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# COUNTRY ROADS BOARD

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## TWELFTH ANNUAL REPORT

FOR YEAR ENDED 30<sup>TH</sup> JUNE, 1925.

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PRESENTED TO BOTH HOUSES OF PARLIAMENT PURSUANT TO ACT No. 2635.

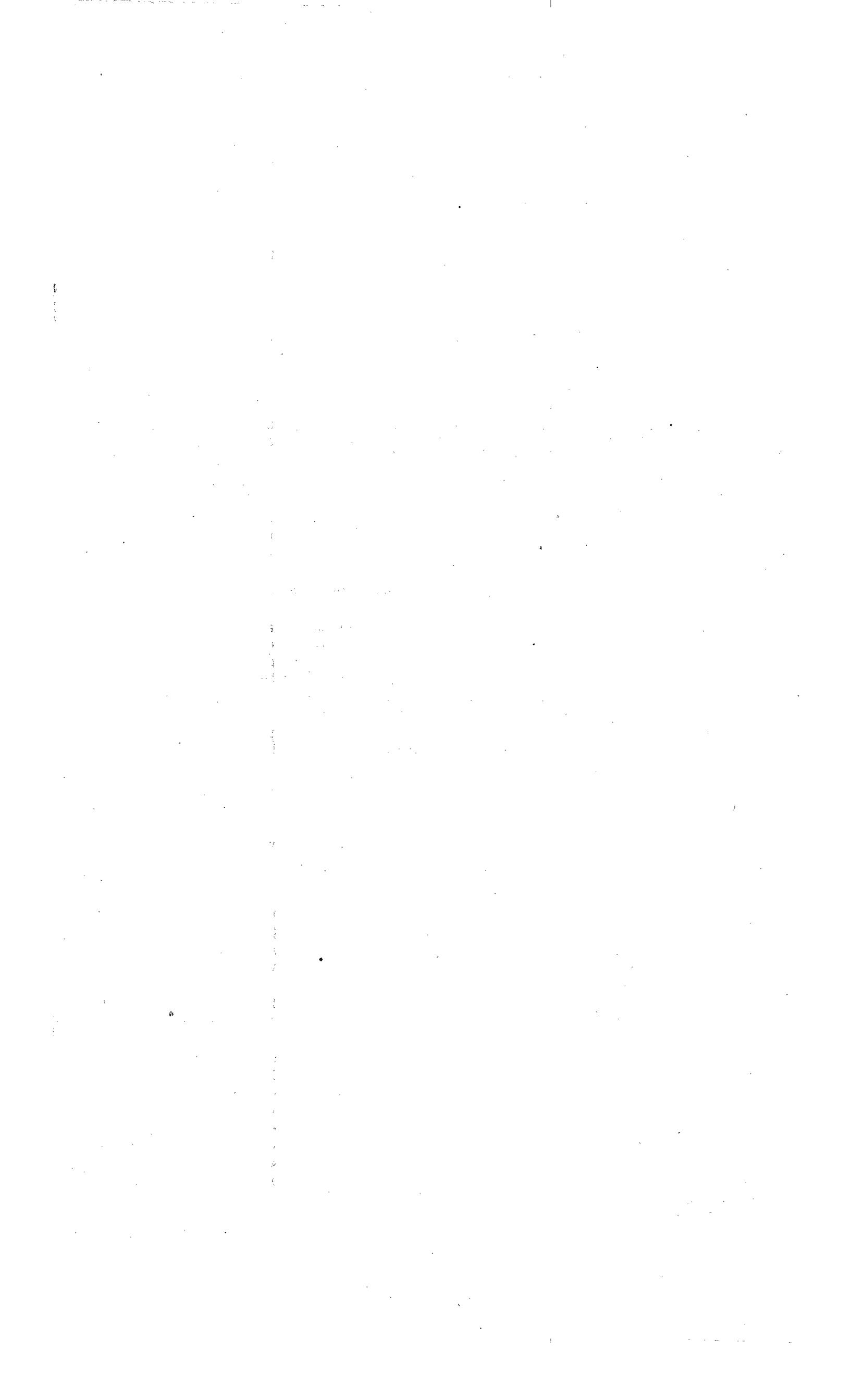
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# COUNTRY ROADS BOARD.

## TWELFTH ANNUAL REPORT.

Melbourne, 31st December, 1925.

*The Honorable G. L. Goudie, M.L.C.,  
Minister for Public Works, Melbourne.*

SIR,

The Board has the honour to submit to you for presentation to Parliament the Twelfth Annual Report setting forth the proceedings of the Board for the financial year ending 30th June, 1925, in compliance with the provisions of Section 73 of the Country Roads Act No. 2635.

### AMENDING LEGISLATION.

The Highways and Vehicles Act No. 3379 passed in December, 1924, became operative during the year, when on the 11th February, 1925, the main road from Bairnsdale to Wodonga through Bruthen, Omeo, and Tallangatta was declared a State Highway under the designation of the Omeo Highway.

From the date mentioned, the financial responsibility for the maintenance of this highway has devolved upon the Board on behalf of the State.

The highway passes through the municipal districts of the Shires of Bairnsdale, Tambo, Omeo, Towong, Yackandandah and Wodonga, and although the whole cost of the work of maintenance is borne by the Board, advantage has been taken of the provisions of Section 8 of the Highways and Vehicles Act to permit of the local municipal authorities carrying out and supervising the work, to which they have readily agreed.

A system of continuous maintenance by employing permanent patrol gangs and patrol men as advocated by the Board in its First Annual Report, but not adopted by the Municipalities to any appreciable extent, has been inaugurated throughout the whole length of 184 miles of road with excellent results.

The following five (5) additional highways were selected and approved during the year, but their declaration did not become effective until 1st July, 1925, viz. :—

*Prince's Highway.*—Traversing the whole length of the State from its western boundary near Mount Gambier in South Australia, thence easterly through Port Fairy, Warrnambool, Camperdown, Colac, Geelong to Melbourne, thence through Dandenong, Warragul, Sale, Bairnsdale and Orbost to the eastern boundary of the State towards Eden in New South Wales. Length, 540 miles.

*North-Eastern Highway.*—From Melbourne north-easterly through Seymour, Benalla, Wangaratta, and Wodonga to the Murray River. Length, 161 miles.

*North-Western Highway.*—From Melbourne north-westerly through Kyneton, Castlemaine, Bendigo, Charlton, Sea Lake, and Ouyen to Mildura. Length, 324 miles.

*Western Highway.*—From Melbourne westerly through Ballarat, Ararat, Stawell, Horsham, and Dimboola to the western boundary of the State at Serviceton. Length, 244 miles.

*Northern Highway.*—From Bendigo northerly through Elmore and Rochester to Echuca. Length, 10 miles. Total length of State Highways, 1,463 miles.

Further amending legislation passed during the year—Act No. 3425—relates to the provision of additional loan funds for the construction of main and developmental roads.

The most important features of this legislation are that it ensures the provision of funds for continuous construction of main and developmental roads over a period of five years at the rate of £1,000,000 per annum. Of this sum £150,000 per annum is to be expended in the construction of roads in what are at present sparsely populated and undeveloped areas such as exist in the hill country of Gippsland, and in the Kinglake, Tolmie and Otway districts, without any contribution from the local councils.

The areas referred to were originally heavily timbered and have been only partially occupied and improved since they were made available for settlement between 30 and 40 years ago. They are eminently suitable for dairying and for the growth of root crops, but owing to the heavy annual rainfall, which is well distributed throughout the year, and to the broken nature of the country, road-making is costly and quite beyond the financial resources of districts in their early stages of development.

By the approved expenditure of £150,000 per annum, it is anticipated that confidence will be restored and that a permanent and prosperous closer settlement will result, without imposing any additional financial burden on existing settlers or on the local authorities.

#### THE CHAIRMAN'S VISIT ABROAD.

In view of the proposed expenditure of further large sums of State funds on road construction in this State, the Chairman of the Board was authorized by the Government to proceed to Europe and America to investigate the road problem and particularly to examine the methods of construction and maintenance adopted in those countries to meet the demands of modern traffic and to provide for rural development.

The results of his investigations are embodied in a comprehensive report to the Government, copies of which have been supplied to every municipal body in the State having the care and management of roads.

It is satisfactory to record the fact that the system laid down by this Board at its inception for providing roads for the development of the rural districts of this State, both as regards types of construction and gradients, are in accordance with the best methods adopted abroad.

#### A RETROSPECT.

When the Board entered upon its duties twelve years ago it selected the eastern portion of the State as being the most necessitous for the commencement of operations; the reasons for this, accompanied by graphic photographs of the then existing road conditions, were clearly set out in the Board's First Annual Report presented to Parliament.

Amongst the reasons advanced, apart from the pitiable condition of the settlers, were the fertility of the soil, the comparatively small holdings, and the stable climatic conditions, which, together, would enable the country to more rapidly respond to expenditure in road construction, and that in consequence the State generally would reap a greater and quicker return from the expenditure contemplated.

That this forecast is already within some measure of realization is indicated by a return furnished by the Government Statistician showing an increase in capital values of land and property based upon the municipal valuations in that territory during the period under review of £28,000,000, while the expenditure in road construction during the same period amounted to only £3,342,000.

It would perhaps be too much to claim that the whole of this great increase in capital values is due solely to road construction, but, inasmuch as apart from the activities of the State Rivers and Water Supply Commission which are destined to effect a wonderful improvement in values in the Maffra district, but which are not yet sufficiently advanced to materially affect the aggregate of valuations of so large an area as that under review, and as other State activities involving State expenditure have not been of such magnitude that they would greatly increase capital values, it may be fairly assumed that the roads already constructed or improved and in course of construction, together with the programme outlined for the next five (5) years, have been a very material factor in contributing to the results certified to by the Statistician.

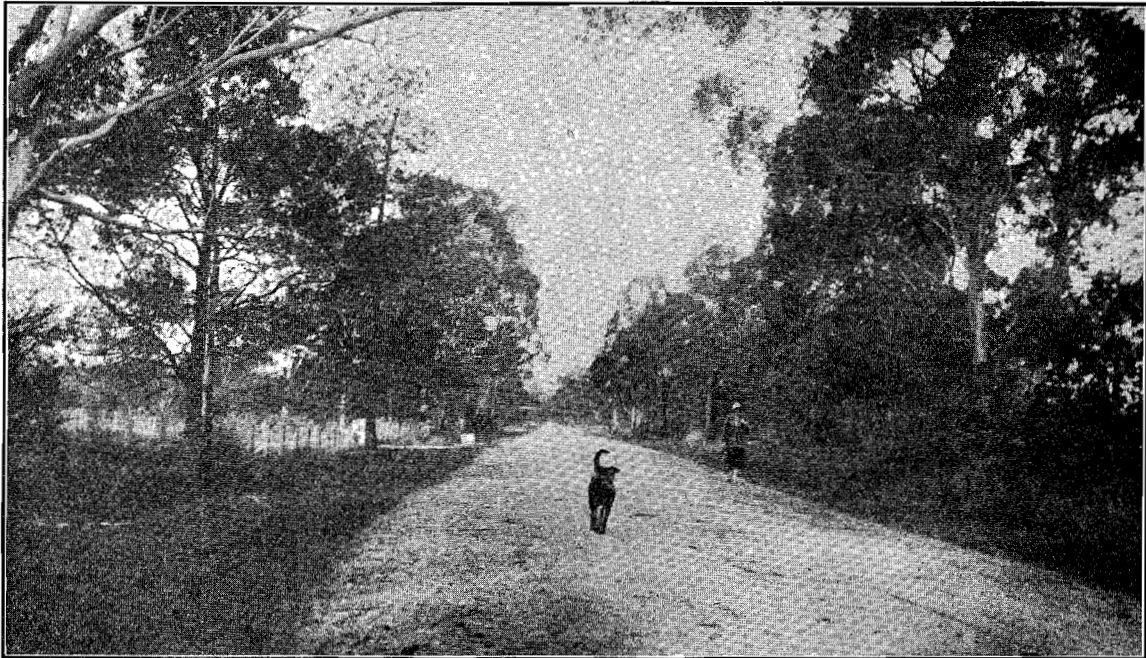
In this connexion the work of the Gippsland Shires and Boroughs Developmental Association has been very valuable and helpful. Formed originally with a view to collecting, for presentation to the Government, data concerning the requirements of the province, including the improvement of means of transport for settlers by roads and the disabilities of the settlers, the Association adopted the telling phrase "Neglected Gippsland" as its slogan, and it has succeeded so well in its efforts that now every conceivable subject relative to the welfare and advancement of the territory is remitted to the Annual Conference of the Association for discussion.

The Association has at all times been prepared to seek from, as well as to give advice to, the various authorities concerning measures and means having for their object the development of Gippsland, and the prevailing prosperity and optimism are in a great measure due to its active propagandism. Certain it is that the general atmosphere of pessimism and despair that existed

throughout the territory when the Board made a detailed investigation of its road requirements twelve years ago has almost disappeared, and has been succeeded by a feeling of confidence and hope for the future.

The principal works that have contributed to these improved conditions embrace—

1. The general improved condition and construction in sections of the Main Gippsland Road, now known as the eastern section of the Prince's Highway. This road is now trafficable at all seasons of the year to the eastern border of the State, a distance of 296 miles, and further extensive improvements necessitated by the increasing traffic are in progress throughout its entire length.



PLATES Nos. 1 and 2.

Showing Restored Section of Princes Highway.

2. The Main South Gippsland Road passing through Cranbourne, Korumburra Leongatha, and Foster. Up to the present the road has been constructed as far easterly as Meeniyan, a distance of 75 miles. Further easterly to Foster extensive works are in progress which are expected to be completed during next summer.

3. The Point Nepean Road from Moorabbin through Mordialloc, Frankston, Mornington and Dromana to Sorrento, and the Flinders Road from Frankston through Somerville and Hastings, which have been practically re-constructed throughout. These works have been responsible for the rapid extension of the residential area between Mordialloc and Frankston and for the increased popularity of the Peninsular both as a residential and productive area, resulting in capital values increasing from £5,070,260 in 1913 to £13,943,604 in 1925.
4. The Main Roads to Healesville and Warburton, which are now trafficable at all seasons, have also added materially to the advancement of those districts as holiday and tourist resorts.

While the improvement of these main arteries has been a great factor in the development of the eastern part of the State, it is the lateral or developmental road works that have been chiefly responsible for the returning confidence and the increasing prosperity of the Province of Gippsland.

Throughout the whole territory immediately north of the Main Gippsland railway line and southerly between that line and the coast it is now possible to travel in summer over a network of newly-located and suitably-graded roads, a number of which has also been completed and metalled or gravelled and is suitable for traffic at all seasons, and it is in bringing the remainder to this condition of service, either with metal or gravel, that portion of the developmental road funds recently voted by Parliament will be expended.

Amongst the most important of these lateral roads may be mentioned that from Frankston to Dandenong, Ferntree Gully to Monbulk, Ferntree Gully to Olinda and Croydon, Frankston to Cranbourne, Pakenham to Gembrook, Drouin and Warragul through Neerim to Noojee, Drouin to Korumburra, Warragul to Korumburra, Warragul to Leongatha, Korumburra to Wonthaggi, Mirboo to Meeniyan, Boolarra to Foster, Boolarra to Welshpool, and many others of lesser importance.



PLATE No. 3.

Showing Gravelled Road near Gunyah.

Apart from the main arteries mentioned which are largely responsible for the present prosperity of Gippsland, a road that is destined to play an important part in the further development of the hill country of South Gippsland and which is now under construction is that



known as the Ridge Road, extending from the terminus of the newly-constructed Strezlecki railway line easterly through Sea View, Mirboo, Gunyah, Ryton, and thence further easterly through Balook, Blackwarri to Carrajung, where it joins the main road under construction from Traralgon through Gormandale to Yarram.

Its location is about midway between and parallel to the two main Gippsland railway lines, where it traverses the watershed of the streams forming the southern tributaries of the Latrobe River and of the rivers running southerly to the coast.

The construction of this road with laterals towards the Main Gippsland and the South Gippsland railway lines, it is confidently anticipated, will do much to solve the problem of resettling the hill country of South Gippsland, and it is the intention of the Board with the funds now available to press the work forward to completion with as little delay as possible.

At present the relocation and grading of the whole length of 66 miles of this Ridge Road is nearing completion and contracts are also in progress which will complete the hard surfacing in either metal or gravel as far easterly as Gunyah, a length of 40 miles.



PLATE No. 4.

Section of Ridge Road, South Gippsland.

These latter contracts and all future work on this road, together with that on the lateral roads connecting with it, will be financed out of the special provision of £150,000 per annum for roads in undeveloped areas previously referred to.

From a developmental aspect—that is, considering the work as an aid to increased settlement and production, as distinct from one designed to improve conditions for the demands of existing traffic—this road is considered by the Board to be one of the most important works it has undertaken; and as the road is intersected every few miles by main or developmental roads that are either completed or in course of construction and lead to the townships and railway stations on the two main lines of railway, its construction, if served with the modern means of transport now available for comparatively short haulage, should obviate the necessity for any further agitation for railway construction in this district.

Further east in the districts of which Bairnsdale and Orbost are the centres, extensive works have either been completed or are in course of construction.

In the Bairnsdale district particularly, the progress of the town and district is very marked.

In the Board's First Annual Report of 1914, page 21, special reference was made to the conditions then existing in Bairnsdale and district in the following terms:—

“The township of Bairnsdale is one of the oldest towns in Gippsland. It has great natural advantages, a good climate, generous rainfall, a fine river with a regular line of steamboats trading with the metropolis. Yet Bairnsdale as a town has been practically stagnant for the past 20 years, the increase in population during that period being only 142.

“The reason for this condition in the opinion of the Board is that the area of first class land which has been in occupation for the past 50 years is limited and no effective effort has been made to develop the enormous area of comparatively light land in the vicinity.”

Since the above was written the road through a section of this light land from Bairnsdale to Bullumwaal has been completed for a distance of 16 miles. Two additional roads running northerly through similar country are in course of construction. Two large bridges have been erected across the Mitchell River and other works are contemplated. Whether or not these works, together with the completion of the Prince's Highway leading to and through Bairnsdale to the east and the improvement of the Omeo Highway, both of which junction at Bairnsdale, are responsible, it is evident that at the present time the Bairnsdale town and district is one of the most prosperous in Gippsland.

In the Orbost Shire the 100 miles section of the Princes Highway will continue to be the main traffic artery.

Since the Board assumed direct control of this road a few years ago and inaugurated the patrol system of maintenance, the improvement in the condition of the road has been very marked, and now that the road is under constant care and supervision, regular and efficient motor services will make accessible additional suitable areas of land along and in the vicinity of the Highway, and the settlement of those areas should be encouraged.

Some years ago the Board completed the construction of the remaining section of the Cann River Valley Road commenced originally by the Public Works Department. This road was designed to develop the rich Cann River Valley and an area of unalienated Crown lands in this State near the border of New South Wales, and at the same time to open up communication with the south-eastern portion of New South Wales through Rockton and on to Bombala. With this object the Board placed itself in communication with the Local Government authorities in Sydney with a view to the construction of several miles of road on the New South Wales side of the border. A joint inspection was made by the then Minister for Local Government for New South Wales, Mr. Fitzgerald, M.L.C., Mr. P. Allen, Chief Engineer for National Works, Mr. J. Garlick, then Under-Secretary for Local Government, and the members of this Board. The New South Wales authorities were not then favorably impressed with the proposal, although the work on the Victorian side was completed to the border. It is, however, satisfactory to record the fact that the section on the New South Wales side has since been completed and the road is now open for traffic between the two States, and carries most of the traffic from Victoria to and from the Monaro tablelands of New South Wales.

Another road of a developmental and interstate character in eastern Gippsland in course of construction is that from Genoa to Wangrabelle, which will eventually be constructed to the border of New South Wales.

When this road is completed there will be four roads of an interstate character in the Shire of Orbost, viz. :—The Prince's Highway between Genoa and Eden, the Wangrabelle Road from Genoa to the border, the Cann River Valley Road and the Orbost-Delegate Road now in course of construction with Federal-State funds.

#### THE ALPINE ROAD.

Between 40 and 50 years ago the Alpine Road between Harrietville and Omeo was an important and much used highway.

It was constructed or partially constructed by the State as a connecting mail coach route between the Ovens River and Omeo gold-fields, but with the decline of the gold-mining industry the traffic disappeared and the road rapidly fell into disuse. It remained in this condition until a few years ago, when a demand arose for the development of the scenic attractions of this part of the State.

By arrangements with the Omeo and Bright Shire Councils, the Board assumed control about three years ago and extensive repairs were at once put in hand.



The road traverses the most mountainous section of the State, reaching an elevation of 6,000 feet above sea level at Mount Hotham, and already the result of re-opening the road has exceeded anticipations. Accommodation houses for tourists have been re-established, and several shelter huts have been erected, with the result that as many as 100 motor cars per day traversed the road during the last summer season.

Owing to its elevation a long section of the road becomes completely snow-bound in winter, when vehicular traffic is suspended. The road, however, is becoming increasingly popular with pedestrians and those who delight in snow sports such as ski-ing and tobogganing in winter.



PLATE No. 5.

Winter Scene on Alpine Road. Elevation, 6,000 feet.

The restoration of this road and the construction of the Omeo Highway are destined to play an important part in the development of the north-eastern part of the State where there is no railway communication, and at the same time will do much to make accessible to tourists the scenic attractions of the mountains with the township of Omeo as a tourist centre.

#### ROADS IN HILLY COUNTRY.

The difficulties that have confronted the Board since its inception in constructing roads in the hilly portions of the States have been referred to in earlier reports. They relate principally to the relocation of a system of roads surveyed in earlier days without regard to engineering difficulties or the requirements of wheeled traffic. Owing to the urgent need of relocating these roads, extensive surveys, land resumption, and costly earthworks have been carried out, which work practically involved a re-design of the whole road system in those areas.

Work of this character has been proceeding continuously, not only in the eastern portions of the State, but also in the Otway and Heytesbury areas, in the western district, and the Kinglake and Tolmie areas, in the north-eastern district.

In the Otway and Heytesbury areas the most important works in progress are the following:—Deans Marsh to Lorne, Forrest to Apollo Bay, Mt. Sabine through Beech Forest to Princetown, Lavers Hill to Hordern Vale and to Chapple Vale, Gellibrand to Carlisle, Timboon to Port Campbell, and Scotts Creek via Timboon to Curdies River. In the Kinglake district the works on the Whittlesea-Kinglake-road, the Yarra Glen-Glenturn-road are the principal ones in hand, whilst in the Tolmie area the Tatong-Toombullup-road and the Mansfield to Tolmie-road are the most important. Every one of these roads had to be relocated throughout. In every case the earthworks, necessary culverts, and drainage works have been completed; the hard surfacing, either in metal or gravel, is in progress, and in some cases nearing completion. In the extension or completion of several of the above works, which are purely of a developmental character, the assistance granted by the Commonwealth Government has been availed of to supplement the efforts of the State. In fact had it not been for this timely assistance, the works could not possibly have reached their present state of progress.

## ROADS IN NORTHERN AND NORTH-WESTERN AREAS.

Road works in these areas are now proceeding more satisfactorily and in some districts more rapidly than in the southern and eastern districts, and there is no reason why good progress should not be made in the future. There are few engineering difficulties, such as expensive bridges and deviations, to contend with as in the other portions of the State. The great disadvantage, however, is the scarcity in some of the northern and north-western Shires of suitable road-making material. This difficulty, however, is being rapidly overcome by the development of gravel deposits and metal quarries in the Midland district and the organization of rail and motor transport, not only by the municipalities themselves and the Board, but to a large extent by private enterprise.

Among the most progressive shires in this connexion may be mentioned Shepparton, Rodney, and Waranga, in the Goulburn Valley; the Shires of Rochester and Cohuna, in the north; and the Shires of Swan Hill, Mildura, Walpeup, Karkaroc, and Wycheproof, in the north-west. For various reasons some of the Shires have been reluctant to avail themselves of the financial assistance provided by the State's road legislation, but chiefly through the dissatisfaction of ratepayers, changes in the personnel of councils, and the publicity given by the press to the great works and consequent development going on in other districts, those municipalities are now a negligible quantity.

A striking instance of progress in the northern part of the State is in the Shire of Cohuna, which was constituted a separate municipality a few years ago.

After the constitution of the new Council it entered with enthusiasm upon a road construction programme, and, with the assistance of the Board, it has in the short period of three years made wonderful progress. To use a metropolitan press comment upon the new Council's policy—"The social and economic advantages that are already accruing from the vastly improved transit disposes the ratepayers to meet the additional rating charges with equanimity. It is confidently anticipated that within three or four years the roads of the Cohuna Shire will be among the very best in rural Australia instead of among the worst."

## FEDERAL-STATE GRANT No. 3.

The policy of the Government of the Commonwealth in assisting the States in developmental road construction was continued during the year by the allotment of a further sum of £90,000 as this State's proportion of a total provision of £500,000, conditionally on the State also contributing £90,000.

The schedule of works submitted for the approval of the Federal authorities was again drawn up by the three Departments controlling rural development works, viz.:—The State Rivers and Water Supply Commission, the Closer Settlement Board, and the Country Roads Board.

It having been unanimously agreed by the Federal Department and the three (3) State Departments that works once entered upon should be carried through to completion, the construction programme submitted and approved was, with some slight additions, an extension of works previously authorized.

These works, as pointed out previously, have been confined chiefly to areas in their early stages of development, and are consequently in the more remote parts of the State.

The districts concerned are as follows:—The South West—between Dartmoor and the western boundary of the State near Mt. Gambier. The North West—between Ouyen and Mildura and between Swan Hill and Euston. The North—embracing the irrigation areas of Bamawm and Cohuna. The North East—including the Tolmie tablelands and the area between Tallangatta and the Murray River. The South—including the Heytesbury and Otway forest areas; and the East embracing South and East Gippsland.

There can be no doubt of the wonderful assistance this Federal grant, together with the State's contribution, has been in laying the foundations of a permanent road system in these districts and in supplementing the programme being carried out with State and municipal funds under State legislation.

A list of the roads subject to Federal assistance, together with amounts allotted and expended, under this heading is shown in Appendix.



PLATES NOS. 6 AND 7.

Tatong-Toombullup-road, Benalla Shire, constructed with Federal-State Funds.



### MAIN ROADS (PERMANENT WORKS).

During the latter part of the previous financial year 1923-4 there was a temporary suspension of new constructional work.

As soon as the financial period 1924-5 opened, the suspension was removed and a fresh programme of work entered upon, which continued uninterruptedly throughout the year.

Owing to the suspension of new works above referred to the liabilities carried forward to the year under review amounted to only £145,861.

Fresh contracts amounting to £146,348 and affecting 76 municipalities were entered into during the year, the expenditure on which, together with that on contracts let previously, but not completed, amounted to a total expenditure for the year on permanent works of £228,872. (See Appendix.)

The above figures do not include expenditure carried out on sections of main roads constructed under Federal-State grant, which is shown in Appendix.

### MAIN ROADS (MAINTENANCE).

The expenditure in maintenance for the year, which is referred to in detail in the attached report of the Board's Chief Engineer, amounted to £468,181, an increase of £124,948 on the previous year's expenditure. It covered a length of 3,445 miles of main roads and affected 161 municipalities. No portion of the Federal-State grant has been expended in the maintenance of main roads.

### DEVELOPMENTAL ROADS (CONSTRUCTION).

As in the case of main road construction, new works on developmental roads were suspended during the latter part of the financial year 1923-4, consequently liabilities on contracts carried forward to the year 1924-5 amounted to only £322,415.

Additional contracts let during the year amounted to £473,128, which, with day labour works, provided a total expenditure for the year amounting to £602,519 and affecting 95 municipalities (See Appendix). The above is exclusive of expenditure on main developmental roads under the Federal-State grant shown in Appendix.

### IMPROVEMENT AND MAKING ACCESSIBLE PLACES OF INTEREST, TOURIST RESORTS, ETC.

The principal expenditure under this heading during the year was incurred on the Warburton-Narbethong road, amounting to £10,226. This road was selected by the Tourist Committee to make accessible some of the most beautiful and picturesque mountain and forest scenery in the State.

Being only 50 miles from Melbourne this road should, when completed, become very popular as an alternative route to the Blacks' Spur-road, through the mountains between the city and the Upper Goulburn River Valley.

Other roads on which expenditure has been incurred by the Board on behalf of the Tourist Committee are as per Appendix, making a total expenditure for the year of £16,164.

