet; it is 15 feet wide between kerbs (see Plate No. 13). The timber

used is bluegum, of which there are valuable tracts in the adjacent hills. The contract price, including cost of approaches, was £1,390. This bridge, besides serving a purely utility purpose, opens to tourist traffic a very beautiful length of the road along the coast.

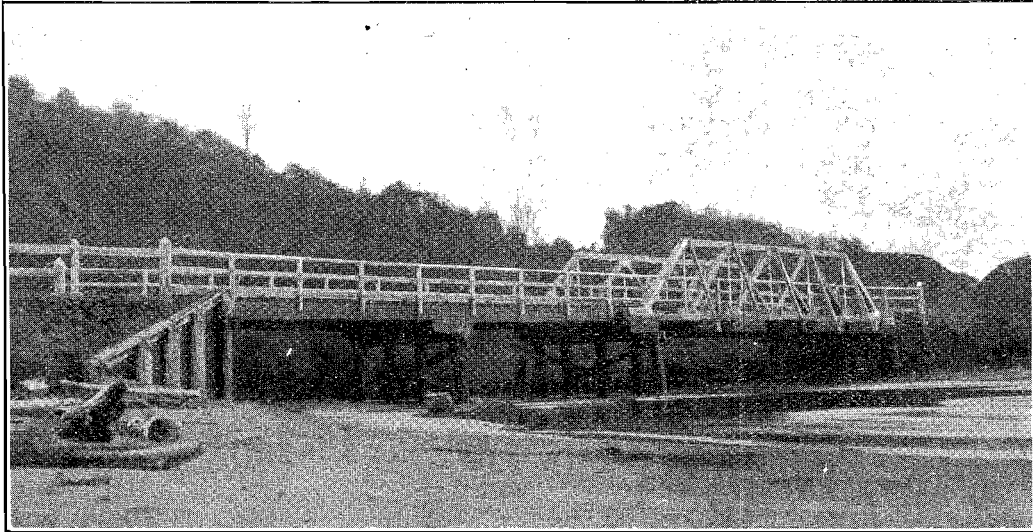


PLATE No. 13.—Bridge over Kennet River, Apollo Bay—Wye River Road.

Another bridge, constructed with the aid of tourist funds, is that over Stony Creek, Hall's Gap, at the northern entrance to the Grampians. This bridge, which has two timber stringer spans of 20 feet, cost £563, inclusive of approaches.

Several small structures have been renewed on State highways, effecting the most urgent of the many replacements necessitated by the lack of a forcible maintenance policy prior to the handing over of complete responsibility for the maintenance of these roads to the Board. Examples are a concrete bridge over Cowie's Creek, near Geelong, and one over Tom's Creek, near Bairnsdale. The former consists of a single span of 30 feet. This bridge is close to the City of Geelong, and a width of 30 feet to provide for traffic to existing industrial establishments in Corio shire, as well as for the certain increase in density of traffic to Melbourne, was adopted, footways 5 feet wide being provided in addition. The contract amount was £2,436. The bridge over Tom's Creek contains three spans of 23 feet and is 22 feet wide between kerbs—the ordinary standard width adopted by the Board for bridges on the principal sections of State highways. The contract amount was £1,753.

Besides urgent structure maintenance on the State highways generally, a special bridge gang operating in the central district has made extensive repairs of three important bridges on the North-western Highway. The three-span bridge over Coliban River at Malmsbury, 96 feet long, is a typical structure of an earlier period when a definite road construction policy existed in this State. The sub-structure is of masonry, still in good order. There are three lines of iron plate girders, which support iron cross beams, and these in turn carry timber stringers and decking. The ironwork had not been painted for many years, and the timber had been neglected until its condition was dangerous to traffic. The macadam wearing surface was also in bad order. Repairs, which cost £663, should economically prolong the life of the structure and make it both safe and pleasant to traffic. The vehicle roadway is 23 feet wide, and repairs were made one-half the width at a time. The new wearing surface is of asphaltic macadam.

On the portion of the same highway between Carlsruhe and Kyneton the Campaspe River is twice crossed. The bridge nearer Carlsruhe is a timber one, with nine spans of 21 feet; the Shamrock bridge has seven spans of 22 feet, of timber, with fine masonry abutments. Repairs to these bridges were undertaken after an investigation had been made of a deviation following the original 3-chain road along the right bank of the river between Carlsruhe and Kyneton. Estimates showed that the cost of reconstructing this road, which crosses low flats subject to floods, would considerably exceed the cost of rebuilding the two bridges and re-surfacing the road on the present route. Moreover, it was found economical to repair the bridges and extend their useful life rather than undertake complete renewal at present. Repairs consisted in replacement of defective stringers and corbels, the laying of new decking, and the provision of wearing surface of asphaltic macadam 9 feet wide, retained between rows of longitudinal planking. The costs were £747 and £522.

An incidental structural work completed is a drainage channel and outfall to relieve flooding on the Geelong—Bacchus Marsh road at the latter town. Here Kennedy's Creek, a stream with a catchment area of  $1\frac{1}{2}$  square miles, subjects the road for a length of about 800 feet to severe annual inundations, owing to its defined channel ending at the road, from which point the water spreads

over a large area, and is prevented from directly entering the Werribee River by reason of the natural silt levee along the river banks. The flood flow was estimated to be as much as 500 cubic feet per second. A complete scheme for draining the creek under and along the road to an artificial outfall was drawn up, and a contract let for the downstream portion of this, comprising an outfall structure and 600 feet of concrete-lined channel, with two small access bridges across it. The free fall type of outfall was adopted, a special portion of the channel being cantilevered well out into the river; this portion is of reinforced concrete, and is carried on two concrete piles which were driven to a depth safe from scour. Plate No. 14 shows the end of the outfall. The contract for the work was supervised by the Shire Engineer, and the price was £1,598.

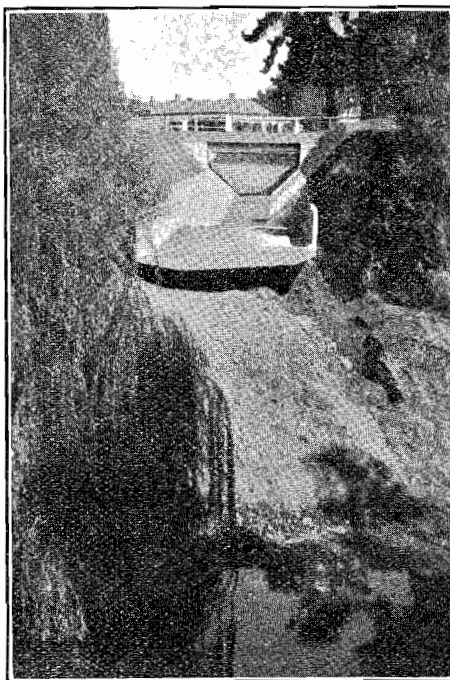


PLATE No. 14.—Outfall Structure, Kennedy's Gully Drain, Bacchus Marsh.

#### OTWAY SHIRE—FORREST—APOLLO BAY ROAD.

Among the list of roads opened to traffic is that of the Forrest to Apollo Bay road, and as this road is of so much importance to the district benefited and has opened up such a large area of fertile country possessing such attractive ocean and sylvan beauty, a special paragraph has been prepared to record the work undertaken in its construction between the railhead at Forrest and Apollo Bay.

The Forrest-Apollo Bay road was gazetted a main road on 1st April, 1914, and a survey made by Mr. Archibald in 1914-15, which followed closely the old coach route from Forrest to Barramunga, thence over Mount Sabine to Tanybryn, to Skene's Creek, and along the ocean frontage to Apollo Bay. This route has not been utilized throughout its whole length, the Board deciding, after inspection, to carry the road from Tanybryn down the Wild Dog Valley, junctioning again with the route as surveyed at the mouth of the Wild Dog Valley Creek. The Wild Dog Valley Road was therefore declared a developmental road on 24th July, 1918, and was surveyed by Mr. Haigh, under Mr. Anderson, in 1918. The total length of this new route is 24.9 miles, as against 26.35 miles as originally surveyed. On 18th October, 1922, after the creation of the Shire of Otway, a section of the main road  $2\frac{1}{2}$  miles long, from Tanybryn towards Forrest, was demained and declared a developmental road.

Construction work was commenced immediately, the first contract being let by the Shire of Colac in 1915 to A. McDonald and Co. for formation works between Forrest and Barramunga. Clearing, grubbing, and formation work was carried on by contract and day labour until 1926, when the last section—near Mount Sabine—was completed under contract by Mr. M. A. Harrington. The first metalling was done along the sea front between Apollo Bay and the Wild Dog Creek under the Shire of Colac, by Mr. M. Walsh, in 1915-16; this metalling was extended as far as Skene's Creek in 1917-18. No further metalling was done until 1920, when a contract was let by the Shire of Otway to Mr. D. J. Lockyer for metalling from Forrest to Barramunga. The first 7,060 feet of this contract was metalled with basalt brought from Lethbridge, the remainder with local sandstone. The work was completed in 1923, but the sandstone section was not a success owing to the poor quality of the stone used, and in 1924-25 it was re-sheeted with Lethbridge stone as part of the work undertaken by the Board of sheeting the road with metal from end to end. This work was completed in 1927 and, as suitable tenders could not be

obtained, was done throughout by day labour, with the exception that much of the metal—Jurassic sandstone quarried locally—was supplied under contract. The total cost of these works to date has been £137,837.

#### TOLL-GATES.

Whilst carrying out record surveys on the several highways, the surveyors have noted the position of several old toll-gates which, in some cases, are defined by the large stone heel-blocks where the gates met on the road, and as the matter was considered of some historical interest the *Gazette* notices proclaiming the tolls prescribed were searched for and are appended to this Report.

It will be noted that the rates fixed strongly encouraged the provision of wide tires, and if tires over 6 inches in diameter were used the rates were reduced by one-half, and, if tires over 9 inches were used, practically making the vehicle a road-roller, a further reduction of 50 per cent. was made.

These gates, it has been noted, were at short intervals, possibly not more than 20 miles apart, so that considerable revenue no doubt was collected, which may account for a letter, which the writer has seen in the records of the Ballarat Shire, from the Duke of Edinburgh, in 1867, complimenting that shire on the excellent condition of its roads.

Modern travellers, especially motorists, may, however, gain much comfort by perusal of those rates and feel very thankful that they are not in force at present. Comparing these rates on the ton-mile basis with the road tax of to-day indicates that the toll-gates rates were four-tenths of 1d. per ton mile, whereas the rate for a 4-ton lorry under the new Act, when loaded to the 8-ton limit, works out at less than half that rate.

*Victoria Government Gazette, 29th July, 1870, page 1127.*

#### BUNGAREE ROAD DISTRICT BOARD.

At a meeting of the Board, held 13th April, 1870, for making Special Order for the erection of toll-gate and check-gate, and fixing the scale of tolls to be collected thereat, on the Ballarat to Daylesford Main Road, via Deadhorse, it was resolved—

That a Special Order of this Board be and is now made to erect a toll-gate on the above-named road, at the north-west angle of the New Cemetery, Ballarat; and also a check-gate on the same road at the south-east angle of allotment 1, section 1, Parish of Bungaree; and that the scale of tolls to be collected thereat be as follows:—

TOLLS FOR CATTLE, ETC.								s. d.
For every sheep, lamb, pig, or goat	..	..	..	..	..	..	..	0 0 $\frac{1}{4}$
For every ox or head of neat cattle	..	..	..	..	..	..	..	0 1
For every horse, mare, ass, or mule	..	..	..	..	..	..	..	0 3

#### TOLLS FOR VEHICLES.

For every gig, chaise, coach, or chariot, or other such carriage, constructed on springs—

If drawn by one horse or other animal	..	..	..	..	..	..	..	0 6
If drawn by two horses or other animals	..	..	..	..	..	..	..	1 0
If drawn by three horses or other animals	..	..	..	..	..	..	..	1 6

And 6d. for each additional horse or other animal.

For every Cart, Dray, Waggon, Wain, or other such Vehicle.	With Tires of Wheels.		
	Not exceeding 6 inches in width.	Exceeding 6 inches and not exceeding 9 inches.	Exceeding 9 inches.
	s. d.	s. d.	s. d.
If drawn by one horse or other animal	1 0	0 6	0 3
If drawn by two horses or other animals	1 6	0 9	0 4 $\frac{1}{2}$
If drawn by three horses or other animals	2 0	1 0	0 6
If drawn by four horses or other animals	2 6	1 3	0 8
If drawn by five horses or other animals	3 0	1 6	0 9
If drawn by six horses or other animals	3 6	1 9	0 11
And for each additional horse or other animal drawing	0 6	0 3	0 2

Tolls payable one way only for going and returning same day.

At a special meeting of the Board, held 11th May, 1870, it was resolved that the Special Order made 13th April, 1870, be confirmed.

(Signed) AND. J. FORBES,  
Chairman.

(Signed) HENRY ACTON,  
Clerk to the Board.

The foregoing Special Order was confirmed by His Excellency the Governor in Council on the 27th June, 1870.

(Signed) WILLIAM WILSON,  
Commissioner of Railways and Roads.

## BITUMEN SURFACING OF ROADS—SEASON 1926–27.

## SUMMARY OF THE WORK CARRIED OUT DURING THE SEASON 1926–27 BY THE BOARD'S MECHANICAL PLANT FOR THE BITUMEN SURFACING OF ROADS.

During the season, 209 miles of road, varying in width from 12 to 20 feet, were surfaced, at a total cost of approximately £59,000. The work comprised double and single coat surfacing in the following proportions :—

	Double Coat.	Single Coat.
Total length—		
Feet .. .. .	431,000 feet	669,000 feet
Miles .. .. .	82 miles ..	127 miles
Width .. .. .	12 to 20 feet, average 17 feet	
Total surface area .. .. .	817,000 sq. yds.	1,251,000 sq. yds.
Total cost (approx.) .. .. .	£32,000 ..	£27,000
Cost per square yard .. .. .	9·35d. ..	5·17d.
Cost per mile 17 feet wide .. .. .	£390 ..	£212

## UNIT COSTS.

Cost data have not been received in all cases, but a careful analysis has been made of the information received, and average unit costs obtained are as under :—

	Cost in pence per sq. yard.	
	Double Coat.	Single Coat.
Bitumen in seal coat .. .. .	2·20	2·00
Bitumen and oil in primer coat .. .. .	2·00	..
Screenings .. .. .	2·62	1·56
Gravel (alternative) .. .. .	2·38	1·95
Fuel and water .. .. .	0·18	0·10
Labour .. .. .	1·54	0·78
Supervision .. .. .	0·12	0·07
Plant charges .. .. .	0·83	0·47
Total .. .. .	9·35	5·17

} 2·48     } 1·75

For the season 1925–26, the average unit cost of single-coat work was 5·41d. per square yard, so that the cost of the work in 1926–27 shows a reduction of 0·34d. per square yard over the previous year. Double-coat treatment was not used to any extent prior to the season 1926–27.

## BITUMEN-SURFACED ROADS AND THEIR MAINTENANCE.

The waterbound macadam road, which is subsequently surface painted with tar or bitumen, is a type of construction intermediate between waterbound macadam and asphaltic macadam. The surface painting is usually undertaken when it is known, or anticipated, that the waterbound road will not withstand the class of traffic which it is to carry in dry weather without disintegration.

Briefly described, the treatment consists of thoroughly sweeping the road surface to expose the stones and remove dust, the application of a light primer coat of a very fluid mixture of bitumen and oil to prepare the surface, and give a grip for the application on the following day of the sealing coat of bitumen. While still warm, the bitumen is covered with clean stone screenings or gravel and thoroughly rolled. An alternative method is to use a single application of tar and screenings, gravel, or coarse sand.

The usual quantity of bitumen or tar used in the seal coat is from one-quarter to one-third gallon per square yard, and the quantity of mineral matter used for covering is about 1 cubic yard to each 75 square yards of road surface ; and, assuming that the whole of the mineral matter is forced into the bitumen, the proportion of bitumen in the surface coat is from 7 per cent. to 9 per cent. by weight or 17 per cent. to 21 per cent. by volume.

The purpose of the bitumen is to act as a binder for the screenings and as a waterproof coat to the road, and that of the mineral matter to take the surface wear of the traffic, and the separate functions of these materials should not be lost sight of. In an ideal mixture of bitumen and mineral matter each particle would be uniformly coated with a thin film of bitumen, and in such a case a very stable pavement results, as such troubles as waving and softening in hot weather are avoided, there being no excess of bitumen to act as a lubricant between the particles, or to soften in warm weather ; this is very important, it being a well-known fact that a mixture of



bitumen and mineral matter has a much higher melting point and is much more stable than pure bitumen. In high class asphaltic pavements the grading of the mineral matter is carefully checked, the proportion of bitumen reduced to just sufficient to coat the particles of mineral matter, and the ingredients heated and uniformly mixed before laying. The proportion of bitumen used, therefore, depends on the grading of the mineral matter, and the usual proportions used are from 10 per cent. to 11 per cent. for sheet asphalt, in which the mineral matter is sand; from 5 per cent. to 6 per cent. for asphaltic concrete, in which the mineral matter is stone and sand; and from  $3\frac{1}{2}$  per cent. to 5 per cent. in pre-mixed asphaltic macadam, in which the mineral matter is stone with very little fine material. On the basis of these figures, and taking account of the variation in the gradings of the materials, it is estimated that the ideal proportion of bitumen in surface painting work is approximately 7 per cent. to 9 per cent. by weight. This means that to obtain a stable pavement at least 1 cubic yard of stone is required to every 75 square yards of surface, and all this mineral matter must be forced into the bitumen.

As has already been pointed out, the purpose of the mineral matter is to take the surface abrasion of traffic, and it is, therefore, necessary that the material should be hard and should have a high resistance to wear. Furthermore, the material has to be forced into the coating of bitumen after it has been spread on the road, and sometimes after it has cooled considerably. For this reason the material must be of such a nature that it will stick to the bitumen at comparatively low temperatures when forced into it. Moisture or dust will effectively prevent the sticking of bitumen, and it is, therefore, necessary that the particles of mineral matter should be free from surface moisture and from a film of fine dust. The presence of very fine material in the aggregate is also deleterious, as it forms a film on the surface of the bitumen and prevents actual contact between the stones and the bitumen. Another important point is that the particles should be of such a size as to be forced through the full thickness of the bituminous coat so that the whole coat may be completely "filled" with mineral matter rather than have a thin film of fine mineral aggregate on the top, and under this practically pure bitumen. Such a condition may be satisfactory for light traffic, but it is found that heavy steel-tired traffic so deforms the surface as to force its way through the well-filled surface skin; the pure bitumen below immediately adheres to the edges of the tires and the surface coat is lifted and damaged.

Summarized, it may, therefore, be stated that the essential properties of mineral matter for this work are (a) hardness, (b) dryness, (c) absence of dust from the surfaces of individual stones, (d) absence of free, fine dusty material, (e) presence of a considerable quantity of material large enough to be forced through the full thickness of the carpet. The first—hardness—is obtained by ensuring that the material is derived from hard stone or consists of quartz, which has a very high wearing quality. The second—dryness—depends, of course, on weather conditions, but clean, coarse, gritty material dries far more rapidly after wetting than finer materials. It is found that the major portion of this harmful dusty material is elutriable, and it is, therefore, not found in river gravels, in heaps of quartz mine tailings, washed river sands, or in artificially-washed materials. Screenings crushed from clean hard stone, from which the dust is removed, are also satisfactory in this respect.

It is found that by specifying a maximum elutriation loss of 3 per cent. and that not more than 50 per cent. of the mineral matter may pass a 10-mesh sieve (and preferably not more than 40 per cent.) the requirements (c) and (d) are provided for.

As the completed carpet is from  $\frac{1}{4}$  inch to  $\frac{3}{8}$  inch in thickness, requirement (e) may be met by the presence of a proportion of material coarser than  $\frac{1}{4}$  inch, and it has been found by experience spread over several years that it is necessary to have at least 25 per cent. of such material, and that 40 per cent. is a most desirable figure. The maximum size allowable is found to be from  $\frac{1}{2}$  inch to  $\frac{3}{4}$  inch after rolling, which means that for screenings and gravels which do not crush under the roller a maximum of  $\frac{3}{4}$  inch is required; whereas for quartz gravel, scoria and clinker a maximum up to  $1\frac{1}{2}$  inches may be allowed, as the roller will crush the material to  $\frac{1}{2}$ — $\frac{3}{4}$  inch, and in doing so drive it well into the carpet.

It may, therefore, be stated that  $\frac{3}{4}$ -in. screenings crushed from hard, clean stone, and free from dust, fulfil the requirements laid down. Quartz gravel and quartz tailings, volcanic scoria and clinker may be allowed a maximum size of  $1\frac{1}{2}$  inches, and must comply with the elutriation test and the grading limits given:—

Elutriation loss	..	..	not more than 3 per cent.
Passing 10-mesh sieve	..	..	not more than 50 per cent., and preferably 40 per cent.
Passing $\frac{1}{4}$ -in. mesh screen	..	..	not more than 75 per cent., and preferably 60 per cent.

River gravels containing hard, tough stones should comply with the above, and be limited to  $\frac{3}{4}$  inch maximum size.

It will be seen that sands, although consisting of a first class wearing material (quartz), fail to comply with the grading requirements set down. Even coarse concrete sands contain insufficient material coarse enough to penetrate the full thickness of the seal coat, and the finer sands usually contain much fine material which will not unite with the bitumen under the temperature conditions in which it is used, and this material forms a skin which effectively prevents the adhesion of the bitumen to any coarser sand present. Experience shows most conclusively that the use of such mineral matter does not give satisfactory results.

To illustrate the results which can be obtained by the use of material complying with the specification outlined, four photographs of road surfaces shortly after the completion of sealing are included. In each case the road was carefully swept before being photographed, and the mineral matter which is seen in the photographs is all securely bound together with bitumen.

Plate 15 shows a surface sealed with bitumen and bound with toscanite screenings from Coldstream Quarries, the section of road being on the North-eastern Highway near Craigieburn.

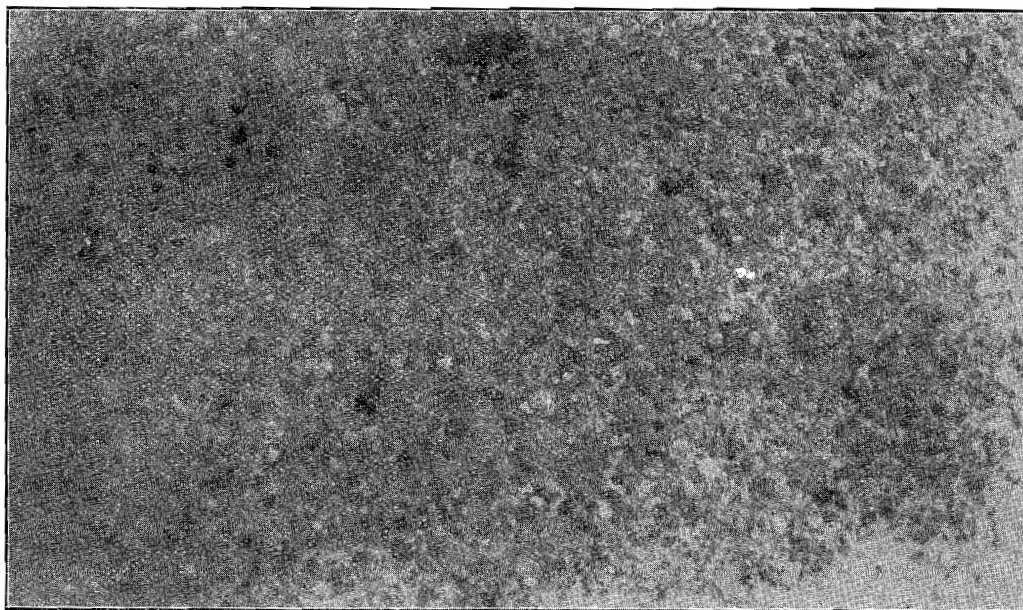


PLATE No. 15.—Seal Coat of Bitumen and Screenings on North-Eastern Highway, near Craigieburn.  
(Approx.  $\frac{1}{4}$  Full Size.)

Plate 16 shows a surface on the same section of road as Plate 15, bound with basalt and quartz river gravel from Seymour; this material contained some over size, as may be seen from the presence of a few large stones in the surface after rolling.

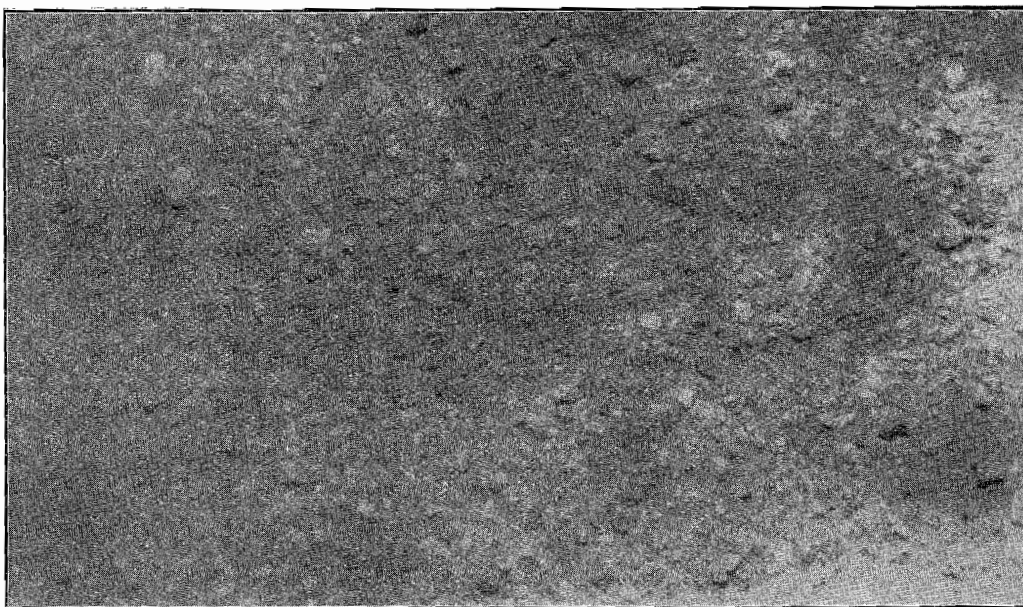


PLATE No. 16.—Seal Coat of Bitumen and River Gravel on North-Eastern Highway, near Craigieburn.  
(Approx.  $\frac{1}{4}$  Full Size.)

Plate 17 shows a surface on the North-eastern Highway, south of Kilmore, where Seymour river gravel was also used but, as may be seen from the texture of the surface, the material is of a better grading than shown in the previous plate.

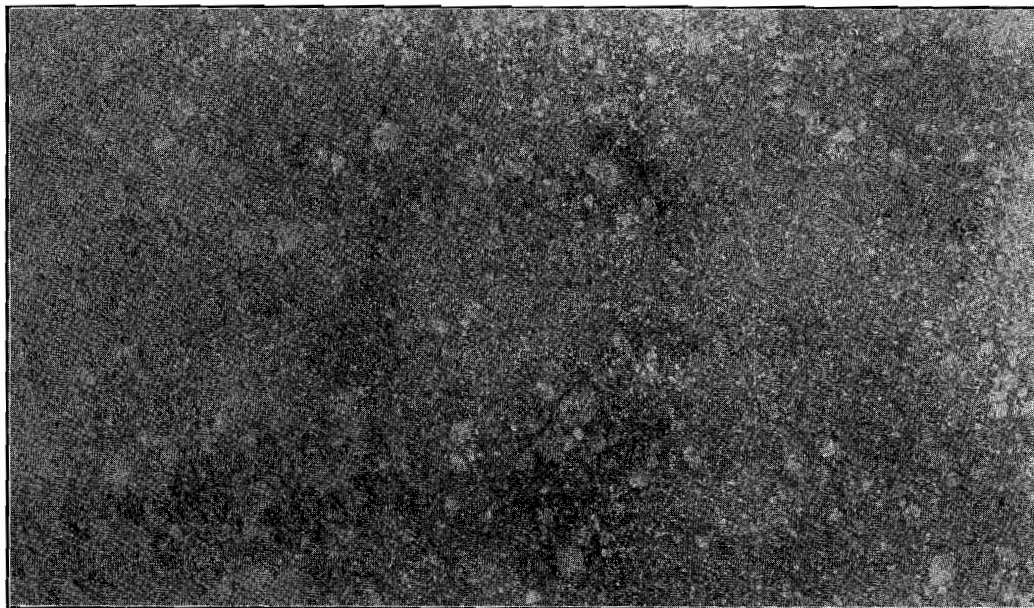


PLATE No. 17.—Seal Coat of Bitumen and River Gravel on North-Eastern Highway, near Kilmore.  
(Approx.  $\frac{1}{4}$  Full Size.)

Plate 18 shows a surface on the Western Highway, near Trawalla, the mineral matter being quartz tailings of excellent quality.

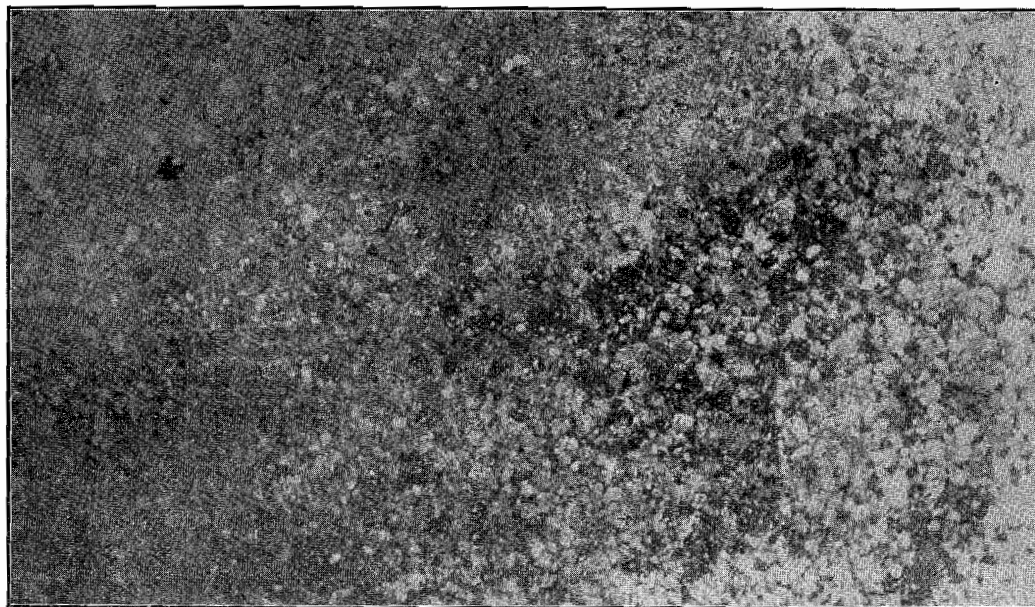


PLATE No. 18.—Seal Coat of Bitumen and Quartz Tailings on Western Highway, Sec. (2), near Trawalla.  
(Approx.  $\frac{1}{4}$  Full Size.)

Although on all these sections the mineral matter was well spread and thoroughly rolled, and from surface appearance the road contains a large amount of mineral matter, it is not anticipated that the surface contains sufficient to avoid any softening in the following summer. Excess bitumen on any class of road shows on the surface in warm weather, no doubt on account of the fact that the mineral matter is forced downwards by traffic, and also tends to go downwards by reason of its higher specific gravity and displaces the warm soft bitumen to the surface.

It is, therefore, *necessary every summer following sealing to provide extra mineral matter on the surface to combine with the excess bitumen.* Should this not be done, the bitumen adheres to steel tires and horses' hoofs, and the carpet is torn and lifted and frequently stones are removed from the macadam, making unsightly holes or ruts which are expensive to repair properly. Under heavy traffic the excess bitumen between the particles of mineral matter act as a lubricant, and the surface waves, becoming uncomfortable to traffic, and through the increased impact induced enhances greatly the destructive effect of traffic on the road.



The usual method of provision of the extra mineral matter is by spreading lightly over the surface from maintenance heaps and maintaining the material on the road by sweeping back that which is forced to the sides by traffic. For the reasons previously set out, the mineral matter should comply with the specification limits already given, except that, unless the material is to be rolled immediately, the maximum size should be limited to  $\frac{3}{4}$  inch so as to cause a minimum of inconvenience to traffic. Sand is not suitable for this work; it affords a temporary preventative to lifting by steel-tired vehicles by preventing actual contact between wheels and bitumen, but the sand does not penetrate the bitumen, and after its removal by the next rain or wind the road is in the same condition as before its application; that is to say, sand effects no permanent improvement in the road surface.

The mineral matter may be maintained on the surface by hand sweeping or horse broom, the latter being, of course, more expeditious and economical. On roads which carry a large proportion of fast pneumatic-tired traffic, which does not force very much of the mineral matter into the road, it is often economical to roll the road, even a light power or horse roller being suitable for such work. A considerable length of road should be covered and the material kept swept on in anticipation of a hot spell, and when such a period eventuates the whole section should be thoroughly rolled; in this manner a considerable proportion of the mineral matter is driven into the bitumen at a low cost. On roads which carry a large proportion of steel-tired traffic rolling is not so necessary, as the steel tires force the material into the bitumen. It is, however, necessary in all cases to anticipate the warm days on which lifting may occur, and covering or gritting should be carried out on the first sign of summer. If left until a hot spell commences it is found that before the length of road can be covered the weather has cooled again and much of the road may have suffered damage.

The above remarks have been written with respect to the sections of road illustrated, and it is pointed out that they apply to all bitumen-sealed roads. To some they apply with much greater force, as it is known that in many cases the initial application and penetration of the mineral matter is less efficient than in the cases illustrated. It is found that gritting must be carried out in successive summers, but it is believed that if efficiently done there comes a time when the surface has absorbed the maximum possible quantity and may be then classed as stable. It is considered that after three years' treatment on the lines set out above sections of the Princes Highway West, between Geelong and Werribee, have practically reached this condition. The condition of the road surface after such treatment is naturally excellent, and by reason of the presence of the large proportion of mineral matter the wearing quality of the surface is very high.

The question also arises, "When does such a surface require re-sealing with bitumen and mineral matter?" and the answer is considered to be "When the bitumen loses its binding power or when the mineral matter wears out," because in either case the value of the surface coat will disappear. It is found that the life of bitumen is much longer than the usual period which elapses between re-sealing of these roads. The wear on the mineral matter depends on the traffic carried, but such wear is usually the factor determining the necessity for re-sealing. It is considered that the proper time for re-sealing is when the surface skin appears thin and the stones in the macadam road begin to show through or are very close to the surface of the road; a general leanness of surface and dullness of colour are the usual indications. With the class of seal-coat now employed, it is anticipated that waterbound macadam roads will withstand at least two years' traffic after double-coat sealing before requiring re-sealing, and subsequent re-sealings may be necessary every two to four years, but under good conditions longer intervals between coatings may be advisable. It has been found that the most satisfactory rate of application of bitumen in a re-sealing coat is about 0.20 gallons per square yard, and certainly not more than 0.25 gallons per square yard, followed by 1 cubic yard of mineral matter to each 100 square yards of surface. After each re-sealing coat the road should receive maintenance similar to that outlined in this Report until stability is established.

During the season 1927-28 it is hoped to conduct further investigations into this type of work and to analyze several examples of both stable and unstable surface coats to determine the proportions of the bitumen and mineral matter present, the grading of the latter, and the reason for their success or inferiority, as the case may be. Bituminous surfacing work of the nature described has been carried out by the Board's plants during season 1926-27, as set out on the summary herewith, the average costs being:—

For double-coat treatment—9.35d. per square yard or £390 per mile of average width 17 feet.

For single-coat treatment—5.17d. per square yard or £212 per mile of average width 17 feet.

It is found that roads so surfaced can be maintained, by the ordinary patrol system, at an average annual cost of £140 per mile, which includes the necessary re-surfacing as the wear demands; such roads are capable of carrying up to 1,500 vehicles per day.

BITUMINOUS SPRAYING, 1926-27.  
Summary of Work Carried Out.

Shire.	Road.	Section.	Treatment.		Length Lineal Feet.	Width Lineal Feet.	Area Square Yards.	Total Cost. £ s. d.	Unit Cost.	Supervision. Council-C. Board-B.
			Coat.	Bitumen, Tar, or Oil.						
Bacchus Marsh	S.H. Western Highway (Sec. 1) S.H. Western Highway (Sec. 1)	Foot of Anthony's cutting .. Myrning to Devil's Elbow, Ch. 197453- 208900	D. D.	Bitumen and oil .. Bitumen and oil ..	Screenings .. Screenings ..	1,320 11,447	20 20	2,933 25,438	111 6 0 956 3 5	C. C.
Ballan	S.H. Western Highway (Sec. 1) S.H. Western Highway (Sec. 1)	Ch. 241600-249010 .. Ch. 258000-292000 (parts) ..	D. D.	Bitumen and oil .. Bitumen and oil ..	Gravel .. Gravel ..	7,410 26,000	20 20	16,430 57,800	.. ..	B. B.
Ballarat	M.R. Ballarat-Lexton ..	Various sections ..	D.	Bitumen and oil ..	Gravel ..	17,160	16	30,507	1,266 14 6	C.
Barrabool	S.H. Princes Highway West (Sec. 1) S.H. Princes Highway West (Sec. 3)	From Wauru Ponds Bridge .. From Moynce River east 20,000 feet (00-202000)	S. S.	Bitumen .. Bitumen ..	Gravel .. Screenings ..	16,964 20,000	20 16	37,698 35,600	790 13 6 826 0 0	C. C.
Benalla	S.H. Princes Highway West (Sec. 3) S.H. Princes Highway West (Sec. 4) S.H. North-eastern Highway (Sec. 3)	20000-34215 (shire boundary) .. 7,000 feet west from borough boundary Ch. 647160-652230 and Ch. 652900- 655133	D. D. S.	Bitumen and oil .. Bitumen and oil .. Bitumen ..	Screenings .. Screenings .. Screenings ..	14,215 7,000 7,303	20 16 18	31,500 12,333 14,606	1,124 0 0 340 0 0 377 9 10	C. C. B.
Berwick	S.H. Princes Highway East (Sec. 1) S.H. Princes Highway East (Sec. 1) S.H. Princes Highway East (Sec. 1) S.H. Princes Highway East (Sec. 1) S.H. Princes Highway East (Sec. 1) M.R. Princes Highway East (Sec. 1) M.R. Woori-Yallock-Pakenham ..	Narrewarren-Berwick .. Narrewarren-Berwick .. Beaconsfield-Pakenham .. Near Pakenham .. Deep Creek-Nar-nar-oon .. Berwick township .. Allotment 5E, Pakenham-Ararat Creek Ch. 59120-85000 ..	D. S. S. S. D. S. S.	Bitumen and oil .. Bitumen .. Bitumen .. Tar .. Bitumen and oil .. Bitumen .. Bitumen and oil ..	Screenings .. Screenings .. Screenings .. Gravel .. Screenings .. Screenings .. Screenings ..	4,216 7,900 23,180 3,740 10,600 3,600 16,170 25,880	20 20 20 20 20 20 12	9,400 17,553 51,506 8,210 23,500 8,000 21,560 57,511	400 12 6 334 5 10 774 18 6 .. .. 93 16 0 687 6 0	B. B. B. B. B. B. C.
Braybrook	S.H. Western Highway (Sec. 1) S.H. North-eastern Highway (Sec. 1)	Ch. 44100-76500 (parts) .. Ch. 76600-107900 ..	D. S.	Bitumen and oil .. Bitumen ..	Gravel .. Screenings ..	10,760 31,300	20 20	23,900 69,700	759 0 0 1,184 14 0	B. B.
Broadmeadows	M.R. Lancefield Road ..	Between Essendon City boundary and Tullamarine	S.	Bitumen ..	Screenings ..	13,600	15	22,665	402 0 0	C.
Bulla	M.R. Melbourne-Lancefield Road ..	South boundary to Bulla ..	D.	Bitumen and oil ..	Gravel ..	25,734	16	45,749	1,574 0 0	C.
Buln Buln	S.H. Princes Highway East (Sec. 1)	Length contracts 202 F.191, 202 F.179, 28 D.48	D.	Bitumen and oil ..	Gravel ..	17,952	20 and 15	36,340	1,038 0 0	C.
Bungaree	S.H. Princes Highway East (Sec. 1) S.H. Princes Highway East (Sec. 1) S.H. Western Highway (Sec. 1)	Drouin to eastern boundary .. Drouin to eastern boundary .. Ch. 316000-328000 ..	S. D. D.	Bitumen .. Bitumen and oil .. Bitumen and oil ..	Gravel .. Gravel .. Gravel ..	6,200 3,370 12,000	20 and 18 20 and 18 20	12,470 7,400 27,780	317 0 0 285 0 0 ..	C. C. B.
Buninyong	S.H. Western Highway (Sec. 1)	Ch. 347000-355000 ..	D.	Bitumen and oil ..	Gravel ..	8,000	20	17,700	..	C.
Carrum Borough	M.R. Point Nepean Road ..	..	S.	Bitumen ..	Gravel ..	29,000	18	58,080	..	C.
Castlemaine Borough	M.R. North-western Highway ..	Ch. 394925-398442 ..	S.	Bitumen ..	Gravel ..	3,517	15	5,862	163 15 11	C.
Ferntree Gully	M.R. Ferntree Gully Road ..	Lower Ferntree Gully-Wheeler's Hill ..	D.	Bitumen and oil ..	Screenings ..	23,200	15	38,667	1,619 1 10	C.
Flinders	M.R. Dromana .. M.R. Dromana-Sorrento .. M.R. Dandenong-Frankston ..	Moat's corner to Dromana .. Dromana-Rocks .. ..	S. S. S.	Bitumen .. Bitumen .. Bitumen ..	Screenings .. Screenings .. Screenings ..	9,300 4,000 18,500	15 15 12	15,400 6,600 24,700	.. .. ..	C. C. C.
Frankston and Hastings	S.H. North-western Highway (Sec. 1) S.H. North-western Highway (Sec. 1)	Ch. 132000-138040 .. Ch. 171400-184352 ..	D. D.	Bitumen and oil .. Bitumen and oil ..	Screenings .. Gravel ..	6,040 12,952	20 20	13,400 28,800	491 11 0 863 0 0	B. B.
Gisborne	M.R. Princes Highway ..	Camperdown township ..	S.	Bitumen ..	Scoria ..	5,280	18	10,560	..	C.
Hampden	M.R. Princes Highway ..	Terang township ..	S.	Bitumen ..	Scoria ..	5,280	18	10,560	..	C.
Healesville	M.R. Terang-Mortlake .. M.R. Healesville-Alexandra ..	Terang-Noorat .. Yarra Flats to Don Road, omitting Church Street to Green Street	D. S.	Bitumen and oil .. Bitumen ..	Scoria .. Screenings ..	18,400 14,850	15 16 and 18	30,800 26,900	.. 678 12 8	C. C.
Heidelberg	M.R. Greensborough-Hurstbridge .. M.R. Greensborough-Hurstbridge ..	.. ..	S. D.	Bitumen .. Bitumen ..	Gravel .. Gravel ..	16,220 11,500	18 20	32,440 23,000	868 0 0 902 13 11	C. C.
Keilor	S.H. North-western Highway (Sec. 1) S.H. North-western Highway (Sec. 1)	Ch. 63000-76126 .. Ch. 56755-76126 ..	S. S.	Tar and bitumen .. Bitumen ..	Screenings .. Gravel ..	13,126 19,371	20 20	29,300 43,047	.. 521 0 0	B. B.
Kilmore	S.H. North-eastern Highway (Sec. 1)	Ch. 158800-207400 (part) ..	D.	Bitumen and oil ..	Gravel ..	15,550	20	34,550	1,139 12 7	B.

BITUMINOUS SPRAYING, 1926-27—continued.

Shire.	Road.	Section.	Coat.	Treatment.		Length Lineal Feet.	Width Lineal Feet.	Area Square Yards.	Total Cost.	Unit Cost.	Supervision. Council—C. Board—B.
				Bitumen, Tar, or Oil.	Covering Material.						
Lilydale ..	M.R. Main Healesville Road	Ringwood boundary to Lilydale	S.	Bitumen	Screenings	36,540	12 and 16	62,390	£ 4.76	C.	
Melton ..	M.R. Main Healesville Road	Coldstream-Yarra	S.	Bitumen	Screenings	31,680	12	42,240	6.38	C.	
Metcalfe ..	S.H. Western Highway (Sec. 1)	Ch. 120680-129330	D.	Bitumen and oil	Gravel	5,050	16 and 20	9,800	..	B.	
Minhamite ..	S.H. North-western Highway (Sec. 1)	Ch. 316290-337580	D.	Bitumen	Gravel	21,290	20	47,500	..	B.	
	M.R. Warrnambool - Hawkesdale - Penshurst	North from South Shire boundary	D.	Bitumen and oil	Gravel	19,800	12	26,400	13.31	C.	
Moorabbin ..	M.R. Point Nepean Road	South Road to Centre Dandenong Road	S.	Bitumen	Screenings	(Irregular)		20,000	6.36	C.	
Mornington ..	M.R. Centre Dandenong Road	Point Nepean to Boundary Road	D.	Bitumen and oil	Screenings	15,400	14 and 15	1,006 16 5	9.73	C.	
Mortlake ..	M.R. Point Nepean Road	North boundary to Mornington	S.	Bitumen	Gravel	22,440	11	27,427	6.21	C.	
	M.R. Terang-Mortlake ..	From 2½-mile post to 4½-mile post from Mortlake and between 6 miles 10 chains and 7 miles 10 chains	D.	Bitumen and oil	Gravel and scoria	14,190	12	18,920	12.8	C.	
Mulgrave ..	M.R. Ferntree Gully Road	Wheeler's Hill (pen)	S.	Bitumen	Gravel and scoria	6,822	16	12,200	..	B.	
	M.R. Ferntree Gully Road	Wheeler's Hill (seal)	S.	Bitumen	Screenings	6,822	16	12,200	..	C.	
	M.R. Ferntree Gully Road	Oakleigh-Wheeler's Hill (part)	D.	Bitumen and oil	Screenings	10,560	16	18,600	..	C.	
	M.R. Ferntree Gully Road	Oakleigh-Wheeler's Hill (part)	S.	Bitumen	Screenings	26,400	16	46,900	..	C.	
Port Fairy Borough	M.R. Princes Highway	West from Moynes River	S.	Bitumen	Screenings	15,200	16	27,022	5.09	C.	
South Barwon ..	S.H. Princes Highway West (Sec. 1)	Settlement Lane to Hobbs Hill	S.	Bitumen	Gravel	7,000	15	11,700	..	C.	
	S.H. Princes Highway West (Sec. 1)	Hobbs Hill to Waurin Ponds Bridge	D.	Bitumen and oil	Gravel	6,200	20	13,700	..	C.	
	S.H. Princes Highway West (Sec. 1)	Shire boundary to Town boundary (part)	S.	Bitumen	Gravel	6,740	20	15,000	5.61	C.	
Warragul ..	S.H. Princes Highway East (Sec. 1)	Shire boundary to Town boundary (part)	D.	Bitumen and oil	Gravel	2,500	20	5,533	169 0 0	C.	
	S.H. Princes Highway East (Sec. 1)	Shire boundary to Town boundary (part)	S.	Bitumen	Gravel	30,360	18	54,648	1,420 0 0	C.	
	S.H. Princes Highway West (Sec. 2)	Warrnambool-Allansford	D.	Bitumen and oil	Gravel and scoria	13,992	16 and 20	26,928	1,352 7 4	C.	
Warrnambool ..	S.H. Princes Highway West (Sec. 3)	Warrnambool-Allansford	S.	Bitumen	Gravel and scoria	7,128	15 and 18	12,210	434 13 2	C.	
	S.H. Princes Highway West (Sec. 3)	Cudgee to Shire boundary	D.	Bitumen and oil	Screenings and gravel	22,506	13 and 15	33,726	1,589 13 0	C.	
	S.H. Princes Highway West (Sec. 3)	Cudgee to Shire boundary	S.	Bitumen	Screenings and gravel	15,312	13 and 15	22,719	764 2 6	C.	
	M.R. Mortlake-Warrnambool	Near Warrnambool	D.	Bitumen and oil	Gravel	6,798	16 and 20	12,863	538 14 8	C.	
	M.R. Mortlake-Warrnambool	Near Warrnambool	S.	Bitumen	Gravel and clinkers	10,230	15 and 20	18,267	442 19 2	C.	
Warrabee ..	M.R. Allansford-Nirranda	Near Allansford	D.	Bitumen and oil	Gravel	7,326	13	10,582	658 17 10	C.	
Whittlesea ..	M.R. Allansford-Nirranda	Near Allansford	S.	Bitumen	Gravel	13,200	12 and 15	17,600	574 7 1	C.	
Whittlesea ..	Point Cooke Road	Full length	S.	Bitumen	Gravel	26,660	16' 6"	48,900	684 0 5	B.	
Whittlesea ..	M.R. Whittlesea Road	Between 2 and 3 miles of South Morang	D.	Bitumen and oil	Gravel	4,100	16	7,289	297 7 4	C.	
Whittlesea ..	S.H. Princes Highway West (Sec. 2)	Length of contract 502 S.31, Ch. 29500-63500	S.	Tar	Gravel	34,000	20	75,500	1,686 0 0	B.	
	S.H. Princes Highway West (Sec. 2)	Length of contract 502 S.31, Ch. 29500-63500	S.	Bitumen	Gravel	34,000	20	75,500	..	C.	
	S.H. Princes Highway West (Sec. 2)	West of Armytage	S.	Tar	Gravel	6,600	13' 6"	9,900	..	C.	
	S.H. Princes Highway West (Sec. 2)	East of Winchelsea	S.	Tar	Gravel	11,900	13' 6"	18,000	..	C.	
	S.H. Princes Highway West (Sec. 2)	East of Winchelsea	S.	Bitumen	Gravel	11,900	13' 6"	18,000	..	C.	