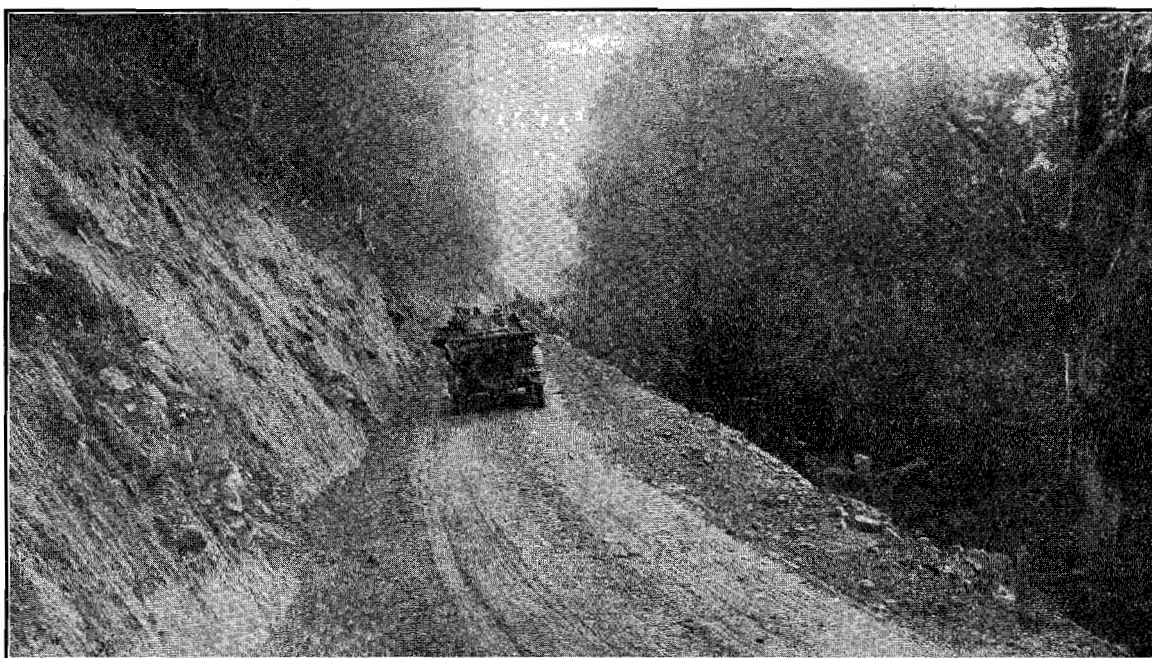


PLATE No. 8.

Section of Warburton-Narbethong Tourist Road through Virgin Forest.

### THE GREAT OCEAN ROAD.

In the older countries of the world there are instances of great highways skirting the sea coast, following its contour for hundreds of miles and giving an ever-changing panorama of delightful scenery which embraces ocean, mountain, and stream. Such well-maintained roads, through health-giving surroundings, are extremely popular, and their varied charms are a constant attraction to the pedestrian or to those who use either horse or motor as a means of conveyance.

It is many years since the construction of such a road along the South Coast as far as Lorne was suggested, but it was not until the close of the Great War that the project took material form and the idea of the formation of a highway running parallel to the verge of the ocean, from Barwon Heads to Warrnambool, was put forward by this Board, and its construction urged as a fitting and permanent memorial to those who had fallen in the Great War.

The magnitude of the conception was acknowledged. The scheme was well supported by the public, money raised, surveys made, and ere long gangs of returned soldiers were busily engaged along the sea front.

The Honorable Howard Hitchcock, C.M.G., O.B.E., M.L.C., Chairman, and Captain Edward Morley, M.L.A., Honorary Secretary of the Great Ocean Road Trust, have spared neither time, energy nor expense in furthering this difficult work. Already the sum of £21,000 has been contributed by the public and expended to advantage upon the construction of 8 miles of side-cutting and 17 miles of track 5 feet wide along the grade line. The whole of the work is being carried out by returned soldiers, and the first section to Lorne is now open to traffic. The soldier gang is now engaged in widening the grade track between Lorne and the Wye River.



PLATE No. 9.

Ocean Road between Anglesea and Lorne.

It is estimated that a further expenditure of £16,900 will be required to complete the earthwork. When this work becomes an accomplished fact, it will be possible to travel by motor from Melbourne to Apollo Bay, a distance of 108 miles through some of the finest coastal and mountain scenery in Australia.

The completion of the Ocean Road should be of great advantage to this remote area and conduce to its settlement, as its comparative isolation would then disappear.

The whole of the works on the Ocean Road have, with the consent of the Government, been surveyed and supervised by the Board's Staff, excellent gradients being secured throughout, the ruling gradient being 1 in 20.



The highest points above sea level reached along the Ocean front at points of interest are as follows :—

Big Hill, Eastern View	..	..	..	200 feet
Mount Defiance	..	..	..	190 „
Mutton Fish, Kenneth River	..	..	..	158 „
Cape Patten	..	..	..	246 „

#### UNEMPLOYMENT RELIEF WORKS.

For the relief of unemployment during the winter of 1924-5 a special provision of £19,400 was made available by the Government for expenditure on road works.

For this purpose three roads of a developmental character were selected, viz. :—

The Basin-road, Tambo Shire.

The Errinundra-road, Orbost Shire ; and

The Healesville-Toolangi-road, in the Shire of Healesville.

The roads were selected because the work consisted of clearing and earthworks, which required the employment of the maximum amount of unskilled manual labour. The total expenditure up to the end of June amounted to £19,282, the work being continued throughout the winter months in the next financial year.

#### ISOLATED SETTLERS' ROADS.

Another small special provision of £2,000, for the making of roads or tracks for settlers in localities isolated from the existing road system, was made available for the first time during the year, and is proving very beneficial for the particular purpose.

#### EXPENDITURE.

The following is a summary of the expenditure incurred during the year under the respective headings and classes of roads under the jurisdiction of the Board, all of which is shown in detail in the appendices :—

1. Main Roads (Construction)	..	..	..	£228,872
2. Main Roads (Maintenance)	..	..	..	463,065
3. Highway Maintenance (Omeo Highway)	..	..	..	5,116
4. Developmental Roads	..	..	..	602,519
5. Federal Assistance for Main Developmental Roads	..	..	..	192,733
6. Unemployment Relief (Special provision)	..	..	..	19,282
7. Roads to Develop Tourist Resorts	..	..	..	16,164
Total	..	..	..	£1,527,751

#### APPORTIONMENT OF COSTS.

The cost of permanent works and maintenance on main roads to the 30th June, 1924, was apportioned during the year under the provisions of section 27 of the *Country Roads Act 1915*. £1,548 18s. was charged to municipal councils on account of interest on permanent works and £154,427 18s. 3d. in respect of expenditure on maintenance.

The number of motor vehicles and motor cycles registered under the Motor Car Act during the year, including renewals, was 70,246 and 19,212 respectively, the net revenue received, including fines and licence-fees, being £402,970 18s. 4d.

#### APPENDICES.

In accordance with the requirements of the Country Roads Act, statements giving details of work carried out on main and developmental roads, together with Statements of Accounts for the year ended 30th June, 1925, are shown in the Appendices to this Report.

I have the honour to be,

Sir,

Your obedient servant,

W. CALDER, Chairman.

W. McCORMACK, Member.

F. W. FRICKE, Member.

W. L. DALE, Secretary.

## CHIEF ENGINEER'S REPORT FOR THE YEAR ENDED 30<sup>TH</sup> JUNE, 1925.

23rd December, 1925.

THE CHAIRMAN—

SIR,

I have the honour to report that during the year ended 30th June, 1925, the following works have been surveyed and constructed under the direct supervision of this office.

### SURVEYS.

(a) Highway Surveys.—For the purpose of record and for future construction, surveys have been made of the Bendigo-road, from the Metropolitan boundary to Bendigo, a distance of 88 miles, and the condition of the road, as it will be taken over by the Board at the end of the financial year, was noted for reference, and shown on record plans.					
(b) Main Road Surveys .. .. .					107·5 miles
(c) Developmental Road Surveys .. .. .				44·1	,,
(d) Tourist Road Surveys .. .. .				9·4	,,
Total .. .. .				161·0	miles

Under the last-named heading special mention might be made of the road from Warburton to Narbethong, which passes through very difficult country, and at the request of the Tourist Committee was located to develop, and to afford access to, the exceedingly picturesque mountain and sylvan scenery of the district. To accomplish this it was necessary to depart at times from a strictly economic location.

### ALIGNMENT AND CURVATURE.

During this year, owing to the increasing amount of motor traffic, greater care has been exercised in securing good alignment and curvature, even at some sacrifice of gradient. Curves on Main Highways have been limited to 200-ft. radius, and on other Main and Developmental Roads to 100-ft. radius.

The following are the Board's regulations for widening and banking on curves of various radii, and diagram illustrative thereof:—

*Banking on Curves.*—On all curves of less than 300-ft. radius, the road shall be banked in accordance with the special cross section attached. The change from the normal cross section shall be effected in a length of 50 feet, commencing 25 feet back from the tangent point, and on all curves of 200-ft. radius or less the formation and metalling must be increased by an additional width on the inside of the curve, as detailed hereunder, such change also to be effected in a length of 50 feet, commencing 25 feet back from the tangent point.



This when plotted gives an almost ideal curve. Gravels containing a greater percentage of fine materials than this show a distinct tendency to rippling, and should, therefore, be avoided, or the proportion of fine material reduced by screening.

Advantage has also been taken of the excellent varieties of river gravel, particularly those from the Gippsland rivers, which, if carefully selected to avoid an excess (say of 20 per cent.) of sand, give excellent results for maintenance purposes. The gravel should be spread in layers not exceeding 3 or 4 inches in thickness and a smooth surface maintained until consolidation by the constant use of a road drag. This very simple road implement has been found most valuable in the maintenance of earth and gravel roads, and is considered indispensable, particularly with gravel construction.

#### QUARRIES.

A survey has been commenced to investigate, collect data, classify, and record the locations, extent, and quality of stone and gravel deposits throughout the State, with a view to using the material that is available in any particular district to the best advantage. It will often be found economically advantageous to incur increased transport charges on a stone of high quality rather than use an inferior local stone. Shire Engineers and the general public have, therefore, been invited to forward samples of stone and particulars of locality wherever deposits appear of value. This has already met with a good response and it is expected that as the work proceeds, a comprehensive knowledge of the State's resources in roadmaking materials will be obtained, and the choice of the best materials for road construction readily made.

As part of this development, full advantage was taken of the road materials testing laboratory at the Melbourne University, and there the testing of stone and gravel samples has been extensively carried out, in order to ascertain the quality and proportion of materials for record or for proposed construction.

Appended is a list of tests on stone already made. The materials of highest value (French co-efficient of wear) so far tested have been basalt from South Gippsland, and quartz porphyry gravels from the King River Valley, giving results of 18·8 and 20·0 respectively. The following table shows the standards adopted in England and America as a means of comparing wearing values :—

#### ENGLAND.

Classification.	F. Dry.	F. Wet.	I.	H.	C.
Very good .. .. .	20 and over	20 and over	19 and over	19 and over	100 and over
Good .. .. .	16-20 ..	13-20 ..	16-18 ..	17-19 ..	75-100
Fairly good .. .	13-16 ..	10-13 ..	13-15 ..	16-17 ..	25-75
Rather poor .. .	10-13 ..	8-10 ..	8-12 ..	15-16 ..	10-25
Very poor .. .	8-10 ..	5-8 ..	4-7 ..	Under 15 ..	Under 10
Unsuitable for roads .. .	8 and under	5 and under	0-3 ..	..	..

F = French Co-efficient of Wear, Deval Rattler or Attrition Test.  
I = Toughness Value, Page Impact Test.  
H = Hardness, Dorry Hardness Test.  
C = Cementing Value, Cementation Test.

#### AMERICA.

Type of Construction.	Traffic.	F. Dry.	I.	H.
Waterbound macadam .. .	Light .. .	5-8 ..	5-9 ..	10-17
	Moderate .. .	8-15 ..	10-18 ..	14 and over
	Heavy .. .	15 and over	18 and over	17 and over
Macadam with bituminous carpet or bituminous macadam .. .	Moderate .. .	5 and over	5 and over	..
	Heavy .. .	7 and over	10 and over	..
Bituminous concrete .. .	Moderate .. .	7 and over	7 and over	..
	Heavy .. .	10 and over	13 and over	..
Broken stone foundation .. .	Any .. .	3 and over	3 and over	8 and over

NOTE.—"Traffic" has been defined as follows :—"Light," less than 100 vehicles per day ; "Moderate," 100 to 250 vehicles per day ; "Heavy," over 250 vehicles per day.

## VICTORIA.

C.R.B. No.	Location.						Tests.					Remarks.
	Univ. No.	Date.	Allot. No.	Parish.	County.	Adjacent Road.	Rock.	F. Dry.	H.	I.	C.	
1	R. 1	5.6.24	..	..	..	..	Sandstone ..	5.96	..	..	..	F = French Coefficient H = Hardness (Dorry) I = Toughness (Impact) C = Cementation
2	R. 2	"	..	..	..	..	Quartz ..	12.05	..	..	..	
3	R. 3	17.7.24	..	..	..	..	Decomposed dacite	5.46	..	..	..	
4	R. 7	17.9.24	..	..	..	Forrest-Apollo Bay	Calcareous sandstone (Mt. Sabine)	17.2	..	..	..	Used to metal Mt. Sabine section of Forrest-Apollo Bay-road
5	R. 8	"	25, west corner	Barramunga	..	..	Grey spotted sandstone (Ivo Taylor's)	10.7	..	..	..	
6	R.10	"	19, south-west corner	"	..	..	Sandstone (Christie's Camp)	9.3	..	..	..	"No. 2 Finlayson's Crusher"; deposit revealed only 2,000 cubic yards
7	R. 9	"	22, south-east corner	"	..	..	Sandstone (opposite Floyd's)	7.7	..	..	..	
8	R. 11	"	12, north corner	"	..	..	Sandstone (Barramunga School)	10.5	..	..	..	"No. 1 Finlayson's Crusher"
9	R.12	2.11.24	..	..	..	..	Quartzite ..	14.7	..	..	..	
10	R.13	"	..	..	..	..	Hornfels ..	14.1	..	..	..	
11	R.14	"	..	..	..	..	"	10.4	..	..	..	
12	R.15	9.12.24	29	Barramunga	..	Forrest-Apollo Bay	Sandstone ..	7.6	..	..	..	
13	R.23	10.3.25	22, south-west corner	"	..	..	"	6.4	..	..	..	Near falls on Gellibrand River
14	R.24	1.4.25	..	..	..	..	Granite ..	14.34	..	..	..	Lake Boga deposit Repeat test on 8/R.11, taken from 50 feet into quarry-hole
15	R.25	20.5.25	12, north corner	Barramunga	..	Forrest-Apollo Bay	Sandstone (behind school at Barramunga)	10.7	17	18	..	
16	R.27	26.5.25	..	Emberton	Dalhousie	..	Honeycomb basalt	..	..	..	..	Metcalfe Shire; Barfold's Quarry
17	R.28	18.6.25	59/60	Olangolah	..	Forrest-Apollo Bay	Sandstone (Marriner's)	9.1	17.2	15	..	
18	R.30	..11.24	..	Korumburra	..	..	Sandstone	7.4	..	..	..	
19	R.31	18.6.24	..	Moorbanool	V.R. Quarry Reserve, on Colac-Beech Forest track Ch., 19m. 11c., 2 miles south of Gellibrand Station	..	Silicified sandstone	6.3	..	..	..	
20	R.29	"	..	..	..	..	..	..	..	..	..	
21	R.332	26.6.25	28A	Barramunga	..	Forrest-Apollo Bay	Sandstone ..	8.5	..	..	..	Taylor's stone on west side of Gellibrand River, in tributary gully; Quarry opened

## FEDERAL GRANT WORKS.

The attached statement indicates the number of Federal Grant Works in progress at the end of the Financial Year, with a short description of the purposes for which they are being carried out.

## WORKS IN PROGRESS.

No.	Shire.	Road.	No. of Miles constructed.	Remarks.
1	Morwell	Morwell River	5.00	This route serves a large area of first class land used for dairying, taps traffic from other roads, and permits the people to reach the railway in a shorter distance and on a good graded road. The farmers will now be able to cultivate and grow root crops profitably, while this was impossible before, owing to the impassable grades on the old routes
2	Woorayland South Gippsland	Turton's Creek	6.60	Links up the north and south Gippsland Railways between Mirboo North and Foster, and now permits traffic between these places. It also serves an area of country suitable for dairying and growing root crops



WORKS IN PROGRESS—*continued.*

No.	Shire.	Road.	No. of Miles constructed.	Remarks.
3	Buln Buln ..	Fumina ..	8·00	Has opened up a neglected area of good dairying country, also suitable for root crops, and has given the farmers access to the Noojee Railway Station
4	Otway ..	Forrest—Apollo Bay	5·10	Serves a large area of first class hill country and rich agricultural flats in the vicinity of Apollo Bay suitable for dairying and root crops. The road will permit of traffic between Apollo Bay and the Forrest Railway Station. When the road between Forrest and Apollo Bay is metalled, it is considered that considerable development, by reason of increased settlement, will take place in this area
5	Portland ..	Portland—Mount Gambier	5·30	Opens up undulating limestone country suitable for general agriculture, grazing and dairying, and provides for road communication between the area and the port of Portland, and will permit of traffic from Portland through Mumbannar to Mount Gambier
6	Warragul and Woorayl	Allambee Estate	3·30	Gives access to the Leongatha and Darnum Railway Stations for the large Allambee Estate acquired for Closer Settlement. This road will also open up an area of hill country suitable for tilling and production of root crops
7	Eltham ..	Yarra Glen—Glenburn	3·80	Opens up an extensive area of hill country suitable for root crops and berry fruit, and gives access to the Yarra Glen Railway Station. It also provides a connecting road between Yea and the Yarra Glen Railway Station
8	Orbost ..	Orbost—Delegate	12·60	Serves a very large and extensive area of country suitable for grazing and settlement, the area being at present only sparsely settled. It will provide an outlet for the products between Monaro Plains, in New South Wales, and Orbost, and will also permit of through traffic to the Federal Capital
9	Morwell ..	Jeeralang West ..	1·00	Provides a connexion between Morwell and Yarram, which was previously impassable, and serves an area of hill country suitable for dairying
10	Orbost ..	Murrungowar ..	15·54	Gives access to a large area of country suitable for agriculture and dairying, and will permit of the settlers using the Orbost Railway. The Closer Settlement Board has already in hand a scheme of closer settlement for this area
11	Otway ..	Aire River—Forrest	2·00	Permits of settlers having communication with the Forrest Railway Station, serves a large area of land suitable for dairying and root crops, and provides a through road from Beech Forest to Forrest and Birregurra
12	Otway ..	Apollo Bay—Wye River	3·16	Will give an outlet to the port of Apollo Bay to a large area of country bordering on the coast line. The land is suitable for grazing and dairying
13	Heytesbury ..	Timboon—Nirranda	2·00	Gives access to the Timboon Station for a large area of dairying and agricultural country to the west of the parishes of Brucknell and Nirranda
14	Mildura ..	Mildura (Lake Hattah)	18·30	Will permit of traffic between Mildura and Ouyen, and allow of access to the railway system from an extensive area of agricultural land between Ouyen and Lake Hattah
15	Cohuna, Swan Hill, Kerang, and Rochester	Murray River Valley	4·00	Gives access to the Murray River Irrigation Settlements
16	Benalla ..	Toombullup ..	2·18	Will give an outlet to the Tatong Railway Station from a large area of land suitable for root crops and dairying
17	Portland ..	Port Fairy—Portland	2·40	Will serve an area of agricultural land not served by a railway and will give an outlet to the ports of Portland and Port Fairy
18	Rochester ..	Rochester—Echuca	1·68	When completed will replace a particularly bad section on this important interstate road, and serves the agricultural settlement along its route with an outlet to the Echuca Railway Station
19	Kowree ..	Elderslie Soldier Settlement	..	Serves a large area acquired by the Government for settlement of soldiers, and gives an outlet for their products to the South Australian railway system
20	Towong ..	Murray River—Tallangatta	..	Will give access to the Tallangatta Railway Station from a large grazing and agricultural area along the Murray Valley cut off by the construction of the Hume Reservoir
21	Grenville and Hampden	Lismore—Pittong	5·70	Serves one of the largest soldier settlements in the State of Victoria, and gives an outlet for produce to the Lismore and Pittong Railway Stations

WORKS IN PROGRESS—*continued.*

No.	Shire.	Road.	No. of Miles constructed.	Remarks.
22	Shepparton, Rochester, Wycheproof	Shepparton-Rochester-Wycheproof	..	Will serve sections of the irrigation settlements devoted to the growing of fruit and wheat
23	Portland ..	Portland-Nelson	3·50	This gives an outlet to the railway and port of Portland to a large area subdivided for closer settlement, and suitable for root crops, fruit-growing, and for dairying
24	Morwell ..	Foster-Boolarra..	2·43	Serves an extensive area of hill country suitable for the production of root crops and for dairying, and will give access to the railway station at Boolarra, and assist in the development of a large area of unsettled country
25	Tambo ..	Bruthen-Omeo ..	7·77	Serves as an outlet for a large extent of country suitable for dairying and grazing, also an important link in the main arterial road from South to Northern Victoria, through Omeo. This area is far removed from a railway centre
		Total ..	121·36	

## UNEMPLOYMENT GRANT.

Work under this Grant during the year was carried out on three roads indicated hereunder, the total length constructed being 5 miles.

1. *Healesville-Toolangi Road.*—This follows the valley of the Chum Creek and opens up a short, direct and well graded route to Toolangi, a well-known tourist resort and saw-milling district, and will serve a large area of fine country in the Yea River Valley.
2. *Basin Road.*—This opens up a large area of grazing and forest country with fine scenery on the Snowy River, east of Buchan, and part of it will serve as a section on the through road from Buchan to Orbost.
3. *Errinundra Valley.*—This road will eventually be a through road to Bendoc and passes through fine timber country, serving a large area with fine flats on the Errinundra River, a branch of the Bemm River, 36 miles from Orbost.

Although the results obtained under this system of working are not nearly so efficient as those obtained by contract work, the work done has served a most useful purpose, and it must be borne in mind that the men employed were not all accustomed to the work, nor was it desirable, in view of the intention of the Grant, to employ the same amount of mechanical plant as would be done otherwise. It is considered that the percentage of efficiency, 65 to 75, as compared with contract labour, was as much as could be expected under the circumstances.

The total expenditure on the three roads was £19,282 18s. 8d.

## MECHANICAL SPRAYING OF ROADS.

During the year ended June, 1925, the Board completed a length of 151 miles of surface spraying, 24 miles being sprayed with tar and 127 miles with bitumen, and it is anticipated that during the next year a length of 217 miles of road will be sprayed and a considerable length of penetration work done. To enable this to be effected two additional sprayers have been designed specially for the work, and details of them are as follows :—

*Machinery.*

The machine for spraying consists of a steam wagon of suitable design, with wheels of ample dimensions, on the chassis of which is mounted a rectangular tank of 800 gallons capacity. From the bottom of this tank a suction pipe leads to a rotary

geared pump which is mounted on the rear of the chassis and driven through gearing off the main driving axle of the wagon. The pump outlet leads to the spraying manifold, which is a horizontal pipe at the rear of the wagon and running its full width, and about 9 inches above the road surface; this is fitted with a number of jets so shaped that the tar or bitumen is sprayed uniformly over the road surface (see Illustrations 1 to 4).

The older machines are capable of spraying widths up to 8 feet at a rate of  $\frac{1}{4}$  to  $\frac{1}{5}$ th gallon per square yard, the pump being driven by sprocket wheels and chain from the main axle with clutch on the pump shaft. To suit the newer widths of roads in use (up to 20 feet) and to allow more flexibility in their treatment, the new machines were designed to spray widths up to 10 feet, and at rates per square yard varying from 0.2 to 1.0 gallon.

The increase in widths sprayed was obtained by extending the manifold and enlarging the pump slightly. The varying rates of spray were obtained by the insertion of extra countershafts and gearing between the main driving axle and the pump axle: one pair of gear wheels was so placed that they could be readily exchanged for other wheels having a varying ratio between the number of teeth, viz. :—Total number of teeth on two wheels—80, wheels provided in pairs as follows :—

40, 40; 46, 34; 54, 26 : 60, 20 : 64, 16.

This gives a wide range of pump speeds.

Other improvements on the previous machines include increase in diameter of the suction, delivery and manifold pipes, and the lagging of these and the pump to reduce heat losses.

The machines have been tested on all classes of work and the following information accumulated :—

Pump gears (see Fig. 2.)			
Overall diameter	..	..	6.5 inches
Width of face	..	..	8.5 inches
Number of teeth	..	..	11
Pitch of teeth	..	..	$1\frac{9}{16}$ inches
Diameter of pitch circle	..	..	5.5 inches
Depth of teeth	..	..	$1\frac{1}{8}$ inches
Diameter of spindle	..	..	$1\frac{1}{2}$ inches
Pump capacity per revolution—172 cubic inches	..	..	0.10 cubic feet

The efficiency of the pump was determined at various speeds and is shown in Figure 3.

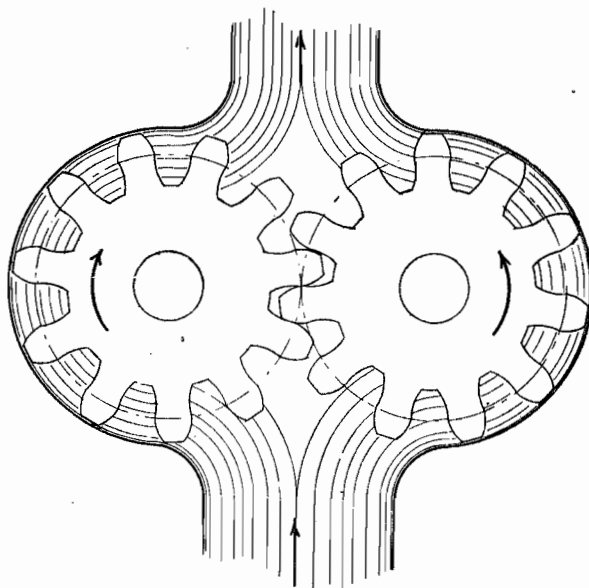
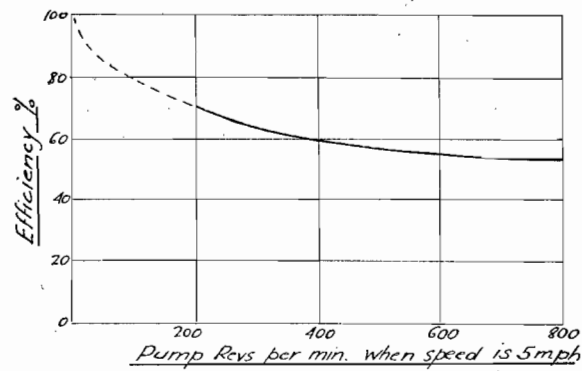


FIG. 2.

Diagram of Rotary Pump.



Note: Efficiency is calculated on a volumetric basis.

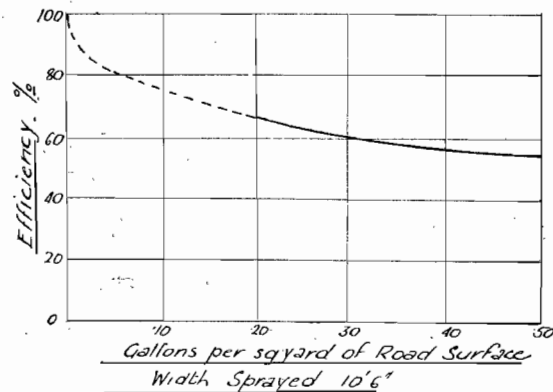


FIG. 3.

Rotary Pump Efficiencies.

In designing the machine, the following were the rates of application of tar or bitumen aimed at:—

Work.	Quantity—Gallons per Square Yard.				
Surface spray roads 12-ft. to 20-ft. width in two strips .. ..	0·20	0·25	0·33	..	..
Penetration on 5-ft. width .. .. .	..	..	..	0·75	1·00

The following table shows the rates of application which have been found possible, with the gear wheels appropriate for each:—

Work.	Quantity—Gallons per Square Yard.					
Spray 20-ft. road in two strips .. .. .	·50	·40	·31	·21	..	..
Spray 18-ft. road in two strips .. .. .	..	·46	·36	·25	·19	..
Spray 16-ft. road in two strips .. .. .	..	·51	·40	·27	·20	..
Spray 15-ft. road in two strips .. .. .	..	·56	·44	·29	·22	..
Spray 12-ft. road in two strips .. .. .	..	..	·54	·37	·28	·21
Penetration on 5-ft. width .. .. .	1·00	·78	..	..	..	..
Gear wheels to be used .. .. .	64/16	60/20	54/26	46/34	40/40	34/46

Cost.—The capital cost of new machines is £1,051 15s., and the charge for hire to the various works controlled by the Board is 70s. per day, which includes wages of driver and operator.

*General Method of Working for Surface Spraying.*

The tar or bitumen is heated in a large boiler of 800 gallons capacity to a temperature of 280° F. in the case of tar, and 350° F. in the case of bitumen. It is then pumped into the spraying machine by a hand pump attached to the heater, screens being provided at the foot of pump and at inlet to sprayer tank to prevent foreign material entering the machine.

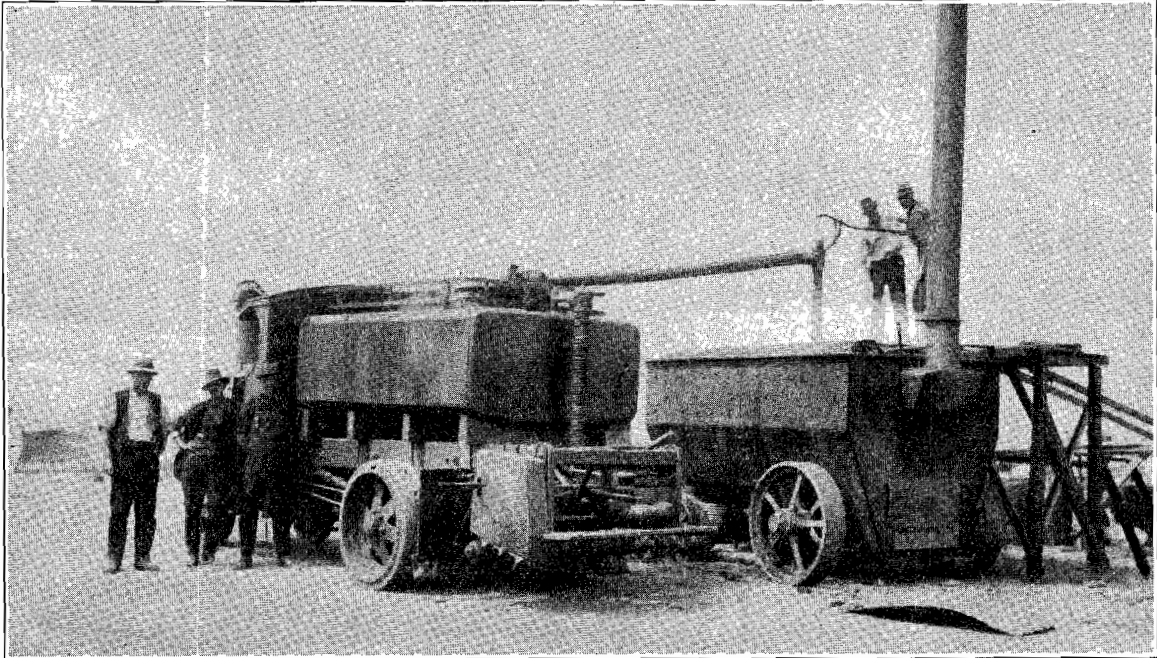


ILLUSTRATION NO. 1.

Pumping Bitumen from Heater to Sprayer.

The machine travels under its own power to its position on the road, and before beginning to spray the pump, piping, manifold and jets are heated by blowing through them superheated steam. The sprayer then travels along the road, which has been previously swept with both horse and hand-brooms, and applies the tar or bitumen over half the width. An operator travels on the rear of the machine to operate pump clutch and attend to the manifold and jets.



ILLUSTRATION NO. 2.

Heating Pump, &c., preparatory to Spraying.



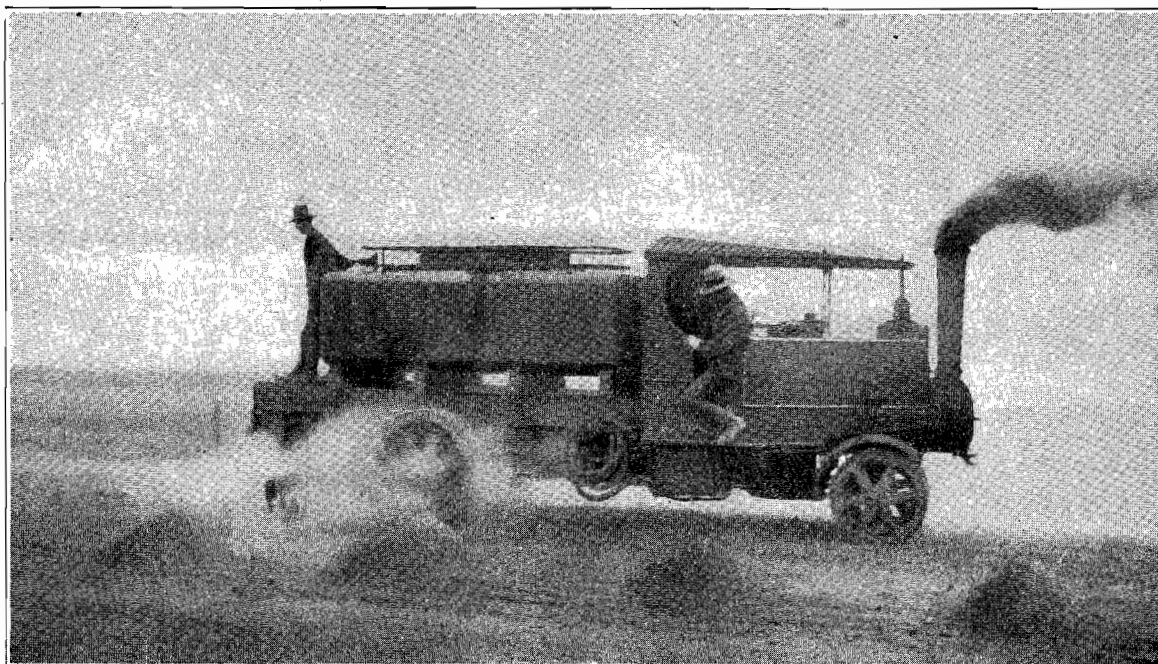


ILLUSTRATION No. 3.

Spraying—Note the small amount of disturbance of the metal by the front wheels; as the driver is seated on the off side an assistant is necessary to guide the machine when spraying the fourth or right-hand strip; a mirror is to be fitted to dispense with this assistance.



ILLUSTRATION No. 4.

Machine Working—Note pump and delivery pipe to right of operator's right foot, vertical pipe with valve at top for return of excess bitumen to tank, countershaft and pump spindle—the change gear wheels are located between these shafts, under the hood to left of operator.

The tar or bitumen is covered with gravel or screenings and rolled. There is no provision for keeping the tar or bitumen in the sprayer hot, but this is unnecessary, as it is possible to travel up to  $4\frac{1}{2}$  miles from the dépôt with bitumen (and further with tar) without appreciable loss of temperature before commencing work. After finishing spraying, the pump, &c., is immediately cleansed with superheated steam, and washed out with tar oil.

When attended by two 800-gallon heaters, a sprayer can distribute four loads per day provided the lead is not more than  $4\frac{1}{3}$  miles. This allows the treatment of 6,000 feet of 20-ft. road per day, or 6 miles per week, given fine weather.

*Quantities of material used.*—For the first coat on a road not previously sprayed, distilled tar is used at the rate of 0·25 gallon per square yard. For subsequent coats 65 penetration bitumen is used at the rate of 0·20 gallon per square yard. Screenings or clean gravel are required at the rate of 1 cubic yard to 80 square yards of road, any surplus material after covering the road being retained for feeding the road surface during subsequent warm weather.

The following table is worked out on the above figures, and shows approximately the quantities of material required :—

Road Width.						Tar.	Bitumen.	Screenings.
						Gallons per Mile.	Tons per Mile.	Cubic Yards per Mile.
15 feet	..	..	..	..	..	2,200	8·1	110
16 feet	..	..	..	..	..	2,350	8·7	117
18 feet	..	..	..	..	..	2,650	9·8	132
20 feet	..	..	..	..	..	2,950	11·0	147

*Tar Oil—*

- 1 drum (40 gals.) to every 20 tons of bitumen.
- 1 drum (40 gals.) to every 120 drums of tar.

Engineers using these materials will find the following approximate figures useful :—

*Bitumen—*

- 216 gallons per ton.
- 1 barrel contains 35 gallons.
- 6 barrels per ton.

*Tar—*

- 1 drum contains 40 to 44 gallons.
- 1 drum when full weighs 5 cwt.
- 1 drum when empty weighs  $\frac{1}{2}$  cwt.
- 28 to 30 drums fill one 10-ton railway truck.

*Cost.*—The cost of this work, when well organized and reasonably close to source of supplies, is at present about 4·5d. per square yard for bitumen spraying and 5·0d. per square yard for tar spraying.

*Penetration Work.*

The machine is filled with bitumen as described above and the manifold so arranged that it sprays on a 5-ft. width, the edge of the spray being outside of the line of the wheels so that junction may be made in the previous strip sprayed without the wheel travelling on the completed work, care being taken not to overlap the previously sprayed strip. On any one section, four trips are necessary to complete 20 feet width of road.

For this work to be successfully carried out, the lower course of metal must be thoroughly sound and firm, the upper or penetration layer of 3-in. consolidated hard clean stone free from small material, and well rolled before penetration. In these circumstances the sprayer disturbs the metal very little when travelling at 2–3 miles per hour and the bitumen is forced through the entire depth of the course.

The machine is capable of treating 1,200 lineal feet of 20-ft. road per day at the rate of one gallon per square yard.

The machine in operation on penetration work is shown in Illustrations Nos. 3 and 4.

**BITUMINOUS EMULSIONS.**

“ This new road material consists of an emulsified form of pure bitumen, which possesses the valuable quality of being capable of application in a cold form at all times of the year, and under all conditions of weather. The emulsion is a coffee-coloured liquid contained in barrels of 42 gallons capacity.”

“ It is therefore of a distinct value as a commodity in the hands of road authorities for urgent repair work which could not be carried out with the class of bituminous materials that it is necessary to apply hot, and which cannot be successfully laid in wet weather.”

"It is considered that 'cold spray' should be a great boon to municipalities not in a position to acquire an expensive plant for drying and mixing hot bituminous mixtures. Being impressed with the possibilities of its usefulness, I obtained a quantity in Great Britain from two different manufacturing firms and forwarded to Melbourne for testing purposes."—Extracts from Chairman's Report on visit to Europe and America in 1924.

There are also on the market some other preparations which possess the first property but not the second—they are bitumens fluxed to fluid consistency with a volatile flux which upon exposure to the atmosphere evaporates and leaves the original bitumen. The Board is now investigating these preparations, but up to date no results can be reported.

The trade names of the emulsions tested are "Coldspray" and "Coldfix" respectively, and both were tested on the Prince's Highway, City of Footscray and Shire of Braybrook, on a short section of road which carries heavy traffic comprising numerous private motor cars, considerable horse traffic and heavy industrial automobiles, some of the latter being large motor lorries conveying metal from the numerous local quarries.

#### *Coldspray.*

1. *Penetration work.*—In October, 1924, a length of 500 feet of 16-ft. road was scarified re-shaped and rolled, after which  $4\frac{1}{2}$  inches (loose depth) of  $1\frac{1}{2}$ -in. metal was spread, rolled, and penetrated with coldspray at the rate of 1.6 gallons per square yard; the surface was covered with  $\frac{3}{4}$ -in. screenings and again rolled. After allowing a day for the emulsion to "dry out" it was rolled, but it was found that the road would not consolidate properly. A further coat of the emulsion was applied (0.73 gallons per square yard), covered with toppings and rolled, and thorough consolidation was then obtained. The total amount of emulsion applied was 2.3 gallons per square yard, but the amount of bitumen in the road is not more than is usually used on penetration work.

The cost of "Coldspray" was 1s. 7d. per gallon landed at Melbourne—made up of 10d. per gallon initial cost in England and 9d. per gallon shipping and freight charges.

The cost of the work described above was 6s. 7d. per square yard, made up as follows:—

Metal, screenings, &c.	..	..	..	..	22d.
"Coldspray"	..	..	..	..	44d.
Labour	..	..	..	..	9d.
Plant	..	..	..	..	4d.
					79d. = 6s. 7d.

It was found that the emulsion had to some extent "broken," i.e., the bitumen and emulsifying agent had partly separated on the long sea voyage.

After three months' traffic in the summer the surface was carefully inspected and found to be in good order except for a few small waves on the side of the road which carries the heavy metal wagons fully loaded. The surface stones were exposed to wear and a sealing coat appeared advisable.

2. *Seal Coat Work.*—A section of 1,340 feet of road 16 feet 6 inches wide, which had been re-sheeted with penetration macadam was sealed with the emulsion at the rate of 0.25 gallons per square yard and bluestone toppings at the rate of 1 cubic yard to 119 square yards of road surface.

The barrel was mounted on a small truck and the liquid run out on to the road near the centre and allowed to flow towards the edge of the road, even distribution being obtained by sweeping with brooms. Owing to the fluidity of the material the quantity applied was about the maximum possible in one coating. The cost per square yard was 6.5d., of which the "Coldspray" costs 4.75d.

After six months' wear this surface was in excellent condition and appeared to wear quite as well as the ordinary seal coat of bitumen applied by the Board's mechanical sprayers.

3. *Patches.*—Some of the material was used in ordinary maintenance patching, and the patches were found to hold well and withstand the effect of the heavy traffic.

*Coldfix.*

This material is supplied in two grades, "Grout" for penetration and "Spray" for sealing work.

*Penetration.*—In February, 1925, a length of 311 feet of 16-foot road was re-sheeted with  $4\frac{1}{2}$  inches (loose depth) of  $1\frac{1}{2}$ -in. metal penetrated with this emulsion. The original surface was not scarified, but was patched with tarred screenings to obtain an even surface. After spreading and rolling the metal the emulsion was applied at the rate of 1.9 gallons per square yard. Screenings were then spread and the sheeting well rolled. The cost of the emulsion landed at Melbourne was 1s. 5d. per gallon, made up of 11d. per gallon initial cost in Scotland, and 6d. per gallon freight and shipping charges. The cost of the work was 5s. 1d. per square yard, made up as follows:—

Metal and screenings	..	..	..	19d.
"Coldfix"	..	..	..	33d.
Labour	..	..	..	8d.
Plant	..	..	..	1d.
				61d. = 5s. 1d.

After three months it was found that the surface had not retained its shape, and considerable waving had occurred on the side carrying the heaviest traffic.

*Supplies.*

At present there is no regular supply of these materials in the State. Both are produced in the United Kingdom, and there is an agent in Melbourne for one of the preparations, but the other has to be imported direct from the manufacturers. If, however, they are to become commercially practicable, considerable reduction in the cost of the materials will be necessary—approximately 50 per cent. of the bulk consists of water and the cost of transport is about equal to the cost of the material at its point of manufacture. It is understood that a company is about to commence the production of cold spray locally, which will overcome this objection.

## BRIDGES.

Three bridges constructed by the Board on main roads in the past year have been of permanent types, two of reinforced concrete and one of structural steel and concrete.

The largest of these is that over Deep Creek, at Carisbrook, on Castlemaine-Maryborough Road. Here there existed an old timber stringer bridge in extremely decrepit condition, and subject to submersion during high floods. The new crossing is raised above flood level and is of viaduct form, with seven spans of 37 feet and end spans 22 feet—total length 304 feet. The practice of dividing a bridge of this type into a series of separate rigid frames has been hitherto followed, and was again adopted in this instance. This practice is safe only where foundation rigidity is assured; a small relative settlement of supports may cause cracking in the superstructure. At Carisbrook a reasonably rigid foundation is provided by reinforced concrete piles driven into stiff clay or gravel strata. The nine piles in each pier are arranged in three groups of three piles each, and the heads of each group are enclosed in a cylindrical mass of concrete extending about 9 feet below low water level so as to prevent the scour during floods from impairing the supporting value of the piles. Just above low water level these cylinders are joined with a massive concrete sill which carries three substantial columns, and these, together with the sill and crosshead, form a rigid frame. For expansion purposes the bridge is divided into five units: the three central spans of the bridge are cast monolithically, likewise the pair of spans at either side, the end spans forming separate units. Thus expansion piers were required at the junctions of units: these piers have half width columns for the adjacent units, with  $1\frac{1}{2}$  inches space to permit the columns to bend from sill level. The end spans were introduced in place of high abutments and the terminal supports consist merely of buried piers; this arrangement is economical and also relieves the bridge of horizontal earth pressure, rendering the design more determinate. The contract amount was £9,874, including raising of approaches.

The Maddingley bridge over Werribee River, at Bacchus Marsh, replaces an old through timber truss of 80 feet span resting on sandstone abutments, with deck level at such a height above the river banks that the bridge formed an obstruction to the clear view along the road. This arrangement was probably necessary 40 years ago so as to give clearance above high flood level, but bed scour has greatly increased the waterway, the deepening being about 6 feet. The present bed is about 10 feet below the foundation level of the old abutments. For this reason and because



of their inadequacy for increased load the old abutments were demolished. The new bridge has two main concrete piers founded on lignite, and located about 50 feet apart. These piers support two steel plate girders which overhang the piers 15 feet at each end. The abutments and wings do not support the girders, but act solely as retaining walls, and are constructed of reinforced concrete resting on the firm material of the river bank. Steel castings and pins are used to transfer the loads to the pier heads. The bearings at the north pier are fixed, and expansion is allowed for over segmental rollers at the south pier. The girders are 6 feet deep and support transverse joists which overhang them and on which the reinforced concrete slab rests. The roadway is 22 feet wide, with footway 5 feet wide on the downstream side. The wearing surface on the roadway is asphaltic macadam. The bridge has a concrete parapet. The new bridge permits a clear view to be obtained from one approach across to the other. Contract amount was £5,089.

At Ironstone Creek, near Nowa Nowa, a high timber viaduct had been destroyed in a bush fire. As the waterway required is small, and the foundation is of crystalline limestone, a structure beneath filling was the most economical, and a reinforced concrete arch was adopted. The span is about 24 feet clear, the rise 9 feet, and the depth of filling over the crown 18 feet. Thickness is  $7\frac{1}{2}$  inches at crown and 15 inches at springing. The arch is designed so that the line of thrust should remain near the arch axis, but reinforcement is added to care for minor and unforeseen bending stresses and to prevent formation of temperature cracks. The wings are of mass concrete.

Crossenvale bridge, over Campaspe River at Echuca, is an example of replacement in timber. The river has a great range between summer and high flood levels typical of the Murray tributaries; at Echuca this range is about 24 feet. The bed of the river consists of alluvial silt, clay, and sand. The length of the bridge is 295 feet. Outline designs were prepared for various types of permanent structure, both concrete and steel, but the height of the crossing and the insecurity of foundation made these types so expensive as to involve much larger capitalized cost than a timber stringer structure, for which the piers could be relatively light while the limiting span of 30 feet satisfied the flood requirements. By using redgum piles and ironbark or Gippsland box stringers, and by adoption of premixed asphaltic macadam wearing surface, it is anticipated that a useful life of 30 years will be obtained. The piles, the longest of which are 50 feet, were obtained on the Murray flats. The stringers were brought from Eden and Nowa Nowa. The asphaltic wearing surface, which is 4 inches thick at the centre and 2 inches at the kerbs, gives a good surface for motor traffic, and minimises impact without unduly loading the stringers. The use of asphaltum instead of tar gives to the covering greater life and resiliency, features which are essential with the deflections and movement inseparable from timber construction. No elaborate plant is required to lay the material, the chief requirements being hot, dry aggregate and ample capacity for the kettle for heating the bitumen. If a power roller is not available, fair results are obtainable by use of hot tamping irons prior to final consolidation by horse roller. The surface is, of course, fairly costly (usually about 7s. 6d. per square yard, of average thickness  $4\frac{1}{2}$  inches). At Echuca, the Contractor improvised a kettle out of an iron tank, and used flat sheets for heating the aggregate. The new bridge is 18 feet wide between kerbs and has six lines of stringers, which were made slightly lighter than for the Board's standard arrangement with 15 feet width and five stringers. There are ten spans of 29 ft. 6 in. Contract amount, including approaches, was £5,454.

Two timber bridges were also erected by the Board over the main canal near Koo-wee-rup—one on the Main Coast Road, and one on Koo-wee-rup-Pakenham Road. Here, also, the insecurity of foundations made timber construction preferable to and more economical than concrete. Moreover, in this district, where the capacity necessary in the drains is proving a subject of uncertainty, use of timber is favoured also on account of its flexibility if adjustments are called for. These bridges consist of seven spans of 26 ft. 6 in. on 45 degrees skew and 4 spans of 28 ft. 6 in. on 10 degrees skew respectively, the latter being flanked by subsidiary bridges.

During the year work on the substructure of the new Barwon Bridge at Geelong proceeded steadily. The new piers, including that in the centre of the main stream, were completed. The concreting of the two abutments was also well advanced. Removal of half the old iron girders was completed, and of the new steelwork about half was delivered on the works. Excavation to improve the course of the river was commenced, the material excavated being placed to widen the south approach bank.

The largest bridge undertaken during the year on developmental roads is that over the Buchan River at Buchan. This has concrete piers founded on rock by sinking steel cylinders through the overlying gravel, a depth of about 7 feet. There are four timber truss spans of 60 feet, and end spans of timber stringers, total length 312 feet. Abutments and wings are of driven piles. This bridge, which replaces a ford, forms an important link in the through route to Murrindal and Gelantipy. The contract amount was £6,480.



A new bridge over Boggy Creek, at the head of Lake Tyers at Nowa Nowa, restores safety of traffic along this section of the Prince's Highway. The old bridge included a truss in which the joints had failed, leaving the span dependent on the beam strength of the lower chord. The foundation is rock, and the new substructure consists of piles planted in the rock. There is a central 60-foot timber truss span and five stringer spans, total length 175 feet. The new bridge is at a higher level than the old so as to be clear of floods, and to permit satisfactory grading of approaches. Plans were prepared by the Board and the work supervised by the Tambo Shire Council.

#### THE EXPERIMENTAL ROAD—OAKLEIGH.

About four years ago the Board, realizing that the time would soon come when more durable types of roads than those then being built would be required to stand up to the ever-increasing traffic without constant and costly maintenance, constructed an experimental length of road comprising several modern types of construction, so that when it was necessary to build modern roads some experience would have been gained concerning their first cost, the maintenance required, and the suitability for various classes of traffic. A short length of the Prince's Highway in the Town of Oakleigh was chosen as the site of this road because it carried a considerable quantity of heavy horse-drawn steel-tired traffic as well as a heavy motor traffic.

#### *Construction.*

The experimental road consists of five different sections, each about 400 feet long, and consisting of a main central pavement 18 feet wide and two shoulders 3 feet wide, giving a roadway 24 feet wide.

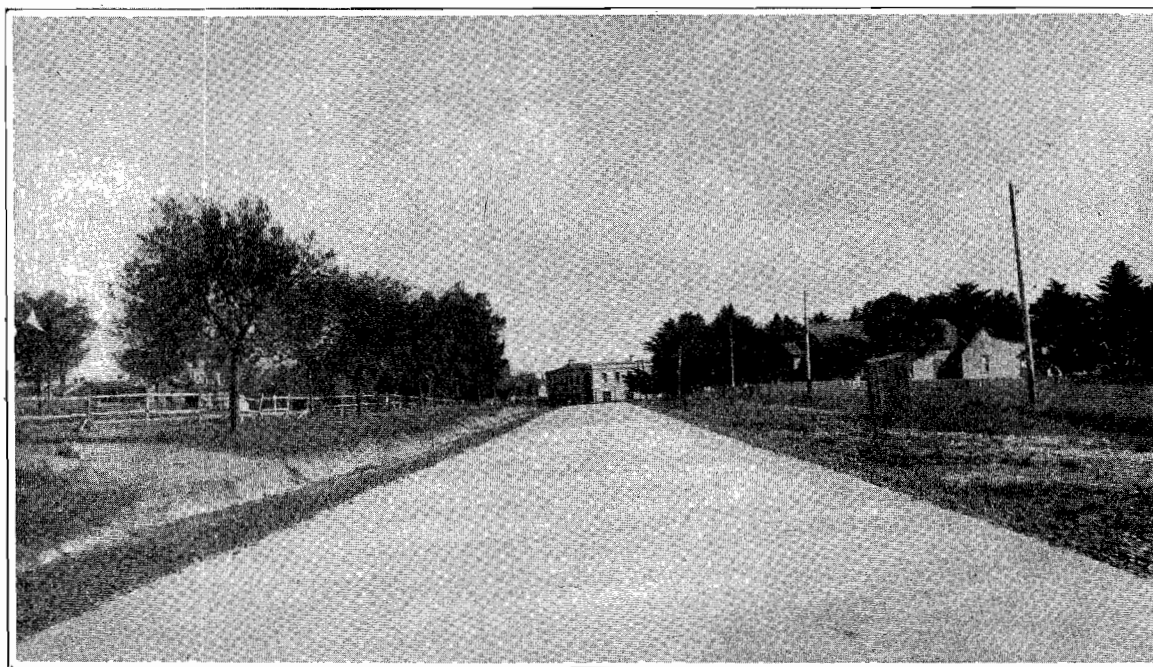


ILLUSTRATION No. 5.

Showing Finished Road.—(Section B.)

Section "A" consists of an asphaltic concrete wearing surface 2 inches thick on a plain cement concrete base. The base is 4 inches thick at the sides and 6 inches at the centre, the bottom being flat and the top cambered.

The sub-grade was trimmed to grade, rolled, and immediately before concrete was placed, watered. The concrete was composed of one part of cement to  $6\frac{1}{2}$  parts of aggregate up to  $2\frac{1}{2}$  inches in size, a minimum quantity of water being used. In all concrete sections particular care was taken to secure as dense a concrete as possible and to that end sand,  $\frac{1}{4}$ -in. toppings,  $\frac{3}{4}$ -in. screenings, and  $2\frac{1}{2}$ -in. metal were mixed in the appropriate proportion. The cement was supplied by the Commonwealth Portland Cement Company, the sand came from Seaford, and the metal (a tough basalt having a French co-efficient of wear of about 10) from Alphington. The concrete was mixed in a 4 cubic foot motor-driven mixer, and the complete gang consisted of one overseer, eleven men, and a boy.

The asphaltic concrete wearing surface was laid by the Sim Paving Co. Pty. Ltd., Melbourne, at a cost of 8s. 1d. per square yard. It consists of a mixture of crushed stone up to  $\frac{3}{4}$ -in. gauge, sand and stone dust passing a 200 mesh sieve, proportioned so as to have a maximum density, to which was added  $12\frac{1}{2}$  per cent. by weight asphaltum; 60-70 penetration. After consolidation the surface was given a second coat of hot asphaltum and, later, a coat of coarse sand. This section was opened to traffic in May, 1922.

Section "B" consists of a plain cement concrete pavement 18 feet wide, 6 inches thick at the sides and 8 inches at the centre, the bottom being flat and the top cambered. It is composed of similar material to the base of section "A," but the mix was one of cement to five of aggregate. After the concrete was deposited, it was well tamped, struck off with a heavy steel shod screed and, just before the initial set took place, the surplus water was squeezed out by rolling with a very light roller 6 feet long, weighing about 10 lb. a linear foot. This left a glazed surface which was removed by "belting" with a 12-in. belt stretched on a light timber frame. The concrete was thoroughly cured by covering with wet bags as soon as possible, and later by damp earth for 20 days. Traffic was allowed on after 30 days. Construction joints were placed at the ends of the morning's and afternoon's work. They were formed by placing a pre-moulded bituminous strip  $\frac{1}{4}$  in. thick, with its upper edge  $\frac{1}{4}$  in. below the surface.