Double Coat.		Single Coat.	
Total length	..	431,000'. 82 miles ..	669,000', 127 miles
Width	..	12' to 20', average 17'	..
Total surface area	..	817,000 square yards	1,251,000 square yards
Total coat (approximate)	..	£32,000	£27,000
Cost per square yard	..	9.35d.	5.17d.
Cost per mile, 17' wide	..	£390	£212

## BITUMINOUS SPRAYING, 1926-27.

## Unit Cost Double Coat Work.—Pence per Square Yard.

Shire.	Road.	Bitumen Seal.	Bitumen and Oil Primer.	Screenings.	Gravel.	Fuel.	Labour.	Supervision.	Plant Hire.	Cost Total.	Supervision. Council (C). Board (B.).	Remarks.
Bacchus Marsh	S.H. Western Highway (Sec. 1)	1.65	1.62	2.57	..	0.11	1.60	0.20	1.39	9.04	C.	
Ballarat	Western Highway (Sec. 1)	1.49	1.54	3.05	..	0.09	1.24	0.19	1.43	9.03	C.	
Belfast	M.R. Ballarat-Lexon	2.15	1.22	..	1.81	0.31	3.38	0.13	0.96	9.96	C.	
	S.H. Princes Highway West (Sec. 3)	2.08	1.75	1.22	..	0.30	2.20	0.11	0.87	8.53	C.	
	S.H. Princes Highway West (Sec. 4)	1.46	1.27	0.97	..	0.30	1.76	0.10	0.76	6.62	C.	
Berwick	S.H. Princes Highway East (Sec. 1)	3.20	2.30	0.90	..	0.40	3.00	..	0.50	10.30	B.	
	M.R. Woori Yallock-Pakenham	2.07	1.80	2.17	..	0.31	0.75	..	0.54	7.64	C.	
Broadmeadows	S.H. North-eastern Highway (Sec. 1)	1.68	1.51	..	2.41	0.26	0.85	0.10	0.79	7.60	B.	
Bulla	M.R. Melbourne-Lancefield	1.93	1.63	..	1.68	0.17	1.75	0.20	0.89	8.25	C.	
Bulu Bulu	S.H. Princes Highway East (Sec. 1)	1.58	1.44	..	1.98	0.09	1.05	0.07	0.66	6.87	C.	
	Princes Highway East (Sec. 1)	1.75	1.20	..	1.65	0.16	2.85	0.23	1.39	9.23	C.	
Ferntree Gully	M.R. Ferntree Gully	2.18	1.77	4.00	..	0.68	1.23	..	0.80	10.06	C.	
Gisborne	S.H. North-western Highway (Sec. 1)	2.25	1.92	2.13	..	0.18	0.90	0.12	1.30	8.80	B.	
	North-western Highway (Sec. 1)	1.72	1.66	..	1.92	0.10	0.68	0.08	1.09	7.25	B.	
Heidelberg	S.H. Greensborough-Hurstbridge	2.03	1.81	..	2.70	0.07	1.89	0.17	0.74	9.41	C.	
Kilmore	S.H. North North-eastern Highway (Sec. 1)	1.98	1.90	..	2.48	0.22	0.61	0.08	0.64	7.91	B.	
Minhamite	M.R. Warrnambool-Hawkesdale-Fenshurst	2.97	2.60	..	3.82	0.18	2.35	0.20	1.19	13.31	C.	
Moorabbin	M.R. Centre Dandenong	3.38	2.07	3.23	..	0.11	0.94	0.24	0.76	9.73	C.	
Mortlake	M.R. Terang-Mortlake	3.04	2.55	..	4.12	0.49	1.68	0.10	0.82	12.80	C.	
Warragul	S.H. Princes Highway East (Sec. 1)	2.07	1.30	..	2.17	0.16	0.60	0.13	0.87	7.30	C.	
Warrnambool	S.H. Princes Highway West (Sec. 3)	3.58	3.96	..	1.98	0.20	1.38	0.12	0.83	12.05	C.	
	Princes Highway West (Sec. 3)	2.50	3.26	3.29	..	0.11	1.34	0.09	0.71	11.30	C.	
	M.R. Mortlake-Warrnambool	2.60	2.71	..	1.53	0.09	2.72	0.19	0.16	10.00	C.	
	M.R. Allansford-Nirranda	3.33	3.63	..	5.27	0.20	1.97	0.30	0.28	14.98	C.	
Whittlesea	M.R. Whittlesea Road	2.37	1.98	..	2.07	0.07	1.53	0.24	1.53	9.79	C.	
Average	..	2.20	2.00	2.62	2.38	0.18	1.54	0.12	0.83	9.35		
					2.48							

## BITUMINOUS SPRAYING, 1926-27.

## Single Coat Work.—Unit Costs in Pence per Square Yard.

Shire.	Road.	Bitumen.	Screenings.	Gravel.	Fuel.	Labour.	Supervision.	Plant Hire.	Unit Cost.	Supervision (Council C., Board B.).
Barrabool .. ..	S.H. Princes Highway West (Sec. 1) ..	1·15	..	2·17	0·09	1·25	..	0·37	5·03	C.
Belfast .. ..	S.H. Princes Highway West (Sec. 3) ..	2·07	1·28	..	0·13	1·48	0·10	0·50	5·56	C.
Benalla .. ..	S.H. North-eastern Highway (Sec. 3) ..	2·66	2·03	..	0·10	0·80	0·11	0·48	6·18	B.
Berwick .. ..	S.H. Princes Highway East (Sec. 1) ..	1·48	1·43	..	0·11	0·92	0·08	0·58	4·60	B.
	" " " " " " ..	1·49	0·94	..	0·05	0·81	0·09	0·23	3·61	B.
	M.R. Princes Highway " " ..	1·51	0·40	..	0·06	0·55	0·06	0·24	2·82	B.
Braybrook .. ..	S.H. Western Highway (Sec. 1) ..	1·57	1·43	..	0·23	0·83	0·10	0·65	4·81	B.
Broadmeadows ..	S.H. North-eastern Highway (Sec. 1) ..	2·08	0·94	..	..	0·45	..	0·60	4·07	B.
	M.R. Lancefield Road ..	1·41	1·43	..	0·15	0·77	0·10	0·40	4·26	C.
Buln Buln .. ..	Princes Highway East (Sec. 1) ..	1·73	..	1·66	0·10	1·68	0·13	0·80	6·10	C.
Castlemaine Borough ..	M.R. North-western Highway ..	3·02	..	0·63	0·18	1·82	..	1·05	6·70	C.
Healesville .. ..	M.R. Healesville—Alexandra ..	2·49	1·98	..	0·12	0·89	..	0·56	6·04	C.
Heidelberg .. ..	M.R. Greensborough—Hurstbridge ..	1·98	..	2·66	0·03	1·30	0·08	0·37	6·42	C.
Keilor .. ..	S.H. North-western Highway (Sec. 1) ..	1·39	..	0·52	0·16	0·44	0·02	0·36	2·90	B.
Lilydale .. ..	M.R. Main Healesville Road ..	1·89	1·98	..	0·08	0·46	0·04	0·29	4·76	C.
	" " " " " " ..	2·63	2·38	..	0·13	0·70	0·09	0·45	6·38	C.
Moorabbin .. ..	M.R. Point Nepean Road ..	1·96	3·17	..	0·07	0·53	0·17	0·46	6·36	C.
Mornington .. ..	" " " " " " ..	2·29	..	2·14	0·10	1·02	..	0·66	6·21	C.
Port Fairy Borough ..	M.R. Princes Highway " " ..	1·78	1·51	..	0·18	1·24	0·09	0·29	5·09	C.
Warragul .. ..	S.H. Princes Highway East (Sec. 1) ..	2·23	..	2·40	0·10	0·32	0·08	0·48	5·61	C.
	S.H. Princes Highway East (Sec. 2) ..	2·12	..	2·57	0·13	0·68	0·07	0·66	6·23	C.
Warrnambool .. ..	S.H. Princes Highway West (Sec. 3) ..	3·60	..	2·18	0·23	1·53	0·12	0·88	8·54	C.
	" " " " " " ..	2·48	..	3·25	0·13	1·57	0·10	0·53	8·06	C.
	M.R. Mortlake—Warrnambool ..	2·23	..	1·76	0·09	1·51	0·10	0·11	5·80	C.
	M.R. Allansford—Nirranda ..	2·17	..	3·48	0·18	1·55	0·23	0·21	7·82	C.
Werribee .. ..	Point Cook Road ..	1·24	..	1·14	0·13	0·37	0·10	0·37	3·35	B.
Winchelsea .. ..	S.H. Princes Highway West (Sec. 2) ..	2·96	..	1·74	0·02	supplied	0·10	0·53	5·35	B.
Averages .. ..	.. .. ..	2·00	1·56	1·95	0·10	0·78	0·07	0·47	5·17	..
			1·75							

## FEDERAL AID WORKS, 1926-27.

The following statement indicates the number of Federal Aid Works in progress or completed during the above financial year, with a short description of the purposes for which they are being carried out :—

No.	Shire.	Road.	Number of Miles constructed.	Remarks.
1	South Gippsland and Woorayl	Turton's Creek ..	2·32	This link completes the construction of the road which connects up the rail systems of North and South Gippsland at Mirboo North and Foster respectively, and permits of greater development of a large area of fertile hill country suitable for dairying and root crops, which was greatly retarded owing to the tracks used previously being on very steep gradients and impassable for a considerable portion of the year
2	Otway .. ..	Forrest—Apollo Bay	8·90	Dealt with fully in a separate section of the Report
3	Warragul .. ..	Allambee Estate ..	2·85	Completes another link in the scheme of development for a considerable area of good hill country south of Warragul and Yarragon
4	Yea .. ..	Yarra Glen—Glenburn	4·18	This road connects the rail systems at Yea and Yarra Glen across the Great Dividing Range, and the section constructed passes through and develops country on the range which is noted for the cultivation of berry and other small fruits
5	Orbost .. ..	Orbost—Delegate ..	8·80	The completion of these important sections will finish the Jones Creek deviation going round Mount Buck and Mount Watt and ending at Sardine Creek, also the Bonang section of this very important road, which is considered to be the most direct road from the Princes Highway to Canberra, and opening up a tract of country possessing great scenic attractions and developmental possibilities
6	Morwell and Traralgon	Jeeralang West ..	4·18	The metalling of this road so far as it has gone has given to the original settlers who remain an outlet for their produce, long anticipated, and without which they could no longer have remained on their holdings, and will enable large areas to be again cleared and made profitable
7	Otway .. ..	Apollo Bay—Wye River	1·38	This work permits of the further development of coastal country previously isolated by connecting it with the port at Apollo Bay, and it also completes an additional link in the Great Ocean Road
8	Heytesbury ..	Timboon—Nirranda	4·47	The metalling and forming of this road has given to the settlers on the Nirranda side of Curdies River an outlet to the Timboon Railway, and without which they have been very seriously handicapped, and for this outlet which has been long promised some of them have patiently waited for 50 years
9	Kerang .. ..	Murray River Valley	·60	Completes a section of roadway retarding local traffic, and forms a section in the main east and west road
10	Swan Hill .. ..	Murray River Valley	7·38	The sections constructed are between Swan Hill and Nyah and between Boundary Bend and Euston, and join up sections previously metalled, and make passable the road over a flooded section forming an outlet for soldier settlers and others on this very important valley road

## FEDERAL AID WORKS, 1926-27—continued.

No.	Shire.	Road.	Number of Miles constructed	Remarks.
11	Mildura .. ..	Murray River Valley	61·07	Clearing work between Mildura and the South Australian border and opening up a large area of fertile Mallee country. It is also part of the scheme for a roadway east and west through the northern portion of State
12	Benalla .. ..	Toombullup ..	3·70	Completes an additional section in the development of the tableland country of the North-eastern Victoria
13	Towong .. ..	Murray River—Tallangatta	1·06	Constitutes the completion of a difficult section of this road which joins the Murray River near Granya with the rail head at Tallangatta
14	Grenville .. ..	Lismore—Pittong ..	·90	Completes a further section on a road leading from a large soldier settlement to the Lismore and Pittong Stations
15	South Gippsland and Woorayl	Foster—Boolarra ..	4·45	Completes the last section in the cross country road connecting North and South Gippsland, and permitting the opening up and developing a large area of fertile country abandoned some years ago owing to the impassable tracks
16	Rochester .. ..	Shepparton—Rochester—Wycheproof—Rainbow	2·00	An additional link in the cross-country road serving the irrigation area
17	Rodney .. ..	Shepparton—Rochester—Wycheproof—Rainbow	1·00	An additional link in the cross-country road serving the irrigation area
18	Otway .. ..	Laver's Hill—Princetown	7·10	Connecting up the coast town of Princetown with the railway terminus at Laver's Hill, and permitting the full development of fertile country in between these points and along the valley of the Gellibrand River
19	Mirboo .. ..	Mt. Vernon and Boorool	1·00	Serving a closely settled fertile area purchased and subdivided by the Closer Settlement Board with an outlet towards Leongatha Railway Station
20	Woorayl .. .	Mt. Vernon and Boorool	1·00	Serving a closely settled fertile area purchased and subdivided by the Closer Settlement Board with an outlet towards Leongatha Railway Station
21	Otway .. ..	Carlisle—Gellibrand	3·30	Connects the settlement at Carlisle with the Gellibrand Railway Station and develops the fertile valley of the Gellibrand River
22	Kowree .. ..	Little Desert ..	7·50	Connects the northern area of the Shire of Kowree with the Goroke Railway Station, via Booroopki township
23	Warrnambool and Heytesbury	Peterborough — Pt. Campbell	..	Timber bridge over Curdie's River, which connects the southern portion of the Heytesbury Shire with the Allansford District, and affords a most valuable road connexion for through traffic on the Ocean Road to Warrnambool from Pt. Campbell
24	Hampden .. ..	Vite Vite .. ..	4·10	Gives an access from the newly subdivided settlement of Vite Vite to the Vite Vite Railway Station along the railway line
25	Gleneleg .. ..	Casterton—Mt. Gambier	2·80	An alternative route between the Western District and the south-eastern portion of South Australia is improved and local traffic facilitated by this work
26	.. ..	Princes Highway West	6·10	The work consists of strengthening the existing road, in order to cope with the heavy arterial road traffic
27	.. ..	Princes Highway East	5·20	The existing road is narrow and thin, and work done has been the widening and strengthening of the worst sections of the road
28	.. ..	Western Highway	11·70	Most of the work done has been the widening of the old cobblestone base on the Melbourne—Ballarat section and re-sheeting it to carry the heavy traffic
29	.. ..	North-western Highway	11·30	The work comprised re-sheeting in asphaltic macadam the roughest sections of road between Melbourne and Charlton
30	.. ..	Northern Highway	1·50	The work consists of reforming and gravelling, in order to make the road usable for motor traffic in wet weather. At present the road is often not available to traffic during rainy periods in the winter
31	.. ..	North-eastern Highway	9·70	The old road has been strengthened and reinforced to provide adequately for heavy road traffic
		Total Mileage ..	191·54	

## THE WILLIAMSTOWN-ROAD, PORT MELBOURNE.

In 1874 a steam-ferry was established across the mouth of the Yarra and connected to Melbourne by road. This reduced the road distance of Williamstown to Melbourne from 9 miles, via North Melbourne, to 6 miles, via Port Melbourne; but by 1890 this road had, as a result of consistent neglect, become impassable for any but the lightest traffic. Large deposits of sand blowing from pits to the north near the river had accumulated to depths of 3 to 4 feet in some places, and it was considered inadvisable to reconstruct the road unless means could be devised for protecting it from this sand. In 1896 the road was reconstructed, in accordance with designs submitted by Messrs. Champion and Heath, at a cost of £3,000. The principal features of the section were a length of 1,300 feet of pitching through the sand drifts and the planting of the drifts with marram grass; pitchers were used, because sand does not settle so readily on their smooth surface. These pitchers were confined in the sand by timber kerbs, with posts and cross ties, and flanked by clay filling, as shown in Fig. 5. This type of construction proved satisfactory, and in 1925 when the road was torn up the timber was sold for £65, while the pitchers have been used to kerb the shoulders for the whole length of the road. From 1897 to 1923 the total expenditure on maintenance was £9,614, shared equally by Port Melbourne and Williamstown Councils, but traffic increased so rapidly that an annual appropriation of £500 was inadequate to keep the road safe for traffic, and in 1925 the Board was requested to construct a modern highway.



It was decided to construct a reinforced concrete road 20 feet wide, with asphaltic macadam shoulders 6 feet wide. Levels showed that 30 per cent. of the existing road was below high tide, and that nearly all was below the level of the adjacent unclaimed land. The road was therefore raised about 2 feet near the river end and graded back to Graham-street. The alignment of the existing metal was adopted, so as to take advantage of the consolidation effected by years of traffic, except that the curves near the river were improved. The construction of the foundation was started in July, 1925, and was carried out by day labour. This work involved obtaining and consolidating 30,000 cubic yards of material, so as to form a bank 32 feet wide and up to 3 feet high. As local material consists entirely of drift sand and silt, wholly unsuitable for foundation work, all the necessary filling had to be transported to the site. Most of it consisted of brick-bats obtained through the demolition of many old buildings in Melbourne. This material was consolidated by two steam-rollers and by traffic. Equalizing culverts were placed at points where the road crosses swamp land that is below high-tide level. This foundation work cost £14,123, and was completed by the end of 1925. There was some delay in starting the construction of the pavement, as the successful tenderer declined to accept the contract, and it was necessary to readvertise the work. In March, 1926, the contract was signed by Concrete Constructions Ltd. of Sydney, who completed the work at the end of 1926.

Fig. 6 shows type cross-sections and part plan of the pavement. The concrete was composed of 1 part of cement,  $\frac{1}{4}$  of fine sand from Clyde,  $1\frac{1}{4}$  of screenings, and  $2\frac{1}{4}$  of metal from Brunswick; the actual volumetric mix being about 1 : 4.1, and the fineness modulus 6.3. The French coefficient of the metal averaged 13.2. Laboratory test cylinders gave average breaking strengths of 1,670 and 2,480 lb. a square inch at seven and 28 days respectively, with a water-cement ratio of 0.65 and a slump of 2 inches. Cylinders cast during the progress of the work showed that this was more than maintained. A 21E Rex paver, with broom and bucket delivery, was used and found very satisfactory, handling a four-bag ( $1\frac{1}{3}$  feet) batch (i.e., 32 cubic feet of loose material) easily. The slump was kept at about  $\frac{5}{8}$  inch. The pavement was laid at an average rate of 200 feet per day, joints being from 60 to 70 feet apart. The surface was obtained by tamping the concrete with a heavy tamper, rolling transversely with a light roller to remove ridges and surplus water, and then belting with a ballata belt to get the final "finish." The finish obtained was satisfactory, but probably different methods will be used in future, as it is thought that by using longitudinal planks for smoothing and finishing a superior surface will be obtained.

Adjacent to the river, where the line deviated from the old road and cut through reclaimed land, the formation was nearly all soft, and 6 inches of hand filling was insufficient to carry a 10-ton roller. It was strengthened by building up with cinders till the roller was supported. The concrete was also strengthened by adding an extra layer of reinforcement 2 inches from the bottom, thus doubly reinforcing it.

Shortly after the road was finished the crack along the centre over the longitudinal joint became visible, showing that it was functioning properly. A black line was then painted along the centre of the road, partly as a guide to traffic, and partly to waterproof the crack.

The total cost of the work was £42,600. The cost of the pavement, consisting of concrete 20 feet wide, and two shoulders each 6 feet wide, was £13,200 per mile, including supervision. The concrete above cost 18s. 5d. per square yard; the cement accounting for 39 per cent. of the cost, water  $3\frac{1}{2}$  per cent., and the other materials about 33 per cent.

An attempt has been made to calculate the actual saving to the community in general due to the building of the road. Traffic statistics have been furnished by the City Engineer of Williamstown for the period up to 21st August, and average running costs have been estimated. No definite information is available for estimating these costs, but they are thought to be on the low side, and are substantially the same as those used by Harger in dealing with similar problems under American conditions.

Assuming that the average saving in distance on one journey is 3 miles, and that total running costs per mile are 6d. for cars and light vans, 2s. for heavy lorries, and 6d. for horse vehicles, then the total reduction in expenditure for the 33 weeks ended 21st August was—

15,000	Horse vehicles, at 1s. (3 miles at 6d., less 6d. ferry charge)	..	£750
21,500	Heavy motor lorries, at 5s. 3d. (3 miles at 2s., less 9d. ferry charge)	..	5,643
94,500	Cars and light vans, at 1s. (3 miles at 6d., less 6d. ferry charge)	..	4,725
36,600	Motor cycles, &c.—negligible	.. .. .	..
<hr/>			<hr/>
167,600			£11,118
<hr/>			<hr/>

Or £17,600 a year.

The whole of this saving cannot be credited to the construction of the new road, because the old one did carry traffic ; it was, however, unsuited to any but horse vehicles, so that it is thought reasonable to deduct the proportion due to them and allow the saving on motor vehicles as being due to the new road. This amounts to about £16,000 a year, or sufficient to pay off the whole cost, with interest, in about three and a half years.

This saving is effected, although the present ferry, which is now showing a handsome profit after many years of unprofitable working, is hopelessly inadequate to carry existing peak loading.

The traffic now using the road is nearly all " through "—to and from Williamstown—but as cheap transport to the City is now assured, it is probable that the lead given by the State Savings Bank, which has built a number of workmen's homes, will be followed, and what is now waste and desolate land will soon be covered with factories and warehouses. The capacity of the road will then be fully appreciated.

Now that this portion of the Williamstown Short-road has been constructed, it is necessary to make provision for its maintenance. The maintenance required should be very slight, but it is extremely important that the little there is should be done systematically and at regular intervals.

Your obedient servant,

ARTHUR E. CALLAWAY,  
Chief Engineer.

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APPENDIX A.

COUNTRY ROADS BOARD FUND.

		RECEIPTS.			PAYMENTS.			1927.		
		£	s.	d.	£	s.	d.	£	s.	d.
1926.	To Balance	..	..	..	..	..	..	..	..	..
July 1, 1927.	To Motor Car Act No. 2702—	..	..	..	..	..	..	..	..	..
June 30, 1927.	Registration Fees	..	769,379	12	2	..	..	..	..	..
	Licence Fees	..	40,964	12	6	..	..	..	..	..
	Fines	..	14,330	8	1	..	..	..	..	..
	Less Refunds and cost of Administration	..	824,674	12	9	..	..	..	..	..
	..	..	27,762	8	7	..	..	..	..	..
	Unused Roads and Water Frontages Act No. 1894—	..	796,912	4	2	..	..	..	..	..
	Licence Fees	..	22,079	4	9	..	..	..	..	..
	Less Refunds and cost of Administration	..	2,536	5	11	..	..	..	..	..
	Country Roads Act No. 2035—	..	2,399	5	6	..	..	..	..	..
	Registration of Traction Engines	..	..	..	..	..	..	..	..	..
	Width of Tires Administration—	..	..	..	..	..	..	..	..	..
	Fines and Costs	..	241	2	6	..	..	..	..	..
	Municipalities Repayments—	..	..	..	..	..	..	..	..	..
	Permanent Works	..	95,194	10	11	..	..	..	..	..
	Maintenance	..	144,393	10	10	..	..	..	..	..
	Appropriation for Maintenance Act No. 2986	..	..	..	..	..	..	..	..	..
	Hire of Plant	..	35,624	17	8	..	..	..	..	..
	Stores and Materials	..	35,639	10	11	..	..	..	..	..
	Sundries	..	57,711	5	5	..	..	..	..	..
	Advance to Loan on Account of Permanent Works, refunded	..	..	..	..	..	..	..	..	..
	Advances on account of Expenditure under Act No. 3235, Item 4, Tourist Vote, refunded	..	..	..	..	..	..	..	..	..
	Advances, Division 72, Construction of Roads to relieve Unemployment in Thrip-infested areas, refunded	..	..	..	..	..	..	..	..	..
	Advances for expenditure to relieve Unemployment, refunded	..	..	..	..	..	..	..	..	..
	Advances under Div. 73, Construction of Roads for Isolated Settlers, refunded	..	..	..	..	..	..	..	..	..
	Advances under Div. 73, Construction of Port Melbourne to Williamstown Short Road, refunded	..	..	..	..	..	..	..	..	..
	Advances for expenditure under Emergency Provision, refunded	..	..	..	..	..	..	..	..	..
	Advances for Construction of Roads under Migrants Grant, refunded	..	..	..	..	..	..	..	..	..
	Advance from Public Account—Act No. 3341	..	..	..	..	..	..	..	..	..
		..	1,197,659	6	9	..	..	..	..	..
		..	427,012	1	7	..	..	..	..	..
		..	4,579	19	2	..	..	..	..	..
		..	6,082	7	2	..	..	..	..	..
		..	1,699	14	11	..	..	..	..	..
		..	2,544	19	6	..	..	..	..	..
		..	26,831	18	5	..	..	..	..	..
		..	8,949	6	1	..	..	..	..	..
		..	9,334	1	3	..	..	..	..	..
		..	100,000	0	0	..	..	..	..	..
		..	£1,790,429	0	3	..	..	..	..	..

£1,790,429 0 3

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APPENDIX A—continued.

RECONCILIATION STATEMENT.

	£	s.	d.	£	s.	d.	£	s.	d.
Balance as per Country Roads Board Account	15,084	10	0	..	..	..	..	..	..
Add outstanding Transfers	3,833	13	4	..	..	..	..	..	..
	18,918	3	4						
Deduct Accounts in Transit	..	..	..	..	..	..	..	..	..
Country Roads Board Fund Dr. Balance	£18,918	3	4	..	..	..	..	..	..

Dr.

REVENUE ACCOUNT, 30TH JUNE, 1927.

	£	s.	d.	£	s.	d.	£	s.	d.
1927.	..	..	..	..	..	..	..	..	..
June 30.	415,719	17	10	..	..	..	..	..	..
To Maintenance Works—General	..	..	..	..	..	..	..	..	..
Maintenance Works—	..	..	..	..	..	..	..	..	..
Woods Point Road	2,348	0	5	..	..	..	..	..	..
Woods Point Road	1,528	17	8	..	..	..	..	..	..
Walhalla Road	837	7	8	..	..	..	..	..	..
Mt. Buffalo Road	844	11	11	..	..	..	..	..	..
Mt. Buffalo Road	3,630	0	0	..	..	..	..	..	..
Bright-Onsco Road	1,800	0	0	..	..	..	..	..	..
State Highways	511,949	1	8	..	..	..	..	..	..
	522,937	19	4	..	..	..	..	..	..
Contributions to Sinking Fund	28,629	8	3	..	..	..	..	..	..
Interest on Loans	85,888	4	10	..	..	..	..	..	..
Payments Consolidated Revenue (Act No. 3425).	..	..	..	..	..	..	..	..	..
Office Furniture	1,294	5	1	..	..	..	..	..	..
Printing and Stationery	34,162	7	4	..	..	..	..	..	..
Salaries	23	4	4	..	..	..	..	..	..
Insurance of Employees	124	5	6	..	..	..	..	..	..
Instruments Account	494	8	9	..	..	..	..	..	..
Plans Purchased	20,833	3	7	..	..	..	..	..	..
Metal Investigation	1,173	0	10	..	..	..	..	..	..
Office Expenses	75	0	0	..	..	..	..	..	..
Audit Fee	803	13	5	..	..	..	..	..	..
Testing Materials	5,782	9	11	..	..	..	..	..	..
Storeyard Account	25	15	9	..	..	..	..	..	..
Erection of Telephone Line, Mount Hotham	..	..	..	..	..	..	..	..	..
Motor Expenses	1,777	10	11	..	..	..	..	..	..
Motor Expenses	464	17	9	..	..	..	..	..	..
Width of Tires Administration	1,424	17	9	..	..	..	..	..	..
Less Fines and Costs	241	2	6	..	..	..	..	..	..
Travelling Expenses	1,183	15	3	..	..	..	..	..	..
Postage and Telegrams	2,867	15	8	..	..	..	..	..	..
Plant Account	910	1	3	..	..	..	..	..	..
F.A.R. Commission (2 per cent.)	37,471	8	3	..	..	..	..	..	..
Camp Equipment	1,728	12	6	..	..	..	..	..	..
Patrolmen's Cottages	5	8	6	..	..	..	..	..	..
Dismantling Old Suspension Bridge—Snowy River	2,633	9	9	..	..	..	..	..	..
Storage Sites	256	0	0	..	..	..	..	..	..
Balance	450	11	0	..	..	..	..	..	..
	117,050	19	10	..	..	..	..	..	..
	209,002	1	5	..	..	..	..	..	..
	£1,399,228	11	6	..	..	..	..	..	..

	£	s.	d.	£	s.	d.	£	s.	d.
1926.	..	..	..	..	..	..	..	..	..
July 1.	..	..	..	..	..	..	..	..	..
June 30.	..	..	..	..	..	..	..	..	..
By Motor Car Act No. 2702—	..	..	..	..	..	..	..	..	..
Registration Fees	769,379	12	2	..	..	..	..	..	..
Licence Fees	40,964	12	6	..	..	..	..	..	..
Fines	14,330	8	1	..	..	..	..	..	..
	824,674	12	9	..	..	..	..	..	..
Less Refunds and Cost of Administration	..	..	..	..	..	..	..	..	..
	27,762	8	7	..	..	..	..	..	..
	796,912	4	2	..	..	..	..	..	..
Unused Roads and Water Frontages Act—	..	..	..	..	..	..	..	..	..
Licence Fees	..	..	..	..	..	..	..	..	..
Less Refunds and Cost of Administration	22,079	4	9	..	..	..	..	..	..
	2,536	5	11	..	..	..	..	..	..
	19,542	18	10	..	..	..	..	..	..
Country Roads Act No. 2635—	..	..	..	..	..	..	..	..	..
Traction Engines—Licence Fees and Fines	2,399	5	6	..	..	..	..	..	..
Forfeited Deposits	470	6	2	..	..	..	..	..	..
Materials, Sale of	345	4	9	..	..	..	..	..	..
Plans, Sale of	182	1	6	..	..	..	..	..	..
Plant Earnings	43,099	6	5	..	..	..	..	..	..
Deduct Working Costs	27,169	15	5	..	..	..	..	..	..
	15,929	11	0	..	..	..	..	..	..
Rents	57	2	6	..	..	..	..	..	..
Royalty on Metal	751	14	4	..	..	..	..	..	..
Timber, etc.—Revenue	105	9	9	..	..	..	..	..	..
Permanent Works—	..	..	..	..	..	..	..	..	..
Contributions payable by Municipalities	105,860	3	5	..	..	..	..	..	..
Adjustment	17	6	0	..	..	..	..	..	..
	105,842	17	5	..	..	..	..	..	..
Maintenance Works—	..	..	..	..	..	..	..	..	..
Contributions payable by Municipalities	..	..	..	..	..	..	..	..	..
Country Roads Act No. 2986/3379	138,706	12	8	..	..	..	..	..	..
Appropriation for Maintenance of Main Roads	10,000	0	0	..	..	..	..	..	..
Federal Aid Roads Act 1926—	..	..	..	..	..	..	..	..	..
Commission (2 per cent.)	3,089	18	9	..	..	..	..	..	..
	1,094,335	7	4	..	..	..	..	..	..
	£1,399,228	11	6	..	..	..	..	..	..

APPENDIX A—continued.

BALANCE-SHEET AS AT 30TH JUNE, 1927.

LIABILITIES.		ASSETS.	
	£ s. d.		£ s. d.
Contractors' Deposits	18,644 11 5	Country Roads Board Fund	15,084 10 0
Sundry Liabilities	20,259 12 5	Add Temporary Advances on account of Permanent Works Expenditure	1,450 15 0
Sinking Fund	224,427 1 5	Transfers Outstanding	2,382 18 4
Advance from Public Account Act 334	100,000 0 0		18,918 3 4
Revenue Account	209,002 1 5	Maintenance Expenditure—	
		Contributions payable by Municipalities	138,706 12 8
		In arrears	522 0 9
		Permanent Works—	
		Contributions payable by Municipalities	105,860 3 5
		In arrears	1,310 4 9
		Federal Aid Roads Act 1926—	107,170 8 2
		Special Advance	22,500 0 0
		Commission Account (1½ per cent.)	3,037 16 4
		Outstanding Accounts	25,537 16 4
		Materials—Stock	25,306 17 2
		Investment Account for Redemption of Loans	13,099 15 5
		Trust Account	224,427 1 5
			18,644 11 5
			<u>£572,333 6 8</u>

COUNTRY ROADS BOARD LOAN ACCOUNT, ACT No. 2635.

RECEIPTS.		PAYMENTS.	
	£ s. d.		£ s. d.
1926.		1927.	
July 1. To Balance	22,627 11 11	June 30. By Permanent Works (Appendix)	402,521 18 6
Proceeds of Loans	426,460 4 1	Deduct Refunds	3,662 12 3
Advance from Country Roads Board Fund	402,106 15 1	" Temporary Advance from Country Roads Board Fund—	
Direct Expenditure from Loan Account (30/6/27)	415 3 5	Refunded	427,012 1 7
		Adjustment	148 9 10
		Balance	426,863 11 9
			25,886 16 6
			<u>£851,609 14 6</u>

RECONCILIATION.

Treasury Balance	£ s. d.	£ s. d.
Add Outstanding Credits	25,687 15 8	
	199 0 10	25,886 16 6
Accounts in transit		2,014 8 3
		<u>£23,872 8 3</u>



APPENDIX A—continued.

DEVELOPMENTAL ROADS INTEREST ACTS Nos. 2944, 2985, & 3255.

RECEIPTS.		EXPENDITURE.	
1927.	1927.	1927.	1927.
June 30.	June 30.	June 30.	June 30.
£	£	£	£
s. d.	s. d.	s. d.	s. d.
To Interest contributed by Municipalities Act No. 2944 ..	6,923 6 1	By Payments to Treasury ..	61,922 12 11
„ Interest contributed by Municipalities Act No. 2985 ..	21,570 0 6		
„ Interest contributed by Municipalities Act No. 3255 ..	33,429 6 4		
	<u>61,922 12 11</u>		<u>61,922 12 11</u>

AUDITOR-GENERAL'S CERTIFICATE.

The Accounts have been audited and compared with the books with which they agree. Recon-  
 citations have also been made with the books of the Treasury. I certify that the statements submitted  
 are correct.

J. A. NORRIS, Auditor-General,  
 20th January, 1928.

EDWIN J. HICKS, Assistant Accountant,  
 22nd December, 1927.

## APPENDIX B.

TOURIST VOTE FOR IMPROVEMENTS AND MAKING ACCESSIBLE PLACES OF INTEREST,  
TOURIST RESORTS, ETC.—ACT No. 3235.

STATEMENT OF EXPENDITURE FOR THE FINANCIAL YEAR ENDED 30TH JUNE, 1927.

		£	s.	d.
Hall's Gap-Wartook ..	.. Construction of Roads into and through Grampians ..	3,434	13	0
Promontory Road ..	.. Constructing road to National Park, Wilson's Promontory ..	555	19	10
Great Ocean Road ..	.. Extension of Great Ocean Road between Lorne and Wye River ..	929	0	9
		4,919 13 7		

NOTE.—Expenditure on Great Ocean Road includes an amount of £214 17s. charged against Country Roads Board Fund. Net Expenditure under Tourist Vote is £4,704 16s. 7d.

SPECIAL GRANT FOR RELIEF OF UNEMPLOYED.

*Appropriation Act 1926-27.*

STATEMENT OF EXPENDITURE FOR THE FINANCIAL YEAR ENDED 30TH JUNE, 1927.

Shire.	Road.	£	s.	d.
Alberton ..	.. Binginwarri-Welshpool—4½ miles of clearing, forming and gravelling ..	718	17	5
Healesville ..	.. Toolangi—Earthworks and clearing between Toolangi and Kinglake ..	827	12	10
Orbost ..	.. Errinundra—Clearing and earthworks south of Boulder Creek ..	719	0	10
Orbost ..	.. Deddick River—Widening turnback to McKeilans Cutting and west of Snowy River ..	90	19	9
		2,356 10 10		

ROADS FOR ISOLATED SETTLERS.—DIVISION 73—ITEM 4.—WILLIAMSTOWN SHORT ROAD,  
DIVISION 73—ITEM 10.—MIGRANTS' GRANT—CHILDER'S SETTLEMENT ROAD.

STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDED 30TH JUNE, 1927.

Shire.	Road.	£	s.	d.
Korumburra ..	.. Outlet on to Loch-Nyora ..	396	9	4
Korumburra ..	.. Beverleys ..	50	0	0
Oxley ..	.. McGibbons ..	300	0	0
Otway ..	.. Outlet on to Carlisle River ..	102	5	0
Otway ..	.. Tuxion ..	299	12	0
Otway ..	.. Harvey's ..	466	16	0
Otway ..	.. Access Road from Aire River, Forrest to Olangolah East ..	37	0	0
Omeo ..	.. Reedy's Lane ..	100	0	0
Omeo ..	.. McNamara's ..	132	8	0
Phillip Island and Woolamai ..	.. Road to Kernot Railway Station ..	150	0	0
Tungamah ..	.. Koonamal Soldier Settlement ..	11	9	2
Tambo ..	.. Gillingall ..	99	0	0
Upper Yarra ..	.. Big Pat's Creek ..	250	0	0
Woorayl ..	.. Central ..	100	0	0
		2,494 19 6		
Williamstown Short Road ..	.. ..	22,261	8	3
Migrant's Grant—Childer's Soldier Settlement Road ..	.. ..	6,410	13	1

## APPENDIX B—continued.

## GRANT TO RELIEVE UNEMPLOYMENT AND THRIP-INFESTED AREAS.—DIVISION 74.—ITEM 11.

STATEMENT OF EXPENDITURE FOR THE FINANCIAL YEAR ENDED 30TH JUNE, 1927.

Municipality.	Road.	£	s.	d.
Emerald and Beaconsfield ..	Formation of 1½ miles near Beaconsfield ..	384	16	0
Castlemaine-Harcourt ..	Widening, forming, improvement in turn of road near Harcourt ..	1,000	0	0
Flinders ..	Main Creek Road ..	304	7	10
Yea ..	Kinglake-Flowerdale ..	1,425	13	5
Frankston and Hastings ..	Baxter-Pearcedale ..	1,500	0	0
Lilydale ..	Monbulk-Seville ..	1,482	3	6
		6,097	0	9

## TRUST FUND—HIGHWAYS AND VEHICLES ACT No. 3379.

EXPENDITURE UNDER THE SUPERVISION OF MUNICIPALITIES, YEAR ENDED 30TH JUNE, 1927.

	Trust Fund.		Special Appropriation.			
	£	s. d.	£	s. d.	£	s. d.
Brighton City ..	..	..	984	0	0	
Camberwell City ..	1,923	0 0	1,282	0	0	
Essendon City ..	..	..	1,532	0	0	
Fitzroy City ..	..	..	1,342	0	0	
Footscray City ..	..	..	1,628	0	0	
Kew City ..	..	..	848	0	0	
Malvern City ..	1,628	0 0	..	..	..	
Northcote City ..	..	..	1,400	0	0	
Port Melbourne ..	..	..	512	0	0	
Prahran City ..	..	..	1,990	0	0	
Richmond City ..	..	..	1,696	0	0	
Sandringham ..	620	0 0	620	0	0	
Williamstown ..	1,308	0 0	872	0	0	
Oakleigh Town ..	..	..	284	0	0	
Box Hill Borough ..	366	0 0	366	0	0	
Blackburn and Mitcham Shire ..	220	0 0	..	..	..	
Braybrook Shire ..	206	0 0	..	..	..	
Heidelberg Shire ..	802	0 0	802	0	0	
Keilor Shire ..	52	0 0	..	..	..	
Mulgrave Shire ..	216	0 0	..	..	..	
Whittlesea Shire ..	140	0 0	..	..	..	
Werribee Shire ..	..	..	208	0	0	
Ballarat City ..	..	..	1,420	0	0	
Warrnambool City ..	..	..	310	0	0	
Geelong West Town ..	..	..	289	18	3	
Newtown and Chilwell Town ..	..	..	296	0	0	
	7,481	0 0	18,681	18	3	26,162 18 3

## RECONCILIATION.

## SPECIAL APPROPRIATION.

	£	s.	d.
Expenditure as per Treasury Books ..	17,261	18	3
Claims in Transit at 30th June, 1927—Ballarat City ..	1,420	0	0
Expenditure as per Country Roads Board Accounts ..	18,681	18	3

## APPENDIX B—continued.

FEDERAL STATE GRANTS—STATEMENT OF EXPENDITURE FOR THE FINANCIAL YEAR ENDED  
30TH JUNE, 1927.

Shire.	Roads.	Federal State Grant No. 1.	Federal State Grant No. 2.	Federal State Grant No. 3.	Federal State Grant, 1925, Act No. 17.	Federal Grant, 1925, Act No. 17, 2nd Schedule.	Federal Aid Roads Act 1926.
		£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Werribee .. ..	Point Cook .. ..	165 10 0	..	..	..	..	..
Belfast and Portland	Portland-Port Fairy ..	..	..	..	..	..	..
Kowree .. ..	Elderslie Soldier Settlement	..	..	..	..	..	..
Otway .. ..	Aire River-Forrest ..	..	..	1,092 0 0	..	..	..
Portland .. ..	Portland-Nelson ..	..	..	43 16 1	2,134 0 0	..	..
Morwell .. ..	Morwell River Valley ..	..	..	..	1,829 1 0	..	28 16 6
Otway .. ..	Forrest-Apollo Bay ..	..	211 13 4	1,410 1 0	47 9 4	..	15,750 0 0
Buln Buln .. ..	Fumina .. ..	..	..	..	4,817 12 8	..	20 10 6
Portland .. ..	Portland-Mt. Gambier ..	..	..	..	3,191 6 7	..	..
Eltham and Yea ..	Yarra Glen-Glenburn ..	..	..	..	921 10 10	..	..
Orbost .. ..	Murrungower .. ..	..	..	..	5,066 2 4	..	..
Otway .. ..	Apollo Bay-Wye River ..	..	..	..	2,162 3 11	..	805 17 0
Heytesbury .. ..	Timboon-Nirranda ..	..	..	..	1,521 9 7	..	562 13 7
Mildura .. ..	Mildura .. ..	..	..	..	4,833 5 4	..	..
Cohuna-Kerang and Swan Hill	Murray River Valley ..	..	..	..	8,416 14 4	..	..
Towong .. ..	Murray River-Tallangatta	..	..	..	1,184 0 0	..	260 0 0
Hampden and Grenville	Lismore-Pittong ..	..	..	..	72 10 9	..	..
South Gippsland ..	Foster-Boolarra ..	..	..	..	278 2 0	..	2,342 13 5
Mirboo and Woorayl	Mt. Vernon and Boorool Soldier Settlement	..	..	..	4,503 0 0	..	..
Otway .. ..	Carlisle Gellibrand ..	..	..	..	3,249 0 0	..	2,476 14 3
	North-Eastern Highway Avenel to Longwood	..	..	..	..	4,316 2 6	..
	Princes Highway East—Nar Nar Goon to Drouin	..	..	..	..	5,336 17 4	..
	Princes Highway—Oakleigh to Dandenong	..	..	..	..	12,800 1 5	..
Woorayl and South Gippsland	Turton's Creek ..	..	..	..	..	..	2,542 7 6
Yea .. ..	Yarra Glen-Glenburn ..	..	..	..	..	..	2,276 14 6
Narracan .. ..	Allambee Estate ..	..	..	..	..	..	4 2 2
Orbost .. ..	Delegate (Sardine Creek to Copper Ore Creek)	..	..	..	..	..	109 13 6
Morwell and Traralgon	Jeeralang West ..	..	..	..	..	..	2,300 17 4
Mildura .. ..	Murray River Valley ..	..	..	..	..	..	115 0 0
Kerang .. ..	Murray River Valley ..	..	..	..	..	..	15 15 1
Cohuna .. ..	Murray River Valley ..	..	..	..	..	..	346 3 9
Swan Hill .. ..	Murray River Valley ..	..	..	..	..	..	2,144 3 6
Benalla and Oxley	Toombullup .. ..	..	..	..	..	..	365 0 0
Grenville .. ..	Lismore-Pittong ..	..	..	..	..	..	743 12 9
Rochester .. ..	Shepparton, Rochester, Wycheproof and Rainbow	..	..	..	..	..	2,216 19 10
Rodney .. ..	Shepparton, Rochester, Wycheproof and Rainbow	..	..	..	..	..	1,631 9 6
Otway .. ..	Lavers Hill-Princetown	..	..	..	..	..	5,320 18 3
Woorayl .. ..	Mt. Vernon and Boorool	..	..	..	..	..	262 0 0
Mirboo .. ..	Mt. Vernon and Boorool	..	..	..	..	..	505 0 10
Heytesbury and Warrnambool	Peterborough - Port Campbell	..	..	..	..	..	513 3 8
Hampden .. ..	Vite Vite .. ..	..	..	..	..	..	3,482 18 11
Heytesbury .. ..	Timboon-Scott's Creek	..	..	..	..	..	661 4 8
Glenelg .. ..	Casterton-Mt. Gambier	..	..	..	..	..	890 0 0
	Princes Highway West..	..	..	..	..	..	39,594 1 0
	Princes Highway East ..	..	..	..	..	..	32,945 13 3
	Western Highway .. ..	..	..	..	..	..	52,736 17 4
	North Western Highway	..	..	..	..	..	32,643 9 11
	Northern Highway ..	..	..	..	..	..	223 11 7
	North-Eastern Highway	..	..	..	..	..	31,730 3 2
	Omeo Highway .. ..	..	..	..	..	..	422 17 5
		165 10 0	211 13 4	2,545 17 1	44,227 8 8	22,453 1 3	238,991 4 8

## NOTE.—

1925-26 .. ..	Excess expenditure on Lavers Hill-Princetown-road—brought forward	£ s. d. 583 10 8
1926-27 .. ..	Net expenditure—Under Federal State Grant, 1925—Act No. 17	43,643 18 0
		<u>44,227 8 8</u>



APPENDIX B—*continued.*

## SUMMARY OF BOARD'S ASSETS AT 30TH JUNE, 1927.

	£	s.	d.
Furniture and Fittings—			
Office .. .. .	5,121	15	4
Motor Registration Branch .. .. .	3,347	0	0
Patrolmen's Cottages .. .. .	6,399	3	2
Plant .. .. .	83,575	15	0
Storeyard Plant .. .. .	1,213	12	3
Storeyard Buildings, &c. .. .. .	4,820	0	0
Instruments—			
Survey .. .. .	327	15	0
Testing Materials .. .. .	162	12	6
	<u>104,967</u>	<u>13</u>	<u>3</u>

ARTHUR E. CALLAWAY,  
Chief Engineer.

22nd December, 1929.

## APPENDIX C.

## COUNTRY ROADS BOARD.

STATEMENT OF APPORTIONMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF MAIN ROADS FOR THE YEAR ENDED 30TH JUNE, 1926.