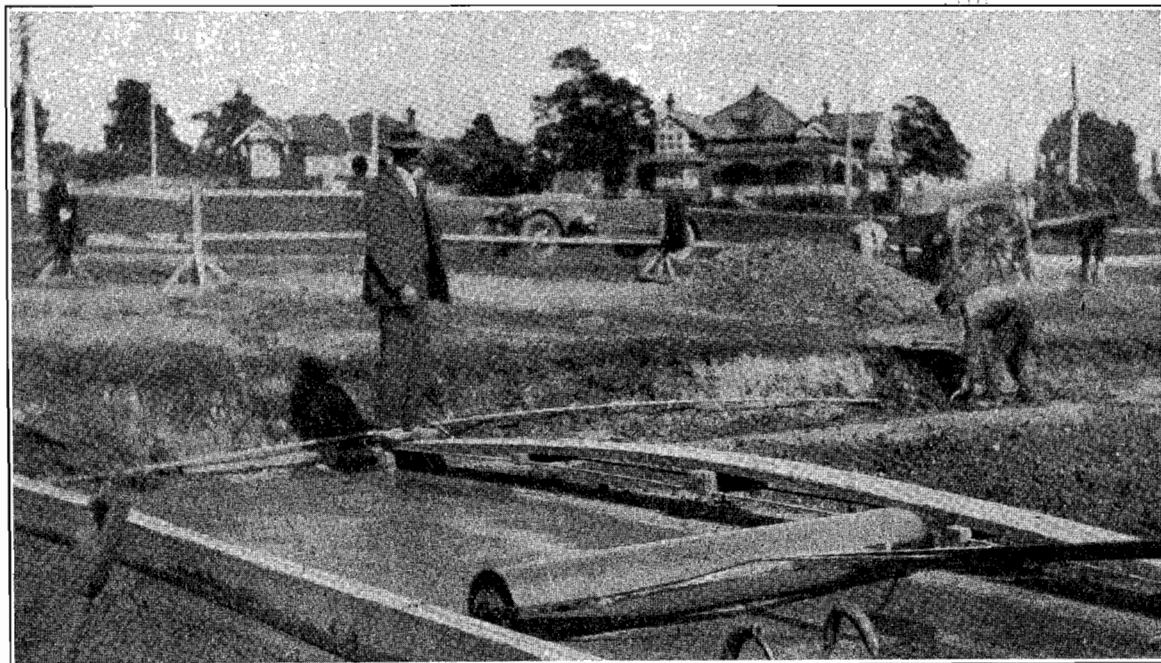


ILLUSTRATION No. 6.

Rolling the Concrete.

Section "C" consists of a two-course reinforced concrete pavement 18 feet wide. The bottom course is composed of 1 part of cement to  $6\frac{1}{2}$  of aggregate up to  $2\frac{1}{2}$ -in. gauge and is 4-in. thick at the sides and 6 inches at the centre. The top course is composed of 1 of cement to  $4\frac{1}{2}$  of aggregate up to  $\frac{3}{4}$ -in. gauge and is uniformly 2 inches thick. The reinforcement—No. 12 B.R.C.—manufactured by the Australian Reinforced Concrete and Engineering Company was placed between the two courses. Care was taken to place the top course before the bottom course had taken its initial set—usually within 30 minutes. The top course was finished off in the same way as section B.

Section "D" is similar to Section "C" except that the bottom course is one inch thinner throughout, that is, the total thickness is 5 inches at the sides and 7 inches at the centre.

Section "E" consists of asphaltic macadam 18 feet wide and 4 inches thick, laid in two courses on a base of Telford pitching 8 inches thick. The Telford pitching was constructed of "undressed dimensioned" spalls from Footscray, as the labour available was not skilful enough to lay a cheaper material. In spite of this, it was very difficult to get the spalls truly laid and well wedged up, and close supervision was required to prevent the men from laying the spalls with the broad edges up. The pitching was then rolled with a 15-ton steam roller, and finally  $\frac{3}{4}$ -in. screenings were water-grouted in. Owing to delays in the earlier part of the work the winter was too far advanced to attempt to construct the asphaltic macadam wearing surface then, and,

as the Telford pitching was true enough to carry traffic this work was postponed till November, 1922. The asphaltic macadam was constructed of 2-in. metal in two courses, each finishing about 2 inches thick. The first course was rolled and penetrated by hand with asphaltum, heated to about 370 degrees F., then  $\frac{3}{4}$ -in. screenings were spread evenly over the surface and rolled in. The second course was then put down in the same way. Including both coats, 1.85 gallons of 96 asphaltum per square yard were used. After the road had been under traffic for about two weeks it was given a seal coat by a power sprayer—about one-third gallon per square yard—and a coat of coarse sand.

Shoulders 3 feet wide were constructed of waterbound macadam 6 inches thick along all sections, but soon failed. Soaking rains fell just before they were laid, softening the ground, making it impossible to consolidate the narrow strip of metal bounded on one side by soft earth and on the other by an unyielding pavement. Observation of the effects of traffic on the shoulders has indicated that waterbound macadam is inherently unsuitable for their construction, because the traffic they get is not distributed sufficiently uniformly to preserve and maintain a waterproof surface—they get isolated wheel loads which tend to cut them up and form ruts, especially at the edge of the pavement. These ruts collect rain from the adjacent waterproof pavement and allow it to percolate through and soften the subgrade, not only destroying the shoulders, but also seriously injuring the stability of the main pavement. In December, 1922, the shoulders were reconstructed by lifting the metal, screening the dirt out and replacing, new metal being added where required. As the ground was hard, the 15-ton roller ran on the shoulder and on the adjacent watertable, so that it was possible to consolidate the metal thoroughly. A coat of hot asphaltum was then applied by hand—three-quarters of a gallon per square yard, coated with  $\frac{3}{4}$ -in. screenings and the surface again rolled. These reconstructed shoulders have stood well and have only required ordinary maintenance to keep them in order.

The total cost of the work was £6,448; of this, £1,500, being the estimated cost of an asphaltic macadam road, was charged to Permanent Works, and the balance, £4,948, was treated as miscellaneous expenditure and debited to the Fund.

Table 1 shows detailed costs of the different sections, which for convenience have been reduced to comparable units. Table 2 gives the percentage costs of the items in each of the sections.

TABLE 1.—UNIT COSTS.

*Cost of Roadway, Type A.—*

Pavement 18 feet wide consisting of plain concrete base 5 inches average thickness, with asphaltic concrete wearing surface 2 inches thick.

Shoulders 3 feet wide each side of asphaltic macadam 6 inches thick.

	£	s.	d.	
Eighteen feet concrete base at 15s. 0.13d. per sq. yd.	1	10	0.26	per lineal foot.
Eighteen feet asphaltic concrete surface at 8s. 1.04d. per sq. yd.	0	16	2.09	per lineal foot.
Six feet asphaltic macadam shoulders at 6s. 0.49d. per sq. yd.	0	4	0.33	per lineal foot.
Earthwork .. .. .	0	6	2	per lineal foot.
Culverts .. .. .	0	1	11.16	per lineal foot.
Guard Fence .. .. .	0	2	8.54	per lineal foot.
Miscellaneous .. .. .	0	2	1.96	per lineal foot.
	3	3	2.34	per lineal foot.

Or £16,680 per mile.

*Cost of Roadway, Type B.—*

Pavement 18 feet wide consisting of plain concrete of 7 inches average thickness.

Shoulders 3 feet wide each side of asphaltic macadam 6 inches thick.

	£	s.	d.	
Eighteen feet concrete slab at 19s. 8.78d. per sq. yd.	1	19	7.56	per lineal foot.
Six feet asphaltic macadam shoulders at 6s. 0.49d. per sq. yd.	0	4	0.33	per lineal foot.
Earthwork .. .. .	0	6	2	per lineal foot.
Culverts .. .. .	0	1	11.16	per lineal foot.
Guard Fence .. .. .	0	2	8.54	per lineal foot.
Miscellaneous .. .. .	0	2	1.96	per lineal foot.
	2	16	7.55	per lineal foot.

Or £14,950 per mile.

*Cost of Roadway, Type C.—*

Pavement 18 feet wide consisting of reinforced concrete of average thickness of 7 inches.  
Shoulders 3 feet wide each side of asphaltic macadam 6 inches thick.

	£	s.	d.	
Eighteen feet reinforced concrete slab at 21s. 5·09d. per sq. yd.	2	2	10·18	per lineal foot.
Six feet asphaltic macadam shoulders at 6s. 0·49d. per sq. yd.	0	4	0·33	per lineal foot.
Earthwork .. .. .	0	6	2	per lineal foot.
Culverts .. .. .	0	1	11·16	per lineal foot.
Guard Fence .. .. .	0	2	8·54	per lineal foot.
Miscellaneous .. .. .	0	2	1·96	per lineal foot.
	<hr/>			
	2	19	10·17	per lineal foot.

Or £15,800 per mile.

*Cost of Roadway, Type D.—*

Pavement 18 feet wide consisting of reinforced concrete of 6 inches average thickness.  
Shoulders 3 feet wide each side of asphaltic macadam 6 inches thick.

	£	s.	d.	
Eighteen feet reinforced concrete slab at 20s. 2·38d. per sq. yd.	2	0	4·76	per lineal foot.
Six feet asphaltic macadam shoulders at 6s. 0·49d. per sq. yd.	0	4	0·33	per lineal foot.
Earthwork .. .. .	0	6	2	per lineal foot.
Culverts .. .. .	0	1	11·16	per lineal foot.
Guard Fence .. .. .	0	2	8·54	per lineal foot.
Miscellaneous .. .. .	0	2	1·96	per lineal foot.
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	2	17	4·75	per lineal foot.

Or £15,150 per mile.

*Cost of Roadway, Type E.—*

Pavement 18 feet wide consisting of asphaltic macadam 4 inches thick on Telford pitched base 8 inches thick, the macadam being supported by pitchers.  
Shoulders 3 feet wide each side of asphaltic macadam 10 inches thick.

	£	s.	d.	
Eighteen feet asphaltic macadam at 9s. 9·68 per sq. yd.	0	19	7·36	per lineal foot.
Eighteen feet Telford pitching at 15s. 0·62d. per sq. yd.	1	10	1·25	per lineal foot.
Six feet asphaltic macadam shoulders at 10s. 0·82d. per sq. yd.	0	6	8·55	per lineal foot.
Earthwork .. .. .	0	6	2	per lineal foot.
Culverts .. .. .	0	1	11·16	per lineal foot.
Guard Fence .. .. .	0	2	8·54	per lineal foot.
Miscellaneous .. .. .	0	2	1·96	per lineal foot.
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	3	9	4·82	per lineal foot.

Or £18,320 per mile.

*Ncte.*—The above costs do not include any "overhead" charges.

TABLE 2.—PERCENTAGE COST OF SECTIONS.

	Section.				
	A.	B.	C.	D.	E.
Asphaltic concrete surface, at 8s. 1d. per square yard laid ..	25·6	..	..	..	..
Cement, at 9s. 6d. per bag (28s. 6d. : £8 11s.), on job ..	19·7	34·3	29·0	28·8	..
Metal, at 13s. 2½d. per cubic yard, on job .. ..	5·1	6·4	5·3	5·1	7·2
Screenings, at 14s. 11d. per cubic yard, on job .. ..	1·4	1·6	3·4	3·5	8·3
Toppings, at 14s. 10d. per cubic yard, on job .. ..	1·2	1·5	1·6	1·5	..
Sand, at 10s. 7½d. per cubic yard, on job .. ..	2·7	3·7	3·5	3·5	..
Reinforcement, at 1s. 9d. per square yard, on job .. ..	..	..	5·8	6·1	..
Pitchers, at 15s. per cubic yard, on job .. ..	..	..	..	..	24·0
Asphaltum, at £12 4s. 6d. per ton, on job .. ..	..	..	..	..	6·5
Miscellaneous .. .. .	·6	2·4	2·9	3·1	1·0
Plant .. .. .	4·7	8·2	7·0	6·9	5·5
Labour .. .. .	12·2	11·9	13·1	12·0	19·2
	<hr/>				
Pavement .. .. .	73·2	70·0	71·6	70·5	71·7
Earthwork, shoulders, &c. .. ..	26·8	30·0	28·4	29·5	28·3
	<hr/>				
	100·0	100·0	100·0	100·0	100·0

*Maintenance.*

Since the road was opened for traffic in 1922, maintenance work has been carried out intermittently as required. This is admittedly an uneconomical way of doing the work, but the short length of the sections renders it impracticable to employ a permanent maintenance gang, and consequently the recorded costs are of very little value in comparing different types of construction.

During the four years that the road has been built, £430 has been expended on maintenance, and of this, £180 has been expended in repairing the shoulders and cleaning the table drains (including the reconstruction of the shoulders referred to before). This work is of the utmost importance because, if the shoulders become waterlogged owing to defective drainage, the sub-grade under the concrete slab softens and its supporting power is weakened, thus tending to cause failure of the pavement.

In the asphaltic concrete section (section A) 36 square yards of the wearing surface have been relaid. The whole of this section has waved more or less, and in several places the material became so thin that it broke up and lifted off the concrete base. It is considered that this waving is due in part to a defective mixture, but mainly to the fact that the surface of the concrete had too smooth a finish to afford an effective bond with the asphalt. This work was carried out under agreement with the Sim Paving Company at a cost of 12s. 9d. per square yard.

In Sections B and C, the only maintenance required has been to the joints. They were formed by a strip of bituminous material placed with its upper edge one quarter of an inch below the surface—a method then in vogue in America, but which has since been abandoned. Shortly after the road was opened, the majority of these joints started to fail—the thin layer of concrete joining the two slabs cracking, and spalling off in small pieces. Repairs were made by cutting out the concrete to a depth of about 2 inches and the minimum width necessary to secure sharp vertical edges. This chase was then filled with toppings and sand mixed, while hot, with asphaltum. The mixture was made fairly soft and rammed with hot rammers, care being taken to finish off the top flush with the concrete. Joints repaired in this way are still in good order where the surface of the concrete is true across the joint and where the subgrade has remained solid, but where there is a tendency for wheels to jump at the joints owing to surface irregularities or to deflection of either slab—caused by a defective sub-grade—they have required further repair. Hollows then become worn at one side of the joint, so that it is necessary to make further repairs by cutting out the joint some inches wide. In one extreme case, in section D, at the junction of two slabs so short that a rocking movement was appreciable, it was necessary to cut the joint out for a width of 15 inches. The cost of the first repairs to the joints was 1s. 7½d. per lineal foot of joint, using hammers and gads to cut out the concrete. Assuming joints spaced on an average 50 feet apart, this would amount to £150 per mile of 18-ft. road. This could be appreciably reduced by using pneumatic chisels. (Actually the joints are spaced at an average distance of 25 feet—this was necessitated by the small plant used.)

Section D has required more maintenance than either Sections B or C because it contains a number of short slabs and is 1 inch thinner. An attempt was made to dispense with joint filler at the contraction joints by constructing a short length by the "alternate bay" method. In this method alternate sections are cast, leaving short gaps between them; then, when the contraction due to setting has taken place, the gaps are filled in. The edges of these joints wore down under traffic, so that it was necessary to repair them in the same way as the others, and, as the connecting slabs are only 7 feet long, the movement under traffic is comparatively large, so that the repair disintegrates rapidly and it has been necessary to continually widen the repair. One joint became so bad that it was cut out entirely and the two slabs concreted together. The work was done half width at a time and appears satisfactory.

Section D is only 5 inches thick at the edges, and several corners have cracked and broken off. The majority of these have been repaired with concrete made with rapid hardening aluminium cement. Concrete made with this cement attains a strength in one or two days equal to that of Portland cement concrete in 28 days, so that it is not necessary to close the repair to traffic for more than 24 hours. The method adopted is to remove the broken concrete and to excavate the sub-grade to a depth of about three inches below the old base. The slab is undercut so as to form a key on to the new work. The concrete is cast and the surface screeded off level with the surrounding concrete. The new work is kept moist for as long as possible, and the barriers removed in about 24 hours. Most of the repairs of this nature have been successful, but some have failed owing no doubt to lack of skill on the part of the repair gang and to insufficient curing. The greatest care is required to make a satisfactory repair, and only a permanent gang could be relied upon to make 100 per cent. successful repairs. The cost of one typical repair was £8. This would be very much reduced if a permanent gang were employed to look after a long length of road.



No repairs have yet been made to Section E (asphaltic macadam on Telford base), except near Section D at the junction with the Ferntree Gully Road. This piece carries heavy brick traffic and is in bad order, but the remainder of the section which does not get much heavy traffic is still in first-class condition.

#### Measurement of Wear.

Before the road was opened, arrangements were made to measure the actual amount of wear which would be caused by traffic, so as to compare the relative merits of different classes of concrete used for this.

The method adopted is that described in *U.S.A. Journal of Agricultural Research*, 14th February, 1916, but the apparatus was modified to suit local conditions.

The apparatus consists essentially of a steel piano wire stretched across the road. The height of the wire above the road surface is measured and compared with the height of the wire above two brass plugs which are grouted into the road with their tops about  $\frac{1}{2}$  inch below the surface. The wire used is 0.022 inch in diameter and is supported by two steel anchors, which are fixed against the edges of the concrete slab and which are arranged so that a spring balance may be used to adjust the tension of the wire to 30 lbs. With this tension the sag of the wire between its supports—18 feet apart—is about 0.03 inch, but as it is always the same for the same sized wire with the same span and pull, this is neglected and the wire assumed to be straight.

The vertical height of the wire above the two brass datum plugs is read to 0.001 inch by means of an inside micrometer screw gauge, contact with the wire being determined electrically as it is impossible to "feel" the wire in the usual way.

In order to average out small irregularities in the road surface a steel block 2 inches in diameter placed on the surface is used to measure from. For convenience, an electric buzzer is mounted on its side and connected to a battery of three dry cells by a flexible wire. The other side of the battery is connected to the piano wire, and the buzzer sounds when the micrometer gauge resting on the top of the steel block touches the underside of the wire.

The details of the apparatus and method of use are shown in Fig. 4.

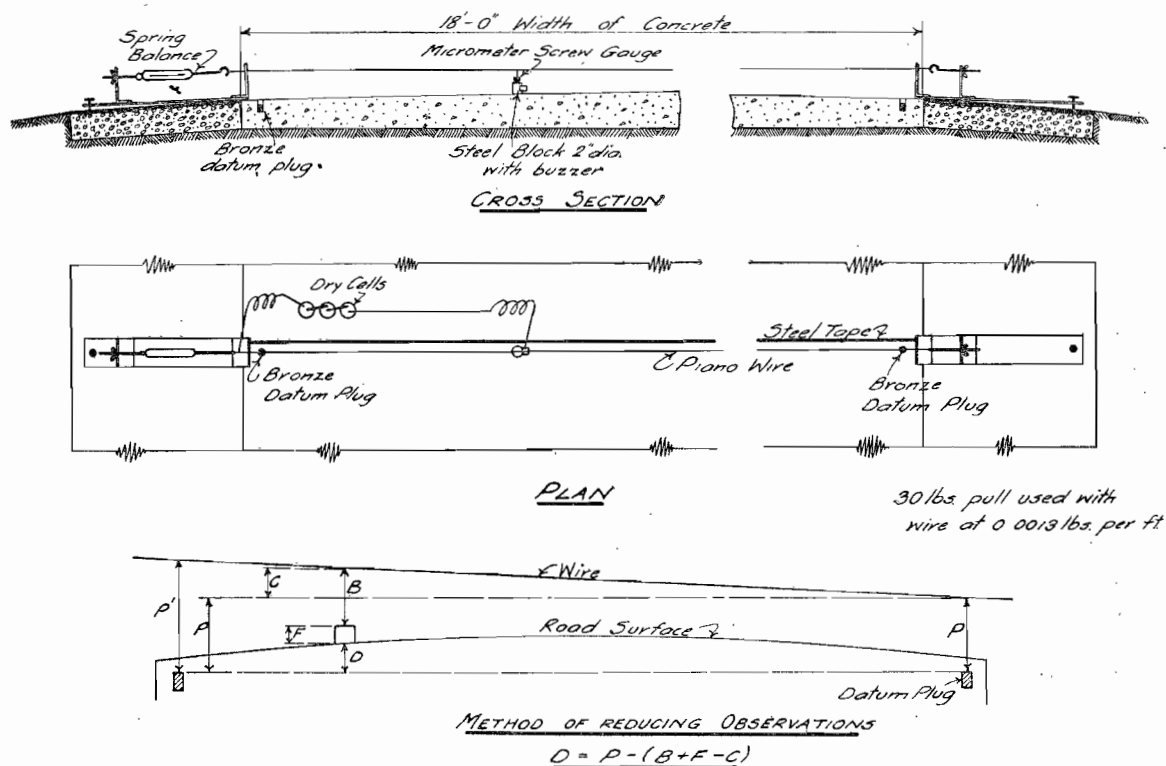


FIG. 4.

Apparatus for Wear Measurement.

Series of measurements have been made at intervals since July, 1922, and though it is still impossible to make any broad generalizations, certain conclusions may be drawn. The asphaltic concrete surface now appears to be wearing at the rate of about 0.025 inch per year, and the average wear over the section is about  $\frac{1}{4}$  inch—portion of this is, however, due to consolidation of the material. Owing to the tendency of this class of material to wave, the measurements are somewhat irregular.

The measurements of the concrete sections are more consistent. Sections B and C are wearing at the rate of about 0·07 inch per year, while Section D is wearing at 0·10 inch per year. A possible explanation of this difference is that Section D being 1 inch thinner is only about 86 per cent. as rigid as the other two. Section C is surfaced with a richer concrete than Section B, and for this reason was expected to wear less, but actually it has worn slightly more than Section B. It is considered that this is because larger metal ( $2\frac{1}{2}$  inches) was used in Section B than in Section C ( $\frac{3}{4}$  inch). The surfaces of all sections have worn quite evenly, the metal wearing at the same rate as the mortar. Except for one or two small patches where dirty toppings were evidently used, there is no sign of any pitting of the surface. The centre of the pavement has worn more than the edges, the average figures for a period of 3 years 8 months being :—

		Wear at Edges.		Centre.		Mean.
Section A	.. ..	0·19	..	0·21	..	0·20
Section B	.. ..	·16	..	·27	..	·22
Section C	.. ..	·21	..	·30	..	·26
Section D	.. ..	·30	..	·51	..	·39

With a view to co-relating the wear on the road with the volume of traffic using the road, traffic counts have been made on two occasions, but so far it has been impossible to make any definite deductions.

A summary of results is given below, the weights and speeds being estimated only :—

	19th December, 1922. 6 a.m. to 10 p.m.			8th June, 1923. 7 a.m. to 10 p.m.		
	No.	Average Speed.	Average Weight.	No.	Average Speed.	Average Weight.
Motor, pneumatic tires.. ..	199	20	Tons. 1·2	256	20	Tons. 1·1
Motor, solid tires .. ..	12	13	3·7	4	13	3·1
Horse-drawn .. ..	328	..	1·1	437	..	1·1
Others .. ..	18	..	1·1	..	..	..
Total .. ..	557	..	640	697	..	760

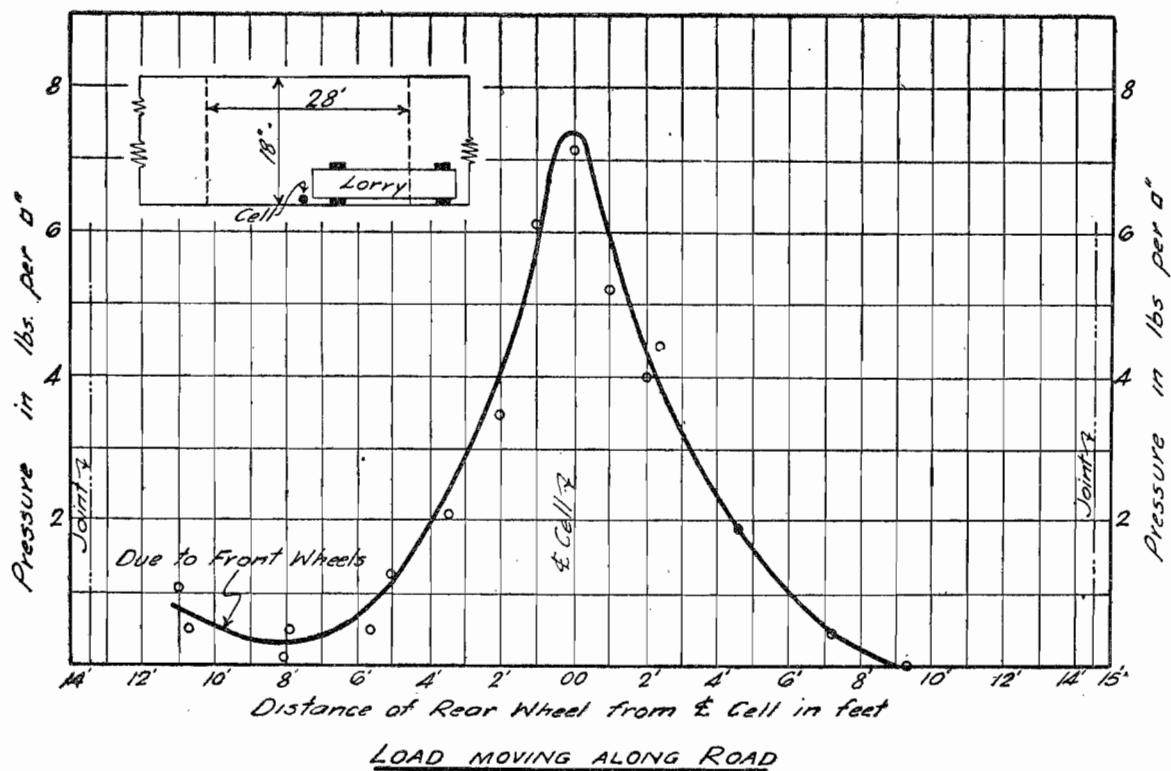
#### Measurements of Pressure.

During the construction of the road, "pressure cells" were built into it, so that the intensity of pressure on the underside of the slab could be measured. Details of the apparatus were obtained from publications of the United States Bureau of Public Roads.

A pressure cell consists essentially of a flat circular box  $5\frac{1}{2}$  inches in diameter and  $1\frac{1}{4}$  inches thick, with one side flexible. This side is made of a sheet of brass 0·005 inch thick, stiffened over the greater part of its area by brass discs.

Normally, when external pressure tends to force this flexible side inwards, it rests against, and the pressure is taken by, a steel button fixed to but insulated from the body of the box. The cell is cast into the concrete slab with the flexible side down and flush with the under surface and is connected to the indicating apparatus by a brass tube  $\frac{1}{8}$  inch diameter, through which passes an insulated wire soldered to the steel button. To use the cell an electric circuit is made up of a lamp and battery connected through the body of the cell, steel button, and the insulated wire. Compressed air is allowed to flow slowly into the cell, and when the pressure on the inside is just greater than that on the outside, i.e., the underside of the slab, the flexible side moves and breaks the circuit, causing the lamp to go out. Immediately this happens the supply of compressed air is shut off and the pressure in the cell is read on a sensitive pressure gauge. This is the intensity of pressure on the underside of the slab.

Typical curves showing the distribution of pressure under a loaded lorry wheel are given in Fig. 5. Unfortunately, some of the pressure cells failed to work, and a complete set of results could not be obtained. The maximum recorded pressure due to a load of  $3\frac{1}{4}$  tons (rear wheel of heavy lorry) was  $7\frac{1}{2}$  lb. per square inch (0·48 tons per square foot) at the edge and 2 lb. per square inch (0·13 tons per square foot) at the third point. It should be understood that these figures refer to static loads and that the pressures under moving loads would be at least twice their value. If the surface were rough, the pressures would be greater still.



Loads:- Rear Wheels  $3\frac{1}{2}$  Tons  
 Front Wheels 1 Ton  
 Concrete 6" thick at edges  
 8" " " centre  
 Date of Test 17.5.23

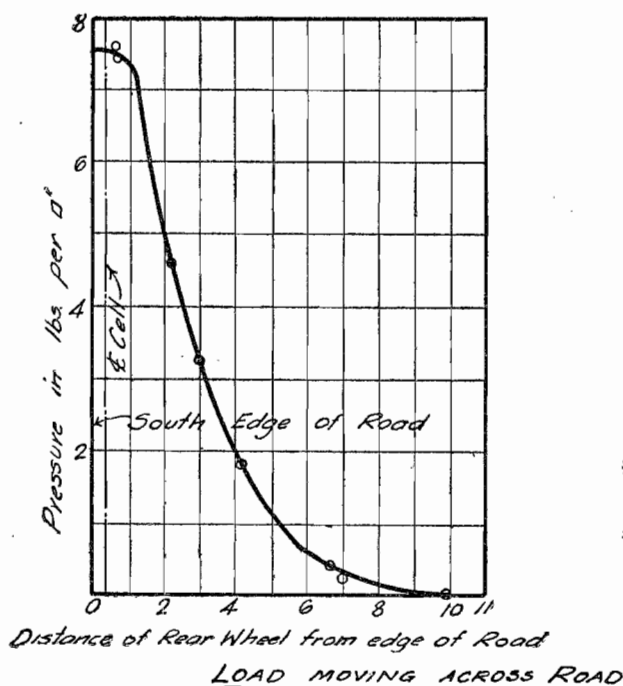


FIG. 5.

Pressure Measurements.

Measurements of Stresses.

In October, 1923, a series of tests was made to determine the stress in a concrete road slab, due to a static load.

The method adopted—believed to be original—was to measure the changes of slope of the slab due to bending caused by a static load, and thence calculate the corresponding moments and stresses.

The following formulae were made use of:—

$$M = E \cdot I \cdot d^2/dl^2$$

$$M = f \cdot I/y$$

$$\text{and } f = E \cdot y \cdot d^2/dl^2$$

where  $M$  = Bending moment (inch pounds).

$E$  = "Young's Modulus" (assumed 2,000,000 lb. per square inch).

$I$  = Moment of Inertia of Section (inch units).

$d^2/dl^2$  = Rate of change of slope at the point considered.

$f$  = Extreme fibre stress (pounds per square inch).

$y$  = Distance from neutral axis to extreme fibre (inches).

The change of slope was measured by means of a very sensitive spirit level—the property of the University of Melbourne, kindly lent by Mr. H. S. Melbourne, the Lecturer in Surveying—supported on an iron plate fitted with three levelling screws, which rest on the slab. The level was calibrated, and it was found that one division of the scale ( $1/40$  of an inch) corresponded very closely to two seconds of arc (about  $\frac{1}{2}$  inch per mile). As the levelling screws were 14·3 inches apart, a change in the position of the bubble of one division would be caused by an alteration in the relative height of the ends of the supporting screws of 0·00014 inches.

The tests were conducted on a slab of plain concrete 22 ft. 6 in. long and 18 feet wide (part of Section B), the load being a loaded motor lorry with 3 tons  $6\frac{1}{2}$  cwt. on the rear wheels and 1 ton  $\frac{1}{2}$  cwt. on the front ones (total 8 tons 14 cwt.).

The first test was carried out with the object of finding the relative movement of adjacent slabs at a joint due to the transference of a load from one side of the joint to the other. The level and stand were placed so as to straddle the joint, and readings were taken with the rear wheels (lorry in centre of road) first on one slab and then just over the joint on the other. The total relative movement at the joint plane was 0·02 inch. This was with a “static load,” with a moving load the movement would not be less than twice this amount. This shows the necessity of providing for shear across joints, so as to prevent relative movement between adjacent slabs, with the resultant wear and disintegration.

The next series of tests was carried out to determine the maximum stresses occurring in a plane parallel to the centre line of the road, near the line of wheels of the lorry when it was in the centre of the road, and also when it was near one edge. As only one spirit level was available it was necessary to plot the results, getting first influence lines for slope at a point due to the load in various positions, and from these to plot slope curves for one particular position of the load.

The level was placed at a point and the load moved along, readings being taken at intervals. The level was then moved to another point and the process repeated. These results were plotted and gave the slope at the point corresponding to every position of the load. A series of these influence lines was obtained, and from these slope curves were plotted giving the slope at points along the slab due to one position of the load. From these curves the maximum change in slope was measured. This occurs at the point of maximum moment, and is a measure of the stress at that point.

The following results were obtained :—

1. Stresses in vertical plane parallel to centre line of road, near wheel track, due to two rear wheels in centre of road, 3·3 tons each.

Distance of Loads from end of 22-ft Slab 8 inches thick.	Maximum Stress.
6 inches .. ..	14 lb. per square inch top in tension
4 ft. 6 in. .. ..	18 lb. per square inch top in compression
9 ft. 6 in. .. ..	22 lb. per square inch top in compression
13 ft. 6 in. .. ..	14 lb. per square inch top in compression
21 ft. 6 in. .. ..	14 lb. per square inch top in tension

2. Stresses in vertical plane parallel to centre line of road, near edge, due to two rear wheels near edge 3·3 tons each.

Distance of Loads from end of 22-ft. Slab 6 inches thick.	Maximum Stress.
6 inches .. ..	47 lb. per square inch top in tension
4 ft. 6 in. .. ..	34 lb. per square inch top in compression
9 feet .. ..	32 lb. per square inch top in compression
13 ft. 6 in. .. ..	46 lb. per square inch top in compression
18 feet .. ..	38 lb. per square inch top in compression
21 ft. 6 in. .. ..	41 lb. per square inch top in tension

It should be noted that the above stresses were due to a static load, and that those produced by a moving load would probably be several times greater. Also the stresses occurring in a plane at right angles to the centre line of the road are probably greater than the ones measured (i.e., parallel to the centre line of the road).

*Conclusions.*

After nearly four years of observation of experimental sections, there are still many questions that remain unanswered, but some conclusions appear quite definite. The first is that a properly constructed concrete surface is quite satisfactory to all classes of traffic, but that, under horse-driven steel-tired traffic it gradually wears down. (American records indicate that wear due to rubber tires is negligible.) If the surface were properly constructed in the first place, this wear is quite uniform and the surface remains true.

The joints in a concrete pavement require constant watching so that any defects are remedied as soon as they appear and before they have time to grow. The method of repair adopted appears to be quite satisfactory. It is obvious that joints should be spaced as far apart as possible; the average spacing is about 25 feet. This could well be doubled.

It is evident from the behaviour of Section "D" that 5 inches at the edge is too thin to stand heavy traffic. Unfortunately the section adopted in all the concrete sections—with edges thinner than the centre—has been proved by research carried out in America to be uneconomical. Modern practice prescribes a pavement with thickened edges. Had the section been constructed with edges 7 inches and centre 5 inches thick, less concrete would have been required and, in all probability, it would have carried very heavy traffic quite satisfactorily.

The waving that has occurred in the asphaltic concrete section indicates that the mixture was not perfect, and that a smooth finish of the concrete base is not desirable. For the reasons given above the base should have thickened edges.

The asphaltic macadam section is quite satisfactory under motor traffic, but it does not appear to stand up to the heavier horse traffic. The Telford Base is economical only for localities where suitable pitching stone is available and where skilled labour can be obtained.

Experience with the shoulders indicates that the road is too narrow for two lanes both of fast and slow moving traffic—too large a percentage of vehicles have to turn out on to the shoulders. This tends to cause rutting of the edge of the main pavement and in turn promotes drainage troubles by providing an entry to the shoulder for the water flowing off the pavement. If the sub-grade becomes water-logged the most scientifically constructed pavement will rapidly fail.

I have the honour to be, Sir,

Your Obedient Servant,

ARTHUR E. CALLAWAY,  
Chief Engineer.

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APPENDIX A.  
COUNTRY ROADS BOARD FUND.

RECEIPTS.		£	s.	d.	£	s.	d.	£	s.	d.
Dr. 1924.	To Balance .. .. .	..	..	..	21,919	14	7	..	..	..
July 1. 1925.	Motor Car Act No. 2702—	..	..	..	..	..	..	..	..	..
June 30.	Registration Fees .. .. .	..	..	..	387,217	1	1	..	..	..
	Licence Fees .. .. .	..	..	..	18,982	16	6	..	..	..
	Fines .. .. .	..	..	..	13,699	14	7	..	..	..
	Less Refunds and Costs of Administration ..	..	..	..	419,899	12	2	..	..	..
	No. 1894—	..	..	..	16,928	13	10	..	..	..
	Unused Roads and Water Frontages Act	..	..	..	402,970	18	4	..	..	..
	Licence Fees .. .. .	..	..	..	21,747	2	7	..	..	..
	Less Refunds and Cost of Administration ..	..	..	..	1,946	0	0	..	..	..
	Country Roads Act No. 2635—	..	..	..	1,561	2	0	..	..	..
	Registration of Traction Engines	..	..	..	82	8	0	..	..	..
	Width of Tyres, Administration Fines and Costs	..	..	..	..	..	..	..	..	..
	Municipalities Repayments—	..	..	..	..	..	..	..	..	..
	Permanent Works .. .. .	..	..	..	82,930	10	7	..	..	..
	Maintenance Works .. .. .	..	..	..	154,557	7	9	..	..	..
	Appropriation for Maintenance of Main Roads	..	..	..	237,487	18	4	..	..	..
	Act, No. 2986 .. .. .	..	..	..	50,000	0	0	..	..	..
	Less Amount Appropriated under Act 3379	..	..	..	20,000	0	0	..	..	..
	Hire of Plant .. .. .	..	..	..	10,888	16	1	..	..	..
	Stores and Materials .. .. .	..	..	..	18,340	5	0	..	..	..
	Sundries .. .. .	..	..	..	6,734	8	11	..	..	..
	Advances to Loan Account on account of Permanent Works	..	..	..	727,866	19	3	..	..	..
	Expenditure refunded .. .. .	..	..	..	222,176	11	3	..	..	..
	Advances on account of Expenditure under Act 3235, Item 4,	..	..	..	13,137	11	10	..	..	..
	Tourist Vote refunded .. .. .	..	..	..	2,132	0	0	..	..	..
	Advances on account of Expenditure under Prov. 72, Mt. Buffalo	..	..	..	19,025	3	4	..	..	..
	Road, refunded .. .. .	..	..	..	..	..	..	..	..	..
	Advances under Prov. 72 for Relief of Unemployment refunded	..	..	..	..	..	..	..	..	..
	Balance .. .. .	..	..	..	£1,006,258	0	3	..	..	..
RECONCILIATION STATEMENT.										
	Balance as per Country Roads Boards Accounts	..	..	..	£	s.	d.	..	..	..
	Add Advances on account of Permanent Works	..	..	..	94,138	4	4	..	..	..
	Transfers Outstanding .. .. .	..	..	..	29,146	9	11	..	..	..
	Less Refunds .. .. .	..	..	..	1,559	15	2	..	..	..
	Country Roads Board Fund Dr. Balance .. .. .	..	..	..	30,706	5	1	..	..	..
PAYMENTS.										
	By Maintenance (Appendix)	..	..	..	471,732	3	6	..	..	..
	Less Refunds .. .. .	..	..	..	3,550	12	5	..	..	..
	Miscellaneous .. .. .	..	..	..	..	..	..	..	..	..
	Advances to Loan Account on account of Permanent Works	..	..	..	..	..	..	..	..	..
	Expenditure .. .. .	..	..	..	..	..	..	..	..	..
	Advances under Act No. 3235, Item 4—	..	..	..	..	..	..	..	..	..
	Tourist Vote .. .. .	..	..	..	13,902	16	11	..	..	..
	Less Refunds .. .. .	..	..	..	28	9	9	..	..	..
	Advances under Prov. 72, Mount Buffalo Road	..	..	..	..	..	..	..	..	..
	Advances under Prov. 72, Works for Relief of—	..	..	..	19,303	6	8	..	..	..
	Unemployment .. .. .	..	..	..	20	8	0	..	..	..
	Less Refunds .. .. .	..	..	..	..	..	..	..	..	..
	Balance .. .. .	..	..	..	19,282	18	8	..	..	..
		..	..	..	94,138	4	4	..	..	..
RECONCILIATION STATEMENT.										
	Balance as per Treasury Books	..	..	..	£	s.	d.	..	..	..
	Add Transfers Outstanding .. .. .	..	..	..	20,809	7	1	..	..	..
	Tourist Vote .. .. .	..	..	..	1,128	15	11	..	..	..
	Mt. Buffalo Road .. .. .	..	..	..	157	14	0	..	..	..
	Special Grant Relief of Unemployment	..	..	..	1,286	9	11	..	..	..
	Williamstown Short Road .. .. .	..	..	..	257	15	4	..	..	..
		..	..	..	15	9	11	..	..	..
	Deduct Accounts in transit	..	..	..	22,369	2	3	..	..	..
	Country Roads Board Fund Dr. Balance .. .. .	..	..	..	184,168	13	1	..	..	..
		..	..	..	59,324	3	8	..	..	..
		..	..	..	£124,844	9	5	..	..	..

APPENDIX A—continued.

Dr.		REVENUE ACCOUNT, 30TH JUNE, 1925.		Cr.	
1925.	1924.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
June 30. To Maintenance Works—General ..	July 1. By Balance ..	..	447,714 18 10	..	286,050 1 11
" Maintenance Works—	June 30. " Motor Car Act No. 2702—	..	..	..	..
Woods Point Road ..	Registration Fees ..	1,991 17 1	387,217 1 1	..	..
Woods Point Road ..	Licence Fees ..	887 15 11	18,982 16 6	..	..
Glen Wills Road ..	Fines ..	350 5 5	13,639 14 7	..	..
Omso Road ..	Less Refunds and cost of Administra-	137 12 8	419,899 12 2	..	..
Bright-Omso Road ..	tion ..	1,300 0 0	16,928 13 10	..	..
Prince's Highway ..	Unused Roads and Water Frontages Act	10,363 14 11	402,970 18 4	..	..
Genoa-Gipsy Point Road ..	No. 1894—	64 1 11	..	..	..
Walhalla Road ..	Licence Fees ..	254 14 0	21,747 2 7	..	..
Omso Highway ..	Less Refunds and cost of Adminis-	5,116 10 4	1,946 0 0	..	..
Contributions to Sinking Fund ..	Country Roads Act No. 2635—	20,466 12 3	19,801 2 7	..	..
Interest on Loans ..	Traction Engines Licences and Fines..	468,181 11 1	1,561 2 0	..	..
Office Furniture ..	Forfeited Deposits ..	22,820 10 9	108 13 3	..	..
Furniture, Patrolman's Cottage ..	Materials, Sale of ..	68,461 11 11	193 4 2	..	..
Printing and Stationery ..	Red Gum Timber, Sale of ..	..	266 6 8	..	..
Salaries ..	Plans, Sale of ..	..	90 3 0	..	..
Instruments ..	Plant Earnings ..	..	11,067 17 5	..	..
Plans Purchased ..	Deduct working costs ..	..	9,637 17 11	..	..
Metal Investigation ..	Rents ..	..	1,429 19 6	..	..
Metal Expenses ..	Royalty on metal ..	..	15 13 2	..	..
Office Expenses ..	Permanent Works—	..	353 4 10	..	..
Audit Fees ..	Contributions payable by Municipalities	..	89,063 4 2	..	..
Testing Materials ..	Maintenance Works—	..	178,210 14 3	..	..
Storeyard Account ..	Country Roads Act No. 2986—	..	..	..	..
Camp Equipment ..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
Motor Expenses ..	Roads ..	..	..	..	..
Width of Tyres Administration ..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
Less Fines and Costs ..	No. 3379 ..	..	..	..	..
Travelling Expenses ..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
Chairman's Mission to Europe and America ..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
Postage and Telegrams ..	Roads ..	..	..	..	..
Plant Account ..	Less Amount Appropriated under Act	..	..	..	..
Experimental Section ..	No. 3379 ..	..	..	..	..
Sundry Expenditure ..	Country Roads Act No. 2986—	..	..	..	..
Interest on Contributions in Arrears, written back ..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
Balance ..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	30,000 0 0	..	..
..	Appropriation for Maintenance of Main	..	724,064 5 11	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	..	..	..
..	No. 3379 ..	..	..	..	..
..	Country Roads Act No. 2986—	..	..	..	..
..	Appropriation for Maintenance of Main	..	50,000 0 0	..	..
..	Roads ..	..	..	..	..
..	Less Amount Appropriated under Act	..	20,000 0 0	..</	





APPENDIX A—continued.

DEVELOPMENTAL ROADS INTEREST ACTS Nos. 2944, 2985, & 3255.

RECEIPTS.		PAYMENTS.	
1925. June 30.	£ s. d.	1925. June 30.	£ s. d.
To Interest contributed by Municipalities Act No. 2944	7,644 17 4	By Payments to Treasury	.. .. .
„ Interest contributed by Municipalities Act No. 2985	22,311 10 0	.. .. .	.. .. .
„ Interest contributed by Municipalities Act No. 3255	9,406 5 9	.. .. .	.. .. .
	<hr/>		<hr/>
	39,362 13 1		39,362 13 1
	<hr/>		<hr/>
	£39,362 13 1		£39,362 13 1

AUDITOR-GENERAL'S CERTIFICATE.

The Accounts have been audited and compared with the books, with which they agree. Reconciliations have also been made with books of the Treasury. I certify that the statements submitted are correct.

J. A. NORRIS, Auditor-General.  
23rd December, 1925.

THOMAS G. BOWDEN, Accountant.  
10th November, 1925.



## APPENDIX B.

FEDERAL STATE GRANTS—STATEMENT OF EXPENDITURE FOR THE FINANCIAL YEAR ENDED  
30TH JUNE, 1925.

## WORKS CARRIED OUT UNDER DIRECT SUPERVISION OF THE BOARD.

District or Shire.	Road.	Federal State Grant No. 1.		Federal State Grant No. 2.		Federal State Grant No. 3.	
		£	s. d.	£	s. d.	£	s. d.
Buln Buln .. ..	Fumina .. ..	..	..	167	16 5	8,000	0 0
Portland .. ..	Mt. Gambier .. ..	..	..	581	2 6	984	16 10
Woorayl and South Gippsland	Turton's Creek .. ..	..	..	3,355	8 11	2,335	19 10
Werribee .. ..	Point Cook .. ..	9,041	1 9	..	..	..	..
South Gippsland ..	Morwell River .. ..	..	..	16	18 9	7,787	8 2
Cape Otway .. ..	Forrest-Apollo Bay ..	1,429	1 0	8,076	2 4	15,767	4 10
South Gippsland ..	Allambee Estate .. ..	..	..	3,931	0 7	7,062	10 9
Central .. ..	Yarra Glen-Glenburn ..	..	..	2,055	2 10	517	15 5
Eastern .. ..	Orbost-Delegate .. ..	..	..	919	0 0	9,978	1 0
South Gippsland ..	Jeeralang West .. ..	..	..	5,324	4 3	..	..
East Gippsland ..	Murrungowar .. ..	..	..	..	..	4,154	14 4
Cape Otway .. ..	Aire River-Forrest ..	..	..	1,889	8 5	1,038	9 3
Cape Otway .. ..	Apollo Bay-Wye River ..	..	..	8,221	11 6	1,279	19 7
South Western ..	Timboon-Nirranda ..	..	..	1,993	13 7	..	..
North-Western ..	Mildura-Lake Hattah ..	..	..	10,350	11 10	..	..
North-Western ..	Murray River Valley ..	..	..	13,597	1 5	3,593	1 3
Northern .. ..	Toombullup .. ..	..	..	539	9 2	2,450	8 7
Northern .. ..	Rochester-Echuca Main ..	..	..	1,585	13 2	..	..
Western .. ..	Elderslie Soldier Settlement	..	..	1,523	18 5	..	..
Western .. ..	Lismore-Pittong .. ..	..	..	4,112	14 8	2,170	3 6
South-Western ..	Portland-Nelson .. ..	..	..	3,533	3 11	..	..
South-Western ..	Portland-Port Fairy ..	..	..	4,500	0 0	850	0 0
North-Eastern ..	Murray River-Tallangatta ..	..	..	3,000	0 0	4,168	12 2
North and North- Western	Shepparton - Rochester - Wycheproof	..	..	3,000	0 0	814	8 2
.. ..	Foster-Boolarra .. ..	..	..	3,400	9 6	..	..
.. ..	Bruthen-Omeo .. ..	..	..	..	..	20,152	4 0
		10,470 2 9		85,674 12 2		93,105 17 8	

## WORKS CARRIED OUT UNDER SUPERVISION OF MUNICIPALITIES.

Beechworth .. ..	.. ..	106	4 11	..	..
Buln Buln .. ..	.. ..	500	0 0	..	..
Deakin .. ..	.. ..	133	6 8	..	..
Eltham .. ..	.. ..	203	4 3	..	..
Ferntree Gully ..	.. ..	308	4 8	..	..
Grenville .. ..	.. ..	665	2 10	..	..
Hampden .. ..	.. ..	207	6 8	..	..
Phillip Island and Woolamai	.. ..	167	0 0	..	..
Ripon .. ..	.. ..	53	6 8	..	..
Shepparton.. ..	.. ..	133	4 8	..	..
Strathfieldsaye ..	.. ..	5	6 8	..	..
Towong .. ..	.. ..	162	19 3	..	..
Warragul .. ..	.. ..	150	0 0	..	..
Whittlesea .. ..	.. ..	569	7 5	..	..
Winchelsea .. ..	.. ..	77	0 8	..	..
Wycheproof .. ..	.. ..	41	6 8	..	..
		3,483 2 0		..	
		13,953 4 9		85,674 12 2	
				93,105 17 8	

APPENDIX B—*continued.*TOURIST VOTE FOR THE IMPROVEMENT AND MAKING ACCESSIBLE PLACES OF INTEREST,  
TOURIST RESORTS, ETC.—ACT No. 3235.

## STATEMENT OF EXPENDITURE FOR FINANCIAL YEAR ENDING 30TH JUNE, 1925.

		£	s.	d.
Hall's Gap-Wartook Road	.. Construction of roads into and through the Grampians	2,747	14	6
Promontory Road	.. Construction of road to National Park, Wilson's Promontory	0	8	0
Warburton-Narbethong Road	.. Construction of road from Mt. Donna Buang Road to Acheron Gap and down the valley of Acheron River to Marysville Road	10,226	0	6
Extension Great Ocean Road	.. Extension of Great Ocean Road between Lorne and Wye River	0	4	2
Beechworth Gorge	.. Completing the road scheme	900	0	0
		13,874	7	2

## RECEIPTS.

Contribution paid by Great Ocean Road Trust towards cost of extension Great Ocean Road	..	175	0	0
Mt. Buffalo Road	.. Metalling and construction of road towards New Dam (Crystal Brook). Repairing the road towards the Horn. Metalling Mt. Buffalo Road (Porepunkah to the Chalet)	2,289	14	0

## SPECIAL GRANT FOR RELIEF OF UNEMPLOYMENT.

*Appropriation Act 1924-25, Division 72.*

## STATEMENT OF EXPENDITURE FOR THE FINANCIAL YEAR ENDED 30TH JUNE, 1925.

		£	s.	d.
Basin Road, Item 9	.. .. .	6,171	11	1
Errinundra Road, Item 6	.. .. .	6,530	12	9
Toolangi Road, Item 9	.. .. .	6,580	14	10
		19,282	18	8
Williamstown Short Road	.. .. .	15	9	11

## APPENDIX C.

## COUNTRY ROADS BOARD.

STATEMENT OF APPORTIONMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF MAIN ROADS FOR THE YEAR ENDED 30TH JUNE, 1924.

Name of Municipality.	Permanent Works.		Maintenance.	Name of Municipality.	Permanent Works.		Maintenance.
	Principal.	Interest.	Amount.		Principal.	Interest.	Amount.
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
Alberton Shire ..	2,196 7 1	32 9 11	1,840 4 10	Brought forward	34,426 16 3	527 6 7	72,162 17 4
Alexandra Shire ..	1,204 13 2	24 1 5	849 6 9	Huntly Shire ..	2,027 3 3	26 17 7	300 5 3
Arapiles Shire ..	1,240 18 5	21 16 11	622 12 4	Inglewood Borough	..	..	110 15 5
Ararat Borough ..	..	..	183 5 10	Kara Kara Shire ..	750 0 0	5 12 6	310 8 2
Ararat Shire ..	25 5 4	0 15 2	1,871 10 0	Karkaroo Shire ..	3,628 14 2	49 5 3	2,107 12 6
Avoca Shire ..	..	..	301 18 11	Keilor Shire ..	..	..	752 9 1
Avon Shire ..	84 7 4	1 13 11	667 10 2	Kerang Shire ..	285 17 7	4 8 1	..
Bacchus Marsh Shire	1,199 3 0	6 8 6	1,399 13 9	Kilmore Shire ..	..	..	356 8 3
Bairnsdale Shire ..	231 1 2	8 3 10	918 7 6	Koroit Borough ..	3 18 7	0 2 4	886 4 2
Ballan Shire ..	17 5 0	0 1 3	1,530 8 2	Korong Shire ..	755 0 10	9 15 9	463 13 3
Ballarat Shire ..	24 1 11	0 1 9	1,875 14 1	Korumburra Shire	616 14 1	5 2 0	1,169 16 1
Bannockburn Shire	..	..	902 14 0	Kowree Shire ..	1,391 3 4	21 13 7	1,109 11 2
Barrarbool Shire	..	..	1,442 18 9	Kyneton Shire ..	..	..	1,262 8 2
Beechworth Shire	564 6 4	13 1 2	807 13 0	Lawloit Shire ..	1,935 3 9	37 3 0	1,737 18 4
Belfast Shire ..	30 4 11	..	1,941 10 6	Leigh Shire ..	314 15 4	9 11 6	726 5 4
Bellarine Shire ..	..	..	1,876 14 0	Lexton Shire ..	..	..	726 6 7
Benalla Shire ..	1,993 10 7	13 6 2	515 12 1	Lilydale Shire ..	2,532 6 2	46 18 3	3,292 15 6
Bendigo City ..	..	..	142 1 5	Lowan Shire ..	3,541 1 0	71 1 9	1,648 7 5
Berwick Shire ..	2,102 11 4	42 6 0	1,634 0 8	Maffra Shire ..	1,439 4 11	16 10 2	727 3 9
Bet Bet Shire ..	..	..	329 8 6	Maldon Shire ..	1 14 2	0 1 0	676 13 0
Birohip Shire ..	207 17 3	6 5 11	276 16 3	Mansfield Shire ..	875 18 0	12 8 2	1,576 0 5
Borong Shire ..	1,174 18 4	26 11 7	459 0 4	Marong Shire ..	1,158 3 0	6 9 2	1,076 5 2
Braybrook Shire ..	..	..	2,034 17 1	Maryborough	..	..	21 11 0
Bright Shire ..	94 12 4	1 5 9	313 5 2	Borough	..	..	814 11 7
Broadford Shire ..	..	..	4 7 5	McIvor Shire ..	0 4 0	0 0 2	..
Broadmeadows	..	..	1,882 3 8	Melton Shire ..	..	..	438 8 3
Shire ..	..	..	741 1 10	Metcalfe Shire ..	..	..	167 7 4
Bulla Shire ..	..	..	2,285 12 2	Mildura Town ..	1,557 19 3	26 16 10	9 11 4
Buln Buln Shire ..	48 0 3	1 15 7	1,129 3 10	Mildura Shire ..	1,135 8 6	18 2 11	628 16 8
Bungaree Shire ..	427 16 11	2 11 0	556 10 4	Minhamite Shire ..	..	..	1,287 4 6
Buninyong Shire ..	61 2 3	1 16 8	177 17 8	Mirboo Shire ..	51 11 0	0 2 4	124 9 8
Carrum Borough ..	..	..	184 0 4	Moorabbin Shire ..	..	..	1,030 1 3
Castlemaine Shire	..	..	604 5 11	Mordialoo Town	..	..	384 0 5
Charlton Shire ..	2,902 9 6	49 5 1	435 4 7	Mornington Shire	1,183 5 7	10 12 9	729 13 1
Chiltern Shire ..	433 17 9	8 4 2	67 18 8	Mortlake Shire ..	..	..	644 5 11
Clunes Borough ..	..	..	3,026 14 1	Morwell Shire ..	656 6 11	15 18 3	664 1 7
Colac Shire ..	2 6 0	0 0 1	2,019 13 4	Mount Rouse Shire	..	..	1,169 10 8
..	..	2 6 0*	908 19 4	Mulgrave Shire ..	346 14 4	2 0 1	1,490 10 11
Corio Shire ..	107 9 7	0 1 4	124 0 0	Narracon Shire ..	953 8 3	17 6 3	918 9 2
Cranbourne Shire	2,181 8 8	36 5 0	828 4 6	Newham and Wood-	..	..	243 6 7
Creswick Borough	..	..	2,085 5 11	end Shire ..	..	..	..
Creswick Shire ..	..	..	73 0 0	Newtown and Chil-	..	..	..
Dandenong Shire ..	..	..	1,455 11 8	well Borough ..	107 9 7	0 1 4	..
Daylesford Borough	..	..	1,194 13 8	Newstead and Mt.	..	..	..
Deakin Shire ..	52 7 2	..	605 7 0	Alexander Shire	0 6 2	0 0 2	335 1 7
Dimboola Shire ..	728 2 10	13 19 0	1,202 4 0	..	..	0 6 2*	..
Donald Shire ..	..	..	2,766 17 8	Nunmurkah Shire ..	1,098 6 8	32 7 10	671 6 1
Doncaster Shire ..	..	..	667 8 0	Numawading Shire	..	..	1,937 3 3
Dundas Shire ..	799 10 7	13 13 2	121 16 6	Oakleigh Town ..	3 17 4	0 2 11	623 11 11
Dunmunkle Shire	52 1 2	0 14 0	..	..	..	3 17 4*	..
East Loddon Shire	1,358 18 4	26 3 5	1,846 11 5	Omeo Shire ..	986 19 9	26 8 2	961 3 4
Echuca Borough ..	1,823 16 2	22 10 9	2,023 17 2	Orbost Shire ..	2,245 12 2	31 5 10	406 0 9
Eltham Shire ..	1,249 6 10	14 6 8	1,131 9 3	Otway Shire ..	408 11 11	5 9 9	549 7 0
Euroa Shire ..	1,705 17 1	25 12 0	..	Oxley Shire ..	1,942 15 3	25 19 3	244 12 9
Ferntree Gully Shire	333 19 11	12 3 11	1,231 3 11	Phillip Island and	..	..	..
Flinders Shire ..	21 18 4	0 0 1	320 17 4	Woolamai Shire	1,722 3 1	23 16 9	674 19 9
Footscray City ..	..	..	1,846 11 5	Port Fairy Borough	..	..	378 4 11
Frankston and Has-	..	..	2,023 17 2	Portland Shire ..	8 1 2	0 2 1	906 2 1
tings Shire ..	1,458 7 1	28 8 8	1,669 0 9	..	..	8 1 2*	..
Geelong City ..	322 8 11	0 4 0	..	Preston Town ..	..	..	1,168 8 2
Geelong West	..	..	489 1 3	Pyalong Shire ..	..	..	455 5 0
Borough ..	107 9 8	0 1 4	1,039 18 11	Queenscliff Borough	..	..	228 4 3
Gisborne Shire ..	1,167 18 5	19 3 5	406 1 0	Ripon Shire ..	16 1 0	0 11 7	1,447 2 9
Glenelg Shire ..	959 7 4	11 2 8	1,051 7 6	Rochester Shire ..	1,251 10 2	14 16 4	281 16 8
Glenlyon Shire ..	93 19 2	2 3 5	433 11 3	Rodney Shire ..	1,949 17 0	32 5 9	1,138 6 5
Goulburn Shire ..	26 0 7	0 19 6	4,070 18 8	Romsey Shire ..	..	..	835 9 2
Grenville Shire ..	..	..	640 15 9	Rosedale Shire ..	63 11 5	1 2 7	696 9 0
Hamilton Borough	..	..	2,630 10 11	Rutherglen Shire ..	252 17 1	4 18 6	591 17 8
Hampden Shire ..	2,719 3 3	28 9 7	1,867 12 8	Sale Borough ..	..	..	354 16 6
Healesville Shire ..	376 10 7	0 19 1	482 10 5	Seymour Shire ..	719 1 0	13 19 4	722 10 5
Heidelberg Shire ..	..	..	..	..	..	..	..
Heytesbury Shire	512 5 6	4 3 10	..	..	..	..	..
Horsham Borough	1 12 11	0 1 0	..	..	..	..	..
..	..	1 12 11*	..	..	..	..	..
Carried forward	34,426 16 3	527 6 7	72,162 17 4	Carried forward	74,345 13 0	1,172 15 10	119,554 3 8