Name of Municipality.	Permanent Works.		Maintenance.	Name of Municipality.	Permanent Works.		Maintenance.
	Principal.	Interest.	Amount.		Principal.	Interest.	Amount.
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
Alberton Shire ..	1,666 14 3	5 15 3	1,240 0 5	Brought forward	48,059 18 10	613 19 3	68,649 12 10
Alexandra Shire ..	846 19 10	10 4 7	1,078 8 6	Inglewood Borough	..	..	57 16 1
Arapiles Shire ..	1,044 16 10	21 12 4	357 11 7	Kara Kara Shire	4,935 19 9	43 10 3	607 12 4
Ararat Borough ..	..	..	565 13 7	Karkaroc Shire ..	3,429 18 1	48 16 11	1,451 10 3
Ararat Shire ..	2,983 7 9	12 0 0	2,789 4 8	Keilor Shire ..	..	..	606 8 8
Avoca Shire ..	348 19 2	3 5 10	350 13 6	Kerang Shire ..	..	..	149 7 9
Avon Shire ..	1,483 15 11	47 14 1	281 10 4	Kilmore Shire ..	..	..	31 11 4
Bacchus Marsh Shire	58 1 1	1 2 10	483 3 9	Koroit Borough ..	..	..	1,170 0 11
Bairnsdale Shire ..	95 2 9	1 0 0	595 6 1	Korung Shire ..	613 7 2	4 5 5	195 0 0
Ballan Shire ..	297 7 0	5 5 11	345 5 7	Korumburra Shire	1,499 6 6	32 10 8	1,822 19 11
Ballarat Shire ..	..	..	2,217 0 4	Kowree Shire ..	1,015 13 0	8 12 3	1,604 17 4
Bannockburn Shire	..	..	1,041 16 7	Kyneton Shire ..	1,478 11 8	10 10 1	598 0 0
Barrarbool Shire ..	..	..	600 0 11	Lawloit Shire ..	1,563 12 0	15 0 11	565 15 4
Beechworth Shire	315 5 3	1 13 6	692 0 6	Leigh Shire ..	374 13 6	3 1 8	883 10 9
Belfast Shire ..	825 10 7	2 10 11	1,085 16 0	Lexton Shire ..	..	..	408 16 10
Bellarine Shire ..	..	..	1,413 8 3	Lilydale Shire ..	3,161 19 1	47 1 4	3,489 0 11
Benalla Shire ..	..	..	1,472 4 9	Lowan Shire ..	1,719 9 6	42 14 0	994 14 5
Berwick Shire ..	3,153 14 6	51 19 9	1,105 12 6	Maffra Shire ..	165 6 0	0 14 7	1,110 5 0
Bet Bet Shire ..	184 17 3	1 9 5	223 6 8	Maldon Shire ..	..	..	285 1 11
Birohip Shire ..	322 1 3	2 8 1	472 13 2	Mansfield Shire ..	1,797 11 5	32 8 3	1,625 0 9
Blackburn and Mit-	..	..	545 4 2	Marong Shire ..	1,152 15 4	13 15 0	1,136 11 9
cham	..	..	414 13 0	Maryborough	..	..	39 17 0
Borong Shire ..	1,822 14 5	17 3 4	1,738 3 6	Borough	..	..	522 8 7
Braybrook Shire ..	309 15 9	3 16 9	478 10 5	MeIvor Shire ..	405 0 0	2 14 0	326 17 10
Bright Shire ..	1,225 0 0	23 1 3	101 9 3	Melton Shire ..	..	..	566 10 5
Broadford Shire ..	..	..	1,277 3 11	Metcalfe Shire ..	..	..	231 18 7
Broadmeadows Shire	..	..	1,358 15 7	Mildura Town ..	1,074 8 8	3 0 8	2,161 7 6
Bulla Shire ..	..	..	1,735 9 11	Mildura Shire ..	3,690 15 8	44 7 5	1,342 13 2
Buln Buln Shire ..	1,286 13 10	27 16 3	50 4 6	Minhamite Shire ..	..	..	583 0 8
Bungaree Shire ..	..	..	363 10 1	Mirboo Shire ..	1,547 12 5	21 10 4	284 12 1
Buninyong Shire ..	..	..	269 19 4	Moorabbin Shire ..	..	..	138 6 1
Carrum Borough	..	..	311 18 10	Mordialloc Town	..	..	1,304 9 7
Castlemaine	..	..	539 7 0	Mornington Shire	185 9 9	5 13 3	915 9 9
Borough	..	..	109 9 10	Mortlake Shire ..	..	..	407 13 5
Charlton Shire ..	2,572 13 1	70 13 3	104 7 0	Morwell Shire ..	3,141 14 2	83 1 8	1,648 8 11
Chiltern Shire ..	1,000 0 0	2 10 7	2,615 1 6	Mount Rouse	..	..	994 7 10
Clunes Borough ..	..	..	1,099 9 7	Shire	..	..	796 8 11
Colac Shire ..	976 6 0	20 2 7	1,706 17 9	Mulgrave Shire ..	..	..	146 15 10
Corio Shire ..	154 0 6	0 3 5	738 6 8	Narraacan Shire ..	133 12 5	4 14 8	..
Cranbourne Shire	2,448 7 5	40 3 7	135 2 2	Newham and Wood-	149 11 2	3 1 2	..
Creswick Shire ..	..	..	789 4 2	end Shire	..	..	..
Creswick Borough	..	..	212 14 3	Newtown and Chil-	..	..	..
Dandenong Shire ..	..	..	2,369 8 11	well Borough	..	..	..
Daylesford Borough	..	..	1,264 5 3	Newstead and Mt.	..	..	..
Deakin Shire ..	..	..	735 10 2	Alexander Shire	..	..	384 8 4
Dimboola Shire ..	2,334 15 6	36 14 9	1,598 19 9	Numurkah Shire ..	1,991 16 3	13 1 11	1,220 3 8
Donald Shire ..	85 0 0	0 1 9	3,252 10 9	Oakleigh Town ..	..	..	422 0 0
Doncaster and Tem-	..	..	578 8 6	Omeo Shire ..	957 11 2	10 1 6	158 10 2
plestowe Shire	..	..	190 8 4	Orbost Shire ..	824 12 3	9 10 10	797 12 0
Dundas Shire ..	1,161 6 3	7 3 0	..	Otway Shire ..	..	..	522 0 9
Dunmunkle Shire	6,220 3 5	65 14 8	247 14 7	Oxley Shire ..	571 11 10	17 16 8	512 4 3
East Loddon Shire	340 15 9	0 17 6	4,364 1 6	Phillip Island and	..	..	..
Echuca Borough ..	102 1 0	3 0 3	1,956 18 3	Woolamai Shire	3,160 1 10	18 17 6	834 19 11
Eltham Shire ..	1,427 6 3	12 14 8	1,176 18 2	Port Fairy Borough	..	..	403 3 11
Euroa Shire ..	2,899 18 10	37 18 2	..	Portland Shire ..	687 12 3	11 15 2	651 7 2
Ferntree Gully Shire	..	..	214 14 3	Preston City ..	..	..	774 13 0
Flinders Shire ..	628 15 0	13 19 1	2,369 8 11	Pyalong Shire ..	..	..	98 17 2
Footscray City ..	..	..	1,264 5 3	Queenscliff Borough	..	..	830 16 10
Frankston and Has-	..	..	735 10 2	Ringwood Borough	..	..	732 16 6
tings Shire	1,119 16 2	7 17 9	1,598 19 9	Ripon Shire ..	667 10 0	7 9 10	476 4 5
Geelong City ..	..	..	3,252 10 9	Rochester Shire ..	4,043 4 8	67 4 10	638 16 9
Geelong West	..	..	578 8 6	Rodney Shire ..	2,399 2 11	45 6 2	702 5 0
Borough	..	..	190 8 4	Romsey Shire ..	68 4 11	2 7 10	312 2 10
Gisborne Shire ..	..	..	..	Rosedale Shire ..	260 8 0	2 3 4	510 1 2
Glenelg Shire ..	1,581 14 1	9 14 0	214 14 10	Rutherglen Shire..	536 19 9	19 12 3	554 14 10
Glenlyon Shire ..	..	..	628 10 3	Sale Town ..	..	..	341 4 10
Goulburn Shire ..	717 12 8	7 6 4	231 9 0	Seymour Shire ..	758 19 1	2 4 6	195 2 1
Grenville Shire ..	53 14 9	1 13 11	1,508 13 6	Shepparton Shire..	567 5 5	15 16 9	515 7 11
Hamilton Borough	..	..	251 13 7	South Barwon Shire	..	..	1,403 7 9
Hampden Shire ..	8 6 6	8 6 6*	3,505 7 7	South Gippsland	..	..	..
Healesville Shire ..	441 16 9	0 18 1	964 9 10	Shire	2,695 18 6	21 15 0	788 0 9
Heidelberg Shire	..	..	2,234 12 1	Stawell Borough ..	322 14 4	2 7 9	356 0 4
Heytesbury Shire	2,492 13 7	19 12 11	970 8 11	Stawell Shire ..	1,107 11 4	18 1 1	671 9 3
Horsham Borough	1,021 17 11	6 12 5	1,176 18 7	Strathfieldsaye Shire	8 13 6	0 1 3	545 6 9
Huntly Shire ..	..	..	180 17 11			8 13 6*	
Carried forward	48,059 18 10	613 19 3	68,649 12 10	Carried forward	102,926 4 1	1,379 11 5	116,208 19 7

\* Liability Liquidated.

STATEMENT OF APPORTIONMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF MAIN ROADS, ETC.—*continued*.

Name of Municipality.	Permanent Works.		Maintenance.	Name of Municipality.	Permanent Works.		Maintenance.
	Principal.	Interest.	Amount.		Principal.	Interest.	Amount.
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
Brought forward	102,926 4 1	1,379 11 5	116,208 19 7	Brought forward	123,075 15 8	1,583 6 8	123,156 12 6
Swan Hill Shire ..	1,787 1 8	22 1 8	1,089 3 7	Wannon Shire ..	865 15 2	14 9 2	1,494 9 1
St. Arnaud Borough	..	..	823 0 3	Waranga Shire ..	1,785 6 0	36 9 2	2,391 19 2
Talbot Shire ..	..	..	59 19 5	Warragul Shire ..	1,085 12 0	6 15 1	3,316 4 2
Tambo Shire ..	491 2 10	8 13 0	884 5 5	Warrnambool Shire	..	..	5,191 4 2
Towong Shire ..	3,666 9 6	34 18 10	496 0 2	Werribee Shire ..	937 10 0	0 11 6	133 12 1
Traralgon Shire ..	2,441 2 10	39 13 9	463 3 2	Whittlesea Shire ..	903 14 1	21 3 0	1,377 16 5
Tullaroop Shire ..	5,847 18 9	2 10 2	815 1 11	Wimmera Shire ..	987 13 3	20 1 0	491 0 6
Tungamah Shire ..	1,522 16 3	21 8 0	224 13 5	Winchelsea Shire	2,445 13 6	27 2 0	451 9 10
Upper Murray Shire	1,300 1 6	20 8 7	493 6 3	Wodonga Shire ..	12,374 12 3	36 0 9	191 14 5
Upper Yarra Shire	1,116 7 1	26 2 9	578 6 7	Wonthaggi Borough	614 5 0	9 19 1	1,044 12 11
Violet Town Shire	349 10 8	5 8 5	112 18 10	Woorayl Shire ..	4,721 9 9	58 16 11	2,227 12 11
Walpeup Shire ..	1,404 3 7	9 3 9	390 7 3	Wycheproof Shire	2,764 1 8	31 18 10	609 2 5
Wangaratta				Yackandandah Shire	2,309 3 5	17 12 3	702 15 9
Borough	217 10 5	7 18 4	381 8 3	Yarrowonga Shire	676 0 0	7 13 11	165 15 9
Wangaratta Shire	5 6 6	{ 0 1 6 }	135 18 5	Yea Shire ..	195 14 7	2 9 5	1,423 15 10
{ 5 6 6* }							
Carried forward	123,075 15 8	1,583 6 8	123,156 12 6		155,742 6 4	1,874 8 9	144,369 17 11

\* Liability Liquidated.

## APPENDIX D.

## COUNTRY ROADS BOARD.

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE  
OF MAIN ROADS FOR THE YEAR ENDED 30th JUNE, 1927.

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
<b>ALBERTON SHIRE—</b>				
Balook-Traralgon Road .. .. .	2,313 17 10		9 17 4	
Boolarra-Welshpool Road .. .. .	11 13 6		111 12 7	
Carrajung-Gormandale Road .. .. .	3,252 3 11		470 3 10	
Foster-Yarram Road .. .. .	933 3 2		66 8 4	
Sale-Yarram Road .. .. .	915 1 8		1,633 14 4	
Yarram-Boolarra Road .. .. .	142 15 3		437 4 7	
Yarram-Port Albert Road .. .. .	..		393 1 9	
Yarram-Won Wron .. .. .	..		882 7 2	
		7,568 15 4		4,004 9 11
<b>ALBERTON AND MORWELL SHIRES (Joint Works)—</b>				
Boolarra-Welshpool Road .. .. .	..		129 5 2	
				129 5 2
<b>ALEXANDRA SHIRE—</b>				
Cathkin-Mansfield Road .. .. .	230 16 0		671 10 10	
Healesville-Alexandra Road .. .. .	1,356 19 5		1,164 4 5	
Upper Goulburn Road .. .. .	432 8 4		1,004 17 11	
Yarek Road .. .. .	..		68 12 8	
		2,020 3 9		2,909 5 10
<b>ALEXANDRA AND YEA SHIRES (Joint Works)—</b>				
Cathkin-Mansfield Road and Upper Goulburn Road ..	49 8 10		..	
		49 8 10		
<b>ARAPILES SHIRE—</b>				
Horsham-Hamilton Road .. .. .	171 0 0		1,115 13 1	
Horsham-Natimuk-Edenhope Road .. .. .	3,116 13 11		143 19 5	
		3,287 13 11		1,259 12 6
<b>ARARAT BOROUGH—</b>				
Ballarat-Stawell Road .. .. .	..		859 14 3	
				859 14 3
<b>ARARAT SHIRE—</b>				
Ararat-Elmhurst Road .. .. .	..		1,030 19 4	
Ararat-Warrambool Road .. .. .	..		2,602 15 2	
Ballarat-Hamilton Road .. .. .	4,304 18 7		3,269 16 8	
Ballarat-Stawell Road .. .. .	..		0 5 0	
Maroona-Glenthompson Road .. .. .	..		2,377 11 11	
		4,304 18 7		9,281 8 1
<b>AVOCA SHIRE—</b>				
Ararat Road .. .. .	..		176 17 10	
Ballarat-St. Arnaud Road .. .. .	495 0 0		688 2 7	
Bealiba Road .. .. .	425 0 0		117 16 5	
Landsborough Road .. .. .	..		..	
Maryborough Road .. .. .	60 3 10		412 18 0	
		980 3 10		1,395 14 10
<b>AVON SHIRE—</b>				
Dargo Road .. .. .	..		471 19 0	
Maffra-Sale Road .. .. .	..		281 11 2	
Prince's Highway .. .. .	..		161 5 8	
				914 15 10
<b>BACCHUS MARSH SHIRE—</b>				
Ballarat Road .. .. .	..		242 18 8	
Geelong-Bacchus Marsh Road .. .. .	1,597 11 3		108 0 9	
Gisborne Road .. .. .	..		1,299 8 7	
		1,597 11 3		1,650 8 0
<b>BAIRNSDALE SHIRE—</b>				
Bairnsdale-Bruthen Road .. .. .	..		..	
Bullumwaal-Tabberahbera Road .. .. .	..		500 19 6	
Prince's Highway .. .. .	..		1,396 5 1	
				1,897 4 7
<b>BALLAN SHIRE—</b>				
Ballarat Road .. .. .	..		1,276 6 3	
Daylesford Road .. .. .	..		1,956 5 4	
Gordons-Meredith Road .. .. .	..		656 4 8	
Mount Wallace Road .. .. .	2,150 0 0		446 14 1	
		2,150 0 0		4,335 10 4
<b>BALLARAT SHIRE—</b>				
Ararat Road .. .. .	..		..	
Ballarat-Creswick Road .. .. .	..		900 14 2	
Ballarat-Lexton Road .. .. .	..		2,766 6 8	
Maryborough-Ballararat Road .. .. .	..		1,240 9 0	
				4,907 9 10
Carried forward .. .. .	..	21,958 15 6	..	33,544 19 2

## STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—continued.

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	21,958 15 6	..	33,544 19 2
<b>BANNOCKBURN SHIRE—</b>				
Geelong-Ballarat Road .. .. .	..		1,262 9 10	
Gordons-Meredith Road .. .. .	..		0 9 5	
Inverleigh Road .. .. .	..		1,301 14 1	
Shelford-Bannockburn Road .. .. .	..		181 9 7	2,746 2 11
<b>BARRARBOOL SHIRE—</b>				
Anglesea Road .. .. .	..		1,490 7 5	
Hendy Main Road .. .. .	0 10 0		423 12 9	
Prince's Highway .. .. .	..		1 13 1	1,915 13 3
		0 10 0		
<b>BEECHWORTH SHIRE—</b>				
Beechworth Road .. .. .	0 13 4		479 17 4	
Bright Road .. .. .	6 1 8		64 17 3	
Everton-Myrtleford Road .. .. .	1,060 14 11		553 19 8	
Stanley Road .. .. .	306 8 10		64 19 1	1,163 13 4
		1,373 18 9		
<b>BEECHWORTH AND BRIGHT SHIRES (Joint Works)—</b>				
Bright Road .. .. .	..		0 10 4	0 10 4
<b>BELFAST SHIRE—</b>				
Hamilton Road .. .. .	186 3 6		1,339 0 3	
Penshurst Road .. .. .	..		1,230 9 1	
Prince's Highway (Portland section) .. .. .	..		6 6 10	
Prince's Highway (Warrnambool-Port Fairy section) .. .. .	..		2 15 6	2,578 11 8
		186 3 6		
<b>BELLARINE SHIRE—</b>				
Geelong-Portarlington Road .. .. .	..		1,849 19 0	
Geelong-Queenscliff Road .. .. .	..		2,150 5 3	4,000 4 3
<b>BENALLA SHIRE—</b>				
Goorambat Road .. .. .	..		732 5 4	
Goorambat-Thoona Road .. .. .	..		563 2 5	
Greta Road .. .. .	..		70 15 11	
Lima Road .. .. .	..		20 4 0	
Mansfield-Benalla Road .. .. .	..		807 0 8	
Sydney Road .. .. .	..		2,612 12 1	
Tatong Road .. .. .	..		65 3 6	
Tatong-Toombullup Road .. .. .	..		416 19 3	5,288 3 2
<b>BERWICK SHIRE—</b>				
Gembrook Road .. .. .	92 15 9		50 0 3	
Gembrook-Beenak Road .. .. .	..		48 6 3	
Hallam-Emerald Road .. .. .	..		178 1 10	
Nar-nar-noon-Longwarry Road .. .. .	369 2 7		1,592 3 10	
Prince's Highway .. .. .	..		2,029 16 6	
Woori Yallock-Pakenham-Koo-wee-rup Road .. .. .	5,090 5 7		3,871 10 2	7,769 18 10
		5,552 3 11		
<b>BET BET SHIRE—</b>				
Avoca-Bealiba Road .. .. .	1,103 11 10		261 1 10	
Betley Road .. .. .	..		42 16 2	
Dunolly Road .. .. .	..		374 16 4	678 14 4
		1,103 11 10		
<b>BIRCHIP SHIRE—</b>				
Beulah-Birchip-Wycheproof Road .. .. .	1,296 14 1		95 0 0	
Donald-Birchip-Sealake Road .. .. .	..		28 0 0	123 0 0
		1,296 14 1		
<b>BORUNG SHIRE—</b>				
Birchip Road .. .. .	2 4 5		13 16 9	
Dimboola Road .. .. .	4,356 14 9		220 18 7	
Hopetoun Road .. .. .	0 5 4		993 3 11	
Minyip Road .. .. .	2,296 18 1		215 17 1	
Rainbow Road .. .. .	..		378 8 8	1,822 5 0
		6,656 2 7		
<b>BLACKBURN AND MITCHAM SHIRE—</b>				
Main Healesville Road .. .. .	..		981 11 0	981 11 0
<b>BRAYBROOK SHIRE—</b>				
Prince's Highway .. .. .	..		Bd. 581 0 5	
Ballarat Road .. .. .	..		2,125 9 7	2,706 10 0
<b>BRIGHT SHIRE—</b>				
Mt. Buffalo Road .. .. .	..		Bd. 3,630 0 0	
Bright Road .. .. .	607 0 8		583 15 6	
Harrietville Road .. .. .	321 12 1		381 10 1	
Mt. Buffalo U.R. Road .. .. .	..		Bd. 844 11 11	5,439 17 6
		928 12 9		
<b>BRIGHT AND OMEO SHIRES (Joint Works)—</b>				
Bright-Omeo Road .. .. .	..		Bd. 2,000 0 0	2,000 0 0
Carried forward .. .. .	..	39,056 12 11	..	72,759 14 9

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	39,056 12 11	..	72,759 14 9
<b>BROADFORD SHIRE—</b>				
Sydney Road .. .. .	..		40 6 1	40 6 1
<b>BROADMEADOWS SHIRE—</b>				
Lancefield Road .. .. .	..		38 12 0	
Sydney Road .. .. .	..		524 16 2	563 8 2
<b>BROADMEADOWS AND KEILOR SHIRES (Joint Works)—</b>				
Lancefield Road .. .. .	..		2,330 14 3	2,330 14 3
<b>BULLA SHIRE—</b>				
Melbourne-Lancefield Road .. .. .	..		3,276 10 2	
Sunbury Road .. .. .	..		811 2 10	4,087 13 0
<b>BULLA AND KEILOR SHIRES (Joint Works)—</b>				
Melbourne-Lancefield Road .. .. .	..		365 4 6	365 4 6
<b>BULN BULN SHIRE—</b>				
Bloomfield Road .. .. .	..		0 1 11	
Fumina Road .. .. .	..		208 7 8	
Loch Valley Road .. .. .	..		65 10 1	
Longwarry-Drouin Road .. .. .	..		1,086 16 10	
Magpie-Duggan Road .. .. .	..		0 2 1	
Main Neerim "A" Road .. .. .	..		333 8 2	
Main Neerim "B" Road .. .. .	..		1,058 2 9	
Main Neerim "C" Road .. .. .	2,699 9 3		89 0 9	
Main South Road .. .. .	..		2,233 18 10	
Neerim East Road .. .. .	..		114 12 2	
Prince's Highway .. .. .	..		1,259 4 8	
Westernport Road .. .. .	78 10 1		60 1 9	
		2,777 19 4		6,509 7 8
<b>BUNGAREE SHIRE—</b>				
Ballarat Road .. .. .	..		1 11 0	
Ballarat-Creswick Road .. .. .	..		813 14 5	
Daylesford-Ballarat Road .. .. .	..		1,494 9 8	2,309 15 1
<b>BUNINYONG SHIRE—</b>				
Ballarat Road .. .. .	..		0 14 8	
Ballarat-Rokewood Road .. .. .	..		437 10 0	
Elaine-Mt. Mercer Road .. .. .	..		1 18 2	
Geelong-Ballarat Road .. .. .	..		1,220 8 6	1,660 11 4
<b>CARRUM BOROUGH—</b>				
Point Nepean Road .. .. .	12,181 10 9		2,367 14 5	2,367 14 5
<b>CASTLEMAINE BOROUGH—</b>		12,181 10 9		
Melbourne-Bendigo Road .. .. .	..		669 14 4	669 14 4
<b>CHARLTON SHIRE—</b>				
Bendigo Road .. .. .	..		211 15 4	
Donald Road .. .. .	705 0 0		1,604 15 0	
St. Arnaud Road .. .. .	..		393 13 4	2,210 3 8
		705 0 0		
<b>CHILTERN SHIRE—</b>				
Chiltern-Howlong Road .. .. .	..		99 7 11	
Rutherglen-Wodonga Road .. .. .	..		133 18 2	
Sydney Road .. .. .	..		112 6 0	345 12 1
<b>CLUNES BOROUGH—</b>				
Maryborough-Ballarat Road .. .. .	..		73 9 10	73 9 10
<b>COLAC SHIRE—</b>				
Colac-Ballarat Road .. .. .	..		3,096 5 2	
Colac-Beech Forest Road .. .. .	5,858 4 4		1,408 12 4	
Cressy-Inverleigh Road .. .. .	..		2,079 10 11	
Prince's Highway .. .. .	..		715 14 7	7,300 3 0
		5,858 4 4		
<b>CORIO SHIRE—</b>				
Ballarat Road .. .. .	..		2,680 14 11	
Fyansford Road .. .. .	..		1 0 3	
Geelong-Bacchus Marsh Road .. .. .	..		464 10 10	
Prince's Highway .. .. .	..		Bd. 196 4 9	3,342 10 9
<b>CRANBOURNE SHIRE—</b>				
Koo-wee-rup-Pakenham Road .. .. .	..		354 11 5	
Lang Lang-Nyora Road .. .. .	1,788 17 2		459 14 9	
Main Coast Road .. .. .	1,574 17 4		3,711 16 0	
Westernport Road .. .. .	79 19 8		407 12 11	4,933 15 1
		3,443 14 2		
<b>CRESWICK BOROUGH—</b>				
Castlemaine-Ballarat Road .. .. .	..		199 11 5	199 11 5
Carried forward .. .. .	..	64,023 1 6	..	112,069 9 5



STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	64,023 1 6	..	112,069 9 5
<b>CRESWICK SHIRE—</b>				
Castlemaine-Ballararat Road .. .. .	..		267 19 4	
Daylesford-Ballararat Road .. .. .	..		494 6 3	762 5 7
<b>DANDENONG SHIRE—</b>				
Cheltenham Road .. .. .	507 0 0		868 12 2	
Prince's Highway .. .. .	..		494 4 4	1,362 16 6
		507 0 0		
<b>DANDENONG AND CRANBOURNE SHIRES (Joint Works)—</b>				
Frankston-Dandenong Road .. .. .	..		2,419 6 3	2,419 6 3
<b>DAYLESFORD BOROUGH—</b>				
Ballan Road .. .. .	..		8 13 9	
Ballarat Road .. .. .	..		154 1 1	
Hepburn-Daylesford Road .. .. .	..		364 13 9	
Malmesbury-Daylesford Road .. .. .	..		201 19 11	729 8 6
<b>DEAKIN SHIRE—</b>				
Echuca-Cornella Road .. .. .	..		41 11 6	
Echuca-Picola Road .. .. .	..		170 14 9	
Kyabram-Nathalia Road .. .. .	..		1,981 18 8	
Kyabram-Tongala Road .. .. .	..		1,072 19 2	
Rochester-Kyabram Road .. .. .	..		1,173 12 8	4,440 16 9
<b>DEAKIN AND NUMURKAH SHIRES (Joint Works)—</b>				
Echuca-Picola Road .. .. .	..		61 18 5	
Kyabram-Nathalia Road .. .. .	..		12 3 7	74 2 0
<b>DEAKIN AND RODNEY SHIRES (Joint Works)—</b>				
Kyabram-Tongala Road .. .. .	..		0 0 11	
Rochester-Kyabram Road .. .. .	..		441 12 11	441 13 10
<b>DIMBOOLA SHIRE—</b>				
Hopetoun-Rainbow Road .. .. .	50 0 0		787 17 5	
Nhill Road .. .. .	..		1 1 7	
Rainbow Road .. .. .	2,607 7 5		1,157 0 5	
Rainbow Rises Road .. .. .	742 0 0		16 14 0	
Warracknabeal Road .. .. .	4,627 12 7		119 3 6	2,081 16 11
		8,027 0 0		
<b>DONALD SHIRE—</b>				
Donald-Charlton Road .. .. .	1,349 2 9		128 17 7	
St. Arnaud-Birchip Road .. .. .	..		523 8 4	652 5 11
		1,349 2 9		
<b>DONCASTER AND TEMPLESTOWE SHIRE—</b>				
Doncaster Road .. .. .	..		2,284 5 7	
Heidelberg-Warrandyte Road .. .. .	..		1,741 18 3	
Warrandyte-Ringwood Road .. .. .	..		238 7 7	4,264 11 5
<b>DUNDAS SHIRE—</b>				
Hamilton-Dunkeld Road .. .. .	3,150 13 3		1,700 12 0	
Hamilton-Horsham Road .. .. .	190 15 0		1,978 6 1	
Hamilton-Mount Gambier Road .. .. .	174 11 6		1,476 14 5	
Hamilton-Port Fairy Road .. .. .	..		2,054 8 8	
Hamilton-Portland Road .. .. .	63 15 0		432 10 5	
Hamilton-Warrnambool Road .. .. .	..		1,295 4 2	8,967 15 9
		3,579 14 9		
<b>DUNDAS AND PORTLAND SHIRES (Joint Works)—</b>				
Hamilton-Portland Road .. .. .	42 7 6		..	
		42 7 6		
<b>DUNMUNKLE SHIRE—</b>				
Minyip-Donald Road .. .. .	1,590 5 8		2 7 6	
Rupanyup-Murtoa Road .. .. .	503 6 5		224 2 9	
Stawell-Warracknabeal Road .. .. .	2,928 0 5		143 14 6	370 4 9
		5,021 12 6		
<b>EAST LODDON SHIRE—</b>				
Dingee Road .. .. .	382 2 0			
Mitiamo Road .. .. .	860 3 0			
		1,242 5 0		
<b>ECHUCA BOROUGH—</b>				
Echuca-Cornella Road .. .. .	300 0 0		67 10 0	
Echuca West Road .. .. .	1 15 10		68 9 6	135 19 6
		301 15 10		
<b>ELTHAM SHIRE—</b>				
Eltham-Yarra Glen Road .. .. .	3,211 8 4		3,192 6 3	
Hurstbridge-Kinglake Road .. .. .	891 19 2		987 15 8	4,180 1 11
		4,103 7 6		
<b>ELTHAM AND LILYDALE SHIRES (Joint Works)—</b>				
Eltham-Yarraglen Road .. .. .	..		0 4 5	0 4 5
Carried forward .. .. .	..	88,197 7 4	..	142,952 19 5

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ d.	£ s. d.
Brought forward .. .. .	..	88,197 7 4	..	142,952 19 5
<b>EUROA SHIRE—</b>				
Arcadia Road .. .. .	..	..	43 5 1	
Euroa—Arcadia Road .. .. .	..	..	70 4 8	
Euroa—Mansfield Road .. .. .	1,118 7 8		310 13 2	
Euroa—Strathbogie Road .. .. .	125 16 2		285 1 7	
Murchison—Shepparton Road .. .. .	591 5 0		245 13 6	
Sydney Road .. .. .	..	..	2,160 16 5	
		1,835 8 10		3,115 14 5
<b>FERNTREE GULLY SHIRE—</b>				
Belgrave—Emerald Road .. .. .	..	..	1,739 1 9	
Emerald Road .. .. .	..	..	572 17 10	
Main Ferntree Gully Road .. .. .	205 0 0		5,253 13 9	
Monbulk Road .. .. .	..	..	2,021 12 10	
Olinda Road .. .. .	22 16 1		3,419 11 3	
		227 16 1		13,006 17 5
<b>FLINDERS SHIRE—</b>				
Dromana Road .. .. .	..	..	237 15 0	
Dromana—Sorrento Road .. .. .	6,026 17 5		1,489 3 0	
Hastings—Flinders Road .. .. .	1 10 0		896 19 3	
Mornington—Flinders Road .. .. .	2 9 8		799 16 4	
Stony Point Road .. .. .	..	..	165 7 3	
		6,030 17 1		3,589 0 10
<b>FOOTSCRAY CITY—</b>				
Prince's Highway .. .. .	..	..	Bd. 315 19 9	
				315 19 9
<b>FRANKSTON AND HASTINGS SHIRE—</b>				
Frankston—Dandenong Road .. .. .	..	..	1,423 19 3	
Frankston—Flinders Road .. .. .	901 11 5		1,609 7 6	
Point Nepean Road .. .. .	194 8 11		5,824 2 1	
		1,096 0 4		8,857 8 10
<b>FRANKSTON AND HASTINGS AND CRANBOURNE SHIRES (Joint Works)—</b>				
Frankston—Dandenong Road .. .. .	..	..	1,811 8 9	
				1,811 8 9
<b>GISBORNE SHIRE—</b>				
Bacchus Marsh Road .. .. .	..	..	119 4 7	
Gisborne Station Road .. .. .	..	..	96 13 8	
Melbourne—Bendigo Road .. .. .	..	..	200 13 2	
				416 11 5
<b>GLENELG SHIRE—</b>				
Casterton—Mount Gambier Road .. .. .	261 8 7		..	
Coleraine—Casterton Road .. .. .	..	..	632 4 5	
Dergholm Road .. .. .	340 0 11		1,133 13 0	
Mount Gambier Road .. .. .	732 11 2		1,032 0 9	
Portland—Casterton Road .. .. .	..	..	1,267 6 5	
Wando Vale Road.. .. .	..	..	379 17 5	
		1,334 0 8		4,445 2 0
<b>GLENLYON SHIRE—</b>				
Ballan Road .. .. .	..	..	39 6 6	
Ballarat Road .. .. .	..	..	123 8 10	
Castlemaine—Daylesford Road .. .. .	..	..	37 3 2	
Hepburn—Daylesford Road .. .. .	..	..	349 5 1	
Malmsbury—Daylesford Road .. .. .	..	..	687 11 3	
				1,236 14 10
<b>GOULBURN SHIRE—</b>				
Avenel—Longwood Road .. .. .	1,267 10 0		306 19 0	
Goulburn Valley Road .. .. .	892 2 0		181 17 6	
Murchison—Shepparton Road .. .. .	..	..	112 2 6	
Vickers-road .. .. .	..	..	1 15 0	
		2,159 12 0		602 14 0
<b>GRENVILLE SHIRE—</b>				
Ballarat—Hamilton Road .. .. .	3 8 6		1,893 10 4	
Cressy Road .. .. .	742 19 3		92 3 9	
Lismore Road .. .. .	..	..	255 7 3	
Pitfield Road .. .. .	..	..	221 14 10	
		746 7 9		2,462 16 2
<b>HAMILTON BOROUGH—</b>				
Ararat Road .. .. .	..	..	22 12 8	
Coleraine Road .. .. .	..	..	271 11 3	
Port Fairy Road .. .. .	..	..	126 9 9	
Portland .. .. .	..	..	4 10 9	
				425 4 5
<b>HAMILTON BOROUGH AND DUNDAS SHIRE (Joint Works)—</b>				
Hamilton—Warnambool Road .. .. .	..	..	165 18 3	
				165 18 3
<b>HAMPDEN SHIRE—</b>				
Camperdown—Ballarat Road .. .. .	..	..	3,862 14 0	
Caramut—Lismore Road .. .. .	..	..	644 2 4	
Lismore—Cressy Road .. .. .	..	..	1,734 8 10	
Prince's Highway .. .. .	..	..	336 0 8	
Terang—Mortlake Road .. .. .	..	..	3,866 15 2	
				10,444 1 0
Carried forward .. .. .	..	101,627 10 1	..	193,848 11 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	101,627 10 1	..	193,848 11 6
<b>HEALESVILLE SHIRE—</b>				
Healesville—Alexandra Road .. .. .	78 19 6		1,941 14 6	
Healesville—Woori Yallock Road .. .. .	..		74 6 8	
Marysville Road .. .. .	2,072 7 0		105 14 1	
		2,151 6 6		2,121 15 3
<b>HEALESVILLE AND LILYDALE SHIRES (Joint Works)—</b>				
Healesville—Alexandra Road .. .. .	..		0 1 3	
				0 1 3
<b>HEIDELBERG SHIRE—</b>				
Greensborough—Hurstbridge Road .. .. .	..		5,545 3 2	
Heidelberg—Warrandyte Road .. .. .	..		41 6 10	
Main Heidelberg—Eltham Road .. .. .	..		1,336 7 1	
Main Whittlesea Road .. .. .	..		555 3 8	
				7,478 0 9
<b>HEYTSBURY SHIRE—</b>				
Camperdown—Cobden Road .. .. .	..		699 15 7	
Cobden—Port Campbell—Prinetown Road .. .. .	1,247 0 2		3,358 6 6	
Prince's Highway .. .. .	..		0 10 4	
Timboon—Port Campbell Road .. .. .	..		3 14 4	
		1,247 0 2		4,062 6 9
<b>HORSHAM BOROUGH</b>				
Dimboola—Horsham Road .. .. .	..		12 19 0	
Dooen Road .. .. .	..		1,009 8 4	
Hamilton Road .. .. .	..		132 9 5	
Natimuk Road .. .. .	1,002 17 9		91 1 4	
		1,002 17 9		1,245 18 1
<b>HUNTLY SHIRE—</b>				
Bendigo—Echuca Road .. .. .	..		1,759 19 11	
				1,759 19 11
<b>INGLEWOOD BOROUGH—</b>				
Bendigo—Charlton Road .. .. .	..		165 8 9	
				165 8 9
<b>KARA KARA SHIRE—</b>				
Avoca—St. Arnaud Road .. .. .	712 16 9		377 13 2	
Charlton Road .. .. .	5,416 9 2		48 3 6	
Navarre Road .. .. .	8 13 0		882 11 10	
St. Arnaud—Donald Road .. .. .	10,806 8 9		918 18 3	
		16,944 7 8		2,227 6 9
<b>KARKAROOC SHIRE—</b>				
Hopetoun—Rainbow Road .. .. .	1,312 13 10		354 14 1	
Hopetoun—Warraeknabeal Road .. .. .	499 13 7		579 12 1	
Hopetoun—Woomelang—Sea Lake Road .. .. .	26 4 7		528 19 5	
Rainbow—Beulah—Birchip Road .. .. .	1,880 6 10		1,907 4 10	
		3,718 18 10		3,370 10 5
<b>KEILOR SHIRE—</b>				
Lancefield Road .. .. .	..		1,067 0 9	
Melbourne—Bendigo Road .. .. .	..		..	
				1,067 0 9
<b>KERANG SHIRE—</b>				
Koondrook Road .. .. .	..		95 4 6	
				95 4 6
<b>KILMORE SHIRE—</b>				
Heathcote Road .. .. .	..		137 14 2	
Sydney Road .. .. .	..		408 15 6	
				546 9 8
<b>KOROIT BOROUGH—</b>				
Koroit—Warrnambool Road .. .. .	..		2,247 0 4	
				2,247 0 4
<b>KORONG SHIRE—</b>				
Borong—Hurstwood Road .. .. .	8 5 0		565 19 1	
Charlton—Bendigo Road .. .. .	..		339 8 1	
Serpentine Road .. .. .	18 7 8		857 18 7	
		26 12 8		1,763 5 9
<b>KORONG AND CHARLTON SHIRES (Joint Works)—</b>				
Charlton—Bendigo .. .. .	..		1 3 10	
				1 3 10
<b>KORUMBURRA SHIRE—</b>				
Bena—Poowong Road .. .. .	536 6 3		137 4 1	
Korumburra—Drouin Road .. .. .	..		45 0 3	
Korumburra—Leongatha Road .. .. .	..		1,818 8 6	
Korumburra—Warragul Road .. .. .	1,794 3 5		702 18 11	
Korumburra—Wonthaggi Road .. .. .	1,308 10 11		2,339 14 0	
Lang Lang—Nyora Road .. .. .	1 3 0		101 16 3	
Loch—Wonthaggi Road .. .. .	1 5 0		34 11 0	
Nyora—Poowong Road .. .. .	..		394 2 7	
		3,641 8 7		5,573 15 7
<b>KORUMBURRA AND WARRAGUL SHIRES (Joint Works)—</b>				
Korumburra—Warragul Road .. .. .	..		0 7 11	
				0 7 11
Carried forward .. .. .	..	130,360 2 3	..	227,574 7 9

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued*.

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	130,360 2 3	..	227,574 7 9
<b>KOWREE SHIRE—</b>				
Booroopki Road .. .. .	..	..	1,319 0 10	..
Edenhope-Goroke Road .. .. .	1,521 15 7	..	1,493 16 8	..
Edenhope-Natimuk Road .. .. .	..	..	..	..
Hamilton-Edenhope-Apsley Road .. .. .	186 5 0	..	2,126 2 1	..
Horsham-Hamilton Road .. .. .	..	..	9 12 9	..
		1,708 0 7		4,948 12 4
<b>KYNETON SHIRE—</b>				
Daylesford Road .. .. .	..	..	0 9 0	..
Melbourne-Bendigo Road .. .. .	..	..	855 5 2	..
Redesdale Road .. .. .	..	..	231 5 7	..
Trentham Road .. .. .	..	..	597 16 3	..
Tylden-Woodend Road .. .. .	..	..	54 18 8	..
				1,739 14 8
<b>LAWLOIT SHIRE—</b>				
Broughton Road .. .. .	..	..	520 14 7	..
Nhill-Kaniva-Border Road .. .. .	..	..	103 8 10	..
South Lillimur Road .. .. .	3,108 11 2	..	541 2 10	..
Yearinga Road .. .. .	240 0 0	..	506 2 4	..
		3,348 11 2		1,671 8 7
<b>LEIGH SHIRE—</b>				
Ballarat-Rokewood Road .. .. .	..	..	341 11 0	..
Cressy-Inverleigh Road .. .. .	..	..	1,009 10 0	..
Cressy-Rokewood Road .. .. .	..	..	232 19 1	..
Inverleigh-Shelford Road .. .. .	..	..	16 14 10	..
Rokewood-Shelford Road .. .. .	..	..	698 10 8	..
Shelford-Bannockburn Road .. .. .	..	..	162 10 6	..
Werneth Road .. .. .	714 16 11	..	68 5 9	..
		714 16 11		2,530 1 10
<b>LEIGH AND COLAC SHIRES (Joint Works)—</b>				
Cressy-Lismore Road .. .. .	..	..	46 14 1	..
Cressy-Rokewood Road .. .. .	..	..	33 5 9	..
Cressy-Shelford Road .. .. .	..	..	37 0 8	..
				117 0 6
<b>LEXTON SHIRE—</b>				
Avoca-Ararat Road .. .. .	..	..	406 8 7	..
Avoca-Ballarat Road .. .. .	..	..	618 17 1	..
				1,025 5 8
<b>LILYDALE SHIRE—</b>				
Evelyn-Lilydale Road .. .. .	1,445 0 10	..	71 14 10	..
Main Healesville Road .. .. .	13 19 6	..	3,504 12 6	..
Main Warburton Road .. .. .	..	..	1,037 4 0	..
Monbulk Road .. .. .	275 13 1	..	970 16 7	..
Mount Dandenong Road .. .. .	5,521 12 8	..	1,407 2 1	..
Yarraglen Road .. .. .	350 13 8	..	386 9 3	..
		7,606 19 9		7,377 19 3
<b>LILYDALE AND UPPER YARRA SHIRES (Joint Works)—</b>				
Main Warburton Road .. .. .	5,928 3 2	..	..	..
		5,928 3 2		
<b>LOWAN SHIRE—</b>				
Dimboola-Kaniva Road .. .. .	..	..	332 1 6	..
Goroke Road .. .. .	19 12 5	..	164 9 4	..
Lorquon West Road .. .. .	1,764 4 1	..	2,288 2 7	..
Yanac Road .. .. .	26 6 11	..	758 14 5	..
		1,810 3 5		3,543 7 10
<b>MAFFRA SHIRE—</b>				
Briagalong-Dargo Road .. .. .	..	..	8 0 10	..
Bushy Park-Valencia Creek Road .. .. .	..	..	284 19 0	..
Licola Road .. .. .	1,703 6 7	..	746 12 5	..
Maffra-Sale Road .. .. .	..	..	1,724 13 0	..
Tinamba-Boisdale Road .. .. .	..	..	3,102 1 0	..
Tinamba-Newry Road .. .. .	586 5 3	..	100 2 8	..
		2,289 11 10		5,966 8 11
<b>MALDON SHIRE—</b>				
Baringhup Road .. .. .	..	..	1,393 9 2	..
Castlemaine-Maldon Road .. .. .	..	..	389 16 8	..
Castlemaine-Newstead Road .. .. .	..	..	19 19 0	..
Maldon-Eddington Road .. .. .	..	..	612 3 1	..
Newstead Road .. .. .	..	..	124 4 8	..
				2,539 12 7
<b>MALDON, MARONG, AND TULLAROOP SHIRES (Joint Works)—</b>				
Maldon-Eddington Road .. .. .	120 4 4	..	..	..
		120 4 4		
<b>MANSFIELD SHIRE—</b>				
Euroa-Merton Road .. .. .	..	..	96 18 6	..
Jamieson Road .. .. .	..	..	58 7 8	..
Mansfield-Tolmie Road .. .. .	106 9 0	..	236 13 6	..
Mansfield Road .. .. .	2,552 10 0	..	2,342 2 3	..
Mansfield-Wood's Point Road .. .. .	..	..	Bd.2,348 0 5	..
Mansfield-Wood's Point Road .. .. .	..	..	1,406 5 3	..
		2,658 19 0		6,488 7 7
Carried forward .. .. .		156,545 12 5		265,522 7 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	156,545 12 5	..	265,522 7 6
<b>MARONG SHIRE—</b>				
Bendigo-Bridgewater Road .. .. .	..	..	27 16 5	..
Bendigo-Eddington Road .. .. .	1,171 18 7	..	1,513 2 2	..
Bendigo-Serpentine Road .. .. .	..	..	477 18 9	..
Maldon-Edlington Road .. .. .	..	..	..	..
Melbourne-Bendigo Road .. .. .	..	..	1 5 3	..
		1,171 18 7		2,020 2 7
<b>MARYBOROUGH BOROUGH—</b>				
Ballarat Road .. .. .	..	..	39 0 9	..
Castlemaine Road .. .. .	..	..	58 17 8	..
				97-18 5
<b>MELTON SHIRE—</b>				
Toolern Road .. .. .	..	..	271 14 9	..
				271 14 9
<b>METCALFE SHIRE—</b>				
Kyneton-Redesdale Road .. .. .	..	..	1,083 1 3	..
Melbourne-Bendigo Road .. .. .	..	..	4 9 3	..
				1,087 10 6
<b>MILDURA TOWN—</b>				
Deakin Avenue .. .. .	..	..	243 3 2	..
Langtree Avenue .. .. .	..	..	114 3 8	..
Punt Road .. .. .	527 8 4	..	..	..
Tenth Street .. .. .	..	..	61 3 2	..
		527 8 4		418 10 0
<b>MILDURA SHIRE—</b>				
Deakin Avenue .. .. .	..	..	195 13 1	..
Irymple Road .. .. .	1 1 0	..	572 12 2	..
Melbourne Road .. .. .	..	..	251 3 0	..
Wentworth Road .. .. .	465 7 11	..	686 5 6	..
		466 8 11		1,705 13 9
<b>MINHAMITE SHIRE—</b>				
Hamilton-Macarthur-Port Fairy Road .. .. .	..	..	1,536 15 1	..
Warrnambool-Hawkesdale-Penshurst Road .. .. .	..	..	2,036 19 1	..
				3,573 14 2
<b>MIRBOO SHIRE—</b>				
Foster-Boolaroo Road .. .. .	..	..	..	..
Leongatha-Mirboo Road .. .. .	..	..	32 15 7	..
Mardan Road .. .. .	1,384 11 9	..	59 7 1	..
Mirboo-Allambee East Road .. .. .	..	..	58 17 5	..
Mirboo South Road .. .. .	..	..	149 14 10	..
		1,384 11 9		300 14 11
<b>MOORABBIN SHIRE—</b>				
Centre Dandenong Road .. .. .	..	..	2,232 11 7	..
Point Nepean Road .. .. .	1,951 17 10	..	1,596 15 9	..
		1,951 17 10		3,829 7 4
<b>MORDIALLOC TOWN—</b>				
Point Nepean Road .. .. .	..	..	5 19 6	..
				5 19 6
<b>MORNINGTON SHIRE—</b>				
Point Nepean Road .. .. .	126 4 2	..	2,821 13 5	..
		126 4 2		2,821 13 5
<b>MORTLAKE SHIRE—</b>				
Caramut-Lismore Road .. .. .	..	..	2,610 15 4	..
Mortlake-Ararat Road .. .. .	..	..	1,233 12 1	..
Mortlake-Warrnambool Road .. .. .	..	..	790 4 4	..
Terang-Mortlake Road .. .. .	..	..	1,716 13 3	..
				6,351 5 0
<b>MORWELL SHIRE—</b>				
Boolarra-Foster Road .. .. .	..	..	463 3 1	..
Boolarra-Welshpool Road .. .. .	345 14 0	..	418 7 7	..
Jeeralang West Road .. .. .	504 12 7	..	188 7 1	..
Prince's Highway .. .. .	..	..	661 7 2	..
		850 6 7		1,731 4 11
<b>MORWELL AND TRARALGON SHIRES (Joint Works)—</b>				
Jeeralang West Road .. .. .	..	..	5 0 6	..
				5 0 6
<b>MOUNT ROUSE SHIRE—</b>				
Ballarat-Hamilton Road .. .. .	..	..	1,245 16 3	..
Hamilton-Dunkeld Road .. .. .	..	..	689 13 1	..
Hamilton-Penshurst Road .. .. .	..	..	1,346 10 2	..
Maroona-Glenhompson Road .. .. .	..	..	237 19 9	..
Penshurst-Caramut Road .. .. .	..	..	1,136 7 1	..
				4,656 6 4
<b>MULGRAVE SHIRE—</b>				
Ferntree Gully Road .. .. .	3,226 12 4	..	2,355 3 8	..
Prince's Highway .. .. .	..	..	12 12 2	..
		3,226 12 4		2,367 15 10
<b>MCIVOR SHIRE—</b>				
Heathcote-Elmore Road .. .. .	271 14 6	..	500 9 6	..
Heathcote-Redesdale Road .. .. .	..	..	525 14 7	..
Kilmore-Heathcote-Bendigo Road .. .. .	811 16 0	..	967 15 5	..
		1,083 10 6		1,993 19 6
Carried forward .. .. .	..	167,334 11 5	..	298,760 18 11

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	167,334 11 5	..	298,760 18 11
<b>NARRACAN SHIRE—</b>				
Allambee-Childers Road .. .. .	0 10 0		..	
Prince's Highway .. .. .	..		313 4 8	
Trafalgar-Willowgrove Road .. .. .	..		181 19 9	
Trafalgar-Thorpdale Road .. .. .	..		250 12 10	
Walhalla Road .. .. .	..		511 1 1	
Walhalla Road .. .. .	..		Bd. 837 7 8	
Yarragon-Shady Creek Road .. .. .	..		208 16 0	
Yarragon-Leongatha Road .. .. .	..		1 160 4 1	
		0 10 0		3,463 6 1
<b>NEWHAM AND WOODEND SHIRE—</b>				
Lancefield Road .. .. .	..		182 13 6	
Melbourne-Bendigo Road .. .. .	..		272 13 10	
Tylden Road .. .. .	806 9 4		70 2 0	
		806 9 4		525 9 4
<b>NEWHAM AND WOODEND AND KYNETON SHIRES (Joint Works)—</b>				
Tylden Road .. .. .	571 13 0		3 12 0	
		571 13 0		3 12 0
<b>NEWSTEAD AND MOUNT ALEXANDER SHIRE—</b>				
Castlemaine-Daylesford Road .. .. .	..		324 8 4	
Castlemaine-Maryborough Road .. .. .	..		324 5 11	
Creswick Road .. .. .	..		353 6 0	
Maldon Road .. .. .	..		17 11 9	
				1,019 12 0
<b>NUMURKAH SHIRE—</b>				
Echuca-Picola Road .. .. .	66 16 8		845 4 8	
Murray Valley Road .. .. .	560 0 0		83 6 11	
Nathalia-Kyabram Road .. .. .	340 10 6		462 15 11	
Nathalia-North Road .. .. .	..		60 3 7	
Nathalia-Picola Road .. .. .	..		300 0 0	
Numurkah-Nathalia Road .. .. .	10 10 0		315 11 5	
Numurkah-Tungamah Road .. .. .	..		124 8 0	
Shepparton-Numurkah-Cobram Road .. .. .	989 2 0		1,074 18 10	
		1,966 19 2		3,266 9 4
<b>NUMURKAH AND DEAKIN SHIRES (Joint Works)—</b>				
Echuca-Picola Road .. .. .	..		97 10 3	
				97 10 3
<b>OAKLEIGH TOWN—</b>				
Ferntree Gully Road .. .. .	..		176 8 8	
Prince's Highway .. .. .	..		319 14 3	
Prince's Highway (Experimental Section M1) .. .. .	..		261 15 11	
				757 18 10
<b>OMEQ SHIRE—</b>				
Benambra Road .. .. .	825 7 7		161 0 10	
		825 7 7		161 0 10
<b>ORBOST SHIRE—</b>				
Cann Valley Road .. .. .	2,578 1 5		904 15 11	
Genoa-Gipsy Point Road .. .. .	..		390 19 6	
Marlo Road .. .. .	19 0 0		339 3 0	
Prince's Highway .. .. .	..		770 19 11	
		2,597 1 5		2,405 18 4
<b>OTWAY SHIRE—</b>				
Beech Forest-Apollo Bay Road .. .. .	..		255 0 7	
Cobden-Port Campbell-Princetown Road .. .. .	..		33 0 2	
Forrest-Apollo Bay Road .. .. .	1 0 0		594 3 7	
Glenaire-Lavers Hill Road .. .. .	..		..	
Princetown Road .. .. .	..		0 8 4	
		1 0 0		882 12 8
<b>OXLEY SHIRE—</b>				
Bright Road .. .. .	2,716 8 5		954 11 9	
Greta-Glenrowan Road .. .. .	..		518 15 6	
Oxley Road .. .. .	..		464 10 2	
		2,716 8 5		1,937 17 5
<b>PHILLIP ISLAND AND WOOLAMAI SHIRE—</b>				
Almurta Road .. .. .	..		94 2 2	
Dalyston-Wonthaggi Road .. .. .	3,073 18 11		183 16 4	
Inverloch-Wonthaggi Road .. .. .	..		97 19 2	
Korumburra-Wonthaggi Road .. .. .	433 4 11		381 8 6	
Main Coast Road .. .. .	4,744 12 0		634 2 8	
Wonthaggi-Loch Road .. .. .	7,548 6 9		863 14 7	
		15,800 2 7		2,255 3 5
<b>PHILLIP ISLAND AND WOOLAMAI SHIRE AND WONTHAGGI BOROUGH (Joint Works)—</b>				
Loch-Wonthaggi Road .. .. .	..		480 1 2	
				480 1 2
<b>PORT FAIRY BOROUGH—</b>				
Hamilton Road .. .. .	..		28 9 6	
Prince's Highway (Portland) .. .. .	..		566 16 6	
Prince's Highway (Warrnambool) .. .. .	..		792 2 1	
				1,387 8 1
Carried forward .. .. .	..	192,620 2 11	..	317,404 18 8



STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	192,620 2 11	..	317,404 18 8
PORTLAND SHIRE—				
Heath Road .. .. .	..	..	188 14 9	..
Portland-Casterton Road .. .. .	240 2 9	..	121 14 3	..
Portland-Hamilton Road .. .. .	..	..	2,038 12 10	..
Portland-Nelson Road .. .. .	312 7 4	..	..	..
		552 10 1		2,349 1 10
PRESTON TOWN—				
Whittlesea Road .. .. .	..	..	522 5 6	522 5 6
PYALONG SHIRE—				
Kilmore-Heathcote-Bendigo Road .. .. .	..	..	210 5 6	210 5 6
QUEENSCLIFF BOROUGH—				
Geelong Road .. .. .	..	..	1,535 18 10	1,535 18 10
RINGWOOD BOROUGH—				
Mt. Dandenong Road .. .. .	..	..	380 7 2	..
Main Healesville Road .. .. .	..	..	1,172 13 0	..
Warrandyte Road .. .. .	8 15 0	..	103 6 3	..
		8 15 0		1,656 6 5
RINGWOOD BOROUGH AND DONCASTER AND TEMPLESTOWE SHIRE (Joint Works)—				
Warrandyte Road .. .. .	..	..	30 18 3	30 18 3
RIPON SHIRE—				
Ballarat-Ararat Road .. .. .	..	..	131 11 9	..
Ballarat-Hamilton Road .. .. .	..	..	540 6 1	..
Skipton Road .. .. .	..	..	869 0 10	1,540 18 8
RIPON AND ARARAT SHIRES (Joint Works)—				
Ballarat-Ararat Road .. .. .	..	..	1 18 10	1 18 10
RIPON AND HAMPDEN SHIRES (Joint Works)—				
Ballarat-Hamilton Road .. .. .	..	..	0 14 1	0 14 1
ROCHESTER SHIRE—				
Bendigo-Echuca Road .. .. .	..	..	622 16 11	..
Rochester-Bamawm-Prairie Road .. .. .	1,952 11 5	..	583 14 3	..
Timmering Road .. .. .	245 12 5	..	197 13 11	..
		2,198 3 10		1,404 5 1
RODNEY SHIRE—				
Kyabram-Nathalia Road .. .. .	..	..	0 3 1	..
Kyabram-Tongala Road .. .. .	..	..	255 6 1	..
Mooroopna-Undera Road .. .. .	6,244 11 9	..	116 12 9	..
Rochester-Kyabram Road .. .. .	..	..	..	..
Shepparton-Tatura Road .. .. .	722 17 0	..	644 2 9	..
Tatura-Byrneside-Kyabram Road .. .. .	1,136 11 4	..	692 8 5	..
Tatura-Murchison Road .. .. .	..	..	116 18 4	..
		8,104 0 1		1,825 11 5
RODNEY AND SHEPPARTON SHIRES (Joint Works)—				
Shepparton-Tatura Road .. .. .	..	..	319 15 11	319 15 11
ROMSEY SHIRE—				
Kilmore-Lancefield Road .. .. .	..	..	243 8 9	..
Melbourne-Lancefield Road .. .. .	..	..	779 3 3	..
Woodend-Lancefield Road .. .. .	..	..	330 7 2	1,352 19 2
ROMSEY AND KILMORE SHIRES (Joint Works)—				
Kilmore-Lancefield Road .. .. .	..	..	215 4 0	215 4 0
ROSEDALE SHIRE—				
Prince's Highway .. .. .	..	..	97 7 4	..
Sale-Yarram Road .. .. .	1,581 0 0	..	434 13 4	..
Seaspray Road .. .. .	2,412 0 0	..	571 6 0	..
Traralgon-Gormandale Road .. .. .	..	..	202 5 6	..
Willung Road .. .. .	..	..	61 4 0	..
		3,993 0 0		1,366 16 2
ROSEDALE AND ALBERTON SHIRES (Joint Works)—				
Carrajung-Gormandale Road .. .. .	..	..	6 17 5	..
Prospect Road .. .. .	5 10 0	..	..	6 17 5
		5 10 0		..
RUTHERGLEN SHIRE—				
Chiltern-Howlong Road .. .. .	0 18 11	..	153 3 4	..
Rutherglen-Wahgunyah Road .. .. .	..	..	748 2 0	..
Springhurst-Rutherglen Road .. .. .	..	..	271 12 6	..
Sydney Road .. .. .	..	..	0 0 5	..
Wodonga Road .. .. .	..	..	462 7 0	..
Yarrowonga Road .. .. .	..	..	509 17 8	..
		0 18 11		2,145 2 11
RUTHERGLEN AND WANGARATTA SHIRES (Joint Works)—				
Yarrowonga Road .. .. .	..	..	0 10 4	0 10 4
Carried forward .. .. .	..	207,483 0 10	..	333,890 9 0

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	207,483 0 10	..	333,890 9 0
<b>SALE TOWN—</b>				
Prince's Highway .. .. .	..		46 11 8	
Sale-Longford Road .. .. .	..		700 15 9	
				747 7 5
<b>SEYMOUR SHIRE—</b>				
Avenel-Longwood Road .. .. .	944 12 6		30 10 2	
Goulburn Valley Road .. .. .	222 16 0		103 5 1	
Sydney Road .. .. .	..		2 14 9	
Upper Goulburn Road .. .. .	582 3 5		188 1 7	
		1,749 11 11		324 11 7
<b>SEYMOUR AND BROADFORD SHIRES (Joint Works)—</b>				
Upper Goulburn Road .. .. .	..		38 5 4	
				38 5 4
<b>SHEPPARTON SHIRE—</b>				
Dookie-Nalinga Road .. .. .	847 18 1		327 15 9	
Shepparton-Mooroopna Road .. .. .	..		14 18 6	
Shepparton-Nagambie Road .. .. .	1,481 0 4		600 9 11	
Shepparton-Nalinga Road .. .. .	1,865 12 6		414 8 1	
Shepparton-Numurkah Road .. .. .	2,115 5 11		303 9 10	
		6,309 16 10		1,661 2 1
<b>SHEPPARTON AND RODNEY SHIRES (Joint Works)—</b>				
Shepparton-Mooroopna Road .. .. .	..		239 8 4	
				239 8 4
<b>SOUTH BARWON SHIRE—</b>				
Barwon Bridge Road .. .. .	13,882 5 8		..	
Barwon Heads Road .. .. .	..		3,614 0 10	
Prince's Highway .. .. .	..		819 17 9	
		13,882 5 8		4,433 18 7
<b>SOUTH BARWON AND BELLARINE SHIRES (Joint Works)—</b>				
Barwon Heads Bridge .. .. .	5,775 16 11		..	
		5,775 16 11		
<b>SOUTH GIPPSLAND SHIRE—</b>				
Boolarra-Foster Road .. .. .	878 15 2		117 11 3	
Boolarra-Welshpool Road .. .. .	..		247 0 0	
Falls Road .. .. .	2,410 9 3		34 19 5	
Foster-Yarram Road .. .. .	2,227 11 6		881 5 4	
Stony Creek-Dollar Road .. .. .	3,142 9 11		59 11 3	
Toora-Gunyah Road .. .. .	..		97 12 9	
		8,659 5 10		1,438 0 0
<b>ST. ARNAUD BOROUGH—</b>				
Avoca-St. Arnaud Road .. .. .	..		833 18 8	
Charlton Road .. .. .	..		43 2 5	
St. Arnaud-Donald Road .. .. .	..		3,604 1 9	
				4,481 2 10
<b>STAWELL BOROUGH—</b>				
Ararat-Stawell Road .. .. .	542 2 4		142 18 2	
Glenorchy Road .. .. .	677 1 2		..	
		1,219 3 6		142 18 2
<b>STAWELL SHIRE—</b>				
Ballarat-Stawell Road .. .. .	..		9 0 0	
Landsborough Road .. .. .	..		54 13 4	
Marnoo Road .. .. .	4,635 7 9		1,209 8 2	
Navarre Road .. .. .	98 4 5		307 11 5	
Stawell-Grampians Road .. .. .	..		598 5 2	
Stawell-Warracknabeal Road .. .. .	1,247 5 5		261 16 3	
		5,980 17 7		2,440 14 4
<b>STAWELL AND KARA KARA SHIRES (Joint Works)—</b>				
Navarre Road .. .. .	..		17 10 3	
				17 10 3
<b>STRATHFIELDSAYE SHIRE—</b>				
Heathcote-Bendigo Road .. .. .	..		433 8 4	
Mandurang Road .. .. .	..		353 6 0	
Strathfieldsaye Road .. .. .	..		243 8 9	
				1,030 3 1
<b>SWAN HILL SHIRE—</b>				
Euston Road .. .. .	2,932 5 7		1,334 15 7	
Murray River Valley Road .. .. .	1,138 5 5		..	
Nyah-Ouyen Road .. .. .	..		1,138 10 8	
Tooleybuc Road .. .. .	..		43 14 2	
Ultima Road .. .. .	1,652 10 2		794 10 0	
Ultima-Sealake Road .. .. .	..		128 2 4	
Swan Hill Road .. .. .	5,069 3 4		2,931 17 4	
		10,792 4 6		6,371 10 1
<b>TALBOT SHIRE—</b>				
Maryborough-Avoca Road .. .. .	..		2 10 0	
Maryborough-Ballarat Road .. .. .	..		341 10 9	
				344 0 9
<b>TAMBO SHIRE—</b>				
Bairnsdale-Bruthen Road .. .. .	..		556 3 0	
Bruthen-Omeo Road .. .. .	..		12 6 5	
Mossiface Road .. .. .	..		23 10 9	
Nowa Nowa-Buchan-Gelantipy Road .. .. .	1,266 4 6		1,892 6 11	
Prince's Highway .. .. .	..		652 4 9	
		1,266 4 6		3,136 11 10
Carried forward .. .. .	..	263,118 8 1	..	360,737 13 8

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	263,118 8 1	..	360,737 13 8
<b>TOWONG SHIRE—</b>				
Murray Valley Road .. .. .	..		965 19 10	
Omeo Road .. .. .	..		313 9 1	
				1,279 8 11
<b>TRARALGON SHIRE—</b>				
Callignee Road .. .. .	5,585 2 7		83 10 3	
Prince's Highway .. .. .	..		269 15 3	
Traralgon-Gormandale Road .. .. .	720 15 8		140 0 5	
Traralgon-Jeeralang Road .. .. .	..		476 5 9	
		6,305 18 3		969 11 8
<b>TULLAROOP SHIRE—</b>				
Avoca Road .. .. .	..		853 9 2	
Ballarat Road .. .. .	..		227 1 9	
Castlemaine-Maryborough Road .. .. .	27 13 9		1,051 8 1	
Eddington Road .. .. .	..		453 4 7	
Natte Yallock Road .. .. .	..		188 9 5	
		27 13 9		2,773 13 0
<b>TUNGAMAH SHIRE—</b>				
Cobram South Road .. .. .	300 0 0		..	
Cobram-Strathmerton Road .. .. .	..		67 10 8	
Numurkah-Tungamah-Wilby Road .. .. .	4,110 2 6		1,314 0 9	
St. James Road .. .. .	522 1 0		483 15 1	
Yarrowonga-Cobram Road .. .. .	1,776 0 6		247 7 6	
		6,708 4 0		2,112 14 0
<b>UPPER MURRAY SHIRE—</b>				
Corryong Road .. .. .	..		2,035 16 0	
Tintalra Road .. .. .	1,144 12 1		276 18 2	
		1,144 12 1		2,312 14 2
<b>UPPER YARRA SHIRE—</b>				
Don Road .. .. .	573 9 9		96 4 6	
Warburton Road .. .. .	405 14 7		3,667 9 10	
Wood's Point Road .. .. .	34 9 11		Bd. 1,528 17 8	
		1,013 14 3		5,292 12 0
<b>VIOLET TOWN SHIRE—</b>				
Shepparton Road .. .. .	2,989 2 3		44 2 0	
Sydney Road .. .. .	..		68 2 3	
Violet Town-Dookie Road .. .. .	4,966 16 5		395 8 3	
		7,955 18 8		507 12 6
<b>WALPEUP SHIRE—</b>				
Mildura Road .. .. .	..		90 0 0	
Ouyen-Pinnaroo Road .. .. .	2,597 7 2		1,055 7 4	
Sea Lake Road .. .. .	..		1 9 4	
		2,597 7 2		1,146 16 8
<b>WANGARATTA BOROUGH—</b>				
Beechworth Road .. .. .	..		222 19 9	
Sydney Road .. .. .	2,490 6 3		2,337 16 3	
		2,490 6 3		2,560 16 0
<b>WANGARATTA SHIRE—</b>				
Beechworth Road .. .. .	..		339 2 8	
Rutherger Road .. .. .	..		36 2 9	
Sydney Road .. .. .	..		0 2 2	
Yarrowonga Road.. .. .	..		217 15 2	
				593 2 9
<b>WANGARATTA AND BEECHWORTH SHIRES (Joint Works)—</b>				
Beechworth Road .. .. .	..		26 14 4	
				26 14 4
<b>WANNON SHIRE—</b>				
Coleraire-Harrow-Apsley Road .. .. .	..		1,236 15 9	
Hamilton-Coleraire-Apsley Road .. .. .	166 18 1		1,675 18 7	
Wannon Bridge .. .. .	..		588 6 4	
		166 18 1		3,501 0 8
<b>WANNON AND GLENELG SHIRES (Joint Works)—</b>				
Hamilton-Coleraire-Casterton Road.. .. .	..		1 3 7	
				1 3 7
<b>WARANGA SHIRE—</b>				
Colbinabbin-Corop Road .. .. .	..		101 6 9	
Colbinabbin-Moora Road .. .. .	..		1,723 10 3	
Elmore-Colbinabbin Road .. .. .	2,081 19 0		196 15 3	
Heathcote-Elmore Road .. .. .	..		3,186 18 2	
Murchison-Rushworth Road .. .. .	402 4 2		637 2 0	
Tatura Road .. .. .	..		52 8 0	
		2,484 3 2		5,898 0 5
<b>WARANGA AND HUNTLY SHIRES (Joint Works)—</b>				
Heathcote-Elmore Road .. .. .	..		68 7 4	
				68 7 4
<b>WARANGA AND GOULBURN SHIRES (Joint Works)—</b>				
Goulburn Valley Road and Murchison-Rushworth Road .. .. .	..		34 3 10	
				34 3 10
Carried forward .. .. .	..	294,013 3 9	..	389,816 5 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Work		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	294,013 3 9	..	389,816 5 6
<b>WARRAGUL SHIRE—</b>				
Bloomfield Road .. .. .	3,704 13 11		140 8 8	
Brandy Creek Road .. .. .	..		454 16 4	
Darnum-Allambee Road .. .. .	..		397 7 11	
Prince's Highway .. .. .	..		705 17 9	
Warragul-Korumburra Road .. .. .	4,747 5 10		958 16 9	
Warragul-Leongatha Road .. .. .	..		235 14 3	
		8,451 19 9		2,893 1 8
<b>WARRAGUL AND KORUMBURRA SHIRES (Joint Works)—</b>				
Warragul-Korumburra Road .. .. .	17 14 2		..	
		17 14 2		
<b>WARRNAMBOOL SHIRE—</b>				
Allansford-Nirranda Road .. .. .	..		5,745 2 0	
Garvoc-Laang Road .. .. .	..		585 8 11	
Caramut-Lismore Road .. .. .	..		75 0 0	
Prince's Highway (Geelong-Warrnambool Section) .. .. .	..		21 11 1	
Prince's Highway (Warrnambool-Port Fairy Section) .. .. .	..		10 14 11	
Mortlake Road .. .. .	..		5,794 14 5	
				12,232 11 4
<b>WERRIBEE SHIRE—</b>				
Geelong-Bacchus Marsh Road .. .. .	1,566 17 8		185 17 6	
Prince's Highway .. .. .	..		249 12 5	
Prince's Highway .. .. .	..		Bd. 6 17 6	
		1,566 17 8		442 7 5
<b>WHITTLESEA SHIRE—</b>				
Epping Road .. .. .	..		677 9 7	
Main Whittlesea Road .. .. .	1,800 10 0		1,969 1 4	
Wallan Road .. .. .	..		171 15 1	
Whittlesea-Kinglake Road .. .. .	4,461 11 0		750 8 10	
		6,262 1 0		3,568 14 10
<b>WIMMERA SHIRE—</b>				
Dimboola Road .. .. .	..		10 4 10	
Dcoen Road .. .. .	..		319 5 1	
Horsham-Wal Wal Road .. .. .	..		10 17 2	
Natimuk Road .. .. .	2,604 3 8		630 18 10	
		2,604 3 8		971 5 11
<b>WIMMERA AND ARAPILES SHIRES (Joint Works)—</b>				
Horsham-Hamilton Road .. .. .	54 0 7		1,023 19 7	
		54 0 7		1,023 19 7
<b>WIMMERA AND HORSHAM SHIRES (Joint Works)—</b>				
Horsham-Hamilton Road .. .. .	..		28 14 0	
				28 14 0
<b>WINCHELSEA SHIRE—</b>				
Birregurra-Forrest Road .. .. .	3,003 6 4		213 17 10	
Lorne Road .. .. .	5,622 6 9		1,581 15 10	
Prince's Highway .. .. .	..		1,359 3 5	
		8,625 13 1		3,154 17 1
<b>WODONGA SHIRE—</b>				
Murray Valley Road .. .. .	..		79 4 9	
Sydney Road .. .. .	..		164 2 3	
Tallangatta Road .. .. .	..		165 5 11	
Wodonga-Yackandandah Road .. .. .	239 13 0		57 1 11	
		239 13 0		465 14 10
<b>WONTHAGGI BOROUGH—</b>				
Loch-Wonthaggi Road .. .. .	..		817 19 9	
Wonthaggi-Inverloch Road .. .. .	..		228 19 3	
Wonthaggi-Korumburra Road .. .. .	898 3 4		22 16 9	
		898 3 4		1,069 15 9
<b>WOORAYL SHIRE—</b>				
Farmer's Road .. .. .	412 16 7		465 18 5	
Inverloch-Wonthaggi Road .. .. .	..		107 1 4	
Inverloch-Leongatha Road .. .. .	..		1,146 6 3	
Leongatha-Yarragon Road .. .. .	1,886 3 2		1,048 4 7	
Lower Tarwin Road .. .. .	2,086 2 10		709 6 9	
Main South Gippsland Road .. .. .	1,518 0 2		1,384 15 9	
Mardan Road .. .. .	18 7 4		440 1 11	
Warragul-Leongatha Road .. .. .	..		54 6 7	
Wild Dog Valley Road .. .. .	1,767 12 3		125 9 0	
		7,689 2 4		5,481 10 7
<b>WYCHEPROOF SHIRE—</b>				
Birchip-Sea Lake Road .. .. .	..		250 9 7	
Birchip-Wycheproof Road .. .. .	925 10 1		1,181 14 4	
Corack Road .. .. .	..		51 7 8	
Sea Lake-Ultima Road .. .. .	30 0 0		219 7 3	
Wycheproof-Sea Lake Road .. .. .	..		0 15 0	
Woomelang-Sea Lake Road .. .. .	..		142 6 1	
		955 10 1		1,845 19 11
<b>WYCHEPROOF AND SWAN HILL SHIRES (Joint Works)—</b>				
Sea Lake-Ultima Road .. .. .	100 11 8		..	
		100 11 8		
Carried forward .. .. .	..	£331,478 14 1	..	422,994 18 5

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	331,478 14 1	..	422,994 18 5
<b>YACKANDANDAH SHIRE—</b>				
Dederang Road .. .. .	4,393 16 9		314 19 6	
Gundowring Road .. .. .	3,938 16 8		684 2 9	
Yackandandah-Wodonga Road .. .. .	2 0 0		301 13 5	
		8,334 13 5		1,300 15 8
<b>YARRAWONGA SHIRE—</b>				
Tungamah-Wilby Road .. .. .	..		3 6 0	
Wangaratta-Yarrawonga Road .. .. .	5 5 0		344 11 8	
Yarrawonga-Cobram Road .. .. .	129 15 0		189 18 7	
Yarrawonga-Rutherglen Road .. .. .	..		38 14 8	
		135 0 0		576 10 11
<b>YARRAWONGA AND WANGARATTA SHIRES (Joint Works)—</b>				
Peechelba Bridge .. .. .	5,510 9 11		..	
		5,510 9 11		
<b>YEA SHIRE—</b>				
Upper Goulburn Road .. .. .	0 14 4		966 2 7	
Yea-Glenburn Road .. .. .	1,007 15 0		647 7 11	
		1,008 9 4		1,613 10 6
<b>YEA AND BROADFORD SHIRES (Joint Works)—</b>				
Upper Goulburn Road .. .. .	..		223 0 0	
				223 0 0
<b>Total .. .. .</b>	<b>..</b>	<b>£346,467 6 9</b>	<b>..</b>	<b>£426,708 15 6</b>

## STATE HIGHWAYS.

Prince's Highway West .. .. .	18,087 7 7	135,851 14 2
Prince's Highway East .. .. .	4,810 0 6	131,780 19 5
Western Highway .. .. .	6,635 16 8	89,076 7 6
North-Western Highway .. .. .	12,268 7 7	70,767 19 5
Northern Highway .. .. .	2,015 17 0	1,078 14 10
North-Eastern Highway .. .. .	1,844 15 8	60,153 16 6
Omeo Highway .. .. .	6,729 14 6	23,239 9 10
	52,391 19 6	511,949 1 8
	398,859 6 3	938,657 17 2

## APPENDIX E.

## COUNTRY ROADS BOARD.

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL  
ROADS FOR YEAR ENDED 30TH JUNE, 1927.

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
ALBERTON SHIRE—				
Albert River Road .. .. .			3,435 7 9	
Balook-Traralgon Road .. .. .			64 13 8	
Binginwarri-Welshpool Road .. .. .			2 0 0	
Blackwarry-Yarram Road .. .. .			82 14 0	
Carrajung-Balook Road .. .. .			1,428 10 2	
Carrajung-Gormandale Road .. .. .			2,893 0 8	
Devil's Pinch Road .. .. .				
Gelliondale Road .. .. .			968 12 3	
Jenkins Road .. .. .			77 0 0	
Lay's Road .. .. .			73 12 7	
Whitelaw's Track .. .. .			3,721 3 7	
Ridge Road .. .. .			15 16 7	
Merriman's Creek Road .. .. .			94 10 10	
				12,857 2 1
ALEXANDRA SHIRE—				
Terip Terip Road .. .. .			155 15 5	
				155 15 5
ARAPILES SHIRE—				
Miga Lake-Gymbowen Road .. .. .			652 8 5	
				652 8 5
AVOCA SHIRE—				
Maryborough-Natte Yallock Road .. .. .			538 19 9	
				538 19 9
AVON SHIRE—				
Bengworden Road .. .. .			2,981 8 0	
Bengworden-Clydebank Road .. .. .			32 17 9	
Dargo Road .. .. .			3,604 19 1	
				6,619 4 10
BACCHUS MARSH AND CORIO SHIRES (Joint Works)—				
Balliang Road .. .. .			0 4 2	
				0 4 2
BAIRNSDALE SHIRE—				
Bairnsdale-Bengworden Road .. .. .			39 0 6	
Bulumwaal-Taberabbera Road .. .. .			324 6 0	
Calulu-Boggy Creek Road .. .. .			540 9 0	
Glenaladale-Lindenow Road .. .. .			902 0 0	
Hodge Estate Road .. .. .			1,251 15 5	
Sarsfield-Water Holes Road .. .. .			3,918 5 0	
				6,975 15 11
BALLAN SHIRE—				
Back Settlement Road .. .. .			650 13 5	
Ballan-Egerton Road .. .. .			345 13 3	
Blakeville Road .. .. .			1,395 9 9	
Bungal Road .. .. .				
Bungeeltap Road .. .. .			423 6 6	
Daylesford Road .. .. .			797 9 0	
Moorarbool West Road .. .. .			1,635 0 0	
				5,247 11 11
BALLAN AND BUNINYONG SHIRES (Joint Works)—				
Boundary Road .. .. .			1,450 0 0	
				1,450 0 0
BANNOCKBURN SHIRE—				
Burnside Road .. .. .			28 14 10	
				28 14 10
BEECHWORTH SHIRE—				
Bowman's Forest Road .. .. .			206 19 7	
Hillsborough Road .. .. .			131 2 5	
Myrtleford-Yackandandah Road .. .. .			5 16 6	
Stanley Road .. .. .			1,371 15 5	
				1,715 13 11
BEECHWORTH AND WANGARATTA SHIRES (Joint Works)—				
Everton .. .. .			380 13 1	
				380 13 1
BEECHWORTH AND BRIGHT SHIRES (Joint Works)—				
Myrtleford-Yackandandah Road .. .. .			36 5 7	
				36 5 7
BELFAST SHIRE—				
Badham's Road .. .. .			1,030 0 0	
				1,030 0 0
Carried forward .. .. .				37,688 9 11



STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	..	..	37,688 9 11
<b>BENALLA SHIRE—</b>				
Benalla-Mansfield Road .. .. .	..	..	600 15 7	
Mollyullah-Tatong Road .. .. .	..	..	25 9 2	
Toombullup Road .. .. .	..	..	4,316 17 5	4,943 2 2
<b>BELLARINE SHIRE—</b>				
Barwon Heads-Ocean Grove Road .. .. .	224 1 2	..	..	
<b>BERWICK SHIRE—</b>		224 1 2		
Beaconsfield-Emerald Road .. .. .	..	..	675 9 7	
Bunyip-Tonimbuk East Road .. .. .	..	..	0 1 0	
Tynong-Tonimbuk Road .. .. .	..	..	20 18 5	
Upper Beaconsfield-Upper Pakenham Road .. .. .	..	..	268 7 9	
Cockatoo-Gembrook Road .. .. .	..	..	1 5 2	966 1 11
<b>BERWICK AND FERNTREE GULLY SHIRES (Joint Works)—</b>				
Reservoir Road .. .. .	..	..	9 7 2	9 7 2
<b>BRIGHT SHIRE—</b>				
Buffalo River Road .. .. .	..	..	748 10 2	
Happy Valley Road .. .. .	..	..	78 10 8	
Harrietville Road .. .. .	..	..	..	
Kiewa Valley Road .. .. .	..	..	2,047 18 11	
Myrleford-Yackandandah Road .. .. .	..	..	1,189 8 10	4,064 8 7
<b>BRIGHT AND OXLEY SHIRES (Joint Works)—</b>				
Buffalo River Road .. .. .	..	..	..	
<b>BIRCHIP SHIRE—</b>				
Morton Plains Road .. .. .	..	..	1,460 10 0	1,460 10 0
<b>BULLA SHIRE—</b>				
Konagadera Road .. .. .	..	..	1,275 2 10	
Riddell Road .. .. .	..	..	1,021 5 3	2,296 8 1
<b>BULLA AND ROMSEY SHIRES (Joint Works)—</b>				
Konagadera Road .. .. .	..	..	393 15 11	393 15 11
<b>BULLA AND BROADMEADOWS SHIRES (Joint Works)—</b>				
Konagadera Road .. .. .	..	..	1,734 6 2	1,734 6 2
<b>BULN BULN SHIRE—</b>				
Apteds Road .. .. .	..	..	0 10 0	
Buln Buln Road .. .. .	..	..	2 10 0	
Fumina Road .. .. .	..	..	46 1 0	
Lock Valley .. .. .	..	..	5 17 0	
Mountain View Road .. .. .	..	..	170 16 2	
Mountain View-McDonald's Track .. .. .	..	..	264 9 1	
Neerim South-Neerim East Road .. .. .	..	..	2,023 6 8	
Neerim North-Noojee Road .. .. .	..	..	4,472 2 5	
Poowong Road .. .. .	..	..	119 1 7	
Robin Hood Road .. .. .	..	..	290 17 1	
Rokey North-Jindivick Road .. .. .	..	..	74 5 7	
Torwood-Topiram Road .. .. .	..	..	2,248 11 1	
Turner's Road .. .. .	..	..	219 3 6	
Jindivick-Neerim South Road .. .. .	..	..	27 6 10	9,964 18 0
<b>BULN BULN AND WARRAGUL SHIRES (Joint Works)—</b>				
Lardner's Track .. .. .	..	..	16 0 0	16 0 0
<b>BUNGAREE SHIRE—</b>				
Bolworrah Road .. .. .	..	..	385 13 5	385 13 5
<b>BUNINYONG SHIRE—</b>				
Elaine-Mt. Mercer Road .. .. .	..	..	31 15 4	31 15 4
<b>BUNINYONG AND BANNOCKBURN SHIRES (Joint Works)—</b>				
Elaine-Mt. Mercer Road .. .. .	..	..	776 9 11	776 9 11
<b>BUNINYONG AND LEIGH SHIRES (Joint Works)—</b>				
Elaine-Mt. Mercer Road .. .. .	..	..	4 3 10	4 3 10
<b>CHARLTON SHIRE—</b>				
Glenloth Road .. .. .	..	..	1,351 17 9	
Lake Marmal .. .. .	..	..	1,745 8 5	
Teddywaddy Road .. .. .	..	..	410 16 6	
Wooroonook Road .. .. .	..	..	323 1 6	
Yeungroon Road .. .. .	..	..	365 0 0	4,196 4 2
<b>CHARLTON AND KORONG SHIRES (Joint Works)—</b>				
Buckrabanyule South Road .. .. .	..	..	8 10 0	8 10 0
Carried forward .. .. .	..	224 1 2	..	68,940 4 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	224 1 2	..	68,940 4 7
<b>COHUNA SHIRE—</b>				
Cohuna-Leitchville Road .. .. .	..	..	973 10 0	..
Cohuna-Mead Road .. .. .	..	..	98 9 2	..
Gunbower-Leitchville Road .. .. .	..	..	5 0 0	..
Gannawarra Road .. .. .	..	..	1,091 10 9	..
Keely's Lane .. .. .	..	..	357 5 4	..
Murray River Valley Road .. .. .	..	..	3,330 2 1	..
Cohuna-McMillan's Road .. .. .	..	..	389 12 10	6,245 10 2
<b>COLAC SHIRE—</b>				
Beeac-Dreeite Road .. .. .	..	..	38 15 8	..
Colac-Forrest Road .. .. .	..	..	744 8 6	..
Dreeite Road .. .. .	..	..	1,012 8 10	1,795 13 0
<b>CORIO SHIRE—</b>				
Cox Road .. .. .	..	..	336 1 11	..
Geelong-Bacchus Marsh Road .. .. .	..	..	3,470 15 6	..
Little River-Ripley Road .. .. .	..	..	15 9 11	..
Stacey's Road .. .. .	..	..	368 2 7	4,190 9 11
<b>CORIO AND BACCHUS MARSH SHIRES (Joint Works)—</b>				
Geelong-Bacchus Marsh Road .. .. .	..	..	267 19 4	267 19 4
<b>CRANBOURNE SHIRE—</b>				
Bullarto Road .. .. .	..	..	112 8 2	..
Dalmore Road .. .. .	..	..	1,293 3 7	..
Koo-wee-rup-Modella Road .. .. .	..	..	587 19 0	..
Manks Road .. .. .	..	..	71 5 5	..
Pearcedale Road .. .. .	..	..	1,714 18 0	3,779 14 2
<b>CRANBOURNE AND FRANKSTON AND HASTINGS SHIRES (Joint Works)—</b>				
Bullarto Road .. .. .	..	..	45 6 5	45 6 5
<b>DEAKIN SHIRE—</b>				
Echuca East Road .. .. .	..	..	12 14 4	..
Echuca-Wyuna Road .. .. .	..	..	264 7 7	..
Girgarre East Road .. .. .	..	..	350 3 7	..
Girgarre North Road .. .. .	..	..	388 0 2	..
Girgarre South Road .. .. .	..	..	256 4 11	..
Girgarre West Road .. .. .	..	..	339 2 9	..
Koyuga Road .. .. .	..	..	22 6 3	..
Tongala East Road .. .. .	..	..	2,021 13 6	..
Tongala North Road .. .. .	..	..	17 3 8	..
Tongala West Road .. .. .	..	..	2 6 7	..
Strathallan East Road .. .. .	..	..	413 5 1	4,087 8 5
<b>DEAKIN AND RODNEY SHIRES (Joint Works) -</b>				
Kyabram-Stanhope Road .. .. .	..	..	1,155 4 0	1,155 4 0
<b>DIMBOOLA SHIRE—</b>				
Albacutya Road .. .. .	..	..	491 14 5	..
Areona Road .. .. .	..	..	109 14 9	..
Depla-Hindmarsh Road .. .. .	..	..	1,579 9 8	..
Glenlee-Jeparit Road .. .. .	..	..	2,917 13 0	..
Hindmarsh-Rainbow Road .. .. .	..	..	1 11 6	5,100 3 4
<b>DONALD SHIRE—</b>				
Donald-Minyip Road .. .. .	..	..	25 0 0	..
Jeffcott Road .. .. .	..	..	1,789 6 0	..
Rich Avon Road .. .. .	..	..	30 0 0	..
Sheep Hills Road .. .. .	..	..	27 0 0	..
Watchem-Warracknabeal Road .. .. .	..	..	80 0 0	1,951 6 0
<b>DUNDAS SHIRE—</b>				
Kanawalla-Noske Shire .. .. .	..	..	0 9 9	..
Melville-Forest Road .. .. .	..	..	1,440 0 0	1,440 9 9
<b>DUNMUNKLE SHIRE—</b>				
Burrum Siding Road .. .. .	..	..	843 5 3	..
Lubeck West Road .. .. .	..	..	495 19 2	..
Minyip-Kewell Road .. .. .	..	..	1,468 7 6	..
Murtoa North Road .. .. .	..	..	1,047 2 7	..
Murtoa South Road .. .. .	..	..	1,811 12 4	..
Rupanyup-Marnoo Road .. .. .	..	..	507 8 9	6,173 15 7
<b>EAST LODDON SHIRE—</b>				
Prairie-Borong Road .. .. .	..	..	307 9 0	..
Tandarra Road .. .. .	..	..	318 0 0	625 9 0
Carried forward .. .. .	..	224 1 2	..	105,798 13 8

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.		Amount.	
	£	s. d.	£	s. d.
Brought forward .. .. .	..	224 1 2	..	105,798 13 8
<b>ELTHAM SHIRE—</b>				
Balee-Kangaroo Ground .. .. .	..	..	293 13 11	
Cottle's Bridge Road .. .. .	..	..	394 7 9	
Cottle's Bridge-Strathewan Road .. .. .	..	..	1,937 8 2	
Kinglake-Kinglake East Road .. .. .	..	..	64 17 9	
Tarrawarra Road .. .. .	..	..	0 14 10	
Whittlesea-Kinglake Road .. .. .	..	..	161 7 3	
Yarraglen-Kinglake East Road .. .. .	..	..	93 4 3	
				2,945 13 11
<b>ELTHAM AND YEA SHIRES (Joint Works)—</b>				
Kinglake-Kinglake East Road .. .. .	..	..	57 2 0	
				57 2 0
<b>EUROA SHIRE—</b>				
Longwood-Ruffy Road .. .. .	..	..	993 11 10	
Merton-Strathbogie Road .. .. .	..	..	..	
Strathbogie Road .. .. .	..	..	894 14 11	
				1,888 6 9
<b>FERNTREE GULLY SHIRE—</b>				
Belgrave-Emerald Road .. .. .	..	..	65 0 0	
Emerald-Macclesfield Road .. .. .	..	..	106 0 5	
Emerald-Monbulk Road .. .. .	..	..	31 4 0	
Reservoir Road .. .. .	..	..	135 0 0	
Sassafras Creek Road .. .. .	..	..	79 0 3	
The Patch Road .. .. .	..	..	1,814 10 2	
				2,230 14 10
<b>FLINDERS SHIRE—</b>				
Baldry Road .. .. .	..	..	8 10 0	
Bittern-Dromana Road .. .. .	..	..	248 19 2	
Brown's Road .. .. .	..	..	1,099 17 8	
Main Creek Road .. .. .	..	..	61 14 0	
Merrick's Road .. .. .	..	..	0 15 0	
				1,419 15 10
<b>FRANKSTON AND HASTINGS SHIRE—</b>				
Frankston-Dandenong Road .. .. .	..	..	13 15 4	
Hodgin's Road .. .. .	..	..	172 18 1	
Moorooduc-Somerville Road .. .. .	..	..	16 5 3	
Tyabb-Mornington Road .. .. .	..	..	18 0 9	
				220 19 5
<b>GLENELG SHIRE—</b>				
Degholm-Elderslie Road .. .. .	..	..	315 3 8	
Glenorchy Estate Road .. .. .	..	..	1,951 16 2	
Merino-Struan-Tahara Road .. .. .	..	..	537 8 6	
				2,804 8 4
<b>GLENLYON SHIRE—</b>				
Daylesford-Trentham Road .. .. .	..	..	1,082 13 5	
				1,082 13 5
<b>GORDON SHIRE—</b>				
Barraport West Road .. .. .	..	..	20 12 3	
Boort-Kerang Road .. .. .	..	..	1,225 14 0	
Durham Ox-Pyramid Hill Road .. .. .	..	..	207 0 0	
Gladfield-Pyramid Hill Road .. .. .	..	..	1,072 19 10	
				2,526 6 1
<b>GRENVILLE SHIRE—</b>				
Pittong Road .. .. .	..	..	1,010 0 10	
				1,010 0 10
<b>HAMPDEN SHIRE—</b>				
Ayersford Road .. .. .	..	..	2,661 11 8	
Berry Bank Road .. .. .	..	..	115 7 1	
Cundare-Duverney Road .. .. .	..	..	613 12 3	
Foxhow Road .. .. .	..	..	379 3 7	
Mt. Bute Road .. .. .	..	..	18 19 0	
Vite Vite .. .. .	..	..	1,492 13 10	
				5,281 7 5
<b>HEALESVILLE SHIRE—</b>				
Buxton-Marysville Road .. .. .	..	..	1,409 13 6	
Healesville-Toolangi Road .. .. .	..	..	384 5 11	
Healesville-Woori Yallock Road .. .. .	..	..	41 3 9	
Myers Creek .. .. .	..	..	17 13 7	
				1,852 16 9
<b>HEALESVILLE AND UPPER YARRA SHIRES (Joint Works)—</b>				
Healesville-Woori Yallock Road .. .. .	..	..	..	
Woori Yallock-Cockatoo Bridge .. .. .	..	..	5,088 2 3	
				5,088 2 3
<b>HEYTESBURY SHIRE—</b>				
Cobden-Port Campbell-Princetown .. .. .	..	..	231 4 9	
Cowley's Creek-Timboon Road .. .. .	..	..	173 17 6	
Timboon-Digney's Bridge Road .. .. .	..	..	172 0 0	
South Ecklin Road .. .. .	..	..	1,912 13 2	
Timboon-Nirranda Road .. .. .	..	..	661 5 11	
Timboon-Port Campbell Road .. .. .	..	..	1,001 19 7	
Timboon-Scott's Creek Road .. .. .	..	..	5,336 12 1	
				9,489 13 0
Carried forward .. .. .	..	224 1 2	..	143,696 14 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	224 1 2	..	143,696 14 6
HEYTESBURY AND WARRNAMBOOL SHIRES (Joint Works)—				
Ayersford .. .. .	..		2,093 0 0	2,093 0 0
HUNTLY SHIRE —				
Dromartin Road .. .. .	..		1,973 9 0	
Holmberg Road.. .. .	..		1,267 4 3	3,240 13 3
KARA KARA SHIRE—				
Coonoer Road .. .. .	..		700 0 0	
Marnoo—St. Arnaud Road .. .. .	..		2,093 19 6	
Sandy Creek Road .. .. .	..		416 0 3	
Swanwater Road .. .. .	..		1,888 12 10	5,098 12 7
KARKAROOC SHIRE—				
Hopetoun—Lascelles Road .. .. .	..		88 16 4	
Hopetoun—Yaapect Road .. .. .	..		659 16 8	
Patchewollock—Speed Road .. .. .	..		548 0 0	
Rosebery West Road .. .. .	..		32 15 11	
Rosebery East Road .. .. .	..		1,752 7 7	
Yaapect—Nypo Road .. .. .	..		26 6 1	3,108 2 7
KERANG SHIRE—				
Murrabit Road .. .. .	..		1,567 0 11	
Murrabit—Myall Road .. .. .	..		413 9 8	
Murrabit West Road .. .. .	..		292 19 3	
Murray River Valley Road .. .. .	..		503 7 7	
Myall Station Road .. .. .	..		2 10 0	2,779 7 5
KORONG SHIRE—				
Buckrabanyule South Road .. .. .	..		493 8 5	
Emu Logan Road .. .. .	..		976 9 3	
Kinypaniel Road .. .. .	..		474 4 2	
Kurting—Rheola Road .. .. .	..		362 17 4	
Mysia West Road .. .. .	..		25 11 11	
Mysia East Road .. .. .	..		465 1 0	
Nine-mile Road .. .. .	..		399 18 9	
Wedderburn—Spring Hill Road .. .. .	..		654 13 4	
Woolshed Road .. .. .	..		328 7 5	
Wyhitella North Road .. .. .	..		255 0 0	4,435 11 7
KORUMBURRA SHIRE—				
Arawata Road .. .. .	..		0 13 6	
Bena—Kongwak Road .. .. .	..		2 4 9	
Ferriers Road .. .. .	..		75 14 0	
Henry's Road .. .. .	..		2 19 0	
Korumburra South Road .. .. .	..		1,249 18 5	
Loch—Nyora Road .. .. .	..		121 0 9	
Poowong Estate Road .. .. .	..		3,367 11 11	
Poowong—Olsen Road .. .. .	..		0 6 3	
Poowong—Ranceby Road .. .. .	..		1 0 0	
Sheepway's Road .. .. .	..		17 4 6	
Timm's Road .. .. .	..		78 13 7	
Wild Dog Valley Road .. .. .	..		866 0 0	5,783 6 8
KOWREE SHIRE—				
Benayeo Road .. .. .	..		330 3 6	
Elderslie Road .. .. .	..		849 0 5	
Elderslie—Narracoorte Road .. .. .	..		128 16 11	
Miga Lake—Gymbowen Road .. .. .	..		1,099 2 11	
Little Desert Road .. .. .	..		35 0 0	2,442 3 9
KYNETON SHIRE—				
Lagoon Road .. .. .	..		296 14 0	296 14 0
LAWLOTT SHIRE—				
Cove Estate Settlement Road .. .. .	..		40 14 0	
Miram Road .. .. .	..		1,223 10 10	
Serviceton North Road .. .. .	..		7 0 10	
Serviceton South Road .. .. .	..		727 7 3	1,998 12 11
LEIGH SHIRE —				
Cressy—Meadows Road .. .. .	..		..	
Werneth Road .. .. .	..		0 7 6	0 7 6
LEIGH AND COLAC SHIRES (Joint Works)—				
Cressy—Sheiford Road .. .. .	..		1,910 4 6	1,910 4 6
LEIGH AND GRENVILLE SHIRES (Joint Works)—				
Gillett's Road .. .. .	..		61 1 4	61 1 4
Carried forward .. .. .	..	224 1 2	..	176,944 12 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road,	Acts. Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	224 1 2	..	176,944 12 7
<b>LILYDALE SHIRE—</b>				
Beenak-Seville Road .. .. .	..	..	6,321 2 11	
Olinda Creek Road .. .. .	..	..	19 0 0	
Wandin Road .. .. .	..	..	686 0 10	
York Road .. .. .	..	..	2,303 3 2	9,329 6 11
<b>LOWAN SHIRE—</b>				
Diapir-Yanac Road .. .. .	..	..	438 14 4	
Netherby Road .. .. .	..	..	1,110 5 7	
Winiam Road .. .. .	..	..	979 6 6	
Woorak Road .. .. .	..	..	0 18 3	
Yanac South Road .. .. .	..	..	47 7 3	
Yanac Station Road .. .. .	..	..	21 17 10	2,598 9 9
<b>MAFFRA SHIRE—</b>				
Bushy Park-Valencia Creek Road .. .. .	..	..	125 18 8	
Maffra-Newry Road .. .. .	..	..	1,143 2 11	1,269 1 7
<b>MANSFIELD SHIRE—</b>				
Benalla-Mansfield Road .. .. .	..	..	1,288 18 4	
Merton-Strathbogie Road .. .. .	..	..	204 11 2	
Tolmie Road .. .. .	..	..	1,230 17 0	2,724 6 6
<b>MARONG SHIRE—</b>				
Bendigo-Serpentine Road .. .. .	..	..	1,239 4 0	
Yarraberb Road .. .. .	..	..	278 19 5	1,518 3 5
<b>MELTON SHIRE—</b>				
Coburn's Road .. .. .	..	..	931 0 1	
Exford Road .. .. .	..	..	1,203 19 5	
Mt. Kororoit Road .. .. .	..	..	1,518 3 11	3,653 3 5
<b>MILDURA SHIRE—</b>				
Merbein West Road .. .. .	..	..	35 6 10	
Redcliff East Road .. .. .	..	..	1,115 8 11	
Redcliffs West Road .. .. .	..	..	73 1 6	
Redcliffs South East Road .. .. .	..	..	2,118 4 9	
Murray River Valley Road .. .. .	..	..	25 14 3	3,367 16 3
<b>MINHAMITE SHIRE—</b>				
Bessiebellie-Yambuk Road .. .. .	..	..	61 4 8	
Condah-Macarthur Road .. .. .	..	..	22 9 11	
Heywood Road .. .. .	..	..	228 5 8	
Lake Gorrie Road .. .. .	..	..	71 0 7	
Nardoo Road .. .. .	..	..	933 12 7	
Orford-St. Helen's Road .. .. .	..	..	146 16 2	
Woodland's Road .. .. .	..	..	40 5 8	1,503 15 3
<b>MIRBOO SHIRE—</b>				
Berry's Creek Road .. .. .	..	..	697 15 0	
Boolarra South-Mirboo Road .. .. .	..	..	48 8 0	
Boorool Road .. .. .	..	..	396 12 6	
Darlumula-Thorpdale Road .. .. .	..	..	238 16 9	
Leongatha-Mirboo Road .. .. .	..	..	36 5 11	
Mardan Road .. .. .	..	..	12 0 0	
Mirboo North-Thorpdale Road .. .. .	..	..	4,962 9 7	
Mirboo-Yarragon Road .. .. .	..	..	2,548 0 4	
Nichol's Road .. .. .	..	..	213 7 9	
Mt. Vernon Soldier Settlement Road .. .. .	..	..	589 17 7	9,743 13 5
<b>MORWELL SHIRE—</b>				
Boolarra-Morwell Road .. .. .	..	..	314 6 1	
Boolarra-Welshpool Road .. .. .	..	..	32 2 6	
Jeeralang West Road .. .. .	..	..	1 10 0	
Jumbuk Road .. .. .	..	..	436 17 1	
Middle Creek Road .. .. .	..	..	2,943 6 6	
Mirboo-Boolarra Road .. .. .	..	..	133 13 9	
Morwell River Road .. .. .	..	..	252 10 1	
Shelcot's Road .. .. .	..	..	102 9 9	
Tyer's Road .. .. .	..	..	1,077 3 3	
Hazelwood Estate Road .. .. .	..	..	245 0 0	5,538 19 0
<b>MORWELL AND TRARALGON SHIRES (Joint Works)—</b>				
Walker's Road .. .. .	..	..	38 5 7	38 5 7
Carried forward .. .. .	..	224 1 2	..	218,229 13 8

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.			
	Amount.		Total.			
	£	s. d.	£	s. d.		
Brought forward .. .. .	..	..	224	1 2	..	218,229 13 8
<b>NARRACAN SHIRE—</b>						
Allambee-Childers Road .. .. .	..	..	..	..	2,122	17 3
Canal Road .. .. .	..	..	..	..	65	14 0
Coalville-Moe Road .. .. .	..	..	..	..	1,188	19 8
Erica Road .. .. .	..	..	..	..	1,270	10 2
Latrobe River Connexion Road .. .. .	..	..	..	..	2	17 3
Mirboo-Yarragon Road .. .. .	..	..	..	..	2,315	19 7
Moe-Moondarra Road .. .. .	..	..	..	..	7	7 10
Narracan Connexion Road .. .. .	..	..	..	..	49	16 2
Nilma-Shady Creek Road .. .. .	..	..	..	..	23	1 8
Shady Creek Road .. .. .	..	..	..	..	4	2 7
Telbit Road .. .. .	..	..	..	..	2	10 0
Thorpdale East Road .. .. .	..	..	..	..	1,346	18 8
Thorpdale-Yarragon Road .. .. .	..	..	..	..	66	13 11
Trafalgar-Willowgrove Road .. .. .	..	..	..	..	363	2 3
Yarragon-Leongatha Road .. .. .	..	..	..	..	0	17 11
Darlimurla-Thorpdale Road .. .. .	..	..	..	..	2,181	18 6
Coalville-Narracan Road .. .. .	..	..	..	..	70	8 6
Platina Road .. .. .	..	..	..	..	450	0 0
<b>NARRACAN AND MIRBOO SHIRES (joint works)—</b>						
Darlimurla-Thorpdale Road .. .. .	..	..	..	..	7	2 0
<b>NEWHAM AND WOODEND SHIRE—</b>						
Campaspe Road .. .. .	..	..	..	..	1,799	5 9
Macedon Village Settlement Road .. .. .	..	..	..	..	1,616	15 9
<b>NEWSTEAD AND MT. ALEXANDER SHIRE—</b>						
Glengowar-Joyce's Creek .. .. .	..	..	..	..	54	13 6
<b>OMEQ SHIRE—</b>						
Bindi Road .. .. .	..	..	..	..	1,951	6 6
Brookville Road .. .. .	..	..	..	..	58	12 1
Glen Wills Road .. .. .	..	..	..	..	74	2 6
Sandy Creek Road .. .. .	..	..	..	..	3,151	17 4
Reedy Creek Road .. .. .	..	..	..	..	55	16 9
Swift's Creek-Cassilis Road .. .. .	..	..	..	..	1,350	17 6
<b>ORBOST SHIRE—</b>						
Bete-Bolong Road .. .. .	..	..	..	..	1,360	13 7
Broome's Road .. .. .	..	..	..	..	1,344	3 6
Combeinbar Road .. .. .	..	..	..	..	2,766	4 4
Deddick River Road .. .. .	..	..	..	..	33	19 7
Groves' Road .. .. .	..	..	..	..	0	10 0
Jarrahmond Road .. .. .	..	..	..	..	918	1 3
Lower Bemm Road .. .. .	..	..	..	..	115	18 10
Lower Bendock Road .. .. .	..	..	..	..	993	4 0
Murrungowar Road .. .. .	..	..	..	..	1	10 0
Orbost-Delegate Road .. .. .	..	..	..	..	92	18 6
Wangrabelle Road .. .. .	..	..	..	..	312	18 11
West Cann Road .. .. .	..	..	..	..	713	18 0
<b>OTWAY SHIRE—</b>						
Barham Valley Road .. .. .	..	..	..	..	11	3 7
Beech Forest-Apollo Bay Road .. .. .	..	..	..	..	1,540	15 5
Beech Forest-Laver's Hill Road .. .. .	..	..	..	..	173	18 3
Beech Forest-Mount Sabine Road .. .. .	..	..	..	..	979	13 3
Cape Patten Road .. .. .	..	..	..	..	88	7 10
Colac-Forrest Road .. .. .	..	..	..	..	156	8 8
Denhart's Road .. .. .	..	..	..	..	255	16 8
Gellibrand-Carlisle Road .. .. .	..	..	..	..	55	8 8
Hordern Vale Road .. .. .	..	..	..	..	336	17 8
Laver's Hill-Chapple Vale-Devondale Road .. .. .	..	..	..	..	239	18 1
Laver's Hill-Glenaire Road .. .. .	..	..	..	..	440	16 8
Phillip's Track .. .. .	..	..	..	..	85	1 9
Port Campbell-Prinetown Road .. .. .	..	..	..	..	37	2 9
Prinetown Road .. .. .	..	..	..	..	14	1 10
Wild Dog Valley Road .. .. .	..	..	..	..	32	4 3
<b>OXLEY SHIRE—</b>						
Abbeyard Road .. .. .	..	..	..	..	53	4 4
Boggy Creek Road .. .. .	..	..	..	..	399	7 11
Fifteen Mile Creek Road .. .. .	..	..	..	..	1	15 0
King Valley Road .. .. .	..	..	..	..	1,689	14 5
<b>PHILLIP ISLAND AND WOOLAMAI SHIRE—</b>						
Bass Road .. .. .	..	..	..	..	10	0 0
Glen Alvie Road .. .. .	..	..	..	..	963	0 0
Korumburra-Wonthaggi Road .. .. .	..	..	..	..	3,273	7 7
Phillip Island Road .. .. .	..	..	..	..	495	0 1
Wonthaggi-Loch Road .. .. .	..	..	..	..	5,153	5 6
Evans Road .. .. .	..	..	..	..	967	19 8
Ventnor Road .. .. .	..	..	..	..	3,138	14 2
<b>Carried forward .. .. .</b>						
	..	..	224	1 2	..	269,131 3 9



STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	224 1 2	..	269,131 3 9
<b>PORTLAND SHIRE—</b>				
Drik Drik—Winnap Road .. .. .	..	..	1,300 0 0	
Gorae Road .. .. .	..	..	1,918 6 3	
Grubbed Road .. .. .	..	..	977 3 9	4,195 10 0
<b>RIPON SHIRE—</b>				
Trawalla Road .. .. .	..	..	277 9 6	
Trawalla East Road .. .. .	..	..	98 5 11	
Trawalla West Road .. .. .	..	..	2,597 13 4	2,973 8 9
<b>ROCHESTER SHIRE—</b>				
Echuca West Road .. .. .	..	..	534 18 1	
Nanneella Road .. .. .	..	..	3,072 16 4	3,607 14 5
<b>RODNEY SHIRE—</b>				
Lancaster—Undera Road .. .. .	..	..	397 1 7	
Mooroopna—Undera Road .. .. .	..	..	2,941 19 0	
Tatura—Rushworth Road .. .. .	..	..	591 14 8	
Tatura—Toolamba Road .. .. .	..	..	411 13 6	4,342 8 9
<b>ROMSEY AND KYNETON SHIRES (Joint Works)—</b>				
Baynton Road .. .. .	..	..	620 15 0	620 15 0
<b>ROSEDALE SHIRE—</b>				
Callignee South Road .. .. .	..	..	991 0 2	
Callignee South Road—U.R. .. .. .	..	..	490 6 9	
Merriman's Creek Road—U.R. .. .. .	..	..	869 18 11	2,351 5 10
<b>SEYMOUR SHIRE—</b>				
Hughes Creek Road .. .. .	..	..	3,590 18 9	3,590 18 9
<b>SHEPPARTON SHIRE—</b>				
Congupna Road .. .. .	..	..	2,146 6 10	
Cosgrove Road .. .. .	..	..	122 7 11	
Graham's Vale Road .. .. .	..	..	429 7 4	
Nathalia Road .. .. .	..	..	3,714 10 11	
Pine Lodge Road .. .. .	..	..	449 0 0	6,861 13 0
<b>SOUTH GIPPSLAND SHIRE—</b>				
Agnes Falls Road .. .. .	..	..	4 13 9	
Amy's Track .. .. .	..	..	15 16 4	
Binginwarri South Road .. .. .	..	..	206 12 5	
Boolarra—Foster Road .. .. .	..	..	4,007 1 0	
Boy's Road .. .. .	..	..	508 11 1	
Devil's Pinch Road .. .. .	..	..	21 12 8	
Dollar—Woorarra West Road .. .. .	..	..	52 16 8	
Foster—Dollar Road .. .. .	..	..	9 4 0	
Foster—Mount Best Road .. .. .	..	..	2,665 3 3	
Foster—Stony Creek Road .. .. .	..	..	4,362 9 1	
Lawson—Harding Road .. .. .	..	..	857 6 1	
McCartin's Road .. .. .	..	..	109 18 8	
O'Grady's Ridge Road .. .. .	..	..	68 3 2	
Stony Creek—Dollar Road .. .. .	..	..	341 7 0	
Toora—Gonyah Road .. .. .	..	..	0 10 0	
Turton's Creek Road .. .. .	..	..	1 10 0	
Waratah Road .. .. .	..	..	484 8 7	
Whitelaw's Track .. .. .	..	..	14 15 10	
Woorarra West Road .. .. .	..	..	748 0 0	14,479 19 7
<b>SOUTH GIPPSLAND AND WOORAYL SHIRES (Joint Works)—</b>				
Boolarra—Foster Road .. .. .	..	..	16 5 10	
Dollar—Dumbalk Road .. .. .	..	..	169 13 0	185 18 10
<b>STAWELL SHIRE—</b>				
Marnoo—St. Arnaud Road .. .. .	..	..	1,051 6 6	
Pomonal Road .. .. .	..	..	179 15 1	1,231 1 7
<b>SWAN HILL SHIRE—</b>				
Fish Point Road .. .. .	..	..	584 4 3	
Manangatang—Euston Road .. .. .	..	..	2,358 11 1	
Spewar Punt Road .. .. .	..	..	312 13 4	
Tresco Road .. .. .	..	..	1,396 14 3	4,652 2 11
<b>TAMBO SHIRE—</b>				
Basin Road .. .. .	..	..	25 6 3	
Bills Road .. .. .	..	..	24 13 9	
Metung Road .. .. .	..	..	1,875 12 3	
Nowa Nowa—Buchan Road .. .. .	..	..	1,521 7 6	
Nowa Nowa—Buchan—Gelantipy Road .. .. .	..	..	213 16 0	
Prince's Highway .. .. .	..	..	27 7 5	
Upper Tambo Road .. .. .	..	..	2,222 2 2	
Nyremilang Road .. .. .	..	..	919 4 9	6,829 10 1
Carried forward .. .. .	..	224 1 2	..	325,053 11 3

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.				
	Amount.		Amount.				
	£	s. d.	£	s. d.			
Brought forward .. .. .	..	..	224	1 2	..	325,053	11 3
<b>TOWONG SHIRE—</b>							
Granya-Tallangatta Road .. .. .	..	..	..	..	71	8 6	
George's Creek Road .. .. .	..	..	..	..	127	13 9	
Shelley-Jingellic Road .. .. .	..	..	..	..	3,410	14 7	
Tallangatta Creek Road .. .. .	..	..	..	..	511	17 7	
Yabba Road .. .. .	..	..	..	..	932	12 6	
Murray River-Tallangatta Road .. .. .	..	..	..	..	253	2 7	
							5,307 9 6
<b>TRARALGON SHIRE—</b>							
Callinglee Road .. .. .	..	..	..	..	0	10 0	
Callinglee Factory Road .. .. .	..	..	..	..	209	5 2	
Traralgon Creek Road .. .. .	..	..	..	..	561	19 8	
Traralgon-Jeeralang Road .. .. .	..	..	..	..	353	8 10	
Tyer's Road .. .. .	..	..	..	..	2	19 4	
Callignee South Road .. .. .	..	..	..	..	51	5 1	
							1,179 8 1
<b>UPPER MURRAY SHIRE—</b>							
Beetoomba Road .. .. .	..	..	..	..	498	7 9	
Benambra-Corryong Road .. .. .	..	..	..	..	1,321	1 8	
Kancobin Road .. .. .	..	..	..	..	684	13 6	
Murray Valley Road .. .. .	..	..	..	..	1,638	8 6	
Thowla Road .. .. .	..	..	..	..	68	16 3	
							4,211 7 8
<b>UPPER YARRA SHIRE—</b>							
Don Road .. .. .	..	..	..	..	532	19 4	
Little Yarra Road .. .. .	..	..	..	..	2,548	7 5	
Wood's Point Road .. .. .	..	..	..	..	3,169	15 6	
Woori Yallock-Cockatoo Road .. .. .	..	..	..	..	685	12 10	
							6,936 15 1
<b>VIOLET TOWN SHIRE—</b>							
Harry's Creek Road .. .. .	..	..	..	..	2,695	18 5	
							2,695 18 5
<b>WALPEUP SHIRE—</b>							
Boinka North Road .. .. .	..	..	..	..	51	5 0	
Boulka-Timberoo Road .. .. .	..	..	..	..	258	17 0	
Booroongie North Road .. .. .	..	..	..	..	380	13 4	
Carina North Road .. .. .	..	..	..	..	825	6 0	
Danyo North Road .. .. .	..	..	..	..	519	1 0	
Galah-Timberoo Road .. .. .	..	..	..	..	61	4 5	
Murrayville North Road .. .. .	..	..	..	..	813	5 0	
Nyang South Road .. .. .	..	..	..	..	4	8 2	
Paninya North Road .. .. .	..	..	..	..	726	15 0	
Underbool South Road .. .. .	..	..	..	..	368	0 2	
Walpeup-North Meridian Road .. .. .	..	..	..	..	10	18 10	
Walpeup South-west Road .. .. .	..	..	..	..	22	0 0	
							4,041 13 11
<b>WANNON SHIRE—</b>							
Melville Forest Road .. .. .	..	..	..	..	95	2 7	
							95 2 7
<b>WANGARATTA SHIRE—</b>							
Boorhaman-Springhurst Road .. .. .	..	..	..	..	2,419	15 11	
							2,419 15 11
<b>WARANGA SHIRE—</b>							
Carag-Stanhope Road .. .. .	..	..	..	..	2,654	10 8	
							2,654 10 8
<b>WARRAGUL SHIRE—</b>							
Bona Vista Road .. .. .	..	..	..	..	150	7 7	
Bona Vista-Nilma Road .. .. .	..	..	..	..	1,795	13 7	
Darnum-Allambee Road .. .. .	..	..	..	..	2	0 0	
Ellinbank Road .. .. .	..	..	..	..	386	11 3	
Ferndale Road .. .. .	..	..	..	..	24	6 8	
Ferndale-Strezlecki Road .. .. .	..	..	..	..	451	0 0	
Korumburra-Seaview Road .. .. .	..	..	..	..	28	18 3	
Mountain View-McDonald's Track .. .. .	..	..	..	..	92	11 6	
Nilma-Shady Creek Road .. .. .	..	..	..	..	2,800	4 5	
Old Sale Road .. .. .	..	..	..	..	10	13 7	
Telegraph Road .. .. .	..	..	..	..	206	10 3	
Warragul-Leongatha Road .. .. .	..	..	..	..	684	0 0	
							6,632 17 1
<b>WARRAGUL AND BULN BULN SHIRES (Joint Works)—</b>							
Lardner's Track .. .. .	..	..	..	..	2,702	12 7	
							2,702 12 7
<b>WARRAGUL AND NARRACAN SHIRES (Joint Works)—</b>							
Nilma-Shady Creek Road .. .. .	..	..	..	..	195	16 6	
							195 16 6
<b>WARRNAMBOOL SHIRE—</b>							
Naringle Road .. .. .	..	..	..	..	274	11 6	
Nullawarre-Timboon Road .. .. .	..	..	..	..	114	15 9	
Panmure Road .. .. .	..	..	..	..	1,532	16 1	
							1,922 3 4
Carried forward .. .. .	..	..	..	..	224	1 2	366,049 2 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.		
	Amount.		Total.		
	£	s. d.	£	s. d.	
Brought forward .. .. .			224	1 2	366,049 2 7
WERRIBEE SHIRE—					
Aviation Road .. .. .			512	10 0	
Exford Road .. .. .			112	0 0	624 10 0
WHITTLESEA SHIRE—					
Chadd's Creek Road .. .. .			1,869	1 5	1,869 1 5
WIMMERA SHIRE—					
Wonwondah Road .. .. .			533	12 6	533 12 6
WINCHELSEA SHIRE—					
Bitreguita-Forrest Road .. .. .			2	0 0	
Boonah Road .. .. .			65	6 3	
Conn's Lane .. .. .			50	10 6	
Dean's Marsh-Retreat Road .. .. .			439	18 9	
Inverleigh-Winchelsea Road .. .. .			143	15 5	
Lorne Road .. .. .			1,476	7 1	
Wymbooliel Road .. .. .			90	17 1	2,268 15 1
WODONGA SHIRE—					
Beechworth-Wodonga Road .. .. .			1,500	0 0	1,500 0 0
WOORAYL SHIRE—					
Boorool Road .. .. .			483	8 3	
Buffalo-Waratah Road .. .. .			554	11 7	
Canavan's Road .. .. .			2,218	4 6	
Dollar-Dumbalk Road .. .. .			306	15 9	
Dumbalk Road .. .. .			3,780	8 9	
Korumburra-Fairbank Road .. .. .			2	6 4	
Inverloch-Lower Tarwin Road .. .. .			151	17 1	
Leongatha-Mirboo Road .. .. .			156	13 10	
Lower Tarwin Road .. .. .			3	12 0	
Meeniyah-Nerrena Road .. .. .			1,042	14 9	
Meeniyah-Tarwin Road .. .. .			2,021	13 5	
Mount Vernon .. .. .			6	0 0	
Turton's Creek Road .. .. .			0	10 0	
Warragul-Leongatha Road .. .. .			71	11 5	
Yarragon-Leongatha Road .. .. .					
Trida-Strezlecki .. .. .			213	8 0	11,013 15 8
WYCHEPROOF SHIRE—					
Culgoa-Lalbert Road .. .. .			792	10 8	
Dumosa Road .. .. .			16	3 6	
Glenloth-Wycheproof Road .. .. .			591	19 1	
Meridian Road .. .. .			1,017	2 2	
Nullawil-Winston Road .. .. .			33	13 6	
Sealake-Tyrell Downs Road .. .. .			733	4 9	3,184 13 8
WYCHEPROOF AND SWAN HILL SHIRES (Joint Works)—					
Nyarrin Road .. .. .			0	19 5	0 19 5
YACKANDANDAH SHIRE—					
Dederang-Tawanga Road .. .. .			1,130	8 2	
Kergunyah Road .. .. .			52	17 2	
Kiewa-Wodonga Road .. .. .			789	6 6	
Myrtleford-Yackandandah Road .. .. .			3	5 7	
Running Creek Road .. .. .			1,610	9 1	
Woololonga Gap Road .. .. .			895	0 0	
Sandy Creek Road .. .. .			12	13 0	4,493 19 6
YEA SHIRE—					
Glenburn-Kinglake East Road .. .. .			1	10 0	
Kilingsworth Road .. .. .			291	7 4	
Molesworth-Dropmore Road .. .. .			2,187	15 8	
Yarraglen-Glenburn Road .. .. .			1,396	10 1	3,877 3 1
			224	1 2	
SPECIAL PROVISION.					
ALBERTON SHIRE—					
Binginwarri-Welshpool Road .. .. .			520	3 7	
Boolarra-Welshpool Road .. .. .			5,334	1 11	
Carrajung-Balook Road .. .. .			4,080	11 3	
Madalya Road .. .. .			13	1 6	
Ridge Road .. .. .			3,133	16 5	
Tarra Valley Road .. .. .			996	16 4	
Whitelaw's Track .. .. .			994	15 8	
Yarram-Boolarra Road .. .. .			340	0 0	15,413 6 8
Carried forward .. .. .			224	1 2	410,828 19 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.		
	Amount.		Total.		
	£	s. d.	£	s. d.	
SPECIAL PROVISION— <i>continued.</i>					
Brought forward .. .. .			224	1 2	410,828 19 7
ALBERTON AND MORWELL SHIRES (Joint Works)—					
Boolarra-Foster Road .. .. .				2 10 1	
Boolarra-Welshpool Road .. .. .				13,744 0 6	
Gonyah-Ryton Road .. .. .				5,282 16 11	
Whitelaw's Track .. .. .				6,087 12 0	25,116 19 6
BENALLA SHIRE—					
Toombulup Road .. .. .				..	
BENALLA AND OXLEY SHIRES (Joint Works)—					
Toombulup Road .. .. .				1,501 12 3	1,501 12 3
BRIGHT SHIRE—					
Happy Valley Road—U.R. .. .. .				759 6 3	759 6 3
BRIGHT AND YACKANDANDAH SHIRES (Joint Works)—					
Happy Valley—Running Creek Road .. .. .				1,353 10 0	1,353 10 0
BULN BULN SHIRE—					
Duggan North Road .. .. .				1,037 16 6	
Fumina Road .. .. .				51 19 9	
Fumina Suspense Road .. .. .				212 14 11	
Lock Valley Road .. .. .				6,385 6 3	
Turners Road .. .. .				1,612 6 6	
Poowong Road .. .. .				0 18 5	
Robinhood Road .. .. .				23 16 1	9,324 18 5
ELTHAM SHIRE—					
Kinglake—Kinglake East Road .. .. .				688 4 2	688 4 2
HEALESVILLE SHIRE—					
Healesville—Toolangi Road .. .. .				2,248 18 1	2,248 18 1
HEYTESBURY SHIRE—					
Peterborough—Port Campbell—U.R. .. .. .				290 14 4	
Timboon—Nirranda .. .. .				241 2 11	531 17 3
HEYTESBURY AND WARRNAMBOOL SHIRES (Joint Works)—					
Ayersford Road .. .. .				1,272 12 1	1,272 12 1
KORUMBURRA AND WARRAGUL SHIRES (Joint Works)—					
Trida—Strezlecki Road .. .. .				5,062 9 7	5,062 9 7
MANSFIELD SHIRE—					
Tolmie Road .. .. .				2,187 0 0	2,187 0 0
MIRBOO SHIRE—					
Allambee East—West Tarwin Road .. .. .				4,908 19 5	
Berry's Creek Road .. .. .				2,627 7 11	
Boorool Road .. .. .				248 11 1	
Clear Creek Road .. .. .				2,480 1 9	
Mirboo—Boolarra South Road .. .. .				2,455 19 6	12,720 19 8
MORWELL SHIRE—					
Boolarra—Foster Road .. .. .				194 4 10	
Boolarra—Stn. Mirboo Road .. .. .				511 15 2	
Boolarra—Welshpool Road .. .. .				544 19 10	
Jeeralang West Road .. .. .				3,286 19 1	
Middle Creek Road .. .. .				2,263 5 11	
Morwell River Road .. .. .				41 12 11	6,842 17 9
MORWELL AND SOUTH GIPPSLAND SHIRES (Joint Works)—					
Gonyah—Ryton Road .. .. .				4,738 9 6	4,738 9 6
MORWELL AND TRARALGON SHIRES (Joint Works)—					
Jeeralang West Road .. .. .				1,035 19 0	1,035 19 0
NARRACAN SHIRE—					
Allambee—Childer's Road .. .. .				1,504 7 8	
Aberfeldy—Matlock Road .. .. .				4,494 9 8	
Allambee Estate Road .. .. .				420 3 8	6,419 1 0
OMEEO SHIRE—					
Bruthen—Omeo Road .. .. .				403 16 11	403 16 11
Carried forward .. .. .			224	1 2	493,037 11 0

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts No. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
SPECIAL PROVISION— <i>continued.</i>				
Brought forward .. .. .	..	224 1 2	..	493,037 11 0
ORBOST SHIRE—				
Bete-Bolong Road .. .. .	..	..	3,741 13 5	
Errinundra Valley Road .. .. .	..	..	988 6 6	
Lower Bemm Road .. .. .	..	..	922 9 7	
Murrungowar Road .. .. .	..	..	333 15 6	
Orbost-Delegate Road .. .. .	..	..	169 6 5	
Wangrabelle Road .. .. .	..	..	237 3 11	
				6,392 15 4
OTWAY SHIRE—				
Barham Valley Road .. .. .	..	..	657 19 6	
Beech Forest-Laver's Hill Road .. .. .	..	..	6,445 0 10	
Beech Forrest-Mt. Sabine Road .. .. .	..	..	194 18 0	
Cape Patten Road .. .. .	..	..	1,156 13 8	
Carlisle-Gellibrand Road .. .. .	..	..	7,927 2 5	
Denhart's Road .. .. .	..	..	343 11 9	
Forrest-Apollo Bay Road .. .. .	..	..	8,876 0 3	
Hordern Vale Road .. .. .	..	..	99 18 1	
Lardner's Track .. .. .	..	..	160 0 0	
Laver's Hill-Glenaire Road .. .. .	..	..	1,678 10 9	
Phillip's Track .. .. .	..	..	1,945 19 7	
Princetown Road .. .. .	..	..	8,198 7 4	
Wild Dog Valley Road .. .. .	..	..	5 5 7	
				37,689 7 9
OXLEY SHIRE—				
Rose River Road .. .. .	..	..	20 0 0	
Whitfield-Tolmie Road .. .. .	..	..	2,611 13 11	
				2,631 13 11
SOUTH GIPPSLAND SHIRE—				
Boolarra-Foster Road .. .. .	..	..	1,146 18 10	
Boolarra-Welshpool Road .. .. .	..	..	3,262 16 8	
Toora-Gunyah Road .. .. .	..	..	2,361 12 6	
Woomera Creek Road .. .. .	..	..	3,582 11 2	
				10,353 19 2
SOUTH GIPPSLAND AND WOORAYL SHIRES (Joint Works)—				
Boolarra-Foster Road .. .. .	..	..	583 1 0	
				583 1 0
TAMBO SHIRE—				
Basin Road .. .. .	..	..	3,679 6 8	
				3,679 6 8
TRARALGON SHIRE—				
Traralgon Creek Road .. .. .	..	..	290 10 10	
Callignee Road .. .. .	..	..	3,628 6 4	
				3,918 17 2
WARRAGUL SHIRE—				
Korumburra-Seaview Road .. .. .	..	..	2,675 11 1	
Trida-Strezlecki Road .. .. .	..	..	280 0 0	
				2,955 11 1
WARRAGUL AND BULN BULN SHIRES (Joint Works)—				
Lardner's Track .. .. .	..	..	5 1 3	
				5 1 3
WHITTLESEA AND YEA SHIRES (Joint Works)—				
Whittlesea-Kinglake Road .. .. .	..	..	3,935 15 11	
				3,935 15 11
WOORAYL SHIRE—				
Turton's Creek Road .. .. .	..	..	2,400 15 9	
Boorool Road .. .. .	..	..	112 5 9	
Trida-Trezlecki Road .. .. .	..	..	292 3 11	
Warragul-Leongatha Road .. .. .	..	..	6,675 11 11	
				9,480 17 4
WOORAYL AND SOUTH GIPPSLAND SHIRES (Joint Works)—				
Turton's Creek Road .. .. .	..	..	1,046 2 0	
				1,046 2 0
YACKANDANDAH SHIRES—				
Running Creek Road .. .. .	..	..	692 0 0	
				692 0 0
YEA SHIRE—				
Yarra Glen-Glenburn Road .. .. .	..	..	3,468 0 6	
				3,468 0 6
	224 1 2	224 1 2	579,870 0 1	579,870 0 1
Suspense Account .. .. .	..	..	..	3,383 4 6
		£224 1 2		£576,486 15 7

## APPENDIX F.

## COUNTRY ROADS BOARD.

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS UNDER THE PROVISIONS OF THE COUNTRY ROADS ACT DURING THE YEAR ENDED 30TH JUNE, 1927.

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES.					
ALBERTON SHIRE— Balook-Traralgon Road ..	John McKenzie, Jnr's., to Pattinson's Saddle ..	1'88	1'88	1'88	1'88
	Willung South .. .. .	1'27	1'27	1'27	1'27
	Gelliondale Road towards Alberton .. ..	1'83	1'83	1'83	1'83
	Deviation through J. E. Chenhall's .. ..	'93	'93	'93	'93
ALEXANDRA SHIRE— Healesville-Alexandra Road ..	Commencing at the boundary of the Healesville and Alexandra Shires and going 5,064 feet northerly	'96	'96	'96	'96
	Between Yarek and Kanumbra .. .. .	'3	'3	'3	'3
ARAPILES SHIRE— Horsham-Hamilton Road ..	Wonwondah, McKeuzie Creek, Brimpaen ..	..	4'96	3'45	3'45
ARARAT SHIRE— Ballarat-Hamilton Road ..	Near Streatham .. .. .	1'65	1'65	1'65	1'65
AVOCA SHIRE— Ballarat-St. Arnaud Road ..	Between 31,300 feet and 35,000 feet on continuous chainage proceeding south-east from Avoca	'51	'51	'51	'51
BACCHUS MARSH SHIRE— Geelong-Bacchus Marsh Road	Maddingley (channel, outfall, and two R.C. culverts)	'33	'33	'12	'12
BALLAN SHIRE— Mount Wallace Road ..	Commencing 3 miles south from Ballan ..	1'16	1'16	1'16	1'16
BEECHWORTH SHIRE— Everton-Myrtleford Road ..	Bowman's Forest and at Taylor's Gap ..	1	1	1'84	'84
BENALLA SHIRE— Goorambat-Thoona Road ..	Forming and gravelling .. .. .	'48	'48	'48	'48
BERWICK SHIRE— Woori Yallock-Pakenham-Koo- wee-rup Road ..	Clearing and forming north of Cockatoo ..	'95	'95	'95	'95
	Metalling north from Ararat Creek .. ..	2'72	2'72	2'72	2'72
BET BET SHIRE— Avoca-Bealiba Road ..	From Main Street, Bealiba, to Body's Corner, Junction Mollagul and Dunolly Roads	1'17	1'17	1'17	1'17
	Section 1—Commencing 6 chains north of Sandy Creek Bridge, thence towards Archdale ..	'92	'92	'92	'92
	Section 2—Opposite Dyer's house and towards Bealiba	..	..	..	..
	Section 3—Connecting 600 feet of roadway between two metalled portions	..	..	..	..
	Re-forming and metalling near northern boundary of the Dunolly Road	'44	'44	'44	'44
BIRCHIP SHIRE— Beulah-Birchip-Wycheproof Road ..	3 miles west of Birchip and 1½ miles east of Birchip	1'5	1'5	1'5	'9
	Donald-Birchip-Sea Lake Road .. .. .	1'14	1'14	1'14	1'14
BORUNG SHIRE— Hopetoun Road .. .. .	Near Galaquil Bridge .. .. .	'8	'8	..	..
	North and south of Brim, formation .. ..	5'1	5'1	5'1	5'1
	Formation 4 miles from Warracknabeal ..	1'36	1'36	1'36	1'36
	Rainbow Road .. .. .	2'9	2'9	2'9	2'9
	Dimboola Road .. .. .	1'15	1'15	1'15	1'15
	Birchip Road .. .. .	'26	'26	..	..
BROADFORD SHIRE— Sydney Road .. .. .	In the Township of Broadford .. .. .	..	..	'25	'25
BULN BULN SHIRE— Westernport Road .. .. .	.. .. .	'69	'69	..	..
	Neerim "C" Road .. .. .	1'5	1'5	1'06	1'06
CHARLTON SHIRE— Bendigo Road .. .. .	2,240 feet of metalling 1 mile from Charlton ..	'42	'42	'42	'42
	1,122 feet of re-sheeting with metal in Township ..	'3	'3	'21	'21
	Wycheproof Road .. .. .	1	'74	'41	'41
	27,700 feet-30,100 feet metalling, Parish of Teddywaddy	..	..	..	..
	25,300 feet-27,700 feet gravelling, Parish of Teddywaddy	..	'45	'45	'45
COLAC SHIRE— Colac-Beech Forest Road ..	Forming and grading between Colac and Kawarren	..	..	6'72	6'72
	Re-forming and gravelling between Colac and Kawarren	..	..	2'84	2'84
CRANBOURNE SHIRE— Main Coast Road .. .. .	Formation gravelling, Pearson's Hill ..	1'99	1'99	1'99	1'99
DANDENONG SHIRE— Cheltenham Road .. .. .	Between sections 25 and 53, Parish of Dandenong	..	..	2 rein. box	concrete culverts.
DIMBOOLA SHIRE— Hopetoun-Rainbow Road ..	Between 1 and 2 miles north of Rainbow ..	'64	'64	'64	'64
	Near Arkona .. .. .	'74	'74	'74	'74
	Rainbow Road .. .. .	1'6	1'6	1'6	1'6
	In three sections between 1 and 4 miles west of Rainbow	..	..	..	..
DONALD SHIRE— Donald-Charlton Road ..	Commencing about 6 miles north-east of Donald, thence towards Shire boundary	4'01	4'01	4'01	4'01
DUNDAS SHIRE— Hamilton-Port Fairy Road ..	At from 4½ to 5½, 11½ to 12½, 17 to 17½, 18 to 18½, 19½ to 20 miles from Hamilton, Parishes of Monivae, Byaduk, and Warracknook—Supply of spalls for 2'85 miles	2'85	..	..	..
	At from 2½ to 3½, 5½ to 6, 6½ to 6¾, 10½ to 11½, 12½ to 14, and 15 to 15½ miles from Hamilton, Parishes of South Hamilton, Warrayure, and Montajup—Supply of spalls for 1'05 miles and construction by day labour	1'05	..	..	2'00 (Day labour)
	Carried forward .. .. .	49'5	50'75	57'07	57'56

## PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES—continued.					
	Brought forward .. ..	49·5	50·75	57·07	57·56
DUNDAS SHIRE—continued. Hamilton-Warrnambool Road ..	At from 3 to 3½, 5½ to 6½, and 8 to 8½ miles from Hamilton, Parishes of South Hamilton and Croxton West—Supply of spalls for '83 miles and construction by day labour	·83	..	..	1·07 (Day labour)
Hamilton-Horsham Road ..	At from 4 to 5, 14 to 14½, 18½ to 19½ and 22½ to 23 miles from Hamilton, Parishes of North Hamilton, Jerrywarook, Cavendish, and Mooralla—Supply of spalls for '87 miles and construction by day labour	1·12	·25	..	1·61 (Day labour)
Hamilton-Mt. Gambier Road ..	At from 7½ to 8½ miles from Hamilton, Parish of Bochara—Supply of spalls for 1 mile and construction by day labour	1	..	..	·68 (Day labour)
Hamilton-Portland Road ..	At from 4 to 4½ miles from Hamilton, Parishes of South Hamilton and Yulecart—Supply of spalls for '62 mile	·62	..	..	..
DUNMUNKLE SHIRE— Stawell-Warracknabeal Road ..	Extension of metal southerly from Rupanyup	1	1	..	..
ECHUCA BOROUGH— Echuca-Cornella Road ..	From List's Gates to Crossing over Tatura line	·57	·57	·57	·57
FERNTREE GULLY SHIRE— Main Ferntree Gully Road ..	R.C. culvert and approaches at the Ferny Creek, between Upwey and Belgrave	..	R.C. culvert	and approaches	..
FLINDERS SHIRE— Dromana-Sorrento Road ..	Rye to Sorrento .. .. .	..	..	2·16	2·16
.. .. .	Near the rocks .. .. .	..	..	·72	·72
GOULBURN SHIRE— Goulburn Valley Road ..	North of Nagambie .. .. .	1·3	1·3	1·3	1·3
Avenel-Longwood Road ..	Near Locksley .. .. .	1·2	1·2	1·2	1·2
GRENVILLE SHIRE— Cressy Road .. .. .	Near McLennan's, Werneth .. .. .	..	·77	..	..
HEALESVILLE SHIRE— Healesville-Woori Yallock Road ..	Between Allotment A5 and Dalry P.R., Parish of Gracedale	·29	·29	·29	..
HEYTESBURY SHIRE— Cobden-Pt. Campbell-Prince- town Road .. .. .	By Cowley's Creek School .. .. .	·4	·4	·4	·4
.. .. .	By Currell's and Magilton's .. .. .	·76	·76	(Day labour) ·76	(Day labour) ·76
HUNTLY SHIRE— Bendigo-Echuca Road ..	Elmore to Rochester at Elmore .. .. .	1	1	..	..
KARA KARA SHIRE— Avoca-St. Arnaud Road ..	Between Redbank and Stuart Mill .. .. .	8·4	8·1	3·8	3·8
Navarre Road .. .. .	At Paradise .. .. .	1·8	1·8	..	..
St. Arnaud-Donald Road ..	Materials supplied and carted for sections 38,000 feet to 43,000 feet, and from 50,269 feet to 57,269 feet	..	2·37	..	..
Charlton Road .. .. .	Between Slaty Creek and Coonoor Bridge .. .. .	2·61	2·37	2·37	2·37
KARKAROOC SHIRE— Hopetoun-Warracknabeal Road ..	Near Goyura Railway Station .. .. .	..	..	..	·82
Rainbow - Beulah - Birchlip Road .. .. .	West of Beulah and west of Kenmare .. .. .	..	..	..	3·51
Hopetoun-Rainbow Road ..	6 miles from Hopetoun and 9 miles from Hope- toun .. .. .	..	..	..	2·56
Hopetoun - Woomelang - Sea- lake Road .. .. .	12 miles from Hopetoun .. .. .	..	..	..	·37
KILMORE SHIRE— Sydney Road .. .. .	In Town of Kilmore—gravelling surface .. .. .	..	..	·25	·25
Heathcote Road .. .. .	At Minogue's, 2 miles from junction with North- Eastern Highway .. .. .	·01	·01	(Day labour) ·01	(Day labour) ·01
KORONG SHIRE— Bendigo-Serpentine Road ..	.. .. .	2·37	2·37	·18	·18
Bendigo-Charlton Road ..	In Wedderburn Township .. .. .	·26	·26	..	..
KORUMBURRA SHIRE— Korumburra-Warragul Road ..	At Reservoir Hill .. .. .	1·3	1·3	1	1
Korumburra-Drouin Road ..	Ranceby section .. .. .	1·5	1·5	1·5	1·5
KOWREE SHIRE— Hamilton - Edenhope - Apsley Road .. .. .	At Harrow Hill, and Bolkerbert S.A. and New- lands .. .. .	2·4	2·4	2·4	2·4
Edenhope-Goroke Road ..	At Kangawall School, Karnak, Awonga, &c. ..	2·25	2·25	2·25	2·25
Boorookpi Road .. .. .	At Morea and Mortat, Pleasant Banks .. .. .	2·08	2·08	2·08	1·16
LAWLOTT SHIRE— South Lillimur Road .. .. .	Metalling between Kaniva and South Lillimur ..	..	·75	·75	·75
Yearinga Road .. .. .	Metalling between Kaniva and Yearinga .. .. .	..	·15	·15	·15
LEIGH SHIRE— Werneth Road .. .. .	Northward from Werneth Post Office .. .. .	..	..	..	·38 (Day labour)
LOWAN SHIRE— Lorquon West Road .. .. .	Between Allotments 6 and 8/9, &c., Parish of Lorquon .. .. .	·44	·44	·44	·44
MAFFERA SHIRE— Tinamba-Newry Road .. .. .	Between Manson's Bridge and Stokes's Corner ..	..	..	..	·42
Licola Road .. .. .	Between Glenmaggie and Molphy's .. .. .	..	..	..	1
Tinamba-Boisdale Road ..	Forbes's deviation .. .. .	..	..	..	·4
MANSFIELD SHIRE— Mansfield Road .. .. .	In two sections, 5 to 10 miles east .. .. .	1	1	1	1
.. .. .	Concrete culvert and approaches at 11½ miles east .. .. .	·1	·1	·1	·1
.. .. .	At Merton Gap .. .. .	1·62	1·62	1·62	1·62
MARONG SHIRE— Bendigo-Eddington Road ..	3,500 feet forming and metalling .. .. .	·7	·7	·7	·7
.. .. .	3,800 feet re-sheeting with gravel at Wallaby Junction .. .. .	·72	·72	·72	·72
MILDURA SHIRE— Wentworth Road .. .. .	17th Street, Riverside to Ontario Avenues ..	·2	·2	·2	·2
.. .. .	Main Avenue, 5th Street to Mena Road .. .. .	1·06	1·06	(Day labour)	(Day labour)
.. .. .	From 3rd Street to Wentworth Bridge .. .. .	4·96	4·96	4·96	4·96
Irymple Road .. .. .	From Irymple to Ginquann Avenues .. .. .	·8	·8	..	..
Melbourne Road .. .. .	South from Red Cliffs Township .. .. .	1	1	·2	·2
MINHAMITE SHIRE— Warrnambool - Hawkesdale - Penshurst Road .. .. .	Maintenance metal coats at various places along road .. .. .	..	..	·38	·38
Hamilton - Macarthur - Port Fairy Road .. .. .	Maintenance metal coats at various places along road .. .. .	..	..	2·12	2·12
	Carried forward .. .. .	97·17	98·9	93·65	105·75



PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads.			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles
UNDER MUNICIPALITIES— <i>continued.</i>					
	Brought forward .. ..	97·17	98·9	93·65	105·75
MIRBOO SHIRE— Mardan Road .. ..	From Webb's to J. Watts's, and Watts's to Campbell's—grading and metalling	3	3	4	4
MORNINGTON SHIRE— Point Nepean Road .. ..	Opposite Strachan's Road .. ..	..	..	·095	·095
SHIRE OF MORWELL— Jeeralang West Road .. ..	Deviation through Ellis's and others .. ..	..	1·35	..	..
MOUNT ROUSE SHIRE— Ballarat-Hamilton Road .. ..	Between Dunkeld and 6 miles east of Glen-thompson .. ..	..	..	1·6	1·6
Hamilton-Dunkeld Road .. ..	Between Dunkeld and 3-mile post .. ..	..	..	·31	·31
Hamilton-Penshurst Road .. ..	Between 1-mile post on south to 7-mile post on north .. ..	..	..	1·35	1·35
Penshurst-Caramut Road .. ..	Between Penshurst and the 10-mile post .. ..	..	..	·88	·88
NUMURKAH SHIRE— Murray Valley Road .. ..	Gravelling west of Strathmerton .. ..	..	..	·9	·9
Nathalia-Kyabram Road .. ..	Gravelling near Ulupna School .. ..	·47	·47	·47	·47
" " " " .. ..	Forming near Kotupna School .. ..	1·5	1·5	1·5	1·5
" " " " .. ..	Embankment near O'Han on's .. ..	·18	·18	·18	·18
Nathalia-Picola Road .. ..	Gravelling near Nathalia Race-course .. ..	·85	·85	·85	·85
" " " " .. ..	Gravelling north from railway line .. ..	1·14	1·14	1·14	1·14
" " " " .. ..	Gravelling eastwards from Picola .. ..	1·02	1·02	1·02	1·02
" " " " .. ..	Gravelling near Mordon's .. ..	·17	·17	·17	·17
Numurkah-Nathalia Road .. ..	Gravelling, fencing, and forming Leaf's deviation .. ..	·58	·58	·58	·58
" " " " .. ..	Gravelling around creek frontage .. ..	·91	·91	·91	·91
" " " " .. ..	Gravelling near Tuckett's pit .. ..	·72	·72	·72	·72
" " " " .. ..	Gravelling west of Leaf's deviation .. ..	1·08	1·08	1·08	1·08
Shepparton-Numurkah-Cobram Road .. ..	Gravelling between Numurkah and Wunghu .. ..	2·41	2·41	1·12	2·41
" " " " .. ..	Gravelling from Wunghu to Shire boundary .. ..	2·65	2·65	2·65	2·65
Nathalia North Road .. ..	Gravelling 2nd mile north of Nathalia .. ..	·85	·85	·85	·85
OMELO SHIRE— Benambra Road .. ..	Wilson's Creek, Sections 1 and 2, and Sugar Loaf, Section 1 .. ..	3	3	3	3
ORBOST SHIRE— Princes Highway .. ..	Township section—metalling in Salisbury Street, Orbost .. ..	..	·33 (Day labour)	·33 (Day labour)	·33 (Day labour)
Cann Valley Road .. ..	Gravelling from Balzer's to Broome's .. ..	4·28	2·95	2·95	2·95
OXLEY SHIRE— Bright Road .. ..	Oxley Flat section, Henley's Gap-Whorouly section, and Yanco section, near Wangaratta .. ..	2·07	2·07	2·07	·84 (1·5 day labour)
PHILLIP ISLAND AND WOOLAMAI SHIRE— Loch-Wonthaggi Road .. ..	Near Archie's Creek and Ryanston .. ..	1·51	1·51	1·51	1·51
Main Coast Road .. ..	Between Grantville and Glen Forbes Road .. ..	1·68	1·68	1·68	1·68
PORTLAND SHIRE— Portland-Hamilton Road .. ..	8,168 feet forming and metalling from Condah to Branxholme .. ..	1·54	1·54	1·54	1·36
Portland-Casterton Road .. ..	North of Hotspur .. ..	1·52	1·52	1·52	1·52
ROCHESTER SHIRE— Rochester-Bamawm-Prairie Road .. ..	Between Allotments 67B, 67A, 26, and 3, 4, Parish of Ballendella .. ..	1·1	1·1	1·1	1·1
" " " " .. ..	Between Allotments 149, 150, 151, 154, and 122B, 123, and 129, Parish of Bamawm .. ..	·94	·94	·94	·94
" " " " .. ..	From western boundary of Allotment 166, to western boundary of Allotment 152B, Parish of Pannoobamawm .. ..	3·09	3·09	3·09	3·09
RODNEY SHIRE— Mooroopna-Undera Road .. ..	Between Allotments 96, 71, 24, 38, and 55, Parish of Mooroopna .. ..	1·74	1·74	1·7	1·7
Tatura-Byrneside-Kyabram Road .. ..	Between Allotments 119 and 116, Parish of Toolamba West .. ..	1·93	1·93	·12	·12
ROSEDALE SHIRE— Sale-Yarram Road .. ..	Southward from Merriman's Creek .. ..	..	..	·64	·64
" " " " .. ..	Continuation of above .. ..	1·88	1·88	..	..
Seaspray Road .. ..	Road through Prospect Estate .. ..	1·89	1·89	1·89	1·89
SEYMOUR SHIRE— Goulburn Valley Road .. ..	Repairing Hughes Creek Bridge .. ..	·645	·645	·645	..
SHEPPARTON SHIRE— Shepparton-Nagambie Road .. ..	Construction of 6,600 feet of metalling .. ..	1·25	1·25	1·25	1·25
Shepparton-Numurkah Road .. ..	Construction of 4,400 feet of metalling .. ..	·83	·83	·83	..
Shepparton-Nalinga Road .. ..	Construction of 3,350 feet of gravelling .. ..	·63	·63	·63	·63
Dookie-Nalinga Road .. ..	Construction of 5,094 feet of gravelling .. ..	·96	·96	·96	·96
" " " " .. ..	Construction of 3,150 feet of gravelling .. ..	·6	·6	·6	·6
SOUTH GIPPSLAND SHIRE— Foster-Boolarra Road .. ..	Through Conder's .. ..	..	..	·95	·95
Dollar-Stony Creek Road .. ..	Through Carmichael's .. ..	..	1·04	1·04	1·04
Fall's Road .. ..	Between Contract 154P-67 and Rich's .. ..	..	2·61	2·61	2·61
Foster-Yarram Road .. ..	Between Franklin River and Toora and construction of Bridge at Deep Creek .. ..	..	1·74 and timber bridge	1·74 and timber bridge	1·74 and timber bridge
ST. ARNAUD BOROUGH— Avoca-St. Arnaud Road .. ..	From Napier Street to Borough boundary .. ..	·96	·96	·96	·96
St. Arnaud-Donald Road .. ..	North-west end of Napier Street to Borough boundary .. ..	1·85	1·85	1·85	1·85
STAWELL BOROUGH— Ararat-Stawell Road .. ..	Patrick Street metalling .. ..	·85	·85	·85	·85
Glenorchy Road .. ..	By Stawell Hospital—metalling .. ..	·38	·38	·38	·38
STAWELL SHIRE— Marnoo Road .. ..	Gravelling north of Marnoo .. ..	1·89	1·89	1·89	1·89
" " " " .. ..	Ironstone experimental section near Evans .. ..	·12	·12	·12	·12
" " " " .. ..	Works between Marnoo and Stawell .. ..	1·85	1·85	1·85	1·85
Stawell-Warracknabeal Road .. ..	Gravelling near Dunmunkle Shire boundary .. ..	1·36	1·36	1·36	1·36
SWAN HILL SHIRE— Murray Valley Road .. ..	North of Piangil .. ..	3·54	3·54	3·54	3·54
" " " " .. ..	Boundary bend .. ..	2·28	2·28	2·28	2·28
Euston Road .. ..	North of Swan Hill .. ..	2·89	2·89	·94	·94
Swan Hill Road .. ..	.. ..	2·17	2·17	1·72	1·72
Piangil Station Road .. ..	.. ..	1·04	1·04	..	..
TAMBO SHIRE— Nowa Nowa-Buchan-Gelantipy Road .. ..	Proudlock's Section .. ..	1	1	1	1
TRARALGON SHIRE— Calligee Road .. ..	Saw Pit Gully, Sections 2, 3, and 4 .. ..	..	..	2·14	2·14
TUNGAMAH SHIRE— Yarrawonga-Cobram Road .. ..	East of Cobram .. ..	1	1	1	1
Cobram-Strathmerton Road .. ..	West of Cobram .. ..	1	1	1	1
Numurkah-Tungamah-Wilby Road .. ..	West of Katamatite railway and near Shire boundary .. ..	1·5	1·5	1·5	1·5
South Cobram Road .. ..	South of Cobram-Yarroweah railway line .. ..	1	1	1	1
	Carried forward .. ..	173·815	181·335	172·54	185·145

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>					
	Brought forward .. ..	173·815	181·335	172·54	185·145
WALPEUP SHIRE—					
Ouyen—Pinnaroo Road ..	Near Torrita (Nyang) .. ..	1·13	1·13	1·13	1·13
	West of Ouyen .. ..	·53	·53	·53	·53
WANGARATTA SHIRE—					
Yarrowonga Road .. ..	At Pre-Emptive Right Section E, Parish of Wangaratta South .. ..	..	..	..	·39
WARANGA SHIRE—					
Elmore—Colbinabbin Road ..	Deviation at McTaggart, forming and metalling .. ..	..	..	·63	·63
	Murchison—Rushworth Road .. ..	..	..	·57	·57
WARRAGUL SHIRE—					
Bloomfield Road .. ..	Commencing at the end of metal opposite C. A. 42, Parish of Drouin East, then along Bloomfield Road, and thence through Crown Allotment 42, to the Old Sale Road .. ..	..	..	1·62	1·62
WHITTLESEA SHIRE—					
Main Whittlesea Road .. ..	From end of tarred road at 13th mile post to Reid's Stud Farm, between South Morang and Genesfield .. ..	..	..	·77	·77
	Whittlesea—Kingleake Road .. ..	..	..	2	2
WIMMERA SHIRE—					
Horsham—Doon Road .. ..	Re-sheeting near Longernong Turnoff .. ..	..	·38	·38	·38
	Horsham—Hamilton Road .. ..	..	·62	..	..
	Horsham—Natumuk Road .. ..	..	·85	·85	·85
	Repairs to Quantong Bridge .. ..	..	·04	·04	·04
WINCHELSEA SHIRE—					
Birregurra—Forrest Road .. ..	New curve at McCullum's Corner, 1½ miles from Birregurra .. ..	·1	·1	·1	·1
	At Dunse's Hill .. ..	..	..	·41	·41
	At Corner near "Studdbrook" .. ..	..	..	·41	·41
	Adjoining Crown Allotment 5, Section 2, Parish of Murtoon .. ..	..	..	·11	·11
	¾ mile north of Birregurra—Forrest Railway Crossing .. ..	..	..	·06	·06
	Lorne Road .. ..	..	..	1·04	1·04
	3½ miles north of Benwerrin Post Office (Tramway section) .. ..	..	..	1·14	1·14
WODONGA SHIRE—					
Wodonga—Yackandandah Road ..	At Baranduda .. ..	·69	·69	·69	·69
WOORAYL SHIRE—					
Main South Gippsland Road ..	Past Morris's and Gwyther's .. ..	..	..	·56	·56
	Lower Tarwin Road .. ..	2·01	2·01	2·01	2·01
	Wild Dog Valley Road .. ..	..	..	1·59	1·59
	Leongatha—Yarragon Road .. ..	..	·76	·76	·76
WYCHEPROOF SHIRE—					
Wycheproof—Corack Road .. ..	West from Wycheproof .. ..	1·72	1·72	1·72	1·72
	Wycheproof—Birchip Road .. ..	1·35	1·35	1·35	1·35
	Sea Lake—Woomelang Road .. ..	..	·22	·22	·22
	Sea Lake—Birchip Road .. ..	·88	·88	·88	·88
YACKANDANDAH SHIRE—					
Dederang Road .. ..	Gravelling near Kinchington's Creek bridge, Parish of Yackandandah .. ..	..	..	..	..
	Gravelling and reinforced concrete bridge and culvert near Back Creek, Parish of Yackandandah .. ..	2·97	2·35	2·35	2·35
	Gravelling approaches and timber bridge at House Creek, Parish of Dederang .. ..	..	..	..	..
	Gundowring Road .. ..	3·02	3·02	2·73	2·73
	Yackandandah—Wodonga Road .. ..	2	..	..	..
YARRAWONGA SHIRE—					
Wangaratta—Yarrowonga Road .. ..	Re-forming and metalling from peg at 9,800 feet to peg at 12,500 feet .. ..	..	·51	·51	·51
YEA SHIRE—					
Yea—Glenburn Road .. ..	Between Kalatha Creek and Glenburn School .. ..	..	1·35	1·35	1·35
	8,591 feet of gravelling near "Azwarby" and "Murrindudi" .. ..	..	1·63	1·63	1·63
	6,775 feet Stewart's Lane to Devlin's Bridge .. ..	..	1·28	1·28	1·28
	Two reinforced concrete pipe culverts and 450 feet gravelling near Rece's and Purvis's .. ..	..	·09	·09	·09
	6,489 feet gravelling near Kerrisdale (boundary work) .. ..	..	1·23	1·23	1·23
	3,705 feet gravelling near Homewood and Sawpit Creek .. ..	..	·7	·7	·7
	Total .. ..	190·435	204·775	205·98	218·975

## UNDER DIRECT SUPERVISION OF THE BOARD.

ALBERTON AND MORWELL SHIRES—					
Boolarra—Welshpool Road ..	Re-forming and metalling from the end of Eckberg's contract to the end of the day labour sanding .. ..	..	2	2	2
ALEXANDRA SHIRE—					
Healesville—Alexandra Road ..	Bridge over Little River .. ..	..	·01	·01	·01
ALEXANDRA AND YEA SHIRES—					
Upper Goulburn Road .. ..	Timber bridge over Home Creek .. ..	·2	·2	..	..
AVOCA SHIRE—					
Maryborough Road .. ..	Three reinforced concrete bridges near Bung Bong .. ..	·22	·22	..	..
	Three-cell reinforced concrete culvert near Avoca .. ..	·007	·007	..	..
BORUNG SHIRE—					
Dimboola Road .. ..	Reinforced concrete bridge over Yarriambiack Creek .. ..	..	..	·13	·13
CARRUM SHIRE—					
Point Nepean Road .. ..	Reinforced concrete bridge over Patterson River at Carrum .. ..	..	..	·15	·15
CRANBOURNE SHIRE—					
Main Coast Road .. ..	Timber bridge over Lang Lang River .. ..	·1	·1	..	..
ELTHAM SHIRE—					
Eltham—Yarra Glen Road .. ..	Reinforced concrete bridge over Diamond Creek .. ..	..	..	·04	·04
	Carried forward .. ..	·527	2·537	2·33	2·33

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>					
	Brought forward .. ..	527	2 537	2 33	2 33
FLINDERS SHIRE— Mornington-Flinders Road ..	Reinforced concrete bridge over Dunn's Creek and approaches	'02	'02	'02	'02
HEALESVILLE SHIRE— Healesville-Alexandra Road	Bridge over Yarra Flats near Healesville ..	'32	..	..	..
LILYDALE SHIRE— Healesville Road .. ..	Reinforced concrete bridge over Olinda Creek at Lilydale	'04	'04	..	..
Yarra Glen Road .. ..	Reinforced concrete bridge over Stringy Bark Creek	..	..	'01	'01
LILYDALE AND UPPER YARRA SHIRE— Main Warburton Road ..	Two reinforced concrete bridges over Woori Yallock Creek	..	..	'09	'09
MALDON, MARONG, AND TULLAROOP SHIRES— Eddington Road .. ..	Bridge over the Loddon River at Eddington ..	'85	..	..	..
MULGRAVE SHIRE— Ferntree Gully Road .. ..	Fencing on Wheeler's Hill deviation ..	..	'5	'5	'5
MANSFIELD SHIRE— Eildon Weir Road .. ..	State Rivers and Water Supply Commission survey, Ch. 00—Ch. 116,160 feet	22	..	..	..
NEWHAM AND WOODEND SHIRE— Tylden Road .. ..	Reinforced concrete bridge near Woodend ..	..	..	'03	'03
OTWAY SHIRE— Forrest-Apollo Bay Road ..	Metalling on Mount Sabine .. ..	..	1 56	(Day labour)	(Day labour)
SEYMOUR SHIRE— Upper Goulburn Road .. ..	Bridge over Fall's Creek .. ..	'02	'02	..	..
SOUTH GIPPSLAND SHIRE— Boolarra-Welshpool Road ..	Reforming and sanding, Ch. 137,600—153,500 feet	..	3	3	3
SWAN HILL SHIRE— Murray River Valley Road ..	Forming between Boundary Bend and Robinvale	..	1 3	1 3	1 3
Murray River Valley Road ..	Forming .. ..	..	'6	'6	'6
TULLAROOP SHIRE— Eddington Road .. ..	Reinforced concrete culvert near Eddington ..	'02	'02	..	..
UPPER YARRA SHIRE— Don Road .. ..	Temporary timber bridge over Yarra River at Launching Place	..	..	'04	'04
Main Warburton Road ..	Timber bridge over Hoddle's Creek .. ..	..	..	'008	'008
WANGARATTA AND YARRAWONGA SHIRES— Peechelba Road .. ..	Seven timber bridges over Ovens River and approaches	..	..	1 94	1 94
	Total .. ..	23 79	9 597	9 868	9 868

## APPENDIX G.

## COUNTRY ROADS BOARD.

PARTICULARS OF SURVEYS, ETC., ON HIGHWAYS DECLARED STATE HIGHWAYS UNDER THE PROVISIONS OF THE HIGHWAYS AND VEHICLES ACT 1924, DURING THE YEAR ENDED 30th JUNE, 1927.