STATEMENT OF APPORTIONMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF MAIN ROADS, ETC.—*continued.*

Name of Municipality.	Permanent Works.		Maintenance.	Name of Municipality.	Permanent Works.		Maintenance.
	Principal.	Interest.	Amount.		Principal.	Interest.	Amount.
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
Brought forward	74,345 13 0	1,172 15 10	119,554 3 8	Brought forward	84,573 16 1	1,346 6 4	131,986 9 3
Shepparton Shire	2,037 12 2	37 4 3	569 3 4	Wangaratta Shire	279 16 7	4 10 9	130 9 1
South Barwon Shire	..	..	1,436 13 10	Wannon Shire ..	..	..	1,368 15 5
South Gippsland Shire ..	3,108 10 10	40 4 11	517 8 2	Waranga Shire ..	2,417 1 4	45 15 2	1,512 18 7
St. Arnaud Borough	..	..	305 3 7	Warragul Shire ..	1,831 7 7	22 6 0	1,656 3 11
Stawell Shire ..	458 15 0	9 19 5	2,549 2 5	Warrnambool Shire	..	..	8,491 10 1
Stawell Borough ..	1,009 1 10	22 0 5	25 11 3	Werribee Shire ..	26 3 2	0 19 7	1,340 11 7
Strathfieldsaye Shire	308 15 9	1 13 1	636 19 11	Whittlesea Shire ..	767 3 4	7 4 7	1,666 11 8
Swan Hill Shire ..	435 8 0	3 2 5	758 11 3	Wimmera Shire ..	1,509 17 8	18 10 0	874 8 10
Talbot Shire ..	..	..	109 16 8	Winchelsea Shire..	303 8 3	6 9 1	984 7 10
Tambo Shire ..	522 18 7	11 0 11	1,036 10 1	Wodonga Shire ..	..	..	1,034 1 5
Towong Shire ..	479 3 0	10 13 10	1,633 4 4	Wonthaggi Borough	..	..	66 5 1
Traralgon Shire ..	260 1 1	1 8 8	460 15 3	Woorayl Shire ..	1,284 18 8	11 4 5	867 15 0
Tullaroop Shire ..	2 0 4	0 1 1	249 9 0	Wycheproof Shire	847 12 6	25 12 3	1,051 4 0
		2 0 4*		Yackandandah Shire	1,215 15 0	26 18 7	488 14 1
Tungamah Shire ..	527 6 8	8 3 2	195 12 1	Yarrawonga Shire	769 0 0	7 1 5	346 13 8
Upper Murray Shire	681 11 0	15 6 8	295 2 1	Yea Shire ..	1,053 18 10	25 19 10	560 18 9
Upper Yarra Shire	75 19 0	1 17 7	698 19 0				
Violet Town Shire	95 0 3	2 2 1	300 17 3			24 0 8*	
Walpeup Shire ..	225 19 7	6 11 8	652 11 4		96,819 19 0	1,524 17 4	154,427 18 3
Wangaratta Borough	..	..	0 14 9			1,548 18 0	
Carried forward	84,573 16 1	1,346 6 4	131,986 9 3				

\* Liability liquidated.

## APPENDIX D.

## COUNTRY ROADS BOARD.

## STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE OF MAIN ROADS FOR THE YEAR ENDED 30th JUNE, 1925.

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
<b>ALBERTON SHIRE—</b>				
Balook-Traralgon Road .. .. .	223 7 11		96 10 1	
Boolarra-Welshpool Road .. .. .	215 11 8		163 8 5	
Carrajung-Gormandale Road .. .. .	734 12 0		1,157 4 0	
Foster-Yarram Road .. .. .	..		1,220 16 8	
Sale-Yarram Road .. .. .	..		2,717 10 0	
Yarram-Boolarra Road .. .. .	26 13 9		1,201 15 5	
Yarram-Port Albert Road .. .. .	..		6,922 2 8	
		1,200 5 4		13,479 7 3
<b>ALBERTON AND MORWELL SHIRES (Joint Works)—</b>				
Boolarra-Welshpool Road .. .. .	..		122 12 11	
Yarram-Boolarra Road .. .. .	..		6 0 0	
				128 12 11
<b>ALEXANDRA SHIRE—</b>				
Cathkin-Mansfield Road .. .. .	210 16 0		166 11 5	
Healesville-Alexandra Road .. .. .	1,183 3 0		744 12 1	
Upper Goulburn Road .. .. .	..		737 11 7	
Yarek Road .. .. .	..		77 0 9	
		1,393 19 0		1,725 15 10
<b>ALEXANDRA AND YEA SHIRES (Joint Works)—</b>				
Cathkin-Mansfield Road and Upper Goulburn Road ..	..		36 7 10	
				36 7 10
<b>ARAPILES SHIRE—</b>				
Horsham-Hamilton Road .. .. .	3 14 3		1,250 4 1	
Horsham-Natimuk-Edenhope Road .. .. .	225 15 11		911 13 0	
		229 10 2		2,161 17 1
<b>ARARAT BOROUGH—</b>				
Ballarat-Stawell Road .. .. .	..		319 10 0	
				319 10 0
<b>ARARAT SHIRE—</b>				
Ararat-Elmhurst Road .. .. .	..		739 13 3	
Ararat-Warnambool Road .. .. .	..		2,733 18 6	
Ballarat-Hamilton Road .. .. .	1,993 10 0		1,679 18 7	
Ballarat-Stawell Road .. .. .	..		600 18 4	
Maroona-Glenthompson Road .. .. .	..		1,575 14 2	
		1,993 10 0		7,330 2 10
<b>ARARAT AND LEXTON SHIRES (Joint Works)—</b>				
Ararat-Elmhurst Road .. .. .	..		269 10 0	
				269 10 0
<b>AVOCA SHIRE—</b>				
Ararat Road .. .. .	..		92 12 9	
Ballarat-St. Arnaud Road .. .. .	..		75 6 6	
Bealiba Road .. .. .	..		81 14 8	
Landsborough Road .. .. .	..		23 8 6	
Maryborough Road .. .. .	..		43 0 3	
Natte Yallock Road .. .. .	..		0 0 2	
				316 2 10
<b>AVON SHIRE—</b>				
Dargo Road .. .. .	903 4 0		211 8 9	
Maffra-Sale Road .. .. .	..		180 13 2	
Prince's Highway .. .. .	5,936 11 9		1,207 0 6	
		6,839 15 9		1,599 2 5
<b>BACCHUS MARSH SHIRE—</b>				
Ballarat Road .. .. .	..		1,305 3 7	
Geelong-Bacchus Marsh Road .. .. .	4,563 15 6		503 9 7	
Gisborne Road .. .. .	..		675 14 10	
		4,563 15 6		2,484 8 0
<b>BACCHUS MARSH AND BALLAN SHIRES (Joint Works)—</b>				
Ballarat Road .. .. .	..		0 1 7	
				0 1 7
<b>BAIRNSDALE SHIRE—</b>				
Bairnsdale-Bruthen Road .. .. .	..		200 9 8	
Bullumwaal-Tabberabbera Road .. .. .	..		340 16 8	
Prince's Highway .. .. .	..		1,227 0 8	
				1,768 7 0
<b>BAIRNSDALE AND TAMBO SHIRES (Joint Works)—</b>				
Bairnsdale-Bruthen Road .. .. .	..		3 3 9	
				3 3 9
<b>BALLAN SHIRE—</b>				
Ballarat Road .. .. .	..		1,668 0 11	
Daylesford Road .. .. .	..		385 14 2	
Gordons-Meredith Road .. .. .	..		268 2 8	
Mount Wallace Road .. .. .	1,229 15 0		313 1 0	
		1,229 15 0		2,634 18 9
Carried forward .. .. .	..	17,450 10 9	..	34,257 8 1



## STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—continued.

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	17,450 10 9	..	34,257 8 1
<b>BALLARAT SHIRE—</b>				
Ararat Road .. .. .	..	..	820 19 4	
Ballarat-Creswick Road .. .. .	..	..	265 13 10	
Ballarat-Lexton Road .. .. .	..	..	1,197 7 5	
Maryborough-Ballarot Road .. .. .	..	..	1,020 0 10	
				3,304 1 5
<b>BANNOCKBURN SHIRE—</b>				
Geelong-Ballarot Road .. .. .	..	..	862 15 6	
Gordons-Meredith Road .. .. .	..	..	73 5 2	
Inverleigh Road .. .. .	..	..	287 0 5	
Shelford-Bannockburn Road .. .. .	..	..	41 10 1	
				1,264 11 2
<b>BARRARBOOL SHIRE—</b>				
Anglesea Road .. .. .	..	..	646 19 4	
Hendy Main Road .. .. .	..	..	336 17 4	
Prince's Highway .. .. .	..	..	2,521 10 4	
				3,505 7 0
<b>BEECHWORTH SHIRE—</b>				
Beechworth Road .. .. .	16 15 3		765 10 5	
Bright Road .. .. .	223 3 11		28 16 7	
Everton-Myrtleford Road .. .. .	..		346 19 6	
Stanley Road .. .. .	24 6 1		252 0 7	
		264 5 3		1,393 7 1
<b>BEECHWORTH AND WANGARATTA SHIRES (Joint Works)—</b>				
Beechworth Road .. .. .	..	..	1 2 3	
				1 2 3
<b>BELEAST SHIRE—</b>				
Hamilton Road .. .. .	..	..	1,153 16 6	
Penshurst Road .. .. .	..	..	495 10 7	
Prince's Highway (Portland section) .. .. .	..	..	1,542 10 10	
Prince's Highway (Warrnambool-Port Fairy section) .. .. .	33 9 1		2,373 2 9	
		33 9 1		5,565 0 8
<b>BELLARINE SHIRE—</b>				
Geelong-Portarlington Road .. .. .	..	..	1,351 5 5	
Geelong-Queenscliff Road .. .. .	..	..	1,991 11 0	
				3,342 16 5
<b>BENALLA SHIRE—</b>				
Goorambat Road .. .. .	..	..	496 3 7	
Goorambat-Thoona Road .. .. .	1,672 19 4		174 1 5	
Greta Road .. .. .	..	..	6 13 0	
Lima Road .. .. .	..	..	..	
Sydney Road .. .. .	..	..	825 0 9	
Tatong Road .. .. .	..	..	540 5 4	
		1,672 19 4		2,042 4 1
<b>BERWICK SHIRE—</b>				
Gembrook Road .. .. .	66 14 9		111 3 5	
Gembrook-Beenak Road .. .. .	..	..	291 5 10	
Hallam-Emerald Road .. .. .	..	..	111 6 8	
Prince's Highway .. .. .	2,357 7 4		6,524 15 4	
Woori Yallock-Pakenham-Koo-wee-rup Road .. .. .	4,451 18 11		1,094 11 6	
		6,876 1 0		8,133 2 9
<b>BET BET SHIRE—</b>				
Avoca-Bealiba Road .. .. .	..	..	353 3 9	
Betley Road .. .. .	..	..	305 8 3	
Dunolly Road .. .. .	..	..	86 0 0	
				744 12 0
<b>BIRCHIP SHIRE—</b>				
Beulah-Birchip-Wycheproof Road .. .. .	..	..	1,219 19 8	
Donald-Birchip-Sealake Road .. .. .	..	..	541 10 11	
				1,761 10 7
<b>BORUNG SHIRE—</b>				
Birchip Road .. .. .	6 1 10		103 13 1	
Dimboola Road .. .. .	23 4 3		77 0 7	
Hopetoun Road .. .. .	6 3 9		290 12 9	
Minyip Road .. .. .	753 3 3		42 0 6	
Rainbow Road .. .. .	79 12 7		31 19 3	
		868 5 8		545 6 2
<b>BRAYBROOK SHIRE—</b>				
Ballarat Road .. .. .	..	..	1,959 7 2	
Prince's Highway .. .. .	..	..	Bd. 1,510 2 8	
				3,469 9 10
<b>BRIGHT SHIRE—</b>				
Bright Road .. .. .	..	..	391 15 1	
Harrierville Road .. .. .	..	..	205 19 6	
				597 14 7
<b>BRIGHT AND OMEO SHIRES (Joint Works)—</b>				
Bright-Omeo Road .. .. .	..	..	Bd. 1,500 0 0	
				1,500 0 0
<b>BROADFORD SHIRE—</b>				
Sydney Road .. .. .	..	..	35 19 0	
				35 19 0
<b>BROADMEADOWS SHIRE—</b>				
Sydney Road .. .. .	..	..	2,409 10 10	
				2,409 10 10
Carried forward .. .. .	..	27,165 11 1	..	73,873 3 11

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	27,165 11 1	..	73,873 3 11
BROADMEADOWS AND KEILOR SHIRES (Joint Works)— Lancefield Road .. .. .	..	..	1,292 0 9	1,292 0 9
BULLA SHIRE— Melbourne-Lancefield Road .. .. . Sunbury Road .. .. .	.. ..	.. ..	2,018 15 9 400 6 6	2,419 2 3
BULLA AND KEILOR SHIRES (Joint Works)— Melbourne-Lancefield Road .. .. .	..	..	426 0 11	426 0 11
BULN BULN SHIRE— Bloomfield Road .. .. . Loch Valley Road .. .. . Magpie-Duggan Road .. .. . Main Neerim "A" Road .. .. . Main Neerim "B" Road .. .. . Main Neerim "C" Road .. .. . Main South Road .. .. . Neerim East Road .. .. . Prince's Highway .. .. . Westernport Road .. .. .	.. .. .. 2,158 10 6 3,132 2 4 .. .. 76 11 5 .. 3,372 5 9	.. .. .. .. .. .. .. .. .. .. 8,739 10 0	12 8 11 269 3 2 15 4 4 732 12 5 779 18 2 194 15 11 1,356 4 4 186 5 10 3,422 4 1 231 5 3	7,200 2 5
BUNGAREE SHIRE— Ballarat Road .. .. . Ballarat-Creswick Road .. .. . Daylesford-Ballarat Road .. .. .	.. 308 19 4 ..	.. 308 19 4 ..	25 2 4 279 16 6 514 0 11	818 19 9
BUNGAREE AND BALLARAT SHIRES (Joint Works)— Ballarat-Creswick Road .. .. .	2 17 10	2 17 10	3 7 5	3 7 5
BUNINYONG SHIRE— Ballarat Road .. .. . Ballarat-Rokewood Road .. .. . Elaine-Mt. Mercer Road .. .. . Geelong-Ballarat Road .. .. .	9 0 1 .. .. ..	9 0 1 .. .. ..	217 19 4 117 8 11 171 14 1 858 13 5	1,365 15 9
CARRUM BOROUGH— Point Nepean Road .. .. .	..	..	1,831 2 4	1,831 2 4
CASTLEMAINE—BOROUGH— Melbourne-Bendigo Road .. .. .	..	..	922 11 1	922 11 1
CHARLTON SHIRE— Bendigo Road .. .. . Donald Road .. .. . St. Arnaud Road .. .. . Wycheproof Road .. .. .	5,126 15 2 34 18 7 63 6 7 465 10 2	5,690 10 6	546 3 7 129 18 8 75 7 11 88 10 2	840 0 4
CHILTERN SHIRE— Chiltern-Howlong Road .. .. . Rutherglen-Wodonga Road .. .. . Sydney Road .. .. .	.. .. 59 9 11	.. .. 59 9 11	94 6 1 151 1 4 400 8 9	645 16 2
CLUNES BOROUGH— Maryborough-Ballarat Road .. .. .	..	..	75 17 9	75 17 9
COLAC SHIRE— Colac-Ballarat Road .. .. . Prince's Highway .. .. .	.. ..	.. ..	601 13 0 2,842 16 4	3,444 9 4
COLAC AND HEYTESBURY SHIRES (Joint Works)— Prince's Highway .. .. .	..	..	36 9 0	36 9 0
CORIO SHIRE— Ballarat Road .. .. . Fyansford Road .. .. . Geelong-Bacchus Marsh Road .. .. . Prince's Highway .. .. .	.. .. .. 2 8 6	.. .. .. 2 8 6	1,037 1 7 299 11 6 481 19 2 Sh. 480 16 10 Bd. 4,350 9 7	6,649 18 8
CRANBOURNE SHIRE— Koo-wee-rup-Pakenham Road .. .. . Main Coast Road .. .. . Lang Lang-Nyora Road .. .. . Westernport Road .. .. .	1,193 4 9 2,260 0 0 98 8 9 20 0 0	3,571 13 6	927 4 1 5,093 19 4 185 18 5 1,226 12 2	7,433 14 0
CRESWICK BOROUGH— Castlemaine-Ballarat Road .. .. .	..	..	222 2 2	222 2 2
CRESWICK SHIRE— Castlemaine-Ballarat Road .. .. . Daylesford-Ballarat Road .. .. .	.. ..	.. ..	780 3 2 553 16 11	1,334 0 1
Carried forward .. .. .	..	45,550 0 9	..	110,834 14 1

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	45,550 0 9	..	110,834 14 1
<b>DANDENONG SHIRE—</b>				
Cheltenham Road .. .. .	..		2,628 3 8	
Prince's Highway .. .. .	..		5,319 10 0	
				7,947 13 8
<b>DAYLESFORD BOROUGH—</b>				
Ballan Road .. .. .	..		6 9 9	
Ballarat Road .. .. .	..		3 1 11	
Castlemaine Road .. .. .	..		7 12 0	
Hepburn—Daylesford Road .. .. .	..		931 3 11	
Malmsbury—Daylesford Road .. .. .	..		400 9 6	
				1,348 17 1
<b>DEAKIN SHIRE—</b>				
Echuca—Cornella Road .. .. .	..		236 3 0	
Echuca—Picola Road .. .. .	..		253 17 3	
Kyabram—Nathalia Road .. .. .	1 14 5		2,091 13 9	
Kyabram—Tongala Road .. .. .	0 17 7		1,743 13 6	
Rochester—Kyabram Road .. .. .	..		798 8 0	
		2 12 0		5,123 15 6
<b>DEAKIN AND NUMURKAH SHIRES (Joint Works)—</b>				
Echuca—Picola Road .. .. .	..		546 9 6	
Kyabram—Nathalia Road .. .. .	..		20 13 0	
				567 2 6
<b>DEAKIN AND RODNEY SHIRES (Joint Works)—</b>				
Rochester—Kyabram Road .. .. .	..		1,274 13 6	
Kyabram—Tongala Road .. .. .	1 1 7		..	
		1 1 7		1,274 13 6
<b>DIMBOOLA AND KARKAROO SHIRES (Joint Works)—</b>				
Rainbow Road .. .. .	..		26 17 9	
				26 17 9
<b>DIMBOOLA SHIRE—</b>				
Hopetoun—Rainbow Road .. .. .	..		82 0 10	
Horsham Road .. .. .	..		12 1 3	
Nhill Road .. .. .	..		405 5 9	
Rainbow Road .. .. .	11 19 3		2,070 1 2	
Rainbow Rises Road .. .. .	..		337 9 2	
Warracknabeal Road .. .. .	..		253 17 11	
		11 19 3		3,160 16 1
<b>DONALD SHIRE—</b>				
Donald—Charlton Road .. .. .	198 14 0		1,165 4 1	
Donald—Minyip Road .. .. .	..		2 6 7	
St. Arnaud—Birchip Road .. .. .	30 1 10		619 11 2	
		228 15 10		1,787 1 10
<b>DONCASTER SHIRE—</b>				
Doncaster Road .. .. .	..		842 12 5	
Heidelberg—Warrandyte Road .. .. .	..		1,177 11 1	
Warrandyte—Ringwood Road .. .. .	5 2 4		283 9 2	
		5 2 4		2,303 12 8
<b>DUNDAS SHIRE—</b>				
Hamilton—Dunkeld Road .. .. .	5 15 2		1,127 8 11	
Hamilton—Horsham Road .. .. .	21 18 4		1,040 3 1	
Hamilton—Mount Gambier Road .. .. .	4 12 8		1,432 10 2	
Hamilton—Port Fairy Road .. .. .	..		1,880 12 1	
Hamilton—Portland Road .. .. .	13 13 4		1,012 17 1	
Hamilton—Warrnambool Road .. .. .	..		681 13 11	
		45 19 6		7,175 5 3
<b>DUNDAS AND HAMILTON SHIRES (Joint Works)—</b>				
Hamilton—Warrnambool Road .. .. .	..		228 14 8	
				228 14 8
<b>DUNDAS AND PORTLAND SHIRES (Joint Works)—</b>				
Hamilton—Portland Road .. .. .	1,591 0 0		92 2 6	
		1,591 0 0		92 2 6
<b>DUNMUNKLE SHIRE—</b>				
Minyip—Donald Road .. .. .	..		129 14 0	
Rupanyup—Murtoa Road .. .. .	900 19 9		238 0 6	
Stawell—Warracknabeal Road .. .. .	1,438 18 4		1,295 19 11	
		2,339 18 1		1,663 14 5
<b>EAST LODDON SHIRE—</b>				
Dingee Road .. .. .	54 0 0		66 9 11	
Mitiamo Road .. .. .	..		0 1 9	
Prairie Road .. .. .	560 5 5		317 11 9	
Prairie—Borong Road .. .. .	..		..	
		614 5 5		384 3 5
<b>ECHUCA BOROUGH—</b>				
Echuca West Road .. .. .	6,374 0 11		..	
		6,374 0 11		..
<b>ELTHAM SHIRE—</b>				
Eltham—Yarra Glen Road .. .. .	38 19 0		1,636 1 1	
Hurstbridge—Kinglake Road .. .. .	557 9 2		864 14 7	
Whittlesea—Kinglake Road .. .. .	..		..	
Yarraglen—Kinglake East Road .. .. .	..		..	
		596 8 2		2,500 15 8
Carried forward .. .. .	..	57,361 3 10		146,420 0 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	57,361 3 10	..	146,420 0 7
<b>EUROA SHIRE—</b>				
Arcadia Road .. .. .	1,830 0 0		13 13 3	
Euroa-Arcadia Road .. .. .	..		136 4 3	
Euroa-Mansfield Road .. .. .	84 7 6		192 16 7	
Euroa-S <sup>t</sup> rathbogie Road .. .. .	..		202 3 2	
Sydney Road .. .. .	..		135 6 10	
		1,914 7 6		680 4 1
<b>FERNTREE GULLY SHIRE—</b>				
Emerald Road .. .. .	..		3 3 2	
Main Ferntree Gully Road .. .. .	..		4,826 8 1	
Monbulk Road .. .. .	..		1,706 3 7	
Olinda Road .. .. .	..		2,331 11 6	
				8,867 6 4
<b>FLINDERS SHIRE—</b>				
Dromana Road .. .. .	..		906 8 11	
Dromana-Sorrento Road .. .. .	6 5 7		3,268 9 10	
Hastings-Flinders Road .. .. .	1,488 2 9		295 11 8	
Mornington-Flinders Road .. .. .	651 10 6		1,417 6 3	
		2,145 18 10		5,887 16 3
<b>FOOTSCRAY CITY—</b>				
Prince's Highway .. .. .	..		Bd. 2,214 18 3	
				2,214 18 3
<b>FRANKSTON AND HASTINGS SHIRE—</b>				
Frankston-Flinders Road .. .. .	..		3,478 2 5	
Point Nepean Road .. .. .	462 0 7		2,803 17 7	
		462 0 7		6,282 0 0
<b>GISBORNE SHIRE—</b>				
Bacchus Marsh Road .. .. .	8 19 6		79 16 10	
Gisborne Station Road .. .. .	..		90 1 6	
Melbourne-Bendigo Road .. .. .	149 12 8		819 4 7	
		158 12 2		989 2 11
<b>GISBORNE AND BULLA SHIRES (Joint Works)—</b>				
Melbourne-Bendigo Road .. .. .	..		238 0 0	
				238 0 0
<b>GLENELG SHIRE—</b>				
Coleraine-Casterton Road .. .. .	18 18 9		428 18 5	
Dergholm Road .. .. .	100 14 7		1,260 6 2	
Mount Gambier Road .. .. .	910 6 8		1,112 13 9	
Wando Vale Road .. .. .	9 10 0		333 11 7	
Portland-Casterton Road .. .. .	..		1,466 14 11	
		1,039 10 0		4,602 4 10
<b>GLENLYON SHIRE—</b>				
Ballan Road .. .. .	..		27 4 8	
Ballarat Road .. .. .	..		47 9 0	
Castlemaine-Daylesford Road .. .. .	..		180 9 8	
Daylesford-Hepburn Road .. .. .	..		70 17 3	
Malmsbury-Daylesford Road .. .. .	..		397 7 11	
				723 8 6
<b>GOULBURN SHIRE—</b>				
Goulburn Valley Road .. .. .	3,144 3 3		255 4 0	
Station Road .. .. .	..		14 14 0	
Sydney Road .. .. .	..		42 2 6	
		3,144 3 3		312 0 6
<b>GRENVILLE SHIRE—</b>				
Ballarat-Hamilton Road .. .. .	..		1,601 11 4	
Cressy Road .. .. .	..		226 5 1	
Lismore Road .. .. .	213 12 6		630 13 6	
Pitfield Road .. .. .	..		685 5 9	
		213 12 6		3,143 15 8
<b>HAMILTON BOROUGH—</b>				
Ararat Road .. .. .	..		175 9 8	
Coleraine Road .. .. .	..		282 10 8	
Port Fairy Road .. .. .	..		84 6 6	
Portland .. .. .	..		110 10 2	
				652 17 0
<b>HAMILTON BOROUGH AND DUNDAS SHIRE (Joint Works)—</b>				
Hamilton-Warrnambool Road .. .. .	..		63 15 1	
				63 15 1
<b>HAMPDEN SHIRE—</b>				
Camperdown-Ballarat Road .. .. .	..		4,855 2 3	
Camperdown-Cobden Road .. .. .	..		..	
Lismore-Cressy Road .. .. .	..		2,100 4 11	
Prince's Highway .. .. .	799 4 0		5,925 0 4	
Terang-Mortlake Road .. .. .	..		2,058 11 5	
		799 4 0		14,938 18 11
<b>HEALESVILLE SHIRE—</b>				
Healesville-Alexandra Road .. .. .	73 2 7		2,491 9 2	
Healesville-Woori Yallock Road .. .. .	..		13 18 7	
Marysville Road .. .. .	..		129 19 3	
		73 2 7		2,635 7 0
<b>HEALESVILLE AND LILYDALE SHIRES (Joint Works)—</b>				
Healesville-Alexandra Road .. .. .	..		1 15 8	
				1 15 8
Carried forward .. .. .	..	67,311 15 3	..	198,653 12 0

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	67,311 15 3	..	198,653 12 0
<b>HEIDELBERG SHIRE—</b>				
Greensborough—Hurstbridge Road .. .. .	..	..	812 11 8	
Heidelberg—Warrandyte Road .. .. .	..	..	138 19 5	
Main Heidelberg—Eltham Road .. .. .	..	..	5,425 14 1	
Main Whittlesea Road .. .. .	..	..	131 2 9	6,508 7 11
<b>HEIDELBERG AND DONCASTER SHIRES (Joint Works)—</b>				
Heidelberg—Warrandyte Road .. .. .	..	..	2 0 5	2 0 5
<b>HEYTESBURY SHIRE—</b>				
Camperdown—Cobden Road .. .. .	..	..	1,418 15 0	
Cobden—Port Campbell—Princetown Road .. .. .	45 4 3	..	3,249 14 1	
Prince's Highway .. .. .	..	..	3,430 17 1	8,099 6 2
<b>HORSHAM BOROUGH</b>				
Dimboola—Horsham Road .. .. .	55 18 0	..	111 13 11	
Dooen Road .. .. .	..	..	268 14 9	
Hamilton Road .. .. .	..	..	716 15 8	
Natimuk Road .. .. .	1,200 0 0	..	..	1,097 4 4
<b>HUNTLY SHIRE—</b>				
Bendigo—Eehuca Road .. .. .	113 1 0	..	1,489 11 5	1,489 11 5
<b>HUNTLY AND WARANGA SHIRES (Joint Works)—</b>				
Elmore—Heathcote Road .. .. .	0 13 0	..	..	0 13 0
<b>INGLEWOOD BOROUGH—</b>				
Bendigo—Charlton Road .. .. .	..	..	83 6 8	83 6 8
<b>KARA KARA SHIRE—</b>				
Avoca—St. Arnaud Road .. .. .	418 19 0	..	38 14 9	
Charlton Road .. .. .	..	..	249 7 4	
Navarre Road .. .. .	1,626 10 3	..	1,886 16 10	
St. Arnaud—Donald Road .. .. .	900 0 0	..	65 10 8	2,240 9 7
<b>KARA KARA AND AVOCA SHIRES (Joint Works)—</b>				
Avoca—St. Arnaud Road .. .. .	278 19 3	..	..	278 19 3
<b>KARKARŌOC SHIRES—</b>				
Hopetoun—Rainbow Road .. .. .	2,014 19 0	..	906 17 8	
Hopetoun—Warracknabeal Road .. .. .	10 12 0	..	1,495 4 6	
Hopetoun—Woomelang—Sea Lake Road .. .. .	112 0 0	..	380 7 1	
Rainbow—Beulah—Birchip Road .. .. .	338 7 4	..	578 2 2	3,360 11 5
<b>KEILOR SHIRE—</b>				
Melbourne—Bendigo Road .. .. .	..	..	1,132 6 2	1,132 6 2
<b>KERANG SHIRE—</b>				
Koondrook Road .. .. .	..	..	319 16 11	319 16 11
<b>KILMORE SHIRE—</b>				
Heathcote Road .. .. .	..	..	45 13 10	
Lancefield—Kilmore Road .. .. .	..	..	137 0 4	
Sydney Road .. .. .	..	..	526 12 5	709 6 7
<b>KILMORE AND ROMSEY SHIRES (Joint Works)—</b>				
Lancefield—Kilmore Road .. .. .	..	..	40 0 0	40 0 0
<b>KILMORE AND PYALONG SHIRES (Joint Works)—</b>				
Heathcote Road .. .. .	..	..	42 5 0	42 5 0
<b>KOROIT BOROUGH—</b>				
Koroit—Warrnambool Road .. .. .	..	..	2,256 9 4	2,256 9 4
<b>KORONG SHIRE—</b>				
Borong—Hurstwood Road .. .. .	269 7 8	..	61 1 0	
Charlton—Bendigo Road .. .. .	77 5 9	..	1,185 3 7	
Serpentine Road .. .. .	675 19 6	..	248 16 6	1,495 1 1
<b>KORUMBURRA SHIRE—</b>				
Bena—Poowong Road .. .. .	3,283 15 6	..	72 1 1	
Korumburra—Drouin Road .. .. .	..	..	107 15 1	
Korumburra—Leongatha Road .. .. .	..	..	559 16 0	
Korumburra—Warragul Road .. .. .	2,708 3 4	..	985 8 1	
Korumburra—Wonthaggi Road .. .. .	179 5 1	..	1,110 9 10	
Lang Lang—Nyora Road .. .. .	..	..	100 10 6	
Loch—Wonthaggi Road .. .. .	..	..	55 5 9	
Nyora—Poowong Road .. .. .	..	..	189 19 3	3,181 5 7
<b>KORUMBURRA AND WARRAGUL SHIRES (Joint Works)—</b>				
Warragul—Korumburra Road .. .. .	7 7 0	..	..	7 7 0
Carried forward .. .. .	..	81,628 2 2	..	230,711 0 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	81,628 2 2	..	230,711 0 7
<b>KOWREE SHIRE—</b>				
Boorookpi Road .. .. .	16 5 6		789 9 9	
Edenhope-Goroke Road .. .. .	493 12 11		750 0 2	
Hamilton-Edenhope-Apsley Road .. .. .	..		1,336 17 4	
Horsham-Hamilton Road .. .. .	77 5 10		6 0 0	
		587 4 3		2,882 7 3
<b>KYNETON SHIRE—</b>				
Melbourne-Bendigo Road .. .. .	..		1,158 8 0	
Daylesford Road .. .. .	..		28 2 6	
Redesdale Road .. .. .	..		262 10 10	
Trentham Road .. .. .	..		633 17 3	
Tylden-Woodend Road .. .. .	..		109 2 0	
				2,192 0 7
<b>LAWLOIT SHIRE—</b>				
Broughton Road .. .. .	1,488 15 11		535 4 2	
Nhill-Kaniva-Border Road .. .. .	3,319 5 2		1,633 4 0	
South Lillimur Road .. .. .	949 18 4		343 9 4	
Yearinga Road .. .. .	993 3 8		436 18 11	
		6,751 3 1		2,948 16 5
<b>LEIGH SHIRE—</b>				
Cressy-Rokewood Road .. .. .	1,691 7 9		153 11 5	
Inverleigh-Shelford Road .. .. .	..		58 3 4	
Rokewood-Shelford Road .. .. .	..		328 0 8	
Shelford-Bannockburn Road .. .. .	..		238 10 9	
Werneth Road .. .. .	15 7 1		90 11 4	
		1,706 14 10		868 17 6
<b>LEIGH AND COLAC SHIRES (Joint Works)—</b>				
Cressy-Lismore Road .. .. .	..		5 10 7	
Cressy-Rokewood Road .. .. .	..		1 3 9	
				6 14 4
<b>LEXTON SHIRE—</b>				
Avoca-Ararat Road .. .. .	..		372 18 1	
Avoca-Ballarat Road .. .. .	..		635 2 3	
				1,008 0 4
<b>LILYDALE SHIRE—</b>				
Main Healesville Road .. .. .	2 1 3		Sh. 6,851 12 5	
			Bd. 4,941 13 6	
Evelyn-Lilydale Road .. .. .	4 12 0			
Main Warburton Road .. .. .	..		539 10 7	
Monbulk Road .. .. .	2,481 18 7		564 1 5	
Mount Dandenong Road .. .. .	1,813 17 11		740 7 3	
Ringwood-Warrandyte Road .. .. .	1 10 0		652 3 9	
Yarraglen Road .. .. .	11 14 2		283 18 0	
		4,315 13 11		14,573 6 11
<b>LILYDALE AND DONCASTER SHIRES (Joint Works)—</b>				
Ringwood-Warrandyte Road .. .. .	..		2 5 11	
				2 5 11
<b>LOWAN SHIRE—</b>				
Dimboola-Kaniva Road .. .. .	787 14 0		1,363 2 10	
Goroke Road .. .. .	8,083 12 7		122 1 6	
Lorquon West Road .. .. .	73 4 8		1,173 8 8	
Yanae Road .. .. .	5 2 8		662 10 9	
		8,949 13 11		3,321 3 9
<b>MAFFRA SHIRE—</b>				
Bushy Park-Valencia Creek Road .. .. .	..		86 19 0	
Briagalong-Dargo Road .. .. .	..		465 19 0	
Licola Road .. .. .	212 4 3		458 9 7	
Maffra-Sale Road .. .. .	..		911 15 1	
Tinamba-Boisdale Road .. .. .	293 8 8		1,616 8 10	
Tinamba-Newry Road .. .. .	..		232 4 4	
		505 12 11		3,771 15 10
<b>MALDON SHIRE—</b>				
Baringhup Road .. .. .	..		512 11 8	
Castlemaine-Maldon Road .. .. .	..		274 6 3	
Castlemaine-Newstead Road .. .. .	..		10 4 0	
Maldon-Eddington Road .. .. .	..		99 10 5	
Newstead Road .. .. .	..		95 6 2	
				991 18 6
<b>MALDON, METCALFE, AND MARONG SHIRES (Joint Works)—</b>				
Melbourne-Bendigo Road .. .. .	..		4,650 0 1	
				4,650 0 1
<b>MANSFIELD SHIRE—</b>				
Euroa-Merton Road .. .. .	..		6 10 6	
Jamieson Road .. .. .	..		1,773 4 5	
Mansfield Road .. .. .	980 12 6		1,125 4 9	
Mansfield-Tolmie Road .. .. .	268 5 8		70 14 11	
Mansfield-Wood's Point Road .. .. .	..		Sh. 464 14 7	
			Bd. 1,991 17 1	
		1,248 18 2		5,432 6 3
<b>MARONG SHIRE—</b>				
Bendigo-Bridgewater Road .. .. .	1,845 12 3		1,650 7 4	
Bendigo-Eddington Road .. .. .	1,098 7 4		914 3 7	
Bendigo-Serpentine Road .. .. .	..		783 0 4	
Melbourne-Bendigo Road .. .. .	..		751 2 1	
		2,943 19 7		4,098 13 4
Carried forward .. .. .	..	108,637 2 10	..	277,459 7 7



STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.				Maintenance.			
	Amount.		Total.		Amount.		Total.	
	£	s. d.	£	s. d.	£	s. d.	£	s. d.
Brought forward .. .. .	..	..	108,637	2 10	..	..	277,459	7 7
MARYBOROUGH BOROUGH—								
Avoca Road .. .. .	..	..	..	..	14	16 0	..	..
Castlemaine Road .. .. .	..	..	..	..	15	0 0	..	..
Eddington Road .. .. .	..	..	..	..	25	0 6	54	16 6
MELTON SHIRE—								
Ballarat Road .. .. .	..	..	..	..	490	6 7	..	..
Melbourne-Bendigo Road .. .. .	..	..	..	..	84	5 6	..	..
Toolern Road .. .. .	..	..	..	..	387	3 0	961	15 1
MELTON AND BULLA SHIRES (Joint Works)—								
Melbourne-Bendigo Road .. .. .	..	..	..	..	6	11 10	6	11 10
MILDURA TOWN—								
Deakin Avenue .. .. .	..	..	451	18 6	222	19 10	..	..
Langtree Avenue .. .. .	..	..	21	16 0	30	17 5	..	..
Punt Road .. .. .	..	..	..	..	29	10 9	283	8 0
MILDURA SHIRE—								
Deakin Avenue .. .. .	..	..	79	4 8	71	3 2	..	..
Irymple Road .. .. .	..	..	1,333	1 11	231	7 8	..	..
Melbourne Road .. .. .	..	..	..	..	7	11 0	..	..
Wentworth Road .. .. .	..	..	2,815	19 10	503	7 3	813	9 1
MINHAMITE SHIRE—								
Hamilton-Macarthur-Port Fairy Road .. .. .	..	..	..	..	3,757	5 9	..	..
Warrnambool-Hawkesdale-Penshurst Road .. .. .	..	..	..	..	3,398	18 3	7,156	4 0
MIRBOO SHIRE—								
Mardan Road .. .. .	..	..	..	..	32	17 10	..	..
Mirboo-Allambee East Road .. .. .	..	..	1,254	5 0	19	15 7	..	..
Mirboo South Road .. .. .	..	..	..	..	198	12 8	251	6 1
MOORABBIN SHIRE—								
Centre Dandenong Road .. .. .	..	..	..	..	2,199	9 8	..	..
Point Nepean Road .. .. .	..	..	..	..	2,005	5 11	4,204	15 7
MORDIALLOC TOWN—								
Point Nepean Road .. .. .	..	..	..	..	1,125	5 9	1,125	5 9
MORNINGTON SHIRE—								
Point Nepean Road .. .. .	..	..	1,252	2 3	1,528	5 3	1,528	5 3
MORTLAKE SHIRE—								
Mortlake-Ararat Road .. .. .	..	..	..	..	488	12 6	..	..
Mortlake-Warrnambool Road .. .. .	..	..	..	..	1,166	8 6	..	..
Terang-Mortlake Road .. .. .	..	..	..	..	735	13 10	2,390	14 10
MORWELL SHIRE—								
Boolarra-Foster Road .. .. .	..	..	11	11 11	324	10 9	..	..
Boolarra-Welshpool Road .. .. .	..	..	29	18 0	579	6 2	..	..
Jeeralang West Road .. .. .	..	..	2,563	17 8	168	3 8	..	..
Prince's Highway .. .. .	..	..	1,753	17 3	499	17 6	4,359	4 10
MORWELL AND TRARALGON SHIRES (Joint Works)—								
Jeeralang West Road .. .. .	..	..	..	..	0	0 9	0	0 9
MORWELL AND ALBERTON SHIRES (Joint Works)—								
Boolarra-Welshpool Road .. .. .	..	..	523	0 0	..	..	523	0 0
MOUNT ROUSE SHIRE—								
Ballarat-Hamilton Road .. .. .	..	..	..	..	1,107	6 6	..	..
Hamilton-Dunkeld Road .. .. .	..	..	..	..	219	10 4	..	..
Hamilton-Penshurst Road .. .. .	..	..	..	..	861	12 4	..	..
Maroona-Glenthompson Road .. .. .	..	..	..	..	187	6 7	..	..
Penshurst-Caramut Road .. .. .	..	..	..	..	1,290	6 10	3,666	2 7
MULGRAVE SHIRE—								
Ferntree Gully Road .. .. .	..	..	566	15 6	1,578	9 3	..	..
Prince's Highway .. .. .	..	..	..	..	Bd. 3,725	16 0	5,304	5 3
McIVOR SHIRE—								
Heathcote-Elmore Road .. .. .	..	..	..	..	310	17 6	..	..
Heathcote-Redesdale Road .. .. .	..	..	..	..	357	12 3	..	..
Kilmore-Heathcote-Bendigo Road .. .. .	..	..	..	..	615	9 11	1,283	19 8
NARRACAN SHIRE—								
Prince's Highway .. .. .	..	..	..	..	1,584	18 4	..	..
Trafalgar-Thorpdale Road .. .. .	..	..	1,522	9 11	225	10 3	..	..
Trafalgar-Willowgrove Road .. .. .	..	..	..	..	117	9 11	..	..
Walhalla Road .. .. .	..	..	..	..	Sh. 327	9 2	..	..
Yarragon-Leongatha Road .. .. .	..	..	11	1 5	Bd. 254	14 0	..	..
Yarragon-Shady Creek Road .. .. .	..	..	..	..	546	18 5	..	..
					155	17 7	1,533	11 4
Carried forward .. .. .	..	..	122,828	2 8	..	..	311,275	3 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	122,828 2 8	..	311,275 3 7
NEWHAM AND WOODEND SHIRE—				
Lancefield Road .. .. .	..		103 19 0	
Melbourne-Bendigo Road .. .. .	..		411 11 7	
Tylden Road .. .. .	..		88 7 8	603 18 3
NEWHAM AND WOODEND AND GISBORNE SHIRES (Joint Works)—				
Melbourne-Bendigo Road .. .. .	..		29 13 1	29 13 1
NEWHAM AND WOODEND AND KYNETON SHIRES (Joint Works)—				
Tylden Road .. .. .	850 0 0	850 0 0	..	
NEWSTEAD AND MOUNT ALEXANDER SHIRES—				
Castlemaine-Daylesford Road .. .. .	..		112 19 11	
Castlemaine-Maryborough Road .. .. .	..		225 0 3	
Creswick Road .. .. .	..		146 12 4	
Maldon Road .. .. .	..		126 17 10	
Melbourne-Bendigo Road .. .. .	..		4 10 6	616 0 10
NEWSTEAD AND MOUNT ALEXANDER AND TULLAROOP SHIRES (Joint Works)—				
Castlemaine-Maryborough Road .. .. .	..		0 1 7	0 1 7
NEWSTEAD AND MOUNT ALEXANDER AND GLENLYON SHIRES (Joint Works)—				
Castlemaine-Daylesford Road .. .. .	..		0 5 2	0 5 2
NUMURKAH SHIRE—				
Echuca-Picola Road .. .. .	..		1 10 11	
Nathalia-Kyabram Road .. .. .	..		665 3 0	
Numurkah-Nathalia Road .. .. .	231 12 5		579 4 4	
Numurkah-Tungamah Road .. .. .	..		67 10 2	
Shepparton-Numurkah-Cobram Road .. .. .	683 13 5	915 5 10	1,554 12 11	2,868 1 4
NUMURKAH AND DEAKIN SHIRES (Joint Works)—				
Echuca-Picola Road .. .. .	..		94 0 1	94 0 1
NUNAWADING SHIRE—				
Main Healesville Road .. .. .	..		3,932 4 5	3,932 4 5
OAKLEIGH TOWN—				
Ferntree Gully Road .. .. .	..		13 5 11	
Prince's Highway .. .. .	..		1,081 11 1	
Prince's Highway (Experimental Concrete Section) .. .. .	..		367 8 3	1,462 5 3
OMEO SHIRE—				
Benambra Road .. .. .	..		281 15 10	
Bruthen-Omeo Road .. .. .	666 4 10		1,625 13 8	
Glen Wills Road .. .. .	..		Sh. 206 3 7	
		666 4 10	Bd. 350 5 5	2,463 18 6
ORBOST SHIRE—				
Cann Valley Road .. .. .	..		282 17 9	
Marlo Road .. .. .	22 1 9		454 6 10	
Genoa-Gipsy Point Road .. .. .	..		Bd. 64 1 11	
Prince's Highway .. .. .	173 16 1	195 17 10	Sh. 1,827 11 2	
			Bd. 10,363 14 11	12,992 12 7
OTWAY SHIRE—				
Beech Forest-Apollo Bay Road .. .. .	19 14 5		373 17 4	
Cobden-Port Campbell-Princetown Road .. .. .	..		31 8 4	
Forrest-Apollo Bay Road .. .. .	12 0 0		874 11 3	
Glenaire-Laver's Hill Road .. .. .	..		0 2 5	
Princetown Road .. .. .	..		222 5 8	1,502 5 0
OXLEY SHIRE—				
Bright Road .. .. .	56 4 10		706 1 9	
Greta-Glenrowan Road .. .. .	912 14 6		54 10 2	
Oxley Road .. .. .	..		7 7 3	
Sydney Road .. .. .	891 11 8	1,860 11 0	..	767 19 2
OXLEY SHIRE AND WANGARATTA BOROUGH (Joint Works)—				
Oxley Road .. .. .	..		0 0 7	0 0 7
PHILLIP ISLAND AND WOOLAMAI SHIRE—				
Almurta Road .. .. .	..		543 3 4	
Dalyston-Wonthaggi Road .. .. .	..		269 10 1	
Inverloch-Wonthaggi Road .. .. .	..		293 2 11	
Korumburra-Wonthaggi Road .. .. .	2,066 6 3		165 18 11	
Main Coast Road .. .. .	..		823 8 5	
Wonthaggi-Loch Road .. .. .	1,691 0 8	3,757 6 11	587 1 5	2,682 5 1
Carried forward .. .. .	..	131,105 3 6	..	341,290 14 6

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	131,105 3 6	..	341,290 14 6
PHILLIP ISLAND AND WOOLAMAI SHIRE AND WONTHAGGI BOROUGH (Joint Works)—				
Loch-Wonthaggi Road .. .. .	..		453 2 11	453 2 11
PORTLAND SHIRE—				
Heath Road .. .. .	..		120 2 4	
Portland-Casterton Road .. .. .	55 15 3		218 16 2	
Portland-Hamilton Road .. .. .	..		795 6 3	
Prince's Highway .. .. .	..		994 11 9	
		55 15 3		2,128 16 6
PORT FAIRY BOROUGH—				
Hamilton Road .. .. .	..		176 0 5	
Prince's Highway (Portland) .. .. .	..		161 17 9	
Prince's Highway (Warrnambool) .. .. .	..		363 18 8	
				701 16 10
PRESTON TOWN—				
Epping Road .. .. .	..		226 10 1	
Whittlesea Road .. .. .	..		1,295 3 0	
				1,521 13 1
PYALONG SHIRE—				
Kilmore-Heathcote-Bendigo Road .. .. .	..		746 18 6	
				746 18 6
QUEENSCLIFF BOROUGH—				
Geelong Road .. .. .	..		465 1 2	
				465 1 2
RIPON SHIRE—				
Ballarat-Ararat Road .. .. .	..		923 0 11	
Ballarat-Hamilton Road .. .. .	..		435 12 2	
Skipton Road .. .. .	..		493 17 1	
				1,852 10 2
RIPON AND ARARAT SHIRES (Joint Works)—				
Ballarat-Ararat Road .. .. .	..		236 <sup>a</sup> 15 6	
Ballarat-Hamilton Road .. .. .	..		26 6 6	
				263 2 0
RIPON AND BALLARAT SHIRES (Joint Works)—				
Ballarat-Ararat Road .. .. .	..		0 4 1	
				0 4 1
RIPON AND HAMPDEN SHIRES (Joint Works)—				
Ballarat-Hamilton Road .. .. .	..		2 11 6	
				2 11 6
RIPON AND LEXTON SHIRES (Joint Works)—				
Ballarat-Ararat Road .. .. .	..		1,216 9 4	
				1,216 9 4
ROCHESTER SHIRE—				
Rochester-Bamawm-Prairie Road .. .. .	400 5 2		652 9 6	
Bendigo-Echuca Road .. .. .	95 2 0		53 9 9	
Timmering Road .. .. .	650 9 4		748 14 7	
		1,145 16 6		1,454 13 10
RODNEY SHIRE—				
Kyabram-Nathalia Road .. .. .	..		98 1 10	
Kyabram-Tongala Road .. .. .	..		394 4 6	
Mooroopna-Undera Road .. .. .	871 1 2		90 14 7	
Shepparton-Tatura Road .. .. .	417 17 10		875 18 1	
Tatura-Byrneside-Kyabram Road .. .. .	616 13 0		576 2 1	
Tatura-Murchison Road .. .. .	..		286 9 1	
		1,905 12 0		2,321 10 2
RODNEY AND SHEPPARTON SHIRES (Joint Works)—				
Shepparton-Tatura Road .. .. .	..		270 14 0	
				270 14 0
ROMSEY SHIRE—				
Kilmore-Lancefield Road .. .. .	..		244 10 2	
Melbourne-Lancefield Road .. .. .	1,025 2 0		637 17 6	
Woodend-Lancefield Road .. .. .	..		103 1 10	
		1,025 2 0		985 9 6
ROSEDALE SHIRE—				
Carrajung-Gormandale Road .. .. .	..		..	
Prince's Highway .. .. .	..		1,515 3 8	
Sale-Yarram Road .. .. .	..		143 8 5	
Seaspray Road .. .. .	..		179 12 2	
Prospect Road .. .. .	4 15 6		..	
Traralgon-Gormandale Road .. .. .	..		14 8 6	
Willung Road .. .. .	..		62 8 4	
		4 15 6		915 1 1
ROSEDALE AND ALBERTON SHIRES (Joint Works)—				
Carrajung-Gormandale Road .. .. .	442 13 6		1 7 3	
Prospect Road .. .. .	22 8 9		..	
		465 2 3		1 7 3
RUTHERGLEN SHIRE—				
Chiltern-Howlong Road .. .. .	680 3 7		102 8 7	
Rutherglen-Wahgunyah Road .. .. .	..		397 12 3	
Springhurst-Rutherglen Road .. .. .	..		172 1 9	
Sydney Road .. .. .	..		9 3 11	
Wodonga Road .. .. .	..		393 19 2	
Yarrowonga Road.. .. .	..		401 0 9	
		680 3 7		1,476 6 5
Carried forward .. .. .	..	136,387 10 7	..	359,068 2 10

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .		136,387 10 7		359,068 2 10
RUTHERGLEN AND WANGARATTA SHIRES (Joint Works)— Yarrowonga Road .. .. .			0 2 3	0 2 3
SALE TOWN— Prince's Highway .. .. . Sale-Longford Road .. .. .			18 13 4 227 10 6	246 3 10
SEYMOUR SHIRE— Goulburn Valley Road .. .. . Seymour-Yea Road .. .. . Sydney Road .. .. . Upper Goulburn Road .. .. .	10 1 9		606 15 4 240 3 3 519 19 6 557 9 5	1,924 7 6
SHEPPARTON SHIRE— Dookie-Nalinga Road .. .. . Shepparton-Mooroopna Road .. .. . Shepparton-Nagambie Road .. .. . Shepparton-Nalinga Road .. .. . Shepparton-Numurkah Road .. .. .	1,018 12 10 6,990 19 8 1,511 19 9	9,521 12 3	35 8 8 85 12 5 165 19 0 212 13 11 1,283 3 7	1,782 17 7
SHEPPARTON AND RODNEY SHIRES (Joint Works)— Shepparton-Mooroopna Road .. .. .			147 19 0	147 19 0
SOUTH BARWON SHIRE— Barwon Bridge Road .. .. . Barwon Heads Road .. .. . Prince's Highway .. .. .	30,593 0 11	30,593 0 11	1,238 18 0 1,683 16 10	2,922 14 10
SOUTH GIPPSLAND SHIRE— Boolarra-Foster Road .. .. . Boolarra-Welshpool Road .. .. . Falls Road .. .. . Foster-Yarram Road .. .. . Stony Creek-Dollar Road .. .. . Toora-Gunyah Road .. .. .	1,160 2 9 909 7 6 15 0 0 64 0 9 30 0 0 152 17 0	2,331 8 0	97 8 2 75 3 2 26 8 4 779 0 7 81 10 3 168 4 7	1,227 15 1
SOUTH GIPPSLAND AND ALBERTON SHIRES (Joint Works)— Boolarra-Welshpool Road .. .. .			2 13 4	2 13 4
SOUTH GIPPSLAND AND WOORAYL SHIRES (Joint Works)— Stony Creek-Dollar Road .. .. .			5 12 6	5 12 6
ST. ARNAUD BOROUGH— Avoca-St. Arnaud Road .. .. . Charlton Road .. .. . Navarre Road .. .. . St. Arnaud-Donald Road .. .. .			130 6 11 25 14 6 513 1 7 177 17 8	847 0 8
STAWELL BOROUGH— Ararat-Stawell Road .. .. .			63 14 6	63 14 6
STAWELL SHIRE— Ballarat-Stawell Road .. .. . Horsham-Wal Wal Road .. .. . Landsborough Road .. .. . Marnoo Road .. .. . Navarre Road .. .. . Stawell-Glenorchy-Horsham Road .. .. . Stawell-Warracknabeal Road .. .. .	474 6 8 537 7 6	1,011 14 2	853 10 6 4 0 5 264 0 7 2,179 9 5 2,036 3 8 336 15 9 442 19 10	6,117 0 2
STAWELL SHIRE AND STAWELL BOROUGH (Joint Works)— Ballarat-Stawell Road .. .. .			8 12 0	8 12 0
STAWELL AND KARA KARA SHIRES (Joint Works)— Navarre Road .. .. .			2 15 1	2 15 1
STRATHFIELDSAYE SHIRE— Heathcote-Bendigo Road .. .. . Mandurang Road .. .. . Strathfieldsaye Road .. .. .	11 3 7 13 10 6 495 15 3	520 9 4	515 0 11 348 11 7 392 4 9	1,255 17 3
SWAN HILL SHIRE— Euston Road .. .. . Nyah-Ouyen Road .. .. . Piangil Station Road .. .. . Swan Hill Road .. .. . Tooleybuc Road .. .. . Ultima Road .. .. . Ultima-Sealake Road .. .. .	2,366 7 1	2,742 9 9	927 15 7 2,940 12 0 389 3 9 15 8 0 236 7 6 71 17 0	4,581 3 10
TALBOT SHIRE— Maryborough-Avoca Road .. .. . Maryborough-Ballarat Road .. .. .			6 14 6 351 0 2	357 14 8
Carried forward .. .. .		183,118 6 9		380,562 6 11