Name of Municipality and Name of Highway.	Locality of Works.	Lengths of Highways—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES.					
ARARAT SHIRE					
Western-Highway .. ..	At Buangor .. ..	·64	·64	·64	·64
" " " " .. ..	Between Buangor and Middle Creek .. ..	2·18	2·18	2·18	2·18
BACCHUS MARSH—					
Western Highway—Section 1 ..	Bacchus Marsh to Myrniong .. ..	3·62	3·62	..	..
COLAC SHIRE—					
Princes Highway West—Section 2 ..	Between Warncoort and Pirron Yaloak ..	14·4	14·4	8·4	8·4
KORONG SHIRE—					
North-Western Highway Section 3 ..	.. .. .	·5	..	..	..
LAWLOIT SHIRE—					
Western Highway—Section 5 ..	Gravelling between Kaniva and Border Fence ..	..	1·61	1·61	1·61
LOWAN SHIRE—					
Western Highway—Section 5 ..	Between allotments 48A and 48B, Parish of Kinimakatka and between allotments 60A and 59-71, Parish of Tarranginnie ..	1·02	1·02	1·02	1·02
MILDURA SHIRE—					
North-Western Highway .. ..	Between 43 and 45 mile posts near Hattah ..	·95	·95	..	..
" " " " .. ..	Between Boonoonar and Carwarp .. ..	1·9	1·9	1·9	..
" " " " .. ..	Between Carwarp and Red Cliffs .. ..	5·0	5·0	5·0	5·0
MORWELL SHIRE—					
Princes Highway East .. ..	Near foot of Haunted Hills .. ..	..	..	·45	·45
PORTLAND SHIRE—					
Princes Highway—Section 5 ..	Between Lyons and Greenwald .. ..	·8	·8	..	..
" " " " .. ..	Between Greenwald and Winnap .. ..	·91	·91	·91	·91
" " " " .. ..	Between Greenwald and Dartmoor .. ..	1·94	1·94	1·94	1·94
" " " " .. ..	Between Winnap and Dartmoor .. ..	·6	·6	·6	·6
ROCHESTER SHIRE—					
Northern Highway .. ..	From North-eastern angle allotment 16, Parish of Ballendella, to north-eastern angle allotment 34, Parish of Culleioa ..	8·19	8·19	8·19	8·19
STAWELL SHIRE—					
Western Highway—Section 3 ..	Works of metalling, &c., north of Glenorchy ..	1·36	1·36	·34	·34
" " " " .. ..	Flood sections, &c., on Wimmera River, south of Glenorchy ..	1·45	1·45	..	..
TOWONG SHIRE—					
Omeo Highway .. ..	At Polnear's, near Eskdale .. ..	..	1·09	1·09	1·09
" " " " .. ..	At Scrubby Creek, near Mitta Mitta .. ..	·15	·15	·15	·15
" " " " .. ..	Between Mitta Mitta and Granite Flat .. ..	·58	·58	·58	·58
WIMMERA SHIRE—					
Western Highway—Section 3 ..	Re-sheeting and widening between 139A and 195C, Parish of Bungalally ..	..	·38	Day labour	
" " " " Section 4 ..	Re-sheeting between 131 and 133, Parish of Vectis East ..	..	·19	..	..
WYCHEPROOF SHIRE—					
North-Western Highway—Section 3 ..	South of Wycheproof .. ..	1·35	1·35	1·35	1·35
" " " " Section 4 ..	Between Wycheproof and Dumosa .. ..	·79	·79	·79	·79
" " " " " " .. ..	" Dumosa and Nullawill .. ..	·11	·11	·11	·11
" " " " " " .. ..	" Nullawill and Culgoa .. ..	2·87	2·87	2·87	2·87
" " " " " " .. ..	" Culgoa and Berriwillock .. ..	1·79	1·79	1·79	1·79
" " " " " " .. ..	" Berriwillock and Sea Lake .. ..	·72	·72	·72	·72
" " " " Section 5 ..	North of Sea Lake .. ..	1·04	1·04	1·04	1·04
	Total .. ..	54·8	57·63	43·67	41·77

## UNDER DIRECT SUPERVISION OF THE BOARD.

BAIRNSDALE SHIRE—					
Princes Highway East—Section 3 ..	Toms Creek Bridge, near S.S., 15 miles from Bairnsdale ..	·03	·03	·03	·03
BALLAN SHIRE—					
Western Highway—Section 1 ..	Re-alignment Pyke's Creek Cutting .. ..	·2	..	..	..
BRAYBROOK AND MELTON SHIRES—					
Western Highway—Section 1 ..	Asphaltic concrete surfacing from Albion to bridge over Kororoit Creek ..	..	2·1	2·1	2·1
BELFAST SHIRE—					
Princes Highway West—Section 4 ..	Timber bridge over Eumeralla River .. ..	..	·22	·22	·22
CHILTERN SHIRE—					
North-Eastern Highway—Section 3 ..	Reinforced concrete culvert over Blind Creek at Wenke's Crossing ..	·02	·02	·02	·02
" " " " " " .. ..	Reinforced concrete culvert near Rutherglen turn-off ..	·03	·03	·03	·03
CORIO SHIRE—					
Princes Highway West—Section 1 ..	Reinforced concrete bridge over Cowie's Creek, near Geelong ..	..	..	·01	·01
" " " " " " .. ..	Concrete surfacing or asphaltic concrete surface from Separation Street to junction Bacchus Marsh Road ..	..	2·0	2·0	2·0
EUROA SHIRE—					
North Eastern Highway—Section 2 ..	Reinforced concrete bridge over 9 mile Creek, near Longwood ..	..	..	·01	·01
	Carried forward .. ..	·28	4·4	4·42	4·42

PARTICULARS OF SURVEYS, ETC., DECLARED STATE HIGHWAYS, ETC.—*continued.*

Name of Municipality and Name of Highway.	Locality of Works.	Lengths of Highways—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>					
	Brought forward .. .. .	.28	4.4	4.42	4.42
GOULBURN SHIRE—					
North Eastern Highway—Section 2	Reinforced concrete culvert near Monea Railway Station	..	..	.002	.002
" " " " ..	Two reinforced concrete culverts over Larry's Hut Creek, near Locksley	..	..	.007	.007
" " " " ..	Reinforced concrete culvert over Burnt Creek..	..	..	.03	.03
GOULBURN AND EUROA SHIRES—					
North Eastern Highway—Section 2	Forming and gravelling Avenel to Longwood ..	..	5.4	5.4	5.4
KELLOR SHIRE—					
North Eastern Highway—Section 1	Reinforced concrete bridge over Maribyrnong River in Kellor	..	.02	.02	.02
KILMORE SHIRE—					
North Eastern Highway—Section 1	Reinforced concrete bridge at turn-off to Bendigo	.01	.01	.01	.01
KORONG AND CHARLTON SHIRES—					
North Western Highway—Section 3	Woosang Flats .. .. .	5.27	1.51	1.51	1.51
MARONG SHIRE—					
North Western Highway—Section 2	Six reinforced culverts near Big Hill .. ..	..	.02	.02	.02
" " " " ..	Five reinforced concrete culverts .. ..	..	.05	.05	.05
" " " " ..	Three reinforced concrete culverts .. ..	..	.01	.01	.01
" " " " ..	At Derby .. .. .	1.6	..	..	..
METCALFE SHIRE					
North Western Highway—Section 2	Reinforced concrete bridge over Barker's Creek	.19	.19	.19	.19
MORWELL AND NARRACAN SHIRES—					
Princes Highway East—Section 2 ..	Haunted Hills alignment .. .. .	3.0	..	..	..
NARRACAN SHIRE—					
Princes Highway East—Section 2 ..	Narracan Creek and approaches .. ..	..	.01	.01	.01
OMEQ SHIRE—					
Omeo Highway—Section 1 ..	Clearing, forming and gravelling near Monkey Creek	..	2.12	2.12	2.12
SRYMOUR SHIRE—					
North Eastern Highway—Section 2	Forming Avenel deviation .. .. .	..	.91	.91	.91
" " " " ..	Culvert over Saunders Creek .. ..	..	..	.001	.001
" " " " ..	Reinforced concrete culvert over Charles Creek	..	..	.007	.007
SWAN HILL SHIRE—					
North Western Highway—Section 5	Re-forming and lime-stoning west of Mittyack ..	..	2.9	2.9	2.9
TRARALGON SHIRE—					
Princes Highway East—Section 2 ..	Loy Yang section .. .. .	4.0	..	..	..
WANGARATTA SHIRE—					
North Eastern Highway—Section 3	From the north-eastern boundary of the borough on for 6.83 miles. Roadworks consisting of re-sheeting with gravel	6.83	3.24	1.8	1.8
" " " " ..	Reinforced concrete bridge over Diddah Diddah Creek at Springhurst	.05	.05	.05	.05
" " " " ..	Reinforced concrete bridge over Springhurst Creek	.01	.01	.01	.01
WODONGA SHIRE—					
North Eastern Highway—Section 3	6½ miles east of Barnawartha, gravelling, &c. ..	..	.5	.5	.5
" " " " ..	Two reinforced concrete bridges over River Murray Flats at Wodonga	.22	.22	.22	.22
	Total .. .. .	21.46	21.57	20.197	20.197

## APPENDIX H.

## COUNTRY ROADS BOARD.

## MAIN ROADS.

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED AND ROADS MAINTAINED DURING THE YEAR ENDED 30TH JUNE, 1927.

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES.</b>				
ALBERTON SHIRE—				
Balook-Traralgon Road .. ..	Gravelling between J. McKenzie's, Junr., and Pattinson's saddle	Pattinson's saddle to Balook .. ..	1'88	1'2
Carrajung-Gormandale Road .. ..	Metalling at Calrossie .. ..	Yarram to Won Wron and Carrajung to Gormandale	'92	17'5
Foster-Yarram Road .. ..	Metalling between Gellondale and Alberton	Alberton to Shire boundary .. ..	1'83	8
Sale-Yarram Road .. ..	Gravelling near Buckley's corner	Yarram to Moukey Creek .. ..	1	28
Yarram-Boolarra Road .. ..	.. ..	Yarram to Gemmill's Hill .. ..	..	17'8
Yarram-Port Albert Road .. ..	.. ..	Yarram to Port Albert .. ..	..	8'5
Yarram-Won Wron Road .. ..	.. ..	Sale-Yarram Road to Carrajung-Gormandale Road	..	4'6
ALEXANDRA SHIRE—				
Healesville-Alexandra Road .. ..	Commencing at boundary of Shires of Healesville and Alexandra—5,064 feet northerly	.. ..	'96	..
Cathkin-Mansfield Road .. ..	.. ..	Between Yarek and Kanumbra .. ..	..	'3
ARAPILES SHIRE—				
Horsham - Natimuk - Edenhope Road .. ..	Poverty Flat, Jacky Sands, Natimuk	.. ..	2'83	..
Horsham-Hamilton Road .. ..	.. ..	Pickford's Bridge .. ..	..	'85
BOROUGH OF ARARAT—				
Ballarat-Stawell Road .. ..	.. ..	Tar painted full length .. ..	..	3'23
ARARAT SHIRE—				
Ararat-Elmhurst Road .. ..	.. ..	General maintenance .. ..	..	23
Ararat-Warrnambool Road .. ..	.. ..	Gravelling 1 mile, re-grading 8 miles, near Ararat; bitumen 2 miles, near Willaura; general maintenance over whole length	..	33
Maroona-Glenthompson Road .. ..	.. ..	Gravelling '66 mile and coating with bitumen 4 miles between Willaura and Glenthompson; and general maintenance	..	25
Ballarat-Hamilton Road .. ..	Metalling near Streatham .. ..	Gravelling 1'51 miles near Wickliffe; tarring 7 miles near Westmere; and general maintenance	1'65	24
AVOCA SHIRE—				
Ballarat-St. Arnaud Road .. ..	Forming and gravelling between 31,300 feet and 35,000 feet south-easterly from Avoca	Re-sheeting between 3,000 feet and 8,000 feet on continuous chainage, north-westerly from Avoca	'51	'95
Bealiba Road .. ..	Forming and gravelling between 45,400 feet and 47,200 feet on continuous chainage, northerly from Avoca	.. ..	'34	..
AVON SHIRE—				
Sale-Maffra Road .. ..	.. ..	General maintenance .. ..	..	2'5
Prince's Highway .. ..	.. ..	.. ..	..	1'5
Dargo Road .. ..	.. ..	.. ..	..	45
BACCHUS MARSH SHIRE—				
Melbourne-Ballarat Road .. ..	.. ..	General maintenance .. ..	..	3'5
Gisborne Road .. ..	.. ..	.. ..	..	7'6
Geelong-Bacchus Marsh Road .. ..	Maddingley drain .. ..	.. ..	'12	3'1
BAIRNSDALE SHIRE—				
Prince's Highway .. ..	.. ..	B.R.C. railway crossing to Mitchell bridge .. ..	..	3'25
Bullumwaal-Tabberabbera Road .. ..	.. ..	Bairnsdale to Boggy Creek .. ..	..	15'17
BALLAN SHIRE—				
Mount Wallace Road .. ..	Forming and metalling .. ..	General maintenance .. ..	2'05	1'25
Daylesford Road .. ..	.. ..	.. ..	..	3
Gordon-Meredith Road—"A" .. ..	.. ..	.. ..	..	'7
" " "B" .. ..	.. ..	.. ..	..	'2
Ballarat Road .. ..	.. ..	Re-sheeting .. ..	..	'65
BALLARAT SHIRE—				
Ballarat-Creswick Road .. ..	.. ..	Re-sheeting and general maintenance .. ..	..	'92
Ballarat-Maryborough Road .. ..	.. ..	.. ..	..	1'25
Ballarat-Lexton Road .. ..	.. ..	.. ..	..	3'07
BANNOCKBURN SHIRE—				
Geelong-Ballarat Road .. ..	.. ..	Re-sheeting .. ..	..	2'94
Inverleigh Road .. ..	.. ..	.. ..	..	2'28
Shelford-Bannockburn Road .. ..	.. ..	General maintenance 2 miles, and re-sheeting '65 mile	..	2'65
Gordons-Meredith Road .. ..	.. ..	General maintenance .. ..	..	2
BARRABOOL SHIRE—				
Prince's Highway .. ..	.. ..	General maintenance .. ..	..	13
Anglesea Road .. ..	.. ..	.. ..	..	17
Hendy Main Road .. ..	.. ..	.. ..	..	10
BEECHWORTH SHIRE—				
Beechworth Road .. ..	.. ..	General maintenance .. ..	..	23
Bright Road .. ..	.. ..	.. ..	..	5
Everton-Myrtleford Road .. ..	Bowman's Forest section	.. ..	'84	20
Stanley Road .. ..	Myrtle Creek .. ..	.. ..	'65	4
BELFAST SHIRE—				
Hamilton Road .. ..	.. ..	Sheeting Port Fairy to Shire of Minhamite .. ..	..	4
Penshurst Road .. ..	.. ..	Sheeting from Borough of Koroit to Shire of Minhamite	..	1'5
BELLARINE SHIRE—				
Geelong-Queenscliff Road .. ..	.. ..	General maintenance .. ..	..	1'5
Geelong-Portarlington Road .. ..	.. ..	.. ..	..	2
BENALLA SHIRE—				
Sydney Road .. ..	.. ..	Maintenance, sheeting, and tarring .. ..	..	2
Goorambat-Thoona Road .. ..	Forming and gravelling .. ..	Gravelling and general maintenance .. ..	'48	11
Goorambat Road .. ..	.. ..	.. ..	..	6
Tatong Road .. ..	.. ..	General maintenance and supply of metal .. ..	..	7
Greta Road .. ..	.. ..	General maintenance .. ..	..	1
Benalla-Mansfield Road .. ..	.. ..	Maintenance, re-forming, and sheeting .. ..	..	3'4
Lima Road .. ..	.. ..	General maintenance .. ..	..	3
BERWICK SHIRE—				
Woori Yallock - Pakenham - Koo-wee-rup Road .. ..	Clearing and forming north of Cockatoo and metalling north from Ararat Creek	Sheeting north from the Prince's Highway, in Main Street, Pakenham, and south of railway line, Pakenham	3'67	1'57
		Carried forward .. ..	19'73	434'43

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>				
		Brought forward .. ..	19·73	434·43
BET BET SHIRE— Avoca-Bealiba Road .. ..	Forming, metalling, and draining from Main Street, Bealiba, to junction of Moliagul and Dunolly Roads	Forming, metalling, and draining three sections between Sandy Creek bridge, Parish of Archdale, and southern boundary of Bealiba Parish, opposite Allotment 38, sec. A, and Allotments 9 and 3B, Parish of Archdale; and general maintenance	1·17	·92
Dunolly Road .. ..	.. .. .	Re-forming and metalling near northern boundary of Dunolly main road; and general maintenance	..	·44
Betley Road .. ..	.. .. .	General maintenance .. .. .	..	·34
BIRCHIP SHIRE— Beulah - Birchip - Wycheproof Road .. ..	3 miles west of Birchip and 1½ miles east of Birchip	Repairs to earth formations west of Birchip .. ..	·9	8
Donald-Birchip-Sealake Road .. ..	.. .. .	Repairs to earth formations north and south of Birchip	..	10
BLACKBURN AND MITCHAM SHIRE— Healesville Road .. ..	.. .. .	General maintenance .. .. .	..	4·16
BORUNG SHIRE— Hopetoun Road .. ..	.. .. .	North and south of Brim—formations .. ..	..	5·1
Minyip Road .. ..	.. .. .	4 miles from Warracknabeal—formations .. ..	..	1·36
Rainbow Road .. ..	.. .. .	8 miles from Warracknabeal—formations .. ..	..	2·9
Dimboola Road .. ..	Gravelling 3 miles from Warracknabeal	.. .. .	1·15	..
BRAYBROOK SHIRE— Ballarat Road .. ..	.. .. .	General maintenance between Footscray municipal boundary and Western Highway	..	3·5
BRIGHT SHIRE— Bright Road .. ..	.. .. .	Re-sheeting and general maintenance .. ..	..	20
Harrietville Road .. ..	.. .. .	General maintenance .. .. .	..	16
BROADFORD SHIRE— Melbourne-Sydney Road .. ..	.. .. .	Gravelling roadway, and repairing handrails and fences on approaches to Sunday Creek Bridge	..	·25
BROADMEADOWS SHIRE— Sydney Road .. ..	.. .. .	Between Coburg boundary and Campbellfield	..	·5
Lancefield Road .. ..	.. .. .	Between Essendon boundary and Tullamarine	..	1·5
BULLA SHIRE— Melbourne-Lancefield Road .. ..	.. .. .	Between Tullamarine and Bulla surfaced with bitumen (double coat)	..	4·75
Sunbury Road .. ..	.. .. .	From Bulla to Clarkefield .. ..	..	10·25
.. .. .	.. .. .	From junction with Melbourne-Lancefield Road to Sunbury Township	..	2·75
BULN BULN SHIRE— Prince's Highway—Town section .. ..	.. .. .	Drouin Township, Town boundary to Town boundary	·5	1
Neerim "A" Road .. ..	.. .. .	From Drouin to Rokeby .. ..	..	6
Neerim "C" Road .. ..	From junction with metal on western boundary of Crown Allotment 98, Parish of Neerim, thence south-easterly	From Rokeby to Neerim South Railway Station	1·06	4
Neerim "B" Road .. ..	.. .. .	From Neerim South to northern side of Allotment 4A, Parish of Neerim	..	9
Bloomfield Road .. ..	.. .. .	From Shire boundary to crossover Railway Station	..	1·5
Neerim East Road .. ..	.. .. .	From junction with Neerim "B" Road at Neerim to south-western corner of Allotment 36A, Parish of Neerim	..	4
Fumina Road .. ..	Parts of Sections 4, 5, 6, and parts of Sections 2, 3, and 7—Metalled 1'37 and Re-formed 2'16 miles	From Noojee to Duggan Bridge .. ..	3·53	·11
Main South Road .. ..	.. .. .	From Drouin Township to southern Shire boundary	..	7
Drouin-Longwarry (old Prince's Highway) .. ..	.. .. .	From Drouin to Bunyip River .. ..	..	3·25
Westernport Road .. ..	.. .. .	From junction with Main South Road to Shire boundary	..	4
BUNGAREE SHIRE— Ballarat-Creswick Road .. ..	.. .. .	General maintenance .. .. .	..	..
Ballarat-Maryborough Road .. ..	.. .. .	.. .. .	..	..
BUNINYONG SHIRE— Geelong-Ballarat Road .. ..	.. .. .	Sheeting with gravel between Ballarat and Buninyong	..	·7
.. .. .	.. .. .	Repairing pot holes between Ballarat and Buninyong	..	4·5
.. .. .	.. .. .	Repairing pot holes between Buninyong and Clarendon	..	3·73
.. .. .	.. .. .	Repairing pot holes between Clarendon and Elaine	..	3·25
.. .. .	.. .. .	Repairing pot holes between Elaine and Meredith	..	2
Ballarat-Rokewood Road .. ..	.. .. .	Repairing pot holes and small repairs to culverts and bridges between Napoleon's and Enfield, small lengths sheeted with gravel	..	12·75
CARRUM BOROUGH— Point Nepean Road .. ..	.. .. .	General maintenance, including surface spraying with bitumen for entire length	..	5·7
CASTLEMAINE BOROUGH— Melbourne-Bendigo Road .. ..	.. .. .	Chainage 384,450 to 385,700 re-sheeted with metal	..	·23
.. .. .	.. .. .	Chainage 388,400 to 394,875 tar-sprayed .. ..	..	1·2
CHARLTON SHIRE— St. Arnaud Road .. ..	.. .. .	20,200 feet—22,000 feet gravelling, in Parish of East Charlton	..	·34
.. .. .	.. .. .	General maintenance .. .. .	..	15
Donald Road .. ..	.. .. .	14,700 feet—19,200 feet of metalling .. ..	..	·85
.. .. .	.. .. .	General maintenance .. .. .	..	11
CHILTERN SHIRE— Sydney Road .. ..	.. .. .	Gravelling and re-tarring .. .. .	..	1·95
Chiltern-Howlong Road .. ..	.. .. .	General maintenance and re-tarring .. ..	..	1·5
Rutherglen-Wodonga Road .. ..	.. .. .	General maintenance .. .. .	..	3·25
CLUNES BOROUGH— Maryborough-Ballarat Road .. ..	.. .. .	Re-sheeting and general maintenance .. ..	..	·32
COLAC SHIRE— Colac-Beech Forest Road .. ..	Between Colac and Kawarren— Forming and grading .. .. Forming and gravelling .. ..	South from Colac Town boundary—Base oil and bitumen painted .. .. .. .. .	6·72 2·84	1·25 ..
Colac-Ballarat Road .. ..	.. .. .	From Cressy towards Beac—Base oil and bitumen painted. General maintenance throughout Cressy and Shire boundary—Waterbound macadam and repairs to culverts	..	1·75
Cressy-Inverleigh Road .. ..	.. .. .	.. .. .	..	6
		Carried forward .. ..	37·60	644·28



STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>				
		Brought forward .. ..	37·60	644·28
CORIO SHIRE—				
Ballarat Road .. ..	.. ..	Geelong to Batesford—General maintenance .. ..	..	4·5
Geelong-Bacchus Marsh .. ..	.. ..	Melbourne Road to Lara—General maintenance .. ..	..	7·62
Fyansford Road .. ..	.. ..	At Fyansford—General maintenance .. ..	..	·16
CRANBOURNE SHIRE—				
Main Coast Road .. ..	Formation and gravelling, Pearson's Hill .. ..	General maintenance .. ..	1·75	38
Koo-wee-rup-Pakenham Road .. ..	.. ..	.. ..	..	5·5
Westernport Road .. ..	.. ..	.. ..	..	9
Lang Lang-Nyora Road .. ..	.. ..	.. ..	..	4·17
CRESWICK BOROUGH—				
Ballarat-Castlemaine Road .. ..	.. ..	Scarifying, gravelling, tarring, and patrolling .. ..	..	·75
CRESWICK SHIRE—				
Ballarat-Castlemaine Road .. ..	.. ..	General maintenance .. ..	..	21
Daylesford-Ballarat Road .. ..	.. ..	General maintenance .. ..	..	12·37
DANDENONG SHIRE—				
Cheltenham Road .. ..	.. ..	Between Prince's Highway and west boundary road .. ..	..	6
Frankston Road .. ..	.. ..	Between Prince's Highway and south-eastern corner of Allotment 58, Parish of Lyndhurst .. ..	..	6·37
DAYLESFORD BOROUGH—				
Daylesford-Ballarat Road .. ..	.. ..	Re-conditioning Leggatt's Bridge .. ..	..	..
Daylesford-Hepburn Road .. ..	.. ..	Re-sheeting .. ..	..	·1
Daylesford-Mainsbury Road .. ..	.. ..	.. ..	..	·1
DEAKIN SHIRE—				
Kyabram-Tongala Road .. ..	.. ..	Forming and gravelling to west of Hotel .. ..	..	·38
Kyabram-Rochester Road .. ..	.. ..	Forming and gravelling at Morrison's .. ..	..	·3
(joint with Rodney Shire)	.. ..	Forming and gravelling on Rushworth Road .. ..	..	·5
Kyabram-Nathalia Road .. ..	.. ..	Forming and gravelling on North and South Road at Wyuna Road .. ..	..	1
DIMBOOLA SHIRE—				
Warracknabeal Road .. ..	Near Woolshed School, about 5 miles north-east of Dimboola .. ..	General maintenance between Dimboola and Woolshed School .. ..	1·25	..
Rainbow Road .. ..	Three sections between Jeparit and Ellam .. ..	Forming and metalling between Jeparit and Antwerp .. ..	·55	·32
Hopetoun-Rainbow Road .. ..	.. ..	Re-sheeting sections of metal between Rainbow and about 3 miles north .. ..	..	·44
DONALD SHIRE—				
Donald-Charlton Road .. ..	Partial construction commencing 6 miles from Donald .. ..	General maintenance .. ..	2·59	10
St. Arnaud-Birchip Road .. ..	.. ..	General maintenance and re-binding bluestone sections north and south of Donald .. ..	..	12·5
Donald-Minyip Road .. ..	.. ..	Re-binding bluestone sections extending over 2 miles from Donald .. ..	..	·15
DONCASTER AND TEMPLESTOWE SHIRE—				
Warrandyte-Ringwood Road .. ..	.. ..	General maintenance .. ..	..	3
Heidelberg Road .. ..	.. ..	.. ..	..	9
Doncaster Road .. ..	.. ..	.. ..	..	6
DUNDAS SHIRE—				
Hamilton-Port Fairy Road .. ..	.. ..	At from 4½ to 5½, 11½ to 12½, 17 to 17½, 18 to 18½, and 19½ to 20 miles from Hamilton, Parishes of Monivae, Byadue, and Warrakook .. ..	..	2·85
Hamilton-Dunkeld Road .. ..	At from 2½ to 3½ and 12½ to 14 miles from Hamilton, Parishes of South Hamilton and Warrayure .. ..	At from 5½ to 6, 6½ to 6¾, 10¾, to 11¾, and 15 to 15½ miles from Hamilton, Parishes of South Hamilton, Warrayure, and Moutajup .. ..	1·86	2·43
Hamilton-Warrnambool Road .. ..	.. ..	At from 3 to 3½, 5½ to 6½, and 8 to 8½ miles from Hamilton, Parishes of South Hamilton and Croxton West .. ..	..	1·07
Hamilton-Horsham Road .. ..	At from 23½ to 24½ and 26 to 27 miles from Hamilton, Parish of Geerak .. ..	At from 4 to 5, 14 to 14½, and 18½ to 19½ miles from Hamilton, Parishes of North Hamilton, Jerrywarook, Cavendish, and Mooralla .. ..	·98	2·63
Hamilton-Mount Gambier Road .. ..	.. ..	At from 7½ to 8½ miles from Hamilton, Parish of Bochara .. ..	..	·54
DUNMUNKLE SHIRE—				
Stawell-Warracknabeal Road .. ..	Extension of metal south-easterly from Minyip .. ..	Bitumen top dressing, commencing at railway crossing and extending southerly .. ..	1·04	·5
.. ..	.. ..	Forming and metalling near Dyer's (adjoining Lot 138, Parish of Marma) .. ..	..	·35
Minyip-Donald Road .. ..	Extension of existing metal north-easterly from Minyip .. ..	.. ..	·76	..
Rupanyup-Murtoa Road .. ..	.. ..	Bitumen top dressing, commencing at Township and extending westerly .. ..	..	1·6
EAST LODDON SHIRE—				
Prairie Road .. ..	Forming and gravelling, Parish of Talambo .. ..	.. ..	·66	..
ECHUCA BOROUGH—				
Echuca West Road .. ..	.. ..	General maintenance .. ..	..	1·6
Echuca-Cornella Road .. ..	.. ..	.. ..	..	·6
ELTHAM SHIRE—				
Hurstbridge-Kinglake Road .. ..	From centre of Hurstbridge township southwards towards Melbourne to Devil's Elbow .. ..	Timber bridge erected over Black Calf Gully; general maintenance from Wattle Glen to Kinglake .. ..	·65	2
Eltham-Yarra Glen Road .. ..	.. ..	60 chains re-sheeting in Eltham township; Research to Kangaroo Ground through Yarra Glen township .. ..	..	2·75
EUROA SHIRE—				
Euroa-Arcadia Road .. ..	Formation and gravelling between Euroa and Miepoll .. ..	General maintenance .. ..	1·45	13
Euroa-Strathbogge Road .. ..	Formation and gravelling near Dark's .. ..	.. ..	·23	15
Euroa-Mansfield Road .. ..	.. ..	.. ..	..	18
Arcadia .. ..	.. ..	.. ..	..	5
Sydney Road .. ..	.. ..	General maintenance and repairs to Seven Creeks Bridge .. ..	..	2
FERNTREE GULLY SHIRE—				
Main Ferntree Gully Road .. ..	Reinforced concrete culvert over Ferny Creek and approaches between Upwey and Belgrave .. ..	Wheeler's Hill to Belgrave .. ..	·05 and culvert	10·81
Monbulk Road .. ..	.. ..	Belgrave to Monbulk .. ..	..	5
Olinda Road .. ..	.. ..	Upper Ferntree Gully to Olinda .. ..	..	6·25
Emerald Road .. ..	.. ..	Emerald township to Avonsleigh .. ..	..	1·63
Belgrave-Emerald Road .. ..	.. ..	Belgrave to Emerald .. ..	..	6·73
FLINDERS SHIRE—				
Dromana-Sorrento Road .. ..	Rye to Sorrento and at the rocks .. ..	Near the rocks .. ..	2·88	..
Hastings-Flinders Road .. ..	.. ..	Near Warrandyte Creek .. ..	..	·78
		Carried forward .. ..	54·30	907·63

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<i>UNDER MUNICIPALITIES—continued.</i>				
		Brought forward .. ..	54·30	907·63
FRANKSTON AND HASTINGS SHIRE— Point Nepean .. ..		Widening from 3 feet to 6 feet, mostly bitumen penetrated; bitumen spraying and painting; patching and general maintenance		16·5
Frankston-Flinders Road .. ..		Spraying and painting, painting and general maintenance		17
Frankston-Dandenong Road .. ..		Spraying and painting (bitumen), and patching and general maintenance		7·5
Frankston-Dandenong Road (joint with Cranbourne Shire)		Sheeting and bitumen penetration) .. ..		1·75
GISBORNE SHIRE— Gisborne-Bacchus Marsh Road .. ..		Generally throughout .. ..		10·18
Gisborne Station Road .. ..		" " .. ..		1·2
GLENELG SHIRE— Wando Vale Road .. ..		Metal sheeting between 8th and 9th mile posts .. ..		·37
Dergholm Road .. ..		Metal sheeting between town and 3rd mile post .. ..		·89
Coleraine-Casterton Road .. ..		Metal sheeting at Bella Vista and near Greenhans' .. ..		·5
Portland-Casterton Road .. ..	Construction of new bridge at Grill's	Metal sheeting near Sandford and between Casterton and Sandford	Construction of bridge .. ..	1·12
Mount Gambier Road .. ..	Forming and metalling between Ardno and South Australian border, and in two sections between Wilderness Church and Penny's		2·57	
GLENLYON SHIRE— Ballan Road .. ..		General maintenance .. ..		6
Ballarat Road .. ..		" " .. ..		3·5
Castlemaine Road .. ..		" " .. ..		14
Hepburn Road .. ..		" " .. ..		1·5
Malmsbury Road .. ..		" " .. ..		15
GOULBURN SHIRE— Goulburn Valley Road .. ..	North of Nagambie .. ..	North and south of Nagambie .. ..	1·3	12
Avenel-Longwood Road .. ..	Between Locksley and Monea .. ..	Near Monea and Locksley .. ..	1·2	8
GRENVILLE SHIRE— Ballarat-Hamilton Road .. ..		General maintenance .. ..		24
Pitfield Road .. ..		" " .. ..		12
Cressy Road .. ..	Metalling near McLennan's .. ..	" " .. ..	·77	9·25
Lismore Road .. ..		" " .. ..		10
HAMILTON BOROUGH— Coleraine Road .. ..		General maintenance in two sections .. ..		·18
Port Fairy Road .. ..		General maintenance near boundary of shire .. ..		·08
Hamilton - Warrnambool Road (joint with Dundas Shire)		General maintenance in two sections .. ..		·12
HAMPDEN SHIRE— Camperdown-Ballarat Road .. ..		General maintenance .. ..		54
Lismore-Cressy Road .. ..		" " .. ..		17
Terang-Mortlake Road .. ..		Re-sheeting and spraying between Terang and Noorat, and general maintenance		7
Caramut-Lismore Road .. ..		General maintenance .. ..		11
HEALESVILLE SHIRE— St. Fillans-Marysville Road .. ..	Cemetery Hill deviation at Marysville	From St. Fillans to Marysville .. ..	1·25	5
Healesville-Alexandra Road .. ..		From shire boundary (Yarra Flats) to Buxton .. ..		19
Healesville-Woori Yallock .. ..		From Alexandra-Healesville Road to Badger's Creek—Dalry P.R. to Allotment 12A, Parish of Gracedale		3·5
HEIDELBERG SHIRE— Heidelberg-Eltham Road .. ..		Road widened to 20 feet with metal penetrated with bitumen		2·11
Greensborough-Hurstbridge Road .. ..		Road surfaced with oil and bitumen 18 feet wide		5·25
Main Whittlesea Road .. ..		Road sheeted with metal .. ..		·25
Heidelberg-Warrandyte Road .. ..		Road surfaced with bitumen .. ..		·47
HEYTESBURY SHIRE— Camperdown-Cobden Road .. ..		Re-sheeting 1·97 miles and repairing balance .. ..		4·84
Cobden - Port Campbell - Princetown Road .. ..	Forming and loaming by Cowley's Creek School, and at Currell's and Magilton's	Re-sheeting and gravelling in detached portions and general maintenance	1·16	30
HORSHAM BOROUGH— Natimuk Road .. ..	Forming and metalling third section adjoining Wimmera Shire	Patching second section .. ..	·5	·5
Dimboola Road .. ..		Scarifying and re-sheeting 17 chains from Railway to High School		·21
Dooen Road .. ..		Widening and re-sheeting Baillie Street; painting with mixture-tar and bitumen, and general maintenance		1·2
Hamilton Road .. ..		Patching and painting with mixture, tar, and bitumen		·27
HUNTLY SHIRE— Bendigo-Echuca Road .. ..	At Epsom and between Huntly and Elmore	General maintenance .. ..	1·25	14
INGLEWOOD BOROUGH— Bendigo-Charlton Road .. ..		At Township of Inglewood .. ..		·12
KARA KARA SHIRE— Avoca-St. Arnaud Road .. ..	Between Stuart Mill and Redbank	Between St. Arnaud and Redbank .. ..	3·8	20
Navarre Road .. ..		Between St. Arnaud and Navarre .. ..		20
St. Arnaud-Donald Road .. ..	From 20,800 to 23,300 and from 8,000 to 14,000	From boundary between Shire and Borough of St. Arnaud and from Cope Cope to boundary	1·6	3·7
Charlton Road .. ..	Between Slaty Creek and Coonoer Bridge	Between St. Arnaud and Coonoer Bridge .. ..	2·37	11
KARKAROO SHIRE— Hopetoun-Warracknabeal Road .. ..	Near Goyura Railway Station .. ..		·82	..
Rainbow-Beulah-Birchip Road .. ..	West of Beulah and west of Kennmare		3·51	..
Hopetoun-Rainbow Road .. ..	6 miles from Hopetoun and 9 miles from Hopetoun		1·56	..
Hopetoun - Woomelang - Sealake Road .. ..		12 miles from Hopetoun .. ..		·37
KELLOR SHIRE— Bendigo Road .. ..		Between Essendon boundary and commencement of State Highway		·56
KERANG SHIRE— Kondrook Road .. ..		At Kondrook .. ..		·99
KILMORE SHIRE— Sydney Road .. ..		Gravelling surface in Town of Kilmore		·25
Heathcote Road .. ..		Construction of three culverts .. ..		..
		Carried forward .. ..	77·96	1812·86

## STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Work Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—continued.				
		Brought forward .. ..	77·96	1312·86
KORONG SHIRE—				
Bendigo-Serpentine Road .. ..	.. ..	General maintenance .. ..	..	..
Borong-Hurstwood Road .. ..	.. ..	Gravelling and general maintenance .. ..	..	7
Bendigo-Charlton Road .. ..	Wedderburn Township .. ..	Gravelling south from High Street and general maintenance .. ..	..	15
KOROIT BOROUGH—				
Koroit-Warrnambool Road .. ..	.. ..	Re-sheeting from Prince's Highway to Belfast Shire .. ..	..	1
" " " " .. ..	.. ..	Tar painting from Prince's Highway to Koroit, east end Commercial Road .. ..	..	3·75
KORUMBURRA SHIRE—				
Korumburra-Wacragul Road .. ..	At Reservoir Hill .. ..	General maintenance .. ..	1	13
Korumburra-Leongatha Road .. ..	.. ..	.. ..	..	4·84
Korumburra-Drouin Road .. ..	.. ..	.. ..	..	4·64
Poowong-Nyora Road .. ..	.. ..	.. ..	..	5·99
Korumburra-Wonthaggi Road .. ..	.. ..	.. ..	..	13·25
Bena-Poowong Road .. ..	.. ..	.. ..	..	6·01
Loch-Wonthaggi Road .. ..	.. ..	.. ..	..	4·64
Lang-Lang-Nyora Road .. ..	.. ..	.. ..	..	5
KOWREE SHIRE—				
Hamilton - Edenhope - Apsley Road .. ..	At Harrow and Harrow Hill .. ..	At Harrow, 85th mile post, Boikerbert, near S.A. border, and Boikerbert Newland's, .. ..	14	2·26
Edenhope-Goroke Road .. ..	At Church Lane, Guthridges and Ullswater and Mitchells, Parishes of Karnak and Awonga .. ..	At Kangawall School .. ..	75	1·5
Booroopki Road .. ..	.. ..	At Allotment 5, Parish of Morea, and Pleasant Bank, Parish of Mortat .. ..	..	1·16
KYNETON SHIRE—				
North-Western Highway .. ..	.. ..	Re-sheeting and surfacing with bitumen .. ..	..	·88
Melbourne-Bendigo Road .. ..	.. ..	Re-sheeting and surfacing in Township of Kyneton .. ..	..	·88
Trentham Road .. ..	.. ..	Re-sheeting .. ..	..	·3
LAWLOIT SHIRE—				
South Lillimur Road .. ..	Metalling between Kaniva and South Lillimur .. ..	General maintenance .. ..	75	1·75
Broughton Road .. ..	.. ..	.. ..	..	1·5
Yearlinga Road .. ..	Metalling between Kaniva and Yearlinga .. ..	.. ..	15	2
Nhill-Kaniva-Border Road .. ..	.. ..	Tarring, &c. .. ..	..	·5
LEIGH SHIRE—				
Bannockburn-Shelford Road .. ..	.. ..	Between Teesdale and Shelford .. ..	..	4
Shelford-Inverleigh Road .. ..	.. ..	From Shelford to 3 miles south .. ..	..	3
Shelford-Rokewood Road .. ..	.. ..	From Shelford to Rokewood .. ..	..	17·5
Rokewood-Cressy Road .. ..	.. ..	Between Rokewood Railway Station and Werneth Railway Station .. ..	..	4·5
Rokewood-Cressy Road (Joint with Colac Shire) .. ..	.. ..	Eastward from Cressy Railway Station .. ..	..	1
Werneth Road .. ..	Northward from Werneth Post Office .. ..	Eastward from Werneth Post Office to Werneth Railway Station .. ..	38	2
Cressy-Lismore Road (Joint with Colac Shire) .. ..	.. ..	Cressy Railway Station to bridge over Woody Yallock River .. ..	..	1
Cressy-Shelford Road (Joint with Colac Shire) .. ..	.. ..	From junction with Cressy-Rokewood Road to junction with Cressy-Inverleigh Road .. ..	..	1
Ballarat-Rokewood Road .. ..	.. ..	Between Rokewood and Buninyong Shire boundary .. ..	..	4·75
Cressy-Inverleigh Road .. ..	.. ..	From junction with Shelford-Inverleigh Road westward to Colac Shire boundary .. ..	..	5·8
LEXTON SHIRE—				
Avoca-Ararat Road .. ..	.. ..	Gravelling 1 mile south from Amphitheatre .. ..	..	1
Avoca-Ballarat Road .. ..	.. ..	Metalling 40 chains between Lexton and Waubra .. ..	..	·5
LILYDALE SHIRE—				
Main Healesville Road .. ..	.. ..	General maintenance .. ..	..	17
Main Warburton Road .. ..	.. ..	.. ..	..	8·5
Yarra Glen Road .. ..	.. ..	.. ..	..	4
Monbulk Road .. ..	.. ..	.. ..	..	7
Lilydale-Evelyn Road .. ..	Swansea Road to Hereford Road .. ..	Near Lilydale .. ..	1·64	1
Mount Dandenong Road .. ..	Keogh's Corner to Log Cabin .. ..	Croyden to Keogh's Corner .. ..	2·05	8·25
LOWAN SHIRE—				
Goroke Road .. ..	.. ..	Between Allotments 26 and 24, Parish of Wujiam .. ..	..	·22
Lorquon West Road .. ..	Between Allotments 6 and 8-9, &c., Parish of Lorquon .. ..	Between Allotments 73 and 134, &c., Parish of Woorak and between Allotments 134-137 and 69, &c., Parish of Woorak .. ..	44	1·26
Yanac Road .. ..	.. ..	Between Allotments 10 and 68, Parish of Yanac .. ..	..	·26
MAFFRA SHIRE—				
Maffra-Sale Road .. ..	.. ..	From Sale Road gates towards Sale .. ..	..	3·1
Licola Road .. ..	.. ..	Near Malphy's .. ..	..	·5
Tinamba-Boisdale Road .. ..	.. ..	Near Mewburn Park .. ..	..	1·25
Bushy Park-Valencia Creek Road .. ..	.. ..	Between Township and gates .. ..	..	·37
.. ..	.. ..	Near Smythe's .. ..	..	·4
MALDON SHIRE—				
Castlemaine-Maldon Road .. ..	.. ..	From Castlemaine boundary to Maldon, repaired with best gravel available .. ..	..	10
Maldon-Eddington Road .. ..	.. ..	General maintenance from Maldon to Eddington Bridge .. ..	..	16
Baringhup Road .. ..	.. ..	Gravelling at Moolort with gravel from Havilah mine and general maintenance .. ..	..	6·5
Newstead Road .. ..	.. ..	General maintenance from junction with Castlemaine-Maldon Road at various points to Newstead Shire boundary .. ..	..	5
Castlemaine-Newstead Road .. ..	.. ..	General maintenance .. ..	..	·5
MANSFIELD SHIRE—				
Mansfield Road .. ..	Two sections metalling 5 to 10 miles easterly .. ..	General maintenance .. ..	1·0	36
" " " " .. ..	Concrete culvert at 11½ miles east and approaches .. ..	.. ..	1	..
Mansfield-Woods Point Road .. ..	.. ..	General maintenance .. ..	..	20
Tolmie Road .. ..	.. ..	.. ..	..	6
Merton-Euroa Road .. ..	.. ..	.. ..	..	5
MARONG SHIRE—				
Bendigo-Eddington Road .. ..	3,500 feet forming and metalling at Shelbourne West .. ..	3,800 feet re-sheeting with gravel at Wailaby Junction .. ..	7	72
Bendigo-Serpentine Road .. ..	.. ..	General maintenance .. ..	..	25
.. ..	.. ..	Patching and re-sheeting by day labour .. ..	..	9
MARYBOROUGH BOROUGH—				
Avoca Road .. ..	.. ..	General maintenance .. ..	..	·25
Ballarat Road .. ..	.. ..	.. ..	..	·15
Castlemaine Road .. ..	.. ..	.. ..	..	·2
Eddington Road .. ..	.. ..	.. ..	..	·1
		Carried forward .. ..	87·06	1633·69

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Work Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<i>UNDER MUNICIPALITIES—continued.</i>				
		Brought forward .. ..	87'06	1633'69
MELIVOR SHIRE— Kilmore-Heathcote-Bendigo Road	Forming, gravelling, and constructing culverts at Knowsley and forming, grading and gravelling at Tooborac	.. .. .	1'49	..
Heathcote-Elmore Road	Erecting timber bridge at Crosbie	.. .. .	Timber bridge	..
MELTON SHIRE— Melton-Toolern Road	.. .. .	From Melton to Toolern .. .. .	..	6
METCALFE SHIRE— Kyneton-Redesdale Road	.. .. .	Metalling between Langley and Redesdale, repairs to bridge over Campaspe River at Barfold, repairs to culverts near Barfold, and painting bridge over Campaspe River at Redesdale	..	8
MILDURA TOWN— Deakin Avenue	.. .. .	Bitumen surfacing .. .. .	..	1
Tenth Street	Previously constructed macadam road surfaced with bitumen only	.. .. .	1	..
Langtree Avenue	.. .. .	Bitumen surfacing .. .. .	..	43
Punt Road	Previously constructed macadam road surfaced with bitumen only	.. .. .	51	..
MILDURA SHIRE— Wentworth Road	17th Street, Riverside to Ontario Avenues	.. .. .	2	..
Deakin Avenue	.. .. .	14th to 15th Streets .. .. .	..	6
Melbourne Road	Yatpool boundary to Sunny Cliffs and opposite Red Cliffs Township	.. .. .	4	..
MINHAMITE SHIRE— Warrnambool-Hawkesdale - Penshurst Road	.. .. .	At Warrong, '17—At Stonefield, '21 ..	..	38
Hamilton-Macarthur-Port Fairy Road	.. .. .	South from north boundary of Shire .. '75 At Macarthur .. .. '19 At Broadwater .. .. '69 North from South boundary of Shire .. '49	..	2'12
MIRBOO SHIRE— Mardan Road	Webb's to J. Watt's .. .. .	General maintenance .. .. .	4	5
Mirboo South Road	.. .. .	.. .. .	..	9
Allambee East Road	.. .. .	.. .. .	..	6
Leongatha-Mirboo Road	.. .. .	.. .. .	..	4
MOORABBIN SHIRE— Centre Dandenong Road	.. .. .	Point Nepean Road to Boundary Road patched and sprayed with bitumen and oil	..	2'94
Point Nepean Road	.. .. .	South Road, Moorabbin, to Oak Grove, patching and portions sprayed with 60/70 bitumen	..	3'19
MORDIALLOC CITY— Point Nepean Road	.. .. .	General maintenance .. .. .	..	3
MORNINGTON SHIRE— Point Nepean Road	.. .. .	Widening out road to 20 feet and laying cement concrete kerbs on each side Frank-ton boundary end and near Bungower Road	..	1'23
MORTLAKE SHIRE— Mortlake-Tarang Road	.. .. .	Sheeting with metal .. .. .	..	2'65
.. .. .	.. .. .	Two-coat bitumen work .. .. .	..	2'69
Mortlake-Ararat Road	.. .. .	Sheeting with metal .. .. .	..	2'21
.. .. .	.. .. .	Sheeting with gravel .. .. .	..	1
Mortlake-Warrnambool Road	.. .. .	Sheeting with metal .. .. .	..	2'35
Caramut-Lismore Road	.. .. .	.. .. .	..	8'72
MORWELL SHIRE— Prince's Highway East	.. .. .	Metalling and general maintenance of Township section	..	1'66
Jeeralang West Road	.. .. .	Scarifying sand road from Prince's Highway, dressing, re-forming and general maintenance	..	17'7
Boolarra-Foster Road	.. .. .	General maintenance .. .. .	..	5
Boolarra-Welshpool Road	Metalling Walsh's Flat near Boolarra	.. .. .	46	7
MOUNT ROUSE SHIRE— Ballarat-Hamilton Road	.. .. .	Between Dunkeld and 6 miles east of Glen-thompson	..	1'6
.. .. .	.. .. .	Between Dunkeld and 3-mile post .. .. .	..	31
.. .. .	.. .. .	Between 1-mile post on the south to 7-mile post on the north	..	1'35
.. .. .	.. .. .	Between Penshurst and the 10-mile post ..	..	88
MULGRAVE SHIRE— Ferntree Gully Road	.. .. .	Tar painting whole length of road and general maintenance	..	5
NARRACAN SHIRE— Walhalla Road	.. .. .	From Trafalgar northerly .. .. .	..	18
.. .. .	.. .. .	From Yarragon .. .. .	..	3
.. .. .	.. .. .	From Yarragon southerly .. .. .	..	10
.. .. .	.. .. .	From Yarragon southerly and from Trafalgar ..	..	12
NEWHAM AND WOODEND SHIRE— Tylden (joint Kyneton Shire)	Metalling .. .. .	General maintenance .. .. .	5	1
Tylden Road	.. .. .	.. .. .	..	3
Lancaster Road	.. .. .	.. .. .	..	9
Melbourne-Bendigo Road	.. .. .	.. .. .	..	1
NEWSTEAD AND MOUNT ALEXANDER SHIRE— Castlemaine-Daylesford Road	.. .. .	Gravelling at Campbell's Creek, Yapeen, and Guildford, and general maintenance	..	7
Maldon Road	.. .. .	General maintenance from Newstead to Welsh-man's Reef	..	2
Creswick Road	.. .. .	Gravelling from Campbelltown to Newstead and general maintenance	..	11
Castlemaine-Maryborough Road	.. .. .	Gravelling Muckleford, Newstead, and general maintenance	..	10
NUMURKAH SHIRE— Shepparton-Numurkah-Cobram Road	Metalling at Wingham .. .. .	Gravelling east and west road, South Numurkah, and general maintenance	88	55
Echuca-Picola Road	.. .. .	Gravelling south from Ross's corner and general maintenance	..	97
Murray Valley	Gravelling, &c., west of Strathmerton	General maintenance .. .. .	9	..
Nathalia-Kyabram Road	.. .. .	Forming near Katupna School and general maintenance	..	1'5
.. .. .	.. .. .	Embankment near O'Hanlon's .. .. .	..	18
Nathalia-Kyabram Road	.. .. .	Gravelling south of Dillon's Lane .. .. .	..	38
Nathalia-Picola Road	.. .. .	Gravelling north from railway line and general maintenance	..	1'14
.. .. .	.. .. .	Gravelling near Mordon's .. .. .	..	17
		Carried forward .. ..	100'10	1838'59

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES— <i>continued.</i>				
		Brought forward .. ..	100·10	1838·59
OAKLEIGH BOROUGH—				
Ferntree Gully Road .. ..		Bitumen surface painting of balance of road ..		·26
Prince's Highway .. ..		Bitumen surface painting Warrigal Road to Box Hill Road ..		·83
OMEQ SHIRE—				
Benambra Road .. ..	Race-course section .. ..	Between Benambra township and Sugarloaf section ..	1·2	3
ORBOST SHIRE—				
Prince's Highway (Township section)	Metalling in Salisbury Street .. ..	From Snowy River Bridge to eastern boundary of Orbost township ..	·33	1·32
Marlo Road .. ..		From Snowy River bridge to Conlon's ..		9
Cann Valley Road .. ..	Gravelling from Balzer's to Broom's .. ..	From Prince's Highway to border line ..	2·95	29
Genoa-Gipsy Point Road .. ..		From Prince's Highway to Mattson's Jetty ..		7
OTWAY SHIRE—				
Beech Forest-Apollo Bay Road .. ..		Between Beech Forest and forks of Barham ..		15
Forrest-Apollo Bay Road .. ..		Between Forrest and Barramunga ..		4
" .. ..		Between Skene's Creek and Apollo Bay ..		2
" .. ..		Formation between Tanybryn and coast ..		8
Cobden - Port Campbell - Princetown Road .. ..		General maintenance .. ..		6
OXLEY SHIRE—				
Bright Road .. ..	Oxley Flats and Henley's Gap—Whorouly sections .. ..	Wangaratta to Rocky Point .. ..	1·94	25
Oxley Road .. ..		Wangaratta boundary to Oxley bridges ..		7
Greta-Glenrowan Road .. ..		From Glenrowan at boundary of Benalla Shire to Greta West ..		10
PHILIP ISLAND AND WOOLAMAI SHIRE—				
Korumburra-Wonthaggi Road .. ..		General maintenance .. ..		3
Wonthaggi-Loch Road .. ..	Near Brown's, Archie's Creek, and Ryanston, and construction of bridge and floodway at Powlett River .. ..		2·23	16·8
Main Coast Road .. ..	Between Grantville and Glen Forbes Road, and in three sections near Grantville ..		4·03	19
Dalyston-Wonthaggi Road .. ..	Timber bridge and approaches, Powlett River .. ..		·13	1·75
Almurta River .. ..				5·2
Wonthaggi-Inverloch River .. ..				4
PORT FAIRY BOROUGH—				
Prince's Highway .. ..		Bitumen spraying, west of Moyne River ..		3
" .. ..		Metal sheeting .. ..		·04
" .. ..		Metal and screenings stacked on road for bitumen penetration ..		·5
PORTLAND SHIRE—				
Portland-Hamilton Road .. ..	Between Condah and Branxholme .. ..	From Portland to Bolwarra and Heywood to Branxholme ..	1·14	25·66
Portland-Casterton Road .. ..	North of Hotspur .. ..	From Drumborg to Digby ..	1·12	18·02
Heath Road .. ..		From Portland Borough toward Cashmore ..		3
PRESTON CITY—				
Epping Road .. ..		General maintenance and bitumen spraying whole length ..		1·45
Whittlesea Road .. ..		General maintenance .. ..		2·75
PYALONG SHIRE—				
Kilmore-Heatcote-Bendigo Road .. ..		General maintenance .. ..		11
QUEENSLIFF BOROUGH—				
Queenscliff-Geelong Road .. ..		General maintenance, re-surfacing and tar painting (2-coat work) ..		
RINGWOOD BOROUGH—				
Main Healesville Road .. ..		From Heatherdale Road to Burnt Bridge ..		3·25
Mount Dandenong Road .. ..		From White Horse Road to Insulation Works ..		1·5
Warrandyte Road .. ..		From White Horse Road to Wonga Road and Wonga Road to Tortoise's Hill ..		2·33
RIPON SHIRE—				
Ballarat-Ararat Road .. ..		In Beaufort Township .. ..		1·37
Ballarat-Hamilton Road .. ..		Re-sheeting and general maintenance ..		16·1
Beaufort-Skipton Road .. ..		Re-sheeting and general maintenance ..		18·
ROCHESTER SHIRE—				
Rochester-Bamawm-Prairie Road .. ..	Between Allotments 67A, 67B, 26, and 3, 4, Parish of Ballendella, & between Allotments 149, 150, 151, 154, and 122B, 128, 129, Parish of Bamawm, and from western boundary of Allotment 166 to western boundary of Allotment 152B, Parish of Pannooobamawm .. ..	1 mile south and 1½ miles east from Township of Lockington ..	5·13	2·5
Rochester-Timmering Road .. ..		Through Rochester Township and easterly from Campaspe Bridge ..	1·01	
Bendigo-Echuca Road .. ..		Through Rochester Township and 1 mile southerly from Township boundary ..		1·41
RODNEY SHIRE—				
Kyabram-Nathalia Road .. ..		General maintenance .. ..		1
Kyabram-Tongala Road .. ..		·16 mile bitumen penetration and patching ..		1
Mooroopna-Undera Road .. ..	Metalling between Allotments 96, 71, 24, 38, and 55, Parish of Mooroopna .. ..	Re-sheeting and patching .. ..	1·7	4
Shepparton-Tatura Road .. ..				5
Tatura-Byrneside-Kyabram Road .. ..	Metalling between Allotments 119 and 116, Parish of Toolamba West .. ..	Bitumen penetration and general maintenance ..	·12	10·48
Tatura-Murchison Road .. ..		Bitumen painting and general maintenance ..		10·15
Shepparton-Tatura (Joint Shepparton Shire) .. ..		Bitumen penetration and general maintenance ..		2·11
ROMSEY SHIRE—				
Melbourne-Lancefield Road .. ..		General maintenance .. ..		15·85
Woodend-Lancefield Road .. ..		" .. ..		5·62
Lancefield-Kilmore Road .. ..		" .. ..		9·71
Lancefield-Kilmore Road (Joint Kilmore Shire) .. ..		" .. ..		2·25
ROSEDALE SHIRE—				
Sale-Yarram Road .. ..	Southwards from Merriman's Creek ..	General maintenance, sheeting and drainage ..	·64	13
Seaspray Road .. ..	In various sections over 8 miles of road ..	" .. ..	5·61	8·25
" .. ..	Through Prospect Estate .. ..	" .. ..	1·89	
Traralgon-Gormandale Road .. ..		General maintenance and drainage ..		5·5
Gormandale-Carrajung Road .. ..		General maintenance and repairs to bridges ..		
Willung Road .. ..		General maintenance .. ..		
Prince's Highway .. ..		Sheeting .. ..		·79
		Carried forward .. ..	131·27	2222·34

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<i>UNDER MUNICIPALITIES—continued.</i>				
		Brought forward	131·27	2222·34
RUTHERGLEN SHIRE—				
Rutherglen—Wodonga Road	..	..	..	..
Rutherglen—Wahgunyah Road	..	..	..	..
Rutherglen—Springhurst Road	..	..	..	..
Rutherglen—Yarrowonga Road	..	..	..	..
Chiltern—Howlong Road	..	..	..	..
SEYMOUR SHIRE—				
Avenel—Longwood Road	..	Gravelling	..	General maintenance
Goulburn Valley Road	..	..	..	..
Upper Goulburn Road	..	Construction of bridge	..	..
Sydney Road	..	..	..	..
Seymour—Yea Road	..	..	..	..
SHEPPARTON SHIRE—				
Shepparton—Nagambie Road	..	2,300 feet metalling	..	..
Shepparton—Nalinga Road	..	2,400 feet metalling	..	..
..	..	3,350 feet gravelling	..	..
..	..	5,200 feet metalling	..	..
..	..	4,400 feet metalling	..	..
..	..	5,094 feet gravelling	..	..
Dookie—Nalinga Road	..	..	..	..
SOUTH BARWON SHIRE—				
Barwon Heads Road	..	..	General maintenance	..
Prince's Highway	..	..	..	..
SOUTH GIPPSLAND SHIRE—				
Welshpool—Boolarra Road	..	Between O'Connor's and the Cliffs; the Cliffs and Welshpool; and Finlay's Cutting	..	General maintenance
Falls Road	..	From Sheedy's Hill	..	..
Foster—Yarram Road	..	Erection of timber bridge over Franklin River	..	..
Dollar—Stony Creek Road	..	Between Dollar and Carnichael's Hotel	..	..
Toora—Gunyah Road	..	..	..	..
Foster—Boolarra Road	..	..	..	..
ST. ARNAUD BOROUGH—				
Avoca—St. Arnaud Road	..	..	From Inkerman Street to Borough boundary—Construction	..
..	..	..	General maintenance	..
St. Arnaud—Donald Road	..	..	From Inkerman Street to Borough boundary—Construction	..
..	..	..	General maintenance	..
Charlton Road	..	..	General maintenance from Town boundary to Borough boundary	..
STAWELL BOROUGH—				
Ararat—Stawell Road	..	Patrick Street, metalling	..	Tar and screenings, and general maintenance London Road section
Glenorehy Road	..	Metalling by Stawell Hospital	..	General maintenance
STAWELL SHIRE—				
Marnoo Road	..	Gravelling north of Marnoo	..	General re-sheeting north of Callawadda
..	..	Ironstone experimental section	..	..
..	..	Works between Marnoo and Stawell	..	..
Stawell—Warracknabeal Road	..	Gravelling near Dumunkle Shire boundary	..	General maintenance
Navarre Road	..	..	..	..
Landsbrough Road	..	..	..	..
Grampians Road	..	..	..	..
STRATHFIELDSAYE SHIRE—				
Mandurang Road	..	..	General maintenance	..
Strathfieldsaye Road	..	..	..	..
Bendigo—Heathcote Road	..	..	..	..
SWAN HILL SHIRE—				
Murray Valley Road	..	Boundary bend Road	..	..
Euston Road	..	At Tyntynder	..	..
..	..	..	..	..
Swan Hill Road	..	South of Swan Hill	..	..
Ultima Road	..	..	..	..
..	..	..	..	..
Ultima—Sealake Road	..	..	..	..
Tooleybuc Road	..	..	..	..
Nyah—Ouyen Road	..	At Towan Plains	..	..
..	..	West of Nyah	..	..
TALBOT SHIRE—				
Maryborough—Ballarat Road	..	..	Metalling Two Mile Hill and near Talbot	..
TAMBO SHIRE—				
Nowa Nowa—Buchan—Gelatinty	..	Proulock's Section—6 miles north from Buchan	..	Between Nowa Nowa and W. Tree
Mossface	..	..	..	From Bairnsdale—Bruthen Road to Mossface Wharf
Bairnsdale—Bruthen	..	..	..	Within Township of Bruthen
Princes Highway	..	..	..	Within Township of Lakes Entrance
Bruthen—Omeo	..	..	..	Within Township of Bruthen
TOWONG SHIRE—				
Omeo Road	..	..	..	Bridge repairs and spreading maintenance gravel
Murray Valley Road	..	..	..	General maintenance
TRARALGON SHIRE—				
Callignee Road	..	At Saw Pit Gully, Sections 2, 3, and 4	..	Commencing at Traralgon Creek Road
Prince's Highway	..	..	..	Within Township of Traralgon
Gormandale Road	..	..	..	Commencing at Township
TULLAROOP SHIRE—				
Avoca Road	..	..	..	Re-sheeting with metal and general maintenance
Ballarat Road	..	..	..	..
Castlemaine Road	..	..	..	..
Eddington Road	..	..	..	..
Natte Yallock Road	..	..	..	..
TUNGAMAH SHIRE—				
Yarrowonga—Cobram Road	..	East of Cobram	..	..
Cobram—Strathmerton Road	..	..	..	..
Numurkah—Tungamah—Wilby Road	..	South of Katamatite and at western boundary of Shire	..	..
St. James Road	..	Metalling	..	..
UPPER MURRAY SHIRE—				
Corryong Road	..	..	..	Tar painting
..	..	..	..	General maintenance
Tintalra Road	..	Forming and Gravelling	..	..
UPPER YARRA SHIRE—				
Main Warburton Road	..	..	..	From Woori Yallock to East Warburton
Don Road	..	..	..	At Launehing Place
VIOLET TOWN SHIRE—				
Violet Town—Dookie Road	..	Formation, metalling &c., near Township	..	General maintenance to roadway and culverts
Violet Town—Shepparton Road	..	Formation, metalling, &c., near Township	..	..
Sydney Road	..	..	..	..
		Carried forward	169·87	2537·27

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<i>UNDER MUNICIPALITIES—continued.</i>				
		Brought forward .. ..	169·87	2537·27
WALPEUP SHIRE— Onyen-Pinaroo Road .. ..	Between Guyen and Underbool .. ..	.. .. .	1·66	..
WANGARATTA BOROUGH— Beechworth Road .. ..	.. .. .	Boxing and gravelling new section with Yellow Creek gravel	..	·5
Sydney Road .. ..	.. .. .	Tarring old section .. .. .	..	·25
WANGARATTA SHIRE— Yarrawonga Road .. ..	At Pre-emptive Section east of Parish of Wangaratta South .. ..	Tar patching .. .. .	..	·75
WANNON SHIRE— Hamilton-Casterton Road .. ..	1,600 feet earthworks at Hurry's; 600 cubic yards stone spalls .. ..	General maintenance .. .. .	·25	17
Coleraine-Harrow Road .. ..	.. .. .	2,838 lineal feet gravelling at J. Hole's; 4,446 lineal feet gravelling at Tulona	..	1·25
Waimon Bridge Road .. ..	.. .. .	General maintenance .. .. .	..	31
WABANGA SHIRE— Murchison-Rushworth Road .. ..	Metalling at Moora .. .. .	.. .. .	..	5
Tatura Road .. ..	.. .. .	General maintenance .. .. .	·57	16
Elmore-Colbinabbin Road .. ..	Metalling at McTaggart .. .. .	.. .. .	..	·97
Colbinabbin-Corop Road .. ..	.. .. .	Gravelling .. .. .	·63	9
Heathcote-Elmore Road .. ..	.. .. .	General maintenance between Toolleen and Elmore .. .. .	..	·41
Colbinabbin-Moora Road .. ..	.. .. .	General maintenance .. .. .	..	17
WARRAGUL SHIRE— Prince's Highway (Town Section) Warragul-Korumburra Road .. ..	Through Allotments 16 and 4, Parish of Warragul .. .. .	From Town boundary to Town boundary .. .. From Warragul Town boundary to Shire boundary at Strezlecki .. .. .	.. .. .. ..	1 12
Warragul-Leongatha Road .. ..	.. .. .	From junction with Warragul-Korumburra Road in Allotment 54, Parish of Warragul, to Shire boundary on southern side of Allotment 32A, Parish of Allanbee .. .. .	..	8·25
Brandy Creek Road .. ..	.. .. .	From Warragul Township to northern boundary of Shire .. .. .	..	8
Bloomfield Road .. ..	.. .. .	From Nilma Township northwards to Shire boundary .. .. .	..	7·5
Darnum-Allanbee Road .. ..	.. .. .	From Darnum Township southwards to Yarragon-Hazeldene Road .. .. .	..	4·5
WARRNAMBOOL SHIRE— Allansford-Nirranda Road .. ..	.. .. .	General maintenance .. .. .	..	17
Mortlake-Warrnambool Road .. ..	.. .. .	Sheeting with metal and gravel .. .. .	..	4·25
WERRIBEE SHIRE— Prince's Highway .. ..	.. .. .	Sheeting with metal and bitumen spraying .. ..	..	3·8
Geelong-Bacchus Marsh Road .. ..	At Balliang East .. .. .	Werribee Town section .. .. .	..	·99
WHITTLESEA SHIRE— Epping-Woodstock Road .. ..	.. .. .	.. .. .	1·28	..
Main Whittlesea Road .. ..	4,100 feet from end of tarred road at 13th mile post to Reid's stud farm and between South Morang and Janefield .. .. .	From Preston at Thomastown to Woodstock—General maintenance .. .. .	..	10·5
Whittlesea-Kinglake Road .. ..	Portion of Scrubby Creek deviation, pitching and metalling .. .. .	From Bundora to Whittlesea Railway Station—General maintenance .. .. .	·77	13·23
Whittlesea-Wallan Road .. ..	.. .. .	From Whittlesea Station to top of Scrubby Creek Deviation—General maintenance .. .. .	2·	7·53
WIMMERA SHIRE— Horsham-Hamilton Road .. ..	.. .. .	From Whittlesea Township to Shire Boundary—General Maintenance .. .. .	..	5·75
Horsham-Natimuk Road .. ..	Gravel construction south of Allotments 16, 17 and 18, Parish of Quantong .. .. .	Shouldering and re-sheeting between Bungallaly Creek and Haven Hall .. .. .	..	1
Horsham-Wal Wal Road .. ..	Gravel construction between Allotments 18 and 19, Parish of Drung Drung .. .. .	Forming and re-forming with machine .. .. .	·85	4·23
WINCHELSEA SHIRE— Birregurra-Forrest Road .. ..	McCallum's Corner 1¼ miles from Birregurra .. .. .	Raking in gravel east of Quantong Bridge .. .. .	..	·85
Lorne Road .. ..	Gravelling and metalling Dunse's Hill (completed during year) .. .. .	General maintenance from Birregurra to Barwon Downs .. .. .	1·09	10
Prince's Highway .. ..	Gravelling at corner near "Studbrook" .. .. .	.. .. .	..	..
WODONGA SHIRE— Murray Valley Road .. ..	Gravelling adjacent to Allotment 5, Section 3, Parish of Murroon .. .. .	Re-sheeting at Deans Marsh and Lorne—General maintenance Deans Marsh to Lorne .. .. .	2·18	11·8
Wodonga-Yackandandah Road .. ..	Gravelling ½ mile north of Birregurra-Forrest railway crossing .. .. .	Re-sheeting between Inverleigh Road and Township and general maintenance throughout Township of Winchelsea .. .. .	..	2·52
Tallangatta Road .. ..	1 mile south of Benwerrin Post Office (Libby's section), and 3¼ miles north of Benwerrin Post Office (Tramway section) .. .. .	.. .. .	..	..
Sydney Road .. ..	.. .. .	General maintenance .. .. .	..	1·1
WONTHAGGI BOROUGH— Wonthaggi-Inverloch Road .. ..	.. .. .	.. .. .	·69	3·25
Loch-Wonthaggi Road .. ..	.. .. .	.. .. .	..	·88
Wonthaggi-Korumburra Road .. ..	.. .. .	.. .. .	..	2·02
WOORAYL SHIRE— Main South Gippsland Road .. ..	Metalling past Morris', &c. .. ..	General maintenance .. .. .	..	2·53
Lower Tarwin Road .. ..	Gravelling near Brown's and Bollom's .. .. .	Re-sheeting and general maintenance .. .. .	..	2·46
.. .. .	Gravelling near Brown's and Shiell's .. .. .	General maintenance .. .. .	..	·37
.. .. .	Gravelling near Middle Tarwin and Cashin's .. .. .	.. .. .	·56	..
Wild Dog Valley Road .. ..	Gravelling through O'Reilly's and Bawden's .. .. .	.. .. .	2·01	..
Leongatha-Yarragon Road .. ..	Metalling near Wooreen School .. .. .	.. .. .	..	2·27
Marden Road .. ..	.. .. .	.. .. .	..	·98
Inverloch-Leongatha Road .. ..	.. .. .	.. .. .	..	1·59
Inverloch-Wonthaggi Road .. ..	.. .. .	.. .. .	..	·76
Farmer's Road .. ..	.. .. .	.. .. .	..	..
Warragul-Leongatha Road .. ..	.. .. .	.. .. .	..	..
		Carried forward .. ..	191·76	2791·21



STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES— <i>continued.</i>				
		Brought forward .. ..	191·76	2791·21
WYCHEPROOF SHIRE—				
Wycheproof—Corak Road .. ..		Earth formation west of Wycheproof .. ..	..	1·72
Wycheproof—Birchip Road .. ..	Gravelling west of Wycheproof .. ..	Gravelling and limestoning west of Wycheproof .. ..	5	85
Sealake—Ultima Road .. ..		General maintenance .. ..	..	22
Sealake—Woomelang Road .. ..		Limestoning south and west from Sea Lake .. ..	..	88
Sealake—Birchip Road .. ..		Limestoning south of Sea Lake .. ..	..	..
YACKANDANDAH SHIRE—				
Yackandandah—Wodonga Road .. ..		Guard fencing in Township of Yackandandah .. ..	..	15·76
		Patrolling Road .. ..	..	..
Dederang Road .. ..	Gravelling near Kinchington's .. ..	Patrolling road for full length .. ..	2·09	20·1
	Gravelling and reinforced concrete .. ..			
	bridge and culvert near Back .. ..			
	Creek, Parish of Yackandandah, .. ..			
	and approaches and bridge at .. ..			
	House Creek, Parish Dederang .. ..			
Gundowring Road .. ..	Gravelling, &c., in six sections .. ..	Patrolling road for full length .. ..	2·73	22·02
	over full length of road .. ..			
YARRAWONGA SHIRE—				
Wangaratta—Yarrowonga Road .. ..		Re-grading and gravelling with river gravel .. ..	..	10
		from Crawford's corner to Township of .. ..		
		Peechelba .. ..		
Yarrowonga—Cobram Road .. ..		Re-grading and gravelling from Wemy's Lane .. ..	..	4
		to Burrumine North State School .. ..		
Yarrowonga—Rutherglen Road .. ..		Surfacing and repairs to Ovens Bridge approaches .. ..	..	2
Tungamah—Wilby Road .. ..		Grading gravelled sections near Wilby Township .. ..	..	1
YEA SHIRE—				
Yea—Glenburn Road .. ..	Between Kolatha Creek and Glen- .. ..	8,591 feet gravelling near "Aywarley" and .. ..	1·35	1·63
" " " " .. ..	burn School .. ..	" Murrudidi " .. ..	1·28	..
" " " " .. ..	6,775 feet gravelling deviation, .. ..			
" " " " .. ..	&c., near "Glenmore" to .. ..			
" " " " .. ..	Devlin's Bridge .. ..			
Upper Goulburn Road .. ..		Two reinforced concrete pipe culverts and 450 .. ..	..	09
" " " " .. ..		feet gravelling, &c. .. ..		
" " " " .. ..		6,489 feet gravelling near Kerrisdale (boundary .. ..	..	1·23
" " " " .. ..		work) .. ..		
" " " " .. ..		3,705 feet gravelling near Homewood and .. ..	..	7
		Sawpit Creek .. ..		
		Total .. ..	199·71	2861·61
UNDER DIRECT SUPERVISION OF THE BOARD.				
ALBERTON SHIRE—				
Boolarra—Welshpool Road .. ..		Generally throughout .. ..	..	13·5
Yarram—Boolarra Road .. ..	Construction .. ..		1·7	..
ALEXANDRA SHIRE—				
Healesville—Alexandra Road .. ..	Bridge over Little River Breakway .. ..		01	..
BET BET SHIRE—				
Avoca—Bealiba Road .. ..	Construction 3-cell reinforced con- .. ..			
	crete culvert .. ..			
BRAYBROOK SHIRE—				
Ballarat Road .. ..	Reinforced concrete culvert near .. ..			
	Albion .. ..			
BORUNG SHIRE—				
Dimboola Road .. ..	Reinforced concrete bridge over .. ..		13	..
	Yarriambiack Creek at Warrack- .. ..			
	nabeal .. ..			
CARRUM BOROUGH—				
Point Nepean Road .. ..	Reinforced concrete bridge over .. ..		15	..
	Patterson River at Carrum .. ..			
ELTHAM SHIRE—				
Eltham—Yarra Glen Road .. ..	Reinforced concrete bridge over .. ..		04	..
	Diamond Creek near Eltham .. ..			
MANSFIELD SHIRE—				
Wood's Point Road .. ..		Generally throughout .. ..	..	40
MORWELL SHIRE—				
Boolarra—Foster Road .. ..		From Boolarra south to Gonyah Junction .. ..	..	8·5
Boolarra—Welshpool Road .. ..		From Greenwood's to south boundary of Shire .. ..	..	16·1
MORWELL AND TRARALGON SHIRE—				
Jeeralang West Road .. ..	Metalling near Brown's Saddle .. ..		1	..
MULGRAVE SHIRE—				
Ferntree Gully Road .. ..		Re-sheeting and forming .. ..	..	1·25
NARRACAN SHIRE—				
Walhalla Road .. ..		Generally throughout .. ..	..	12
OMEO SHIRE—				
Bright—Omeo Road .. ..		Generally throughout .. ..	..	28
PORTLAND SHIRE—				
Portland—Mount Gambier Road .. ..	Forming and metalling from Sage's .. ..		2·63	..
	Corner northwards .. ..			
SWAN HILL SHIRE—				
Mildura Road .. ..	Forming and marling Section 50 .. ..		1·52	..
TOWONG SHIRE—				
Glen Wills—Granite Flat Road .. ..		Generally throughout .. ..	..	31
UPPER YARRA AND ALEXANDRA SHIRES—				
Wood's Point Road .. ..		McVeigh's to Matlock .. ..	..	30
WANGARAGGA BOROUGH—				
Sydney Road .. ..	1,500 feet re-sheeting between .. ..	6,434 feet trimming and re-sheeting near .. ..	28	1·21
	Ovens and Loag Bridges .. ..	Cemetery .. ..		
		Total .. ..	7·46	181·56

## APPENDIX J.

## COUNTRY ROADS BOARD.

## STATEMENT SHOWING MILEAGE LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED ON DEVELOPMENTAL ROADS FOR THE YEAR ENDED 30TH JUNE, 1927.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER MUNICIPALITIES.						
ALBERTON SHIRE—						
Albert River Road .. ..	Gravelling McAuley's to Egan's .. ..	1 '32	..	..	..	..
Carrajung-Gormandale Road .. ..	Metalling between Carrajung and Blackwarry .. ..	2 '2	4	4	4	4
Gelliondale Road .. ..	Gravelling between Collis' Corner and Keating's Hill .. ..	1 '38	..	..	1 '17	1 '17
Whitelaw's Road .. ..	Earthworks at Womerah, and gravelling near North Devon .. ..	3 '22	..	..	..	..
ALEXANDRA SHIRE—						
Terrip Terrip Road .. ..	Terrip Terrip .. ..	'06	'06	'06	'06	'06
ARAPILES SHIRE—						
Miga Lake-Gymbowen Road .. ..	.. ..	1 '37	'47	'47	'47	'47
Arapiles-Grassy Flat Road .. ..	.. ..	..	..	..	..	..
AVOCA SHIRE—						
Maryborough-Natte Yallock Road .. ..	Between 15,375 feet and 42,714 feet on continuous chainage proceeding east from Natte Yallock .. ..	3 '94	..	..	..	..
AVON SHIRE—						
Bengworden Road .. ..	Between Clydebank-Bengworden Road and Perry Bridge and Perry Bridge and Tom's Creek .. ..	4 '59	..	..	1 '43	1 '43
Dargo Road .. ..	At Lennie's Hill .. ..	2 '21	..	..	..	..
BACCHUS MARSH SHIRE—						
Balling Boundary Road .. ..	Little River (bridge and approaches) .. ..	'08	'08	'08	'08	'08
BAIRNSDALE SHIRE—						
Glenaladale-Lindenow Road .. ..	Between Lindenow Station and Mitchell River .. ..	1	..	1	..	1
Bulmwaal-Tabberabbera Road .. ..	Reinforced concrete culvert and approaches .. ..	..	..	..	..	..
BALLAN SHIRE—						
Boundary Road .. ..	South from Old Ballarat Road to Green's Road .. ..	'79	'79	'79	'79	'79
Moorabool West Road .. ..	West from Daylesford Road .. ..	'31	'31	'31	'31	'31
(East Section)	.. ..	..	..	..	..	..
Moorabool West Road (West Section)	3 miles north of Gordon Township .. ..	1 '04	..	..	..	..
BEECHWORTH SHIRE—						
Bowman's Forest Road .. ..	Bowman's Section, near Patton's .. ..	'6	'6	'6	'6	'6
Everton Road .. ..	Section near Owens River .. ..	'5	..	..	..	..
Hillsborough Road .. ..	Stanley to end settlement .. ..	'6	..	..	..	..
Myrtleford-Yackandandah Road .. ..	Myrtleford Creek to Healey's .. ..	1	1	..	..	..
Stanley Road .. ..	Beechworth Railway Station to Silver Creek .. ..	1 '12	..	..	..	..
BENALLA SHIRE—						
Benalla-Mansfield Road .. ..	Parish of Samaria, 6 miles south of Benalla .. ..	..	2 '7	..	..	..
Toombullup Road .. ..	Parish of Toombullup, 26 miles south of Benalla .. ..	3 '39	..	..	3 '39	3 '39
BERWICK SHIRE—						
Upper Beaconsfield-Upper Pakenham Road .. ..	Opposite Packing Shed .. ..	'41	'41	'41	'41	'41
BIRCHIP SHIRE—						
Berrillook Road .. ..	East from Birchip-Sea Lake Road .. ..	..	1 '42	1 '42	1 '42	..
Kinnabulla Road .. ..	West from Railway .. ..	..	1 '42	1 '42	1 '42	..
Morton Plains Road .. ..	West from Donald-Birchip Road .. ..	'94	1 '42	1 '42	1 '42	'94
Watchupga East Road .. ..	East from Railway .. ..	..	'75	'75	'75	'75
BORUNG SHIRE—						
Boolite-Sheep Hills Road .. ..	Gravelling .. ..	..	'2	'2	..	..
Donald-Warracknabeal Road .. ..	Gravelling 2 miles from Warracknabeal .. ..	'45	..	..	..	..
BRIGHT SHIRE						
Buffalo River Road .. ..	Metalling 30,500 to 36,500 feet .. ..	1 '14	..	..	..	..
Kiewa Valley Road .. ..	Metalling 14,000 to 21,900 feet .. ..	1 '5	..	..	1 '23	1 '23
Myrtleford-Yackandandah Road .. ..	Sanding at Mudgogonga .. ..	1 '41	..	..	1 '41	1 '41
" " " " .. ..	Bridge at Sandy Creek .. ..	..	..	..	Bridge	Bridge
" " " " .. ..	Road protection works at 16,000-feet peg .. ..	..	..	..	..	..
BULLA SHIRE—						
Konagaderra Road .. ..	At Konagaderra, near Miekieham .. ..	'6	1	1	..	..
" " " " .. ..	At Fentona, near Clarkfield .. ..	'62	..	..	..	..
BULN BULN SHIRE—						
Duggan North Road .. ..	.. ..	'37	2 '4	2 '4	(Day labour)	(Day labour)
Fumina Road .. ..	Metalling .. ..	1 '37	..	1 '68	(Day labour)	(Day labour)
" " " " .. ..	Re-forming .. ..	2 '16	..	..	..	..
Loch Valley Road .. ..	From Noojee Township to portion of the road opposite C.A. 25B, Parish of Neerim East—	..	..	..	..	..
	Metalling .. ..	1 '20	..	..	..	..
	Earthworks .. ..	1 '71	..	..	..	..
Neerim South-Neerim East Road .. ..	From junction with metal opposite C.A. 39, Parish of Neerim, to Crown Allotment 77G, Parish of Neerim .. ..	'71	..	..	..	..
Neerim-North Noojee Road .. ..	From junction with Neerim "C" Road, opposite Crown Allotment 4, Parish of Neerim, to Noojee Township .. ..	1 '3	..	..	..	..
Torwood-Topiram Road .. ..	Part Section 4, Torwood-Topiram Road .. ..	1 '97	4 '16	4 '16	1 '97	1 '97
BUNGAREE SHIRE—						
Bolwarrah Road .. ..	Peg 00 to Peg 6,300 .. ..	1 '19	..	2 '38	2 '38	2 '38
BUNINYONG SHIRE—						
Elaine-Mt. Mercer Road .. ..	On top of hill east side of Leigh River. Boundary with Bannockburn Shire .. ..	'66	'66	'66	(Day labour)	(Day labour)
CHARLTON SHIRE—						
Glenloth Road .. ..	Gravelling from 5,100 feet to 10,200 feet, Parish of Glenloth .. ..	'06	2	1 '92	1 '92	1 '92
Marmal Road .. ..	Gravelling and metalling from 5,700 feet to 11,065 feet, Parish of East Charlton .. ..	1 '01	1 '5	1 '13	1 '01	1 '01
Wooroonook Road .. ..	Gravelling and metalling from 11,600 feet to 14,800 feet, Parish of Wooroonook .. ..	'2	1 '77	'75	'66	'66
" " " " .. ..	Metalling from 7,500 feet to 11,600 feet, Parish of Wooroonook .. ..	'77	1 '77	1 '52	1 '43	1 '43
COHUNA SHIRE—						
Keeley's Lane .. ..	Clearing and forming from Peg 16,000 to Peg 26,500 .. ..	1 '99	..	..	..	1 '99
Gannawarra Road .. ..	Clearing and forming from Peg 5,000 feet to Peg 29,100 feet .. ..	3 '37	2 '39	2 '39	2 '39	2 '39
" " " " .. ..	Gravelling from 200 feet to 5,200 feet .. ..	..	..	'98	..	..
Murray River Valley Road .. ..	Metalling from Cohuna towards Wee Wee Rup .. ..	'38	..	..	..	..
" " " " .. ..	Metalling from Gunbower towards Leitchville .. ..	1 '51	..	..	..	..
" " " " .. ..	Clearing, forming, &c., from Gunbower past Cohuna .. ..	1 '35	..	..	..	..
COLAC SHIRE—						
Colac-Forrest Road .. ..	Between Colac and Forrest .. ..	1 '23	..	..	..	..
Beac-Dreite Road .. ..	Between Beac and Dreite .. ..	1 '45	..	..	..	..
Dreite Estate .. ..	Through Soldier Settlement .. ..	1 '88	..	..	..	..
CORIO SHIRE						
Geelong-Bacchus Marsh Road .. ..	At Balliang and Ripley .. ..	1 '47	..	..	1 '02	1 '02
Cox Road .. ..	.. ..	1 '02	..	..	(Day labour)	(Day labour)
Carried forward .. ..		68 '02	33 '28	33 '90	34 '14	32 '81

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<i>UNDER MUNICIPALITIES—continued.</i>						
	Brought forward .. ..	68·02	33·28	33·9	34·14	32·81
CRANBOURNE SHIRE—						
Dalmore Road .. ..	Near Dalmore .. ..	·56	1·24	1·24	1·24	1·24
Fearcedale Road .. ..	Near Langwarrin .. ..	·25	1·04	1·04	1·04	1·04
Koowecup—Modella Road .. ..	Near Bayles .. ..	1·11	1·58	1·58	1·58	1·58
DEAKIN SHIRE—						
Kyabram West Road .. ..	To west of Kyabram .. ..	..	1·1	1·1	1·1	1·1
Kyabram—Stanhope Road .. ..	On the N.S. Rushworth Road .. ..	1	..	..	..	..
Strathallan Road .. ..	To east of Strathallan Railway Station .. ..	·6	..	..	..	..
Tongala East Road .. ..	To east of State School .. ..	·76	..	..	..	..
DIMBOOLA SHIRE—						
Detpa—Hindmarsh Road .. ..	About 1 mile north of Jeparit—Yanac railway line .. ..	·64	..	..	..	..
.. ..	Between Detpa and Lake Hindmarsh School .. ..	..	·37	·37	·37	·37
Glenlee—Jeparit Road .. ..	Near Glenlee .. ..	1·57	..	..	..	..
Albacutya Road .. ..	Two sections between Albacutya station and Lake Albacutya .. ..	·34	..	..	..	..
DONALD SHIRE—						
Corack East Road .. ..	Commencing about 14 miles from Donald, thence northward .. ..	1·7	3·31	3·31	3·31	3·31
Jeffcott Road .. ..	Commencing about 2 miles from Donald, thence eastward .. ..	1·89	1·89	1·89	1·89	1·89
DUNDAS SHIRE—						
Melville Forest Road .. ..	At from 3 to 9 miles from Cavendish, Parish of Urangara .. ..	1·43	..	..	..	..
DUNMUNKLE SHIRE—						
Minyip—Kewell Road .. ..	Commencing at Township boundary (Minyip) and extending south-westerly .. ..	1·04	..	..	..	..
EAST LODDON SHIRE—						
Prairie—Borong .. ..	Parish of Talambe .. ..	1·47	·72	·72	..	..
ELTHAM SHIRE—						
Cottles' Bridge—Strathewan Road .. ..	Schultz's to Apted's .. ..	·77	..	..	..	Two timber bridges
Cottle's Bridge Road .. ..	Cottle's Bridge to the deviation .. ..	..	..	..	..	·6
Balee—Kangaroo Ground Road .. ..	.. ..	·05	..	..	..	(Day labour)
Yarra Glen—Kingslake East Road .. ..	Scott's Hill .. ..	..	..	..	..	·81
Kingslake—Kingslake East Road .. ..	.. ..	1·45	..	..	..	..
EUROA SHIRE—						
Longwood—Ruffly Road .. ..	Gravelling Longwood—Ruffly deviation .. ..	·51	..	..	..	..
Strathbogie Road .. ..	Formation and sidecutting, near Strathbogie Township .. ..	·57	..	..	..	..
FERN TREE GULLY SHIRE—						
Patch Road .. ..	Between Cannm's Road and Patch Post Office .. ..	·69	..	·69	·69	·69
Reservoir Road .. ..	Near Green's Corner, Narre Warren .. ..	·15	·15	·15	·15	·15
FLINDERS SHIRE—						
Brown's Road .. ..	Over swamp .. ..	·91	..	..	·91	·91
FRANKSTON AND HASTINGS SHIRE—						
Quarry Road .. ..	Between Frankston—Flinders Road and Moorooduc Railway Station, in the Parish of Frankston .. ..	..	3	3	3	3 (Day labour)
GLENELG SHIRE—						
Merlino—Struan—Tahara Road .. ..	.. ..	..	2·49	..	..	..
Glenorchy Estate Road .. ..	Forming and gravelling on Northcott's Deviation and through Allotment 8 .. ..	·74	1·71	·34	·34	·34
GLENLYON SHIRE—						
Daylesford—Treatham .. ..	Parish of Bullarto .. ..	·57	·95	·95	·95	·95
GORDON SHIRE—						
Gladfield—Pyramid Hill Road .. ..	Gravelling west from Pyramid Hill .. ..	·45	..	..	..	..
Boort—Kerang Road .. ..	Gravelling on road north-east from Boort Township .. ..	·39	..	..	..	..
HAMPDEN SHIRE—						
Ayresford Road .. ..	Between Emu Creek and Princes Highway .. ..	1·5	..	2·14	..	(Day labour)
Candare—Duvernay Road .. ..	Boundary of Parishes of Pollah South and Wilgul South .. ..	·62	..	..	·62	·62
Foxhow Road .. ..	South of Allotments 73, 84 and 89, Parish of Corangamite .. ..	·87	..	·87	·87	·87
Werneth Road .. ..	North of Allotment 8, Parish of Wilgul North .. ..	·25	..	·87	·25	·25
Vite Vite Road .. ..	Between Derrinallum—Skipton Road and Vite Vite Railway Station .. ..	4·12	4·12	4·12	4·12	4·12 (Day labour)
HEALESVILLE SHIRE—						
Buxton—Marysville Road .. ..	Sherwell's Hill, at Buxton .. ..	·66	..	..	..	..
HEYTESBURY SHIRE—						
Cowley's Creek—Timboon Road .. ..	Fencing roadside lines .. ..	2·75	2·75	2·75	2·75	2·75
Digney's Bridge Road .. ..	Completion of forming and grading .. ..	2·13	2·13	2·13	2·13	2·13
Cobden—Ecklin Road .. ..	Formation .. ..	6·57	6·57	6·57	6·57	6·57
Ayresford Road .. ..	Metalling .. ..	1·51	1·51	1·51	1·51	1·51
Timboon—Scott's Creek Road .. ..	Metalling .. ..	1·26	2·27	2·27	2·27	2·27 (Day labour)
Timboon—Nirranda Road .. ..	Fencing through Heins Bros' .. ..	1	1	1	1	1
HUNTLY SHIRE—						
Holmberg Road .. ..	Metalling through Kelson's .. ..	·53	·98	·98	·98	·98
Dromartin Road .. ..	Minto .. ..	..	..	·75	..	..
KARA KARA SHIRE—						
Swanwater Road .. ..	Minto .. ..	..	..	·75	..	..
KARKAROO SHIRE—						
Hopetoun—Yaapeet Road .. ..	Between St. Arnaud—Donald Rd. and Swanwater Rail. Stn. .. ..	·95	1·26	·95	·95	·95
Patchewollock—Speed Road .. ..	Works commencing at Nyppo Road .. ..	..	..	..	..	1·5
Patchewollock—Yarto Road .. ..	At Patchewollock Township .. ..	1	..	..	..	1
KERANG SHIRE—						
Murrabit Road .. ..	Two miles from Patchewollock .. ..	·47	..	..	..	·47
Murray River Valley Road .. ..	Between Kerang and Westby .. ..	·66	1·2	·66	·66	·66
KORONG SHIRE	Near Kerang .. ..	2·58	2·58	2·58	2·58	2·58
Korong Vale—Kinypaniel Road .. ..	Opposite Crown Allotment 125, Kinypaniel Parish; and opposite Crown Allotment 126, Kinypaniel Parish .. ..	·62	..	..	..	..
Korong Vale—Woolshed Flat Road .. ..	Opposite Crown Allotments 9 and 32, Borung Parish .. ..	·37	..	..	..	..
Mysia East Road .. ..	Opposite Crown Allotment 147, Mysia Parish .. ..	·43	..	..	..	..
Mysia West Road .. ..	Opposite Crown Allotment 147c, Parish of Mysia .. ..	·37	..	..	..	..
Emu—Logan Road .. ..	Opposite Crown Allotments 6, 3, 5, Schubert Parish; and opposite Crown Allotments 10, 11, Kooroc Parish; and opposite Crown Allotment 45, Kooroc Parish .. ..	1·33	..	..	..	..
Nine Mile Road .. ..	Opposite Crown Allotment 103B, Barrakee Parish .. ..	·60	..	..	..	..
Kurting—Rheola Road .. ..	In Kingower Township .. ..	·28	·14	·14	..	..
Wychitella North Road .. ..	Opposite Crown Allotment 65, Buckrabanyule Parish .. ..	·30	..	..	..	..
Buckrabanyule South Road .. ..	Opposite Crown Allotment 4, Woosang Parish .. ..	·43	·36	·36	·36	..
KORUMBURRA SHIRE—						
Poo Wong Estate Road .. ..	Soldiers' Settlement .. ..	1·91	1·91	1·91	1·91	1·91
KOWREE SHIRE—						
Benayeo Road .. ..	At Benayeo (Thompson's Creek) Bridge .. ..	·01	·01	·01	·01	·01
Edenhope—Natimuk Road .. ..	At Turandury (Pine Hut Lake) .. ..	·5	·5	·5	·5	·5
Elderslie Road .. ..	At Meereek, Edenhope, &c. .. ..	·41	2·15	2·15	2·15	2·15
Little Desert .. ..	At Minimay and Beewar (Sections 1 to 7) .. ..	·01	7·05	7·05	6·85	5·85
Miga Lake—Gymbowen Road .. ..	At Turandury (Nelsons) .. ..	·87	·87	·87	·87	·87
KYNETON SHIRE—						
Lagoon Road .. ..	Cleating, forming and metalling .. ..	..	..	1·6	·56	·56
Carried forward .. ..		127·50	92·19	95·77	91·18	92·87

## STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES—continued.</b>						
	Brought forward .. ..	127·57	92·19	95·77	91·18	92·87
LAWLOIT SHIRE—						
Miram Road .. ..	Metalling between Miram and Miram North .. ..	·19	..	·19	·19	·19
LEIGH SHIRE—						
Cressy—Shelford (joint with Colac Shire)	From junction with Rokewood—Winchelsea Road westward to near Mount Rebecca .. ..	1·1	1·1	1·1	..	1·1 (Day labour)
LILLYDALE SHIRE—						
Monbulk—Seville Road .. ..	East Wandin .. ..	4	..	..	..	..
LOWAN SHIRE—						
Netherby Road .. ..	Between Allotments 40—39, Warraquil, and 44—25, Lorquon .. ..	·28	·28	·28	·28	·28
Winlam Road .. ..	Between Allotments 14—15 and 21—19, Winlam .. ..	1·32	1·32	1·32	1·32	1·32
MANSFIELD SHIRE—						
Mansfield—Tolmie Road .. ..	Reformation and metalling from 6½ to 9 miles .. ..	2·04	2·04	2·04	2·04	2·04
Mansfield—Benalla Road .. ..	Clearing, formation, gravelling two sections .. ..	·66	·66	·66	·66	·66
MARONG SHIRE—						
Raywood—Kamarooka Road .. ..	6 chains of formation and installing one concrete box culvert .. ..	·07	·07	·07	·07	·07
Bendigo—Serpentine Road .. ..	Forming and metalling 8,000 lineal feet .. ..	1·51	1·51	1·51	1·51	1·51
Yarraberb Road .. ..	3,500 feet forming and metalling .. ..	..	·7	..	..	..
MELTON SHIRE—						
Coburn's Road .. ..	Extension of existing construction northerly to Coimadai Road .. ..	2	..	..	..	..
.. ..	Extension of existing work .. ..	..	..	..	..	·38
Exford Road .. ..	Two miles south of Melton Railway Station .. ..	·57	..	..	..	..
Mount Kororoit Road .. ..	Adjoining Wallace's Bridge .. ..	·57	..	..	..	..
MILDURA SHIRE—						
Red Cliffs South-East Road .. ..	Red Cliffs Township to Channel and to pumps .. ..	1·5	5·5	5·5	..	..
Red Cliffs West Road .. ..	Red Cliffs Township to Block 351 .. ..	·5	2	2	..	..
MINHAMITE SHIRE—						
Bessiebell—Yambuk Road .. ..	Portion of the road leading south from Bessiebell to Yambuk .. ..	1·04	..	..	..	..
Nardoo Road .. ..	From main Warrnambool—Hawkesdale—Penshurst Road westward, near Hawkesdale .. ..	1·65	..	..	..	..
MIRBOO SHIRE—						
Allambee East-West Tarwin Road .. ..	Mirboo—Yarragon Road to Leongatha—Yarragon Road .. ..	4·75	4·75	4·75	4·75	4·75
Berry's Creek .. ..	Farmer's to Ashworth's and Tackaberry's to Molr's .. ..	2·5	3·5	3·5	..	..
Boolarra South Road .. ..	Martin's to Boolarra—Foster Road .. ..	3·5	..	..	..	..
Clear Creek Road .. ..	Bridge to Jacka's .. ..	1·4	3	3	..	..
Mirboo—Boolarra Road .. ..	Mirboo North to Farmer's .. ..	..	2	2	..	..
Mirboo North—Thorpdale Road .. ..	Mirboo North to Francoine's .. ..	3·85	·75	·75	..	..
Mirboo—Yarragon Road .. ..	Dyke's to Jerni's, and Jerni's to boundary .. ..	2·75	3·2	3·2	1·35	1·35
MORWELL SHIRE—						
Boolarra—Morwell Road .. ..	Metalling near Boolarra .. ..	·33	..	..	..	..
Hazelwood Estate Road .. ..	From Boolarra—Morwell Road easterly .. ..	..	2·1	2·1	1·55	1·55
Jumbuk Road .. ..	Metalling O'Reilly's Hill .. ..	..	..	..	..	..
Middle Creek Road .. ..	From Jumbuk Road southerly and cuttings beyond Yinnar South .. ..	·88	·7	·7	·7	·7
Tyers Road .. ..	Bridge and flood crossings, Latrobe River Flats .. ..	·3	..	..	..	..
NARRACAN SHIRE—						
Allambee—Childers Road .. ..	Metalling .. ..	·68	..	..	..	..
Coalville—Moe Road .. ..	Sanding portions of Coalville Road from David Railway Station to Coalville Road .. ..	1·36	..	..	1·36	1·36
Coalville—Narracan Road .. ..	Forming from Coalville Railway Station thence in a westerly direction .. ..	..	..	..	·55	·55
Darlimurla—Thorpdale Road .. ..	Forming .. ..	1·82	..	..	..	..
Mirboo—Yarragon Road .. ..	Through Crown Allotment 19, 9, and 10, Allambee Estate 118B/15 .. ..	1·32	..	..	..	..
Moe—Yallourn Road .. ..	From 00 peg, Prince's Highway, through Crown Allotment 5D, 5E and 4C, Parish of Narracan .. ..	..	2·17	2·17	2·17	2·17
Platina Road .. ..	Commencing at peg 5,100, Walhalla Road to Cooper's Creek Township .. ..	·27	..	..	·65	·65
Thorpdale East Road .. ..	Metalling .. ..	1·68	..	..	..	..
Trafalgar—Wilowgrove Road .. ..	Two sections .. ..	·63	..	..	..	..
Walhalla Road .. ..	Through Crown Allotment 4, Allotment 4, Parish of Moondarra .. ..	1·4	..	..	1·91	1·91
NEWHAM AND WOODEND SHIRE—						
Campaspe Road .. ..	Metalling .. ..	·5	..	..	·5	·5
Macedon Village Settlement Road .. ..	Metalling .. ..	1·5	..	..	1·5	1·5
OMBO SHIRE—						
Bindi Road .. ..	.. ..	1·9	..	..	..	..
Reedy Creek Road .. ..	.. ..	..	..	..	..	..
Reedy Flat Road .. ..	.. ..	..	·8	·8	..	..
Sandy Creek Road .. ..	Sections 3 and 4 .. ..	2	2	2	2	2
Swift's Creek—Cassilis Road .. ..	.. ..	1·8	..	..	..	..
ORBOST SHIRE—						
Bete Bolong Road .. ..	Continuation of construction towards Buchan .. ..	2·77	..	2·77	2·77	2·77
Combiobar Road .. ..	Extension of widening from Pine Creek .. ..	·44	..	·44	·44	·44
Errinundra Valley Road .. ..	Continuation northerly towards Errinundra .. ..	·38	..	..	·38	·38
Jarrahmond Road .. ..	Section 2, to complete road .. ..	..	2·06	..	..	..
Lower Bendoc Road .. ..	Construction from Big Flat to Craigie Bog .. ..	2·69	·42	·42	2·69	2·69
OTWAY SHIRE—						
Carlisle—Gellibrand Road .. ..	Boxing out, metalling and gravelling .. ..	7·62	..	..	3·31	3·31
Beech Forest—Mount Sabine Road .. ..	Boxing out and metalling .. ..	·37	..	..	..	..
Beech Forest—Laver's Hill Road .. ..	.. ..	2·52	..	..	1·82	1·82
Amiet's Track .. ..	Grubbing and clearing .. ..	..	2·4	2·4	1·43	1·43
Apollo Bay—Wye River Road .. ..	Forming and grading near Wood's and Chapman's, boxing out and metalling, and low-level bridge over Smythie's Creek .. ..	·78	..	..	1·42	1·42
Barham Valley Road .. ..	Grubbing, clearing, forming and grading parts of Sections 5 and 6 .. ..	..	1·9	1·9	1·27	1·27
Dehnert's Road .. ..	Clearing, grubbing, forming and grading .. ..	·82	..	..	..	..
Glen Aire—Laver's Hill Road .. ..	Boxing out and metalling at Beauty Spot, &c. .. ..	·91	..	..	..	..
Hordern Vale Road .. ..	Grubbing, clearing, forming and grading .. ..	..	..	..	·31	·31
Lardner's Track .. ..	Deviation at Gellibrand end and grubbing, clearing, forming and grading .. ..	..	2·37	2·37	1·16	1·16
Phillips' Track .. ..	Boxing out and metalling .. ..	·76	..	..	..	..
Prinetown Road .. ..	Boxing out and metalling .. ..	..	..	..	1·14	1·14
Sabine—Sunnyside Road .. ..	Grubbing, clearing, forming and grading .. ..	..	..	..	1	1
OXLEY SHIRE—						
Boggy Creek Road .. ..	Section No. 2, Myrthee .. ..	·15	..	..	..	..
King Valley Road .. ..	Section 3, between Whitfield and Cheshung, and Section near Post Office and King Valley Railway Station .. ..	1·07	..	..	..	..
Rose River Road .. ..	Section 59,000 feet and 71,000 feet—Rose River—Gentle Annie Gap .. ..	..	2·3	2·3	2·3	2·3
	Carried forward .. ..	204·90	143·79	148·71	136·78	141·55



STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES—continued.</b>						
	Brought forward .. ..	272·55	187·13	193·38	172·43	175·06
WARRNAMBOOL SHIRE—						
Panmure Road .. ..	At Panmure .. ..	·8	..	..	..	..
WERRIBEE SHIRE—						
Aviation Road .. ..	From Beach Road to Point Cook Road .. ..	1·12	..	..	..	..
Exford Road .. ..	Continuation of Road towards Parwan .. ..	..	..	..	·42	·42
WHITTLESEA SHIRE—						
Whittlesea—Kingslake Road .. ..	From Kingslake West State School at chainage 38,000 to Shire boundary at chainage 43,200 feet .. ..	·97	..	..	..	·97 (Day labour)
WIMMERA SHIRE—						
Wonwondah East Road .. ..	Gravel construction from north end of 45 Bungalally to between 41 and 49 Bungalally .. ..	..	1·52	1·52	1·52	1·52
WINCHELSEA SHIRE—						
Lorne Road .. ..	Near Lorne (between chainages 64,000 feet and 67,000 feet) .. ..	·55	..	..	·55	·55
Wymboliel Road .. ..	Between Lorne and St. George River—Clearing slips .. ..	·7	..	..	..	..
WODONGA SHIRE—						
Beechworth—Wodonga Road .. ..	.. ..	1·03	..	..	1·03	1·03
WOORAYL SHIRE—						
Boorool Road .. ..	Through Boorool Estate .. ..	2·65	..	..	..	..
Canavans Road .. ..	Along Wilkur Creek .. ..	·79	..	..	..	..
Coulter's Road .. ..	Near McColl's .. ..	..	·8	·8	·8	·8
Dollar—Dumbalk Road .. ..	Mrs. Harrison's outlet .. ..	..	·17	·17	·17	·17
Dumbalk Road .. ..	Through Dumbalk Township .. ..	1·02	1·34	1·34	1·34	1·34
Warragul—Leongatha Road .. ..	From Pearson's Bridge to Dumbalk .. ..	·78	..	..	..	..
" .. ..	From Worth's to Shire boundary .. ..	1·45	..	..	..	..
" .. ..	Through Adam's, Pope's and Worth's .. ..	1·42	..	..	..	..
WYCHEPROOF SHIRE—						
Culgoa—Lalbert Road .. ..	East of Culgoa .. ..	1·06	1·06	1·06	1·06	1·06
Meridian Road .. ..	South of Berriwillock .. ..	·53	·53	·53	·53	·53
Nullawill—Winston Road .. ..	.. ..	..	1·48	1·48	1·48	1·48
Sea Lake—Tyrrell Downs Road .. ..	North of Sea Lake .. ..	·82	·82	·82	·82	·82
Wycheproof—Glenloth Road .. ..	Between Wycheproof and Glenloth .. ..	4·22	4·45	4·45	4·45	4·45
YACKANDANDAH SHIRE—						
Dederang—Tawanga Road .. ..	Gravelling near Tawanga P.R., Parish of Tawanga .. ..	1·55	..	..	1·55	1·55
Kiewa—Wodonga Road .. ..	Timber bridge with gravel approaches over Yackandandah Creek at Baranduda .. ..	·28	..	·28	·28	·28
Running Creek Road .. ..	Gravelling from connexion with Dederang—Tawanga Road and reinforced concrete culvert .. ..	1·44	..	..	..	..
Wollolonga Gap Road .. ..	Earthworks and fencing deviation, Section IX., Parish of Kergunyah .. ..	1·74	..	..	..	..
YEA SHIRE—						
Glenburn—Yarra, Glen Road .. ..	Matchett's Hotel to West's Bridge .. ..	1·71	1·71	1·71	1·71	1·71
Molesworth—Dropmore Road .. ..	North end to Watershed Road .. ..	1·45	1·45	1·45	1·45	1·45
	Total .. ..	300·63	202·46	208·99	191·59	195·19

UNDER DIRECT SUPERVISION OF THE BOARD.

ALBERTON SHIRE—						
Tarra Valley Road .. ..	Re-forming and metalling from the Ridge Road southwards .. ..	1	1	1	1	1
Binginwarri—Welshpool Road .. ..	Construction .. ..	..	2	2	..	Done by day labour
Madalya Road .. ..	Formation from ch. 10,000 feet to ch. 19,000 feet .. ..	..	1·7	1·7	1·7	1·7
The Ridge Road .. ..	Construction from Balook to Tarra Valley .. ..	2·95	..	..	2·95	2·95
Whitelaw's Track .. ..	Formation .. ..	1·07	..	..	..	..
ALBERTON AND MORWELL SHIRES—						
Gonyah—Ryton Road .. ..	Clearing and grubbing .. ..	4·15	..	..	4·15	4·15
Boolarra—Welshpool Road .. ..	Metalling .. ..	6·9	..	..	6·9	6·9
Gonyah—Ryton Road .. ..	Metalling .. ..	4·	..	..	4·	4·
Whitelaw's Track .. ..	Forming and grading .. ..	1·77	..	..	1·77	1·77
AVOCA SHIRE—						
Maryborough—Natte Yallock Road .. ..	Bridge over Homebush Creek .. ..	·06	..	..	..	..
BELLARINE SHIRE—						
Barwon Heads—Ocean Grove Road .. ..	.. ..	..	1·75	1·75	1·75	1·75
BENALLA SHIRE—						
Toombullup Road .. ..	Formation ch. 36,000 to ch. 45,240 .. ..	1·75	1·75	1·75	1·75	1·75
BRIGHT AND YACKANDANDAH SHIRES—						
Happy Valley Road .. ..	Formation ch. 58,000 feet to ch. 92,570 feet .. ..	2·3	6·55	6·55	2·3	2·3
BELLARINE AND SOUTH BARWON SHIRES—						
Barwon Heads—Ocean Grove Road .. ..	Timber bridge over Barwon River at Barwon Heads .. ..	·29	..	..	·29	·29
HEALESVILLE SHIRE—						
Healesville—Toolangi Road .. ..	Fencing .. ..	1·22	..	..	1·22	1·22
" .. ..	Formation, &c. .. ..	4·75	4·75	4·75	4·75	4·75
" .. ..	Metalling 5 miles from Healesville .. ..	·9	·9	·9	·9	·9
Woori Yallock—Cockatoo Road .. ..	Four timber bridges over Yarra River .. ..	·58	..	..	·58	·58
HEYTESBURY SHIRE—						
Timboon—Nirranda Road .. ..	Re-forming and metalling in Parish of Brucknell .. ..	..	..	1	1	1
" .. ..	Forming in the Parish of Brucknell .. ..	..	..	3·3	3·3	3·3
" .. ..	Forming and metalling .. ..	·1	·16	·16	·16	·16
HEYTESBURY AND WARRNAMBOOL SHIRES—						
Ayesford Road .. ..	Clearing and forming from Brucknell's Creek to Timboon—Nirranda Road .. ..	2	..	4	4	4
Peterborough—Port Campbell Road .. ..	Timber bridge over Curdie's Inlet .. ..	..	·32	·32	·32	·32
LILLYDALE SHIRE—						
Monbulk—Seville Road .. ..	Formation in the Parish of Wandin .. ..	1·8	..	1·8	..	(Done by day labour)
MORWELL SHIRE—						
Boolarra—Foster Road .. ..	Supply of gravel .. ..	..	..	..	..	..
" .. ..	Construction of fencing .. ..	·86	..	..	·86	·86
Gonyah—Ryton Road .. ..	Metalling contract 1 S.P. 115 .. ..	1·7	..	1·7	1·7	1·7
Morwell River Road .. ..	Fencing through Allotment 22, Parish of Budgerec .. ..	·6	..	·6	·6	·6
Morwell River Road .. ..	Re-forming and sanding .. ..	·92	..	..	·92	·92
Boolarra—Welshpool Road .. ..	Metalling near "English's" .. ..	1·93	..	..	1·93	1·93
MORWELL AND ALBERTON SHIRES—						
Boolarra—Welshpool Road .. ..	Forming and metalling .. ..	4·15	..	..	4·15	4·15
MORWELL AND SOUTH GIPPSLAND SHIRES—						
Gonyah—Ryton Road .. ..	Formation, &c. .. ..	2·26	..	..	2·26	2·26
NARRACAN SHIRE—						
Childers—Settlement Road .. ..	Construction of Sections 1, 2, 3, 4 from Leviston's to Diver's .. ..	6·75	..	..	6·75	6·75
" .. ..	Metalling of Sections 1, 2, 3, 4, from Leviston's to Diver's .. ..	..	..	6·75	6·75	6·75
Childers Settlement Connexion .. ..	Across Childers Estate .. ..	..	1·74	..	..	..
Childers Settlement Road .. ..	From Diver's to Elizabeth Creek .. ..	..	..	1·7	1·7	1·7
Allambee—Childers Road .. ..	From Childers Settlement Road to Childers .. ..	2·85	..	..	..	..
Aberfeldy—Matlock Road .. ..	Near Aberfeldy .. ..	2·76	..	..	..	..
	Carried forward .. ..	63·27	22·62	41·73	72·41	72·41

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>						
Brought forward .. .. .		63.27	22.62	41.73	72.41	72.41
OTWAY SHIRE—						
Wild Dog Valley Road .. .. .	Bushy Access Road .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Forrest-Apollo Bay .. .. .	Metalling from Tanybryn to the coast .. .. .	7.35	.. .. .	7.35	(Done by day labour)	(Done by day labour)
Mount Sabine-Laver's Hill Road .. .. .	Near Tanybryn .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Apollo Bay-Wye River Road .. .. .	Supply 13,000 cubic yards metal between Beech Forest and Laver's Hill .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Laver's Hill-Princetown Road .. .. .	Forming near Snyth's Creek .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Beech Forest-Laver's Hill Road .. .. .	Timber bridge over Kennet River .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Hordern Vale-Apollo Bay Road .. .. .	Forming and metalling .. .. .	4.33	.. .. .	.. .. .	4.33	4.33
Glen Aire-Laver's Hill Road .. .. .	Metalling Section 1 .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Princetown Road .. .. .	Forming Section 8 .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Apollo Bay-Wye River Road .. .. .	Forming Section 9 .. .. .	1.23	.. .. .	.. .. .	.. .. .	.. .. .
ORBOST SHIRE—	From Hordern Vale to within 3 miles of Apollo Bay .. .. .	.. .. .	12	1.9	.. .. .	.. .. .
Orbost-Delegate Road .. .. .	From ch. 31,250 feet to ch. 48,833 feet .. .. .	.. .. .	.. .. .	2.2	2.2	2.2
Wangarabelle Road .. .. .	Reforming and metalling from Wattle Hill towards Gellibrand River .. .. .	.. .. .	.. .. .	1.7	1.7	1.7
Erinundra Road .. .. .	Reinforced concrete bridge over Brown's Creek .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Orbost-Delegate Road .. .. .	Reinforced concrete bridge over Petticoat Creek .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
OXLEY SHIRE—						
Whitfield-Tohmie Road .. .. .	Construction .. .. .	1.82	.. .. .	.. .. .	1.82	1.82
OXLEY AND BENALLA SHIRES—	From Wangarabelle Creek to New South Wales border—Formation .. .. .	2.85	5.86	2.85	2.85	2.85
Toombullup Road .. .. .	Formation 10 miles north of Club Terrace .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
ROSEDALE AND TRARALGON SHIRES—	Formation near Bonang .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Callignee South Road .. .. .	Forming ch. 41,600 feet to ch. 66,303 feet .. .. .	5	.. .. .	5	5	5
ROSEDALE SHIRE—	Forming and metalling .. .. .	2	.. .. .	.. .. .	3.6	3.6
Callignee South Road .. .. .	Forming and metalling along the Shire boundary .. .. .	2	.. .. .	2	2	2
ROSEDALE AND ALBERTON SHIRES—						
Merrinian's Creek Road .. .. .	Forming ch. 00 feet to 6,400 feet and ch. 00 to 19,246 feet .. .. .	1.21	4.38	1.21	1.21	1.21
SOUTH GIPPSLAND SHIRE—	Fencing through Allotment 21, Parish of Callignee .. .. .	.. .. .	.. .. .	1.35	.. .. .	.. .. .
Turton's Creek Road .. .. .	Along Merrinian's Creek—Construction, including two timber bridges and two cattle subways .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Foster-Boolarra Road .. .. .	Metalling from Turton's Creek State School to Dollar-Woorara West Road .. .. .	2.45	.. .. .	2.45	2.45	2.45
Woorarra West Road .. .. .	From Boolarong to Gunyah Junction—metalling .. .. .	.. .. .	.. .. .	4.4	4.4	4.4
SWAN HILL SHIRE—	Re-forming and metalling near the Turton's Creek .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Manangatang-Euston Road .. .. .	Formation, &c. .. .. .	1	.. .. .	.. .. .	1	1
TAMBO SHIRE—						
Basin Road .. .. .	Forming from ch. 44,000 feet to ch. 47,800 at the Gap .. .. .	1.1	.. .. .	1.1	(Done by day labour)	(Done by day labour)
Basin Road .. .. .	Timber bridge over the Murrindal River .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Nyerimilang Road .. .. .	Forming .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
TRARALGON SHIRE—						
Traralgon Creek Road .. .. .	Hogg's Bridge to Brown's Saddle .. .. .	.. .. .	5.25	.. .. .	.. .. .	.. .. .
TRARALGON AND ALBERTON SHIRES—						
Ridge Road .. .. .	Valley View to Tarra Valley Saddle .. .. .	.. .. .	2.15	.. .. .	.. .. .	.. .. .
UPPER YARRA SHIRE—						
Woori Yallock-Cockatoo Road .. .. .	Re-forming, &c., near Woori Yallock Railway Station .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
WARRAGUL SHIRE—						
Darnum-Allambee Road .. .. .	McDonald's Track to Ellinbank Road—Re-forming and sanding .. .. .	.. .. .	.. .. .	.. .. .	3	3
WARRAGUL AND KORUMBURRA SHIRE—						
Trida-Sztelecki Road .. .. .	Forming from ch. 6,200 feet to ch. 10,000 feet .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
WOORAYL SHIRE—						
Dollar-Dumbalk Road .. .. .	Fencing .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Dollar-Dumbalk Road .. .. .	.. .. .	1.96	.. .. .	.. .. .	.. .. .	.. .. .
Boortool Road .. .. .	.. .. .	1.89	.. .. .	.. .. .	.. .. .	.. .. .
YEA SHIRE—						
Yarra Glen-Glenburn Road .. .. .	Forming and gravelling from Mount Slide to Yea River .. .. .	.. .. .	.. .. .	.. .. .	.. .. .	.. .. .
Totals .. .. .		105.2	57.97	87.36	116.63	114.43



## APPENDIX K.

## COUNTRY ROADS BOARD.

## STATE HIGHWAYS.

## STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED AND HIGHWAYS MAINTAINED DURING THE YEAR ENDED 30TH JUNE, 1927.

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES.				
ARARAT SHIRE— Western Highway—Section 2 ..	Metalling between Buangor and Middle Creek	Putting bitumen on road at Buangor—4 miles, and general maintenance	2·18	22·0
"    "    "    Section 3 ..	At Buangor .. .. .	Bitumen coating, &c., 2 miles .. .. .	·64	2
BACCHUS MARSH— Western Highway—Section 1 ..	.. .. .	Re-construction and general maintenance .. .. .	..	11·58
BELFAST SHIRE— Prince's Highway West—Section 3 ..	.. .. .	Bitumen spraying from Shire of Warrnambool to Moyne River	..	6·48
"    "    "    Section 4 ..	.. .. .	Bitumen spraying west from Borough of Port Fairy	..	1·32
"    "    "    "    " ..	.. .. .	From Borough of Port Fairy to Yumbuk—7½ miles of metal for bitumen penetration	..	7·5
BULN BULN SHIRE— Prince's Highway East—Section 1 ..	.. .. .	From Bunyip River to Western boundary of Drouin Township, then from Eastern boundary of Township to Shire boundary	..	8·75
BUNINYONG SHIRE— Western Highway—Section 1 ..	.. .. .	Pot holes repaired between Brewery Tap Hotel and Bungaree	..	2·91
"    "    "    "    " ..	.. .. .	Dressed and penetrated with bitumen between Ballarat and Brewery Tap Hotel	..	1·53
"    "    "    "    " ..	.. .. .	Widened to 20 feet. Sheeted with metal and penetrated with bitumen	..	2·89
COLAC SHIRE— Prince's Highway West—Section 2 ..	.. .. .	Between Colac and Cororooke Road re-sheeted and penetrated with bitumen and general maintenance	..	1·91
DANDENONG SHIRE— Prince's Highway .. .. .	.. .. .	Between Mulgrave and Berwick Shire boundaries	..	6·25
DIMBOOLA SHIRE— Western Highway—Section 4 ..	.. .. .	General maintenance .. .. .	..	..
HAMPDEN SHIRE— Prince's Highway .. .. .	.. .. .	General maintenance .. .. .	..	28
HEYTESBURY SHIRE— Prince's Highway West—Section 2 ..	.. .. .	General maintenance .. .. .	..	11·5
HUNTLY SHIRE— Northern Highway .. .. .	.. .. .	Through Epsom and Huntly .. .. .	..	6
KORONG SHIRE— North-Western Highway—Section 2 ..	.. .. .	Flanking and sheeting with gravel off Crown allotments 1, 2, and 3, Section 3, Parish of Inglewood, and general maintenance	..	1
"    "    "    Section 3 ..	Metalling opposite Crown allotments 11B, Parish of Woosang, and 8, Section 1, Parish of Borung	Sheeting and flanking opposite Crown allotment 55A, Parish of Woosang, and general maintenance	·53	·5
LAWLOIT SHIRE— Western Highway—Section 5 ..	Gravelling between Kaniva and Border fence	Tarring, &c., and general maintenance	1·61	5
LOWAN SHIRE— Western Highway—Section 5 ..	Between allotments 48A and 48B, Parish of Kinimakatka, and 60A and 59-71, Parish of Tarraginnee	Between allotments 70 and 71, Parish of Tarraginnee, 2 and 1, Kinimakatka, and A and B, Parish of Kinimakatka	1·02	·5
"    "    Section 4 ..	.. .. .	Between allotments 47A and 22 and 24 and 65, Parish of Balrootan	..	·37
MILDURA SHIRE— North-Western Highway .. .. .	Carwarp to Yatpool .. .. .	General maintenance, Trinita and Irymple .. .. .	7	54
MORWELL SHIRE— Prince's Highway East—Section 2 ..	Metalling near foot of Haunted Hills	.. .. .	·45	..
OMEQ SHIRE— Omeo Highway .. .. .	Bald Hill Creek section .. .. .	Between Ramrod Creek and Glen Wills .. .. .	1·25	120
ORROST SHIRE— Prince's Highway East—Sections 4 and 5 ..	.. .. .	Bridges re-conditioned and general maintenance	..	28·68
PORTLAND SHIRE— Prince's Highway—Section 4 ..	.. .. .	General maintenance from boundary of Port Fairy Shire, Tyrendarra East to Narrawong	..	..
"    "    "    "    " ..	.. .. .	From Narrawong to Bolwarra Junction—metal sheeting and road repairs	..	27·4
"    "    "    "    " ..	.. .. .	From Bolwarra Junction to Heathmere—metal sheeting and general maintenance	..	..
"    "    "    "    " ..	.. .. .	From Heathmere to Heywood—gravel sheeting and general maintenance	..	..
"    "    Section 5 ..	Greenwald towards Dartmoor .. .. .	Heywood to Lyons—gravel sheeting and general maintenance	3·45	..
"    "    "    "    " ..	.. .. .	Lyons to Greenwald—gravel sheeting and general maintenance	..	..
"    "    "    "    " ..	.. .. .	Greenwald towards Dartmoor—metal sheeting and general maintenance	..	26·5
"    "    "    "    " ..	.. .. .	Dartmoor towards Mumbannar—metal sheeting and general maintenance	..	..
ROCHESTER SHIRE— Northern Highway .. .. .	From north-east angle allotment 16, Parish of Ballendella, to north-east angle allotment 34, Parish of Milleva	.. .. .	3·1	..
Carried forward .. .. .			21·23	384·59

## STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER MUNICIPALITIES—continued.				
		Brought forward .. ..	21·23	384·59
STAWELL SHIRE— Western Highway—Section 3 ..	Invert, North of Glenorchy ..	General maintenance and re-sheeting throughout where required ..	·34	23
TAMBO SHIRE— Omeo Highway—Section 1 ..	.. .. .	Nicholson River and Ramrod Creek .. ..	..	8
Prince's Highway—section 3 ..	.. .. .	Nicholson River and Nowa Nowa .. ..	..	37
TOWONG SHIRE— Omeo Highway .. ..	.. .. .	3 miles re-conditioned in various sections between Mitta Mitta and Tallandoon, and general maintenance .. ..	·58	70
" .. ..	.. .. .	.. .. .	1·09	..
" .. ..	.. .. .	.. .. .	·15	..
" .. ..	.. .. .	.. .. .	·58	..
WALPEUP SHIRE— North-Western Highway—Section 5 ..	.. .. .	South of Ouyen .. ..	..	2·25
WARRAGUL SHIRE— Prince's Highway—Sections 1 and 2 ..	.. .. .	From Shire boundary to Western town boundary, and from Eastern Town boundary to Shire boundary .. ..	..	7·75
WARRNAMBOOL SHIRE— Prince's Highway .. ..	.. .. .	Sheeted and sprayed .. ..	..	6·8
WIMMERA SHIRE— Western Highway—Section 3 ..	.. .. .	Shouldering, widening, re-sheeting south of 5 and 7B, Parish of Horsham .. ..	..	·73
" .. ..	.. .. .	Re-sheeting near Drung South School .. ..	..	·12
" .. ..	.. .. .	Trimming clay formation west of Burnt Creek .. ..	..	·4
" .. ..	.. .. .	Widening and re-sheeting between 139A and 195C, Parish of Bungally .. ..	..	·38
" .. ..	.. .. .	Machine work between end of Gravel and Three Bridges .. ..	..	2·75
" .. ..	Section 4 ..	Re-forming between 81 and 82, Parish of Wail .. ..	..	·31
" .. ..	.. .. .	Re-forming between 131 and 136, Parish of Wail .. ..	..	·37
" .. ..	.. .. .	Re-forming between 67/68 and 60/58, Parish of Wail .. ..	..	·12
" .. ..	.. .. .	Re-forming east of 83, Parish of Wail .. ..	..	·25
" .. ..	.. .. .	Dragging (3 times) .. ..	..	12
" .. ..	.. .. .	Loaming east of 154, Parish of Wail .. ..	..	·36
" .. ..	.. .. .	Re-sheeting between 163 and 170, Parish of Vectis East .. ..	..	·25
WODONGA SHIRE— North-Eastern Highway .. ..	.. .. .	General maintenance .. ..	..	6·91
Omeo Highway .. ..	.. .. .	General maintenance .. ..	..	11
WYCHEPROOF SHIRE— North-Western Highway—Section 3 ..	South Wycheproof ..	South Wycheproof .. ..	·33	·97
" .. ..	Section 4 ..	Between Wycheproof and Dumosa, Dumosa and Nullawil Nullawil and Culgoa, Culgoa and Berrillock, and Berrillock and Sea Lake .. ..	·46	5·06
" .. ..	Section 5 ..	North of Sea Lake .. ..	..	1·04
		Total .. ..	24·73	582·41

## UNDER DIRECT SUPERVISION OF THE BOARD.

AVON AND BAIRNSDALE SHIRES— Prince's Highway East—Section 3 ..	Bridge over Toms Creek .. ..	Generally between Old Turf Hotel and B.R.C. .. ..	..	50
" .. ..	.. .. .	Between Lucknow and Sarsfield .. ..	..	7
BAIRNSDALE SHIRE— Omeo Highway—Section 1 ..	.. .. .	Between Mitchell and Nicholson Rivers .. ..	..	5
Prince's Highway East—Section 4 ..	.. .. .	5,112 cubic yards metal, screenings, and toppings at Ballan and Gordon Railway Sidings .. ..	..	..
BALLAN SHIRE— Western Highway—Section 1 ..	.. .. .	.. .. .	..	..
BALLARAT SHIRE— Western Highway—Section 2 ..	At Burrumbeet Park .. ..	Ballarat to Burrumbeet Park .. ..	·65	2·36
" .. ..	.. .. .	Ballarat to Shire boundary .. ..	..	2·75
BENALLA SHIRE— North-Eastern Highway—Section 2 ..	3,318 lin. feet of grubbing, clearing, forming, and metalling .. ..	.. .. .	·63	..
" .. ..	.. .. .	Supply of 14,300 cubic yards of metal and screenings .. ..	..	..
" .. ..	.. .. .	Trimming and re-sheeting with gravel 4,000 lineal feet .. ..	..	·75
" .. ..	Section 3 ..	Re-sheeting 6,000 lineal feet with granite sand .. ..	..	1·13
" .. ..	.. .. .	Re-sheeting 6,000 lineal feet with gravel .. ..	..	1·13
" .. ..	.. .. .	Grubbing, clearing, forming, and metalling 3,000 lineal feet .. ..	..	·57
BERWICK SHIRE— Prince's Highway East—Section 1 ..	.. .. .	12,710 cubic yards of metal, screenings, and toppings between Narre Warren and Berwick and Cardinia Creek and Pakenham .. ..	..	..
" .. ..	.. .. .	6,000 cubic yards of metal, &c., Narre Warren and Berwick .. ..	..	..
" .. ..	.. .. .	8,160 cubic yards of metal, &c., Pakenham and Deep Creek .. ..	..	..
BRAYBROOK AND MELTON SHIRES— Western Highway—Section 1 ..	.. .. .	14 440 cubic yards of metal screenings and toppings between Sunshine and Melton .. ..	..	..
BROADMEADOWS SHIRE— North-Eastern Highway—Section 1 ..	.. .. .	Supply 19,644 cubic yards of metal, screenings, and toppings between Craigieburn and Wallan .. ..	..	..
CHARLTON SHIRE— North-Western Highway—Section 3 ..	.. .. .	6,250 cubic yards of metal and screenings .. ..	..	..
" .. ..	.. .. .	Patrol only .. ..	..	18
CHILTERN SHIRE— North-Eastern Highway—Section 3 ..	Three-cell reinforced concrete culvert 22 feet long near Wenkes .. ..	.. .. .	·008	..
EUROA SHIRE— North-Eastern Highway—Section 2 ..	Construction of reinforced concrete bridge over Nine Mile Creek with approaches .. ..	.. .. .	·02	..
GOULBURN SHIRE— North-Eastern Highway—Section 2 ..	Construction three reinforced concrete culverts near Larry's Hut Creek .. ..	.. .. .	..	..
" .. ..	2 miles forming and grading between Avenel and Longwood .. ..	.. .. .	2	..
" .. ..	Five-cell reinforced concrete culvert over Burnt Creek .. ..	.. .. .	·08	..
		Carried forward .. ..	3·383	88·69

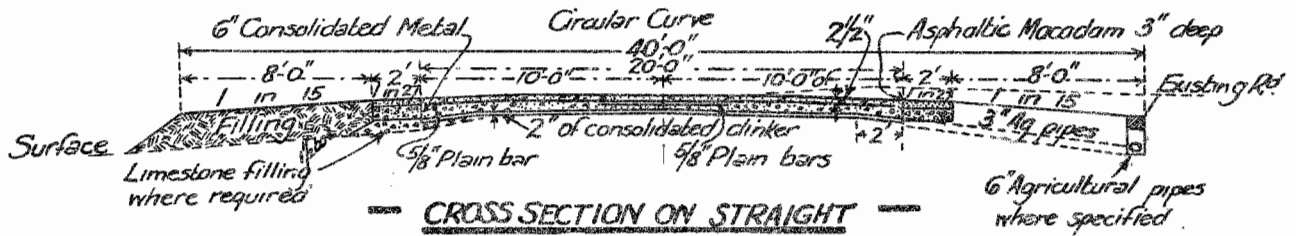
STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Highway.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER DIRECT SUPERVISION OF THE BOARD—<i>continued.</i></b>				
		Brought forward .. ..	3·383	88·69
KEILOR AND BULLA SHIRES— North-Western Highway—Section 1	.. .. .	16,000 cubic yards of metal and screenings .. ..	..	..
KORONG AND CHARLTON SHIRES— North-Western Highway—Section 3	Woosang Flats .. ..	.. .. .	1·33	..
METCALFE AND KYNETON SHIRES— North-Western Highway—Section 1	.. .. .	Supply 6,200 cubic yards of metal and screenings between Taradale and Malmesbury .. ..	..	..
MARONG SHIRE— North-Western Highway—Section 2	.. .. .	Castlemaine to Bridgewater. (Patrol only) .. ..	..	31
" " " " "	Kangaroo Flats .. ..	.. .. .	55	..
" " " " "	Big Hill .. ..	.. .. .	1	..
MELTON SHIRE— Western Highway—Section 1	.. .. .	Supply 22,180 cubic yards of metal, screenings, and toppings between Sunshine and Melton .. ..	..	..
MORWELL AND TRARALGON SHIRES— Prince's Highway East—Section 2	.. .. .	Generally throughout .. .. .	..	60
" " " " "	Metalling between Yallourn and Morwell .. ..	.. .. .	..	..
MULGRAVE SHIRE— Prince's Highway East—Section 1	.. .. .	Widening, re-sheeting, and asphaltic concrete 15,265 lineal feet .. ..	..	2·89
" " " " "	.. .. .	18,900 feet widening, re-sheeting, and surfacing with asphaltic concrete .. ..	3·6	..
NARRACAN SHIRE— Prince's Highway East—Section 2	.. .. .	Haunted Hills alignment between Moe Creek and Pearson's Bridge .. ..	..	..
NEWHAM AND WOODEND SHIRE— North-Western Highway—Section 1	.. .. .	5,000 cubic yards of metal unloaded and carted at Woodend .. ..	..	..
" " " " "	.. .. .	5,500 cubic yards of metal and screenings i.o.r. Black's Siding .. ..	..	..
North-Western Highway—Section 1	.. .. .	5,500 cubic yards of metal carted and stacked at Woodend .. ..	..	..
OMEQ SHIRE— Omeo Highway .. ..	.. .. .	Construction of approaches to bridge over St. Patrick's Creek .. ..	·03	..
ORBOST SHIRE— Prince's Highway East—Section 5	.. .. .	Between Brodribb River and Cann River .. ..	..	50
" " " " " 6	.. .. .	Bridge over Genoa River .. ..	..	..
" " " " " 6	.. .. .	Between Cann River and N.S.W. Border .. ..	..	43
PORTLAND SHIRE— Prince's Highway West—Section 5	.. .. .	Between Dartmoor and S.A. Border .. ..	6·79	..
RIPON SHIRE— Western Highway—Section 2	.. .. .	West of Beaufort .. ..	..	1·88
RIPON AND LEXTON SHIRES— Western Highway—Section 2	.. .. .	Between Burrumbeet and Trawalla .. ..	..	56
" " " " "	.. .. .	Between Burrumbeet and Trawalla .. ..	71	..
ROSEDALE SHIRE— Prince's Highway East—Section 2	.. .. .	Bitumen work east and west of Traralgon .. ..	7	..
SEYMOUR SHIRE— North-Eastern Highway—Section 2	.. .. .	4,400 feet clearing and forming .. ..	85	..
" " " " "	.. .. .	Reinforced concrete culvert, 6 feet span, over Saunders Creek .. ..	·001	..
" " " " "	.. .. .	Three-cell reinforced concrete culvert over Charles Creek .. ..	·003	..
SEYMOUR AND GOULBURN SHIRES— North-Eastern Highway—Section 2	.. .. .	6·29 miles forming and gravelling between Avenel and Longwood .. ..	6·29	..
WANGARATTA SHIRE— North-Eastern Highway—Section 3	.. .. .	6,887 feet trimming and re-sheeting from Northern boundary of Wangaratta Borough .. ..	..	1·3
" " " " "	.. .. .	6,000 feet re-sheeting with gravel .. ..	..	(day labour) 1·13
WINCHELSEA SHIRE— Prince's Highway West—Section 2	.. .. .	34,000 feet excavating, trenching, and widening of existing metal bed .. ..	6·5	..
WODONGA SHIRE— North-Eastern Highway—Section 3	.. .. .	1,900 feet clearing, forming, at Barnawartha Subway .. ..	·36	..
BENALLA, WANGARATTA, OXLEY, CHILTERN, AND RUTHERGLEN SHIRES— North-Eastern Highway—Section 3	.. .. .	Generally throughout .. ..	..	55·45
SEYMOUR, GOULBURN, VIOLET TOWN, AND BENALLA SHIRES— North-Eastern Highway—Section 2	.. .. .	Generally throughout .. ..	..	55·65
SHIRES NOT STIPULATED— Western Highway—Section 2	.. .. .	Generally throughout between Ballarat and Middle Creek .. ..	..	36·5
SHIRE NOT STIPULATED— North-Western Highway—Section 1	.. .. .	1,614 cubic yards of metal and screenings .. ..	..	..
" " " " "	.. .. .	4,065 feet of metal and screenings .. ..	..	..
		Total .. ..	38·397	428·05

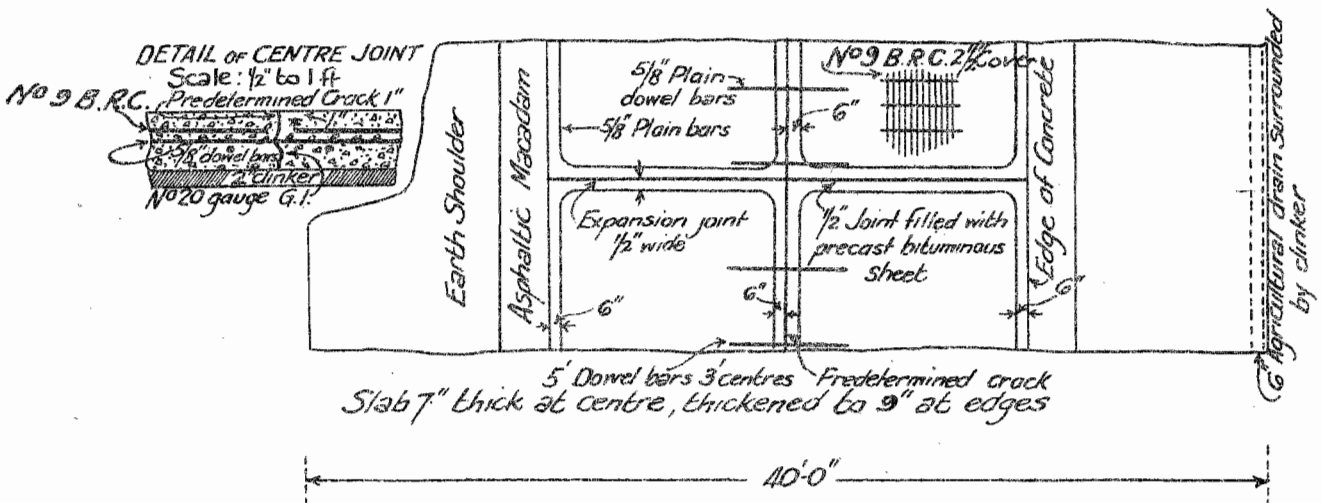
# — Typical Cross Sections —

— Scale: 1/8 in to 1 ft —

— FIGS 1&2 CONTRACT NO 38FP60 —

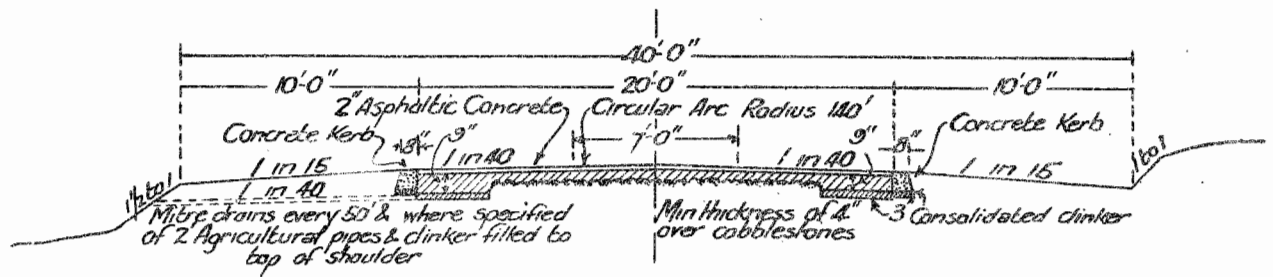


— Fig. 1 —

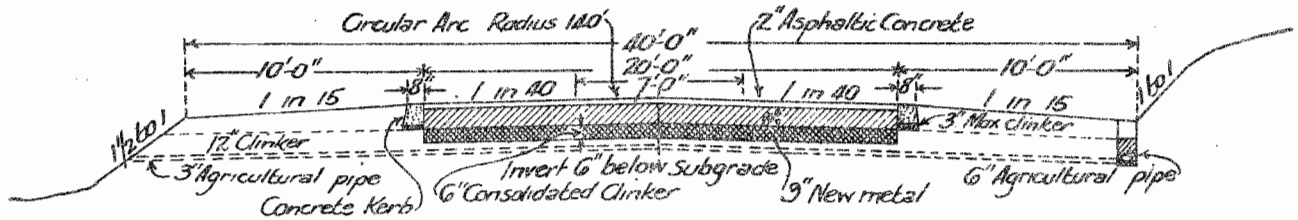


— Fig. 2 —

— FIGS 3&4 CONTRACT NO 23F.H.9 —



— Fig. 3 —

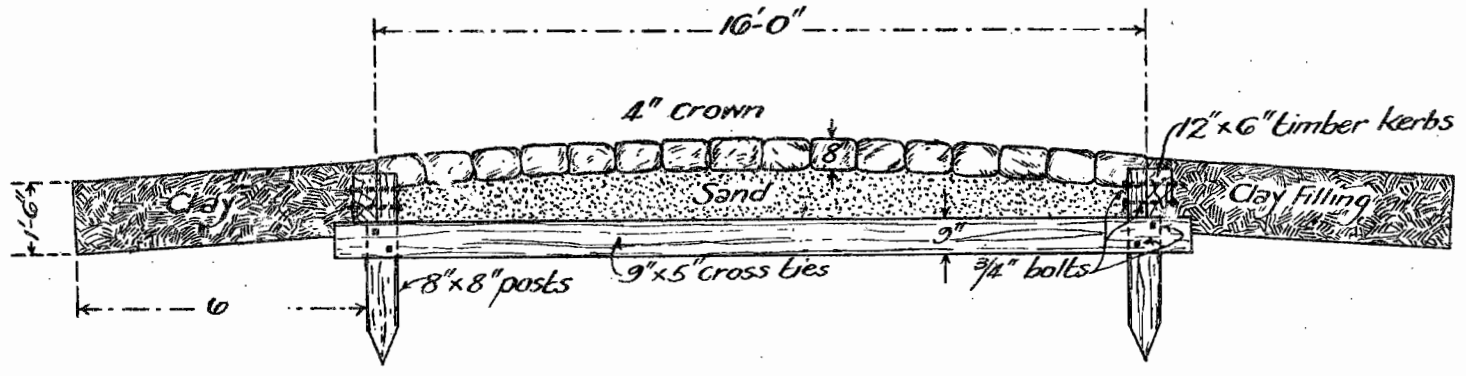


— Fig. 4 —

*Section of the paved portion of the Old  
Williamstown Short Road before reconstruction*

*— Scale: 1/4 inch = 1ft —*

*-Fig. 5-*

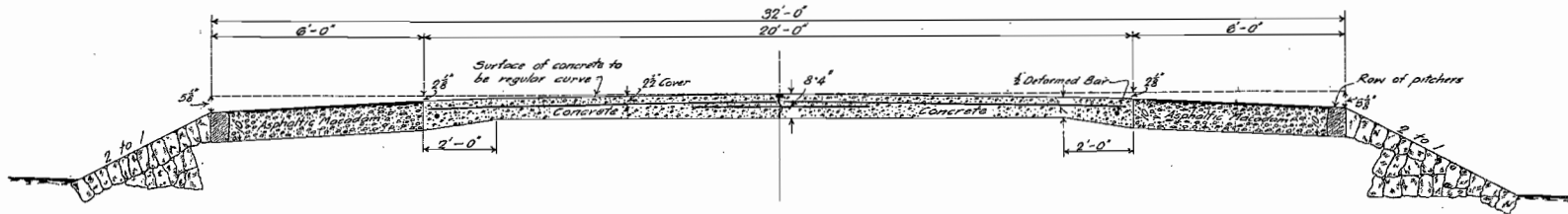


— C. R. B. —

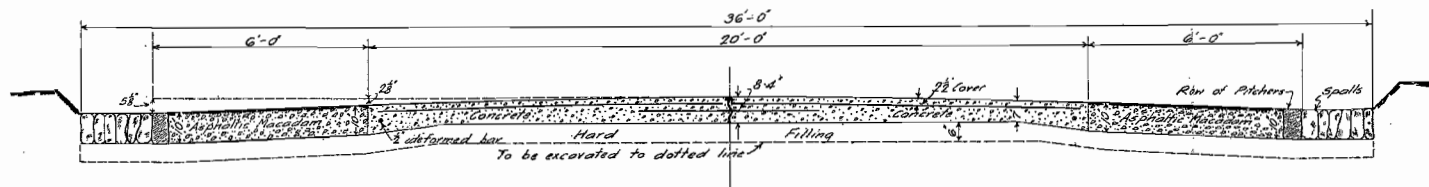
— WILLIAMSTOWN SHORT ROAD. — GRAHAM ST TO R. YARRA. —

— TYPE DRAWINGS. —

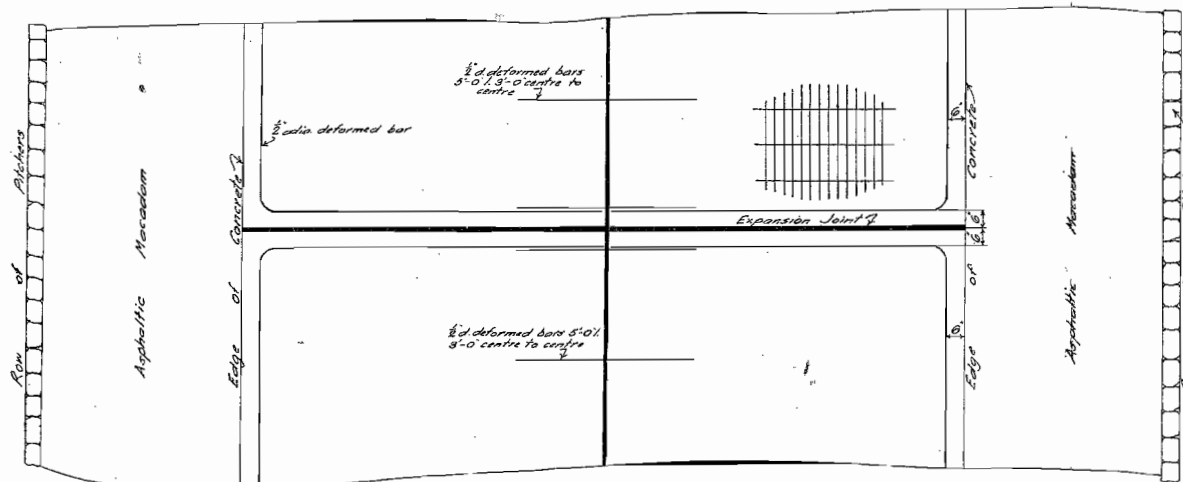
— Fig 6 —



— CROSS SECTION IN FILL —



— CROSS SECTION IN CUT —



— PART PLAN —

Trim to outer line