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	183,118 6 9	..	380,562 6 11
<b>TAMBO SHIRE—</b>				
Bairnsdale-Bruthen Road .. .. .	22 17 6		264 16 2	
Bruthen-Omeo Road .. .. .	3 13 2		270 4 7	
Mossiface Road .. .. .	..		7 9 2	
Nowa Nowa-Buchan-Gelantipy Road .. .. .	1,661 8 5		671 13 10	
Prince's Highway .. .. .	2,864 9 4		2,629 4 3	
		4,552 8 5		3,843 8 0
<b>TOWONG SHIRE—</b>				
Murray Valley Road .. .. .	569 11 5		1,143 5 8	
Omeo Road .. .. .	2,018 2 5		Sh. 1,121 9 11 Bd. 137 12 8	
Wodonga Road .. .. .	..		131 10 3	
		2,587 13 10		2,533 18 6
<b>TRARALGON SHIRE—</b>				
Calligee Road .. .. .	777 18 2		57 14 10	
Prince's Highway .. .. .	..		568 0 5	
Traralgon-Gormandale Road .. .. .	215 1 8		272 15 2	
Traralgon-Jeeralang Road .. .. .	..		376 10 0	
		992 19 10		1,275 0 5
<b>TULLAROOP SHIRE—</b>				
Avoca Road .. .. .	..		156 15 8	
Ballarat Road .. .. .	..		126 18 9	
Castlemaine-Maryborough Road .. .. .	6,163 17 5		1,438 0 3	
Dunolly Road .. .. .	..		..	
Eddington Road .. .. .	..		233 10 8	
Natte Yallock Road .. .. .	..		150 12 1	
		6,163 17 5		2,105 17 5
<b>TUNGAMAH SHIRE—</b>				
Cobram-Strathmerton Road .. .. .	..		6 10 0	
Numurkah-Tungamah-Wilby Road .. .. .	50 0 0		262 19 2	
St. James Road .. .. .	..		10 16 0	
Yarrawonga-Cobram Road .. .. .	1,852 1 0		148 8 5	
		1,902 1 0		428 13 7
<b>UPPER MURRAY SHIRE—</b>				
Corryong Road .. .. .	..		970 1 9	
Tintaldra Road .. .. .	756 10 6		382 7 6	
		756 10 6		1,352 9 3
<b>UPPER YARRA SHIRE—</b>				
Don Road .. .. .	..		21 16 2	
Warburton Road .. .. .	590 9 1		1,561 11 8	
Wood's Point Road .. .. .	16 14 6		Sh. 17 9 3 Bd. 887 15 11	
		607 3 7		2,488 13 0
<b>VIOLET TOWN SHIRE—</b>				
Shepparton Road .. .. .	..		151 0 0	
Sydney Road .. .. .	1,343 6 7		325 6 2	
Violet Town-Dookie Road .. .. .	345 0 0		57 7 1	
		1,688 6 7		533 13 3
<b>WALPEUP SHIRE—</b>				
Cowangie Road .. .. .	..		..	
Ouyen-Pinnaroo Road .. .. .	1,883 13 6		1,642 14 6	
Sea Lake Road .. .. .	..		152 5 9	
		1,883 13 6		1,795 0 3
<b>WANGARATTA BOROUGH—</b>				
Beechworth Road .. .. .	832 13 7		39 15 8	
Sydney Road .. .. .	1,500 6 3		1,963 8 9	
		2,332 19 10		2,003 4 5
<b>WANGARATTA BOROUGH AND SHIRE (Joint Works)—</b>				
Sydney Road .. .. .	..		38 6 1	
				38 6 1
<b>WANGARATTA SHIRE—</b>				
Beechworth Road .. .. .	..		200 2 1	
Rutherglen Road .. .. .	..		3 6 9	
Sydney Road .. .. .	..		390 10 7	
Yarrawonga Road .. .. .	..		130 19 5	
				724 18 10
<b>WANGARATTA AND BEECHWORTH SHIRES (Joint Works)—</b>				
Beechworth Road .. .. .	..		7 3 9	
				7 3 9
<b>WANNON SHIRE—</b>				
Coleraine-Harrow-Apsley Road .. .. .	..		2,483 8 6	
Hamilton-Coleraine-Casterton Road .. .. .	26 17 5		1,064 4 8	
Wannon Bridge .. .. .	..		443 4 11	
		26 17 5		3,990 18 1
<b>WANNON AND GLENELG SHIRES (Joint Works)—</b>				
Hamilton-Coleraine-Casterton Road .. .. .	..		0 2 10	
				0 2 10
<b>WARANGA SHIRE—</b>				
Colbinabbin-Corop Road .. .. .	36 10 1		115 3 0	
Elmore-Colbinabbin Road .. .. .	1,827 6 2		998 5 6	
Heathcote-Elmore Road .. .. .	..		804 2 2	
Murchison-Rushworth Road .. .. .	..		654 7 5	
Tatura Road .. .. .	..		81 13 6	
		1,863 16 3		2,653 11 7
Carried forward .. .. .	..	208,476 14 11	..	406,337 6 2

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	208,476 14 11	..	406,337 6 2
WARANGA AND GOULBURN SHIRES (Joint Works)— Goulburn Valley Road and Murchison-Rushworth Road	..		105 4 4	105 4 4
WARRAGUL SHIRE— Bloomfield Road .. .. .	..		182 7 9	
Brandy Creek Road .. .. .	..		267 12 8	
Darnum-Allambee Road .. .. .	..		594 7 10	
Prince's Highway .. .. .	..		4,863 11 4	
Warragul-Korumburra Road .. .. .	3,775 12 5		407 0 8	
Warragul-Leongatha Road .. .. .	..		1,429 13 7	
		3,775 12 5		7,744 13 10
WARRAGUL AND KORUMBURRA SHIRES (Joint Works)— Warragul-Korumburra Road .. .. .	1,310 8 3		..	
		1,310 8 3		
WARRNAMBOOL SHIRE— Allansford-Nirranda Road .. .. .	..		2,536 11 2	
Garvoc-Laang Road .. .. .	..		1,056 2 10	
Mortlake Road .. .. .	..		3,978 0 9	
Prince's Highway (Geelong-Warrnambool Section) .. .. .	..		6,498 6 7	
Prince's Highway (Warrnambool-Port Fairy Section) .. .. .	..		3,525 11 9	
				17,594 13 1
WARRNAMBOOL AND HAMPDEN SHIRES (Joint Works)— Garvoc-Laang Road .. .. .	..		0 9 4	0 9 4
WERRIBEE SHIRE— Geelong-Bacchus Marsh Road .. .. .	..		22 5 0	
Prince's Highway .. .. .	..		Sh. 145 16 4 Bd. 6,558 1 2	
				6,726 2 6
WHITTLESEA SHIRE— Epping Road .. .. .	..		2,037 6 11	
Main Whittlesea Road .. .. .	..		2,826 15 5	
Wallan Road .. .. .	12 4 6		502 3 3	
Whittlesea-Kinglake Road .. .. .	1,956 3 0		400 7 11	
		1,968 7 6		5,766 13 6
WIMMERA SHIRE— Dimboola Road .. .. .	2 17 9		689 14 0	
Dcoen Road .. .. .	6 19 10		63 17 8	
Horsham-Wal Wal Road .. .. .	2,531 18 6		741 11 2	
Natimuk Road .. .. .	12 9 0		138 4 4	
		2,554 5 1		1,633 7 2
WIMMERA AND ARAPILES SHIRES (Joint Works)— Horsham-Hamilton Road .. .. .	350 0 0		1,105 6 11	
		350 0 0		1,105 6 11
WINCHELSEA SHIRE— Birregurra-F Forrest Road .. .. .	1,083 2 11		161 1 5	
Lorne Road .. .. .	786 10 5		670 16 10	
Prince's Highway .. .. .	..		1,044 1 8	
		1,869 13 4		1,875 19 11
WINCHELSEA AND BARRABOOL SHIRES (Joint Works)— Anglesea Road .. .. .	..		10 11 7	10 11 7
WODONGA SHIRE— Murray Valley Road .. .. .	..		31 8 9	
Sydney Road .. .. .	147 8 0		1,122 16 8	
Tallangatta Road .. .. .	..		389 4 1	
Wodonga-Yackandandah Road .. .. .	..		19 14 5	
		147 8 0		1,563 3 11
WONTHAGGI BOROUGH— Loch-Wonthaggi Road .. .. .	..		24 1 0	
Wonthaggi-Inverloch Road .. .. .	..		460 15 8	
Wonthaggi-Korumburra Road .. .. .	..		7 13 6	
				492 10 2
WOORAYL SHIRE— Farmer's Road .. .. .	..		282 19 9	
Inverloch-Leongatha Road .. .. .	..		777 8 0	
Inverloch-Wonthaggi Road .. .. .	..		46 6 11	
Leongatha-Yarragon Road .. .. .	..		1,016 12 11	
Lower Tarwin Road .. .. .	1,306 18 1		1,042 16 5	
Main South Gippsland Road .. .. .	672 8 7		514 15 4	
Mardan Road .. .. .	3,235 12 9		278 3 1	
Warragul-Leongatha Road .. .. .	1 10 0		40 13 10	
Wild Dog Valley Road .. .. .	..		144 17 5	
		5,216 9 5		4,144 13 8
WYCHEPROOF— Birchip-Sea Lake Road .. .. .	..		284 9 10	
Birchip-Wycheproof Road .. .. .	8 2 5		174 14 0	
Ouyen-Sea Lake Road .. .. .	..		309 4 2	
Sea Lake-Ultima Road .. .. .	..		20 7 3	
Wycheproof-Sea Lake Road .. .. .	603 13 7		2,465 10 11	
		611 16 0		3,254 6 2
Carried forward .. .. .	..	226,280 14 11	..	458,355 2 3



STATEMENT OF EXPENDITURE IN CONNECTION WITH CONSTRUCTION AND MAINTENANCE, ETC.—*continued.*

Municipality and Road.	Permanent Works.		Maintenance.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	226,280 14 11	..	458,355 2 3
YACKANDANDAH SHIRE—				
Dederang Road .. .. .	1,792 5 6		513 8 7	
Gundowring Road.. .. .	299 8 5		481 6 4	
Tallangatta Road .. .. .	..		110 15 9	
Yackandandah-Wodonga Road .. .. .	170 7 2		631 2 4	
		2,262 1 1		1,736 13 0
YARRAWONGA SHIRE—				
Tungamah-Wilby Road .. .. .	7 4 9		1 7 1	
Wangaratta-Yarrawonga Road .. .. .	27 9 0		520 9 6	
Yarrawonga-Cobram Road .. .. .	12 12 0		266 11 4	
Yarrawonga-Rutherglen Road .. .. .	..		24 0 6	
		47 5 9		812 8 5
YEA SHIRE—				
Upper Goulburn Road .. .. .	282 2 9		1,550 9 5	
Whittlesea-Kinglake Road .. .. .	..		..	
Yea-Glenburn Road .. .. .	..		609 13 3	
		282 2 9		2,160 2 8
YEA AND ALEXANDRA SHIRES (Joint Works)—				
Upper Goulburn Road .. .. .	..		0 10 0	
				0 10 0
YEA AND BROADFORD SHIRES (Joint Works)—				
Upper Goulburn Road .. .. .	..		0 4 5	
				0 4 5
	..	£228,872 4 6	..	£463,065 0 9

## HIGHWAYS AND VEHICLES ACT No. 3379.

## MAINTENANCE OF STATE HIGHWAYS, 30TH JUNE, 1925.

	Amount Expended.
	£ s. d.
Omoo Highway .. .. .	5,116 10 4

## APPENDIX E.

## COUNTRY ROADS BOARD.

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL  
ROADS FOR YEAR ENDED 30TH JUNE, 1925.

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.		Amount.	
	£	s. d.	£	s. d.
ALBERTON SHIRE—				
Albert River Road .. .. .			1,022	19 3
Balook-Traralgon Road .. .. .			2,898	0 6
Binginwarri-Welshpool Road .. .. .			1,383	12 11
Blackwarry-Yarram Road .. .. .			2,307	2 0
Carrajung-Balook Road .. .. .			1,145	6 0
Carrajung-Gormandale Road .. .. .			947	7 9
Devil's Pinch Road .. .. .				
Gelliondale .. .. .			4,665	13 2
Jenkin's Road .. .. .			1,031	7 7
Lay's Road .. .. .			1,533	7 11
Whitelaw's Track .. .. .			4,762	1 11
				21,696 19 0
ALBERTON AND TRARALGON SHIRES (Joint Works)—				
Balook-Traralgon Road .. .. .			1,373	3 0
				1,373 3 0
ALBERTON AND SOUTH GIPPSLAND SHIRES (Joint Works)—				
Binginwarri-Welshpool Road .. .. .			172	8 4
Devil's Pinch .. .. .			637	8 0
				809 16 4
ALBERTON, MORWELL, AND SOUTH GIPPSLAND SHIRES (Joint Works)—				
Gunyah-Ryton Road .. .. .			10	13 0
				10 13 0
ALEXANDRA SHIRE—				
Terip Terip Road .. .. .			931	18 6
				931 18 6
ARAPILES SHIRE—				
Miga Lake—Gymbowen Road .. .. .			1,680	0 0
				1,680 0 0
ARARAT SHIRE—				
Pomonal Road .. .. .				
AVOCA SHIRE—				
Maryborough-Natte Yallock Road .. .. .			3,855	6 6
				3,855 6 6
AVON SHIRE—				
Bengworden Road .. .. .			667	5 4
Bengworden-Clydebank Road .. .. .			2,223	15 8
Clydebank Road .. .. .			3,794	6 3
Dargo Road .. .. .			2,048	15 6
Talbotville Road .. .. .			298	6 6
				9,032 9 3
BACCHUS MARSH SHIRE—				
Brisbane Road .. .. .			137	2 9
Geelong-Bacchus Marsh Road .. .. .			10	18 6
McArthur's Road .. .. .				
Staughton Vale Road .. .. .			4	1 3
				152 2 6
BACCHUS MARSH AND CORIO SHIRES (Joint Works)—				
Balliang Road .. .. .			2,802	9 1
				2,802 9 1
BAIRNSDALE SHIRE—				
Bairnsdale-Bengworden Road .. .. .			660	0 0
Bullumwaal-Tabberabbera Road .. .. .			667	2 10
Colulu-Boggy Creek Road .. .. .			1,836	17 7
Glenaladale-Lindenow Road .. .. .			1,634	0 0
Sarsfield-Waterholes Road .. .. .			41	7 5
				4,839 7 1
BALLAN SHIRE—				
Back Settlement Road .. .. .			2,277	11 3
Ballan-Egerton Road .. .. .			2	18 0
Blakeville Road .. .. .			216	1 3
Bungal Road .. .. .			98	13 4
Bungeeltap Road .. .. .			612	15 7
Daylesford Road .. .. .			1,002	5 7
Moorarbool West Road .. .. .			1,130	0 7
		78 9 0		5,340 5 7
BANNOCKBURN SHIRE—				
Burnside Road .. .. .			1,429	1 3
				1,429 1 3
Carried forward .. .. .				53,953 11 10
		78 9 0		

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.			
	Amount.		Total.			
	£	s. d.	£	s. d.		
Brought forward .. .. .	..	..	78	9 0	..	53,953 11 10
<b>BEECHWORTH SHIRE—</b>						
Bowman's Forrest Road .. .. .	..	..	1,081	19 1		
Hillsborough Road .. .. .	..	..	10	0 0		
Myrtleford-Yackandandah Road .. .. .	..	..	330	10 5		
Stanley Road .. .. .	..	..	22	2 0		
						1,444 11 6
<b>BEECHWORTH AND WANGARATTA SHIRES (Joint Works)—</b>						
Everton Road .. .. .	..	..	2,170	13 8		
						2,170 13 8
<b>BEECHWORTH AND BRIGHT SHIRES (Joint Works)—</b>						
Myrtleford-Yackandandah Road .. .. .	..	..	395	19 0		
						395 19 0
<b>BEECHWORTH AND YACKANDANDAH SHIRES (Joint Works)—</b>						
Myrtleford-Yackandandah Road .. .. .	..	..	0	12 2		
						0 12 2
<b>BELFAST SHIRE—</b>						
Spencer Road .. .. .	..	..	937	2 8		
						937 2 8
<b>BENALLA SHIRE—</b>						
Benalla-Mansfield Road .. .. .	..	..	25	13 7		
Mollyullah-Tatong Road .. .. .	..	..	408	0 0		
Tatong Road .. .. .	..	..				
Toombullup Road .. .. .	..	..	2,104	8 2		
						2,538 1 9
<b>BERWICK SHIRE—</b>						
Beaconsfield-Emerald Road .. .. .	..	..	282	7 0		
Bunyip-Tonimbuk East Road .. .. .	..	..	990	14 9		
Longwarry-Modella Road .. .. .	..	..	1,500	8 4		
Nar-nar-noon-Gembrook Road .. .. .	..	..	1,000	0 0		
Tynong-Tonimbuk Road .. .. .	..	..	280	0 0		
Upper Beaconsfield-Upper Pakenham Road .. .. .	..	..	1,039	11 8		
						5,093 1 9
<b>BERWICK AND FERNTREE GULLY SHIRES—</b>						
Reservoir Road .. .. .	..	..	100	0 0		
						100 0 0
<b>BRIGHT SHIRE—</b>						
Buffalo River Road .. .. .	..	..	99	8 7		
Happy Valley Road .. .. .	..	..	1,768	7 9		
Harrietville Road .. .. .	..	..	7	10 0		
Kiewa Valley Road .. .. .	..	..	2,271	11 7		
Myrtleford-Yackandandah Road .. .. .	..	..	2,367	16 0		
						6,514 13 11
<b>BRIGHT AND OXLEY SHIRES (Joint Works)—</b>						
Buffalo River Road .. .. .	..	..	..	..		
<b>BULLA SHIRE—</b>						
Konagadera Road .. .. .	..	..	958	11 7		
Riddell Road .. .. .	..	..	1,792	10 7		
						2,751 2 2
<b>BULLA AND ROMSEY SHIRES (Joint Works)—</b>						
Konagadera Road .. .. .	..	..	2,685	10 3		
						2,685 10 3
<b>BULN BULN SHIRE—</b>						
Apted's Road .. .. .	..	..	26	9 10		
Buln Buln Road .. .. .	..	..	0	2 0		
Fumina Road .. .. .	..	..	879	9 7		
Labertouche Road .. .. .	..	..				
Lardner's Track .. .. .	..	..	0	15 10		
Loch-Valley Road .. .. .	..	..	18	4 5		
Longwarry-Modella Road .. .. .	..	..	95	14 11		
Mountain View Road .. .. .	..	..	7	18 8		
Mountain View-McDonald's Track .. .. .	..	..	1,728	19 0		
Neerim North-Noojee Road .. .. .	..	..	4,406	15 8		
Neerim South-Neerim East Road .. .. .	..	..	471	4 4		
Poowong Road .. .. .	..	..	1,897	1 10		
Rokeyby-North Jindivick Road .. .. .	..	..	1,414	10 10		
South Road .. .. .	..	..	926	8 10		
Torwood-Topiram Road .. .. .	..	..	1,138	4 9		
						13,012 0 6
<b>BULN BULN AND BERWICK SHIRES (Joint Works)—</b>						
Longwarry-Modella Road .. .. .	..	..	91	19 10		
						91 19 10
<b>BULN BULN AND KORUMBURRA SHIRES (Joint Works)—</b>						
Torwood-Topiram Road .. .. .	..	..	16	3 3		
						16 3 3
<b>BULN BULN AND WARRAGUL SHIRES (Joint Works)—</b>						
Lardner's Track .. .. .	..	..	341	7 8		
						341 7 8
<b>BUNGAREE SHIRE—</b>						
Bolwarrah Road .. .. .	..	..	1,043	6 0		
						1,043 6 0
<b>BUNINYONG SHIRE—</b>						
Elaine-Mt. Mercer Road .. .. .	..	..	149	9 9		
						149 9 9
Carried forward .. .. .	..	..	78	9 0	..	93,239 7 8

## STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—continued.

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.					
	Amount.		Total.					
	£	s. d.	£	s. d.				
Brought forward .. .. .	..	..	78	9 0	..	93,239	7 8	
BUNINYONG AND LEIGH SHIRES (Joint Works)—								
Elaine-Mt. Mercer Road .. .. .	..	..	..	..	2,820	17 6	2,820	17 6
CHARLTON SHIRE—								
Barakee Road .. .. .	..	..	..	..	24	0 7		
Teddywaddy Road .. .. .	..	..	..	..	560	15 6		
Wooroonook Road .. .. .	..	..	..	..	523	11 5	1,108	7 6
COHUNA SHIRE—								
Cohuna-Leitchville Road .. .. .	..	..	..	..	1,232	9 11		
Cohuna-Mead Road .. .. .	..	..	..	..	37	2 6		
Cohuna-McMillan's Road .. .. .	..	..	..	..	7,216	0 9		
Gunbower-Leitchville Road .. .. .	..	..	..	..	2,402	7 2		
Keeley's Lane .. .. .	..	..	..	..	6,817	3 11		
Leitchville South Road .. .. .	..	..	..	..	3,002	6 10	20,707	11 1
COLAC SHIRE—								
Beeac-Dreeite Road .. .. .	..	..	..	..	3,121	12 0		
Colac-Forrest Road .. .. .	..	..	..	..	5,686	11 3		
Dreeite Road .. .. .	..	..	30	0 0	1,664	9 10		
Stoneyhurst-Estate Road .. .. .	..	..	..	..	2,768	2 0	13,240	15 1
COLAC AND OTWAY SHIRES (Joint Works)—			30	0 0				
Colac-Forrest Road .. .. .	..	..	..	..	48	10 8	48	10 8
CORIO SHIRE—								
Avalon Road .. .. .	..	..	..	..	1,268	6 8		
Cox Road .. .. .	..	..	..	..	891	2 9		
Geelong-Bacchus Marsh Road .. .. .	..	..	..	..	3,072	1 1		
Little River-Ripley Road .. .. .	..	..	..	..	3,506	3 10		
Stacey's Road .. .. .	..	..	..	..	1,373	4 9	10,110	19 1
CORIO AND BACCHUS MARSH SHIRES (Joint Works)—								
Geelong-Bacchus Marsh Road .. .. .	..	..	..	..	176	7 11	176	7 11
CRANBOURNE SHIRE—								
Bullarto Road .. .. .	..	..	..	..	..	..		
Dalmore Road .. .. .	..	..	..	..	3,519	10 5		
Koo-wee-rup-Modella Road .. .. .	..	..	..	..	795	16 7		
Mank's Road .. .. .	..	..	..	..	2,379	16 1		
Pearcedale Road .. .. .	..	..	..	..	2,203	11 8	8,898	14 9
CRANBOURNE AND FRANKSTON AND HASTINGS SHIRES (Joint Works)—								
Bullarto Road .. .. .	..	..	..	..	750	0 0	750	0 0
DANDENONG SHIRE—								
Worsely Road .. .. .	..	..	..	..	3,110	17 2	3,110	17 2
DEAKIN SHIRE—								
Echuca East Road .. .. .	..	..	..	..	1,855	13 8		
Girgarre East Road .. .. .	..	..	..	..	237	0 0		
Girgarre North Road .. .. .	..	..	..	..	140	0 0		
Girgarre West Road .. .. .	..	..	..	..	170	0 0		
Koyuga Road .. .. .	..	..	..	..	5,652	8 4		
Tongala East Road .. .. .	..	..	..	..	2,021	16 2		
Tongala North Road .. .. .	..	..	..	..	1,958	8 11		
Tongala West Road .. .. .	..	..	..	..	3,013	0 10	15,048	7 11
DIMBOOLA SHIRE—								
Albacutya Road .. .. .	..	..	..	..	418	11 10		
Depla-Hindmarsh Road .. .. .	..	..	..	..	227	8 7		
Glenlee-Jeparit Road .. .. .	..	..	..	..	1,075	5 3		
Hindmarsh-Rainbow Road .. .. .	..	..	..	..	522	18 7		
Pepper's Plains Road .. .. .	..	..	..	..	1,356	11 2	3,600	15 5
DONALD SHIRE—								
Donald-Minyip Road .. .. .	..	..	..	..	1,036	18 1		
Rich Ayon Road .. .. .	..	..	..	..	1,291	13 3	2,328	11 4
DUNDAS SHIRE—								
Kanawalla-Noske Road .. .. .	..	..	..	..	810	11 7	810	11 7
EAST LODDON SHIRE—								
Prairie-Borong Road .. .. .	..	..	..	..	17	2 0		
Tandarra Road .. .. .	..	..	..	..	1,358	18 4	1,376	0 4
ELTHAM SHIRE—								
Balee-Kangaroo Ground Road .. .. .	..	..	..	..	3,260	1 4		
Cottle's Bridge Road .. .. .	..	..	..	..	11	4 3		
Kinglake-Kinglake East Road .. .. .	..	..	..	..	389	0 0		
Tarrawarra Road .. .. .	..	..	..	..	..	..		
Whittlesea-Kinglake Road .. .. .	..	..	..	..	148	19 2		
Yarra Glen-Kinglake East Road .. .. .	..	..	4	14 0	1,048	7 6		
Yarra Glen-Toolangi Road .. .. .	..	..	..	..	..	..	4,857	12 3
Carried forward .. .. .	..	..	113	3 0	..	..	182,234	7 3

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	113 3 0	..	182,234 7 3
ELTHAM AND LILLYDALE SHIRES (Joint Works)— Tarrawarra Road .. .. .	..	..	148 12 7	148 12 7
ELTHAM AND YEA SHIRES (Joint Works)— Kinglake-Kinglake East Road .. .. .	..	..	1,142 0 0	1,142 0 0
EUROA SHIRE— Euroa-Strathbogie Road .. .. . Longwood-Ruffy Road .. .. . Merton-Strathbogie Road .. .. . Strathbogie Road .. .. .	.. .. .. ..	.. .. .. ..	1,959 0 8 2,631 6 6 .. 1,693 14 10	6,284 2 0
FERNTREE GULLY SHIRE— Belgrave-Emerald Road .. .. . Emerald-Macclesfield Road .. .. . Emerald-Monbulk Road .. .. . Lysterfield Road .. .. . Reservoir Road .. .. . Sassafras Creek Road .. .. . The Patch Road .. .. . Wantirna Road .. .. .	.. .. .. .. .. 12 0 6 .. 15 0 0	.. .. .. .. .. .. .. ..	8,016 16 0 .. 1,360 15 9 1,391 19 0 132 6 8 909 1 4 432 14 5 ..	12,243 13 2
FLINDERS SHIRE— Baldry Road .. .. . Bittern-Dromana Road .. .. . Boneo Road .. .. . Brown's Road .. .. . Main Creek Road .. .. . Merrick's Road .. .. . Red Hill .. .. . Stony Point Road .. .. .	.. .. .. .. .. .. .. ..	75 10 11 .. .. .. .. .. .. ..	.. 1,133 19 9 829 14 0 4,634 17 1 1,893 17 6 3,175 8 6 10 10 8 2,560 11 6	14,249 16 0
FRANKSTON AND HASTINGS SHIRE— Baxter-Pearcedale Road .. .. . Cranbourne-Frankston Road .. .. . Frankston-Dandenong Road .. .. . Hodgin's Road .. .. . Moorooduc-Somerville Road .. .. . Tyabb-Mornington Road .. .. .	.. .. .. .. 243 14 7 385 13 0	.. .. .. .. .. ..	.. 210 1 3 334 7 5 2,981 5 11 23 1 10 65 5 7	3,614 2 0
GLENELG SHIRE— Dergholm-Elderslie Road .. .. . Glenorehy Estate Road .. .. . Merino-Struan-Tahara Road .. .. .	.. .. ..	.. .. ..	3 15 10 1,746 5 5 1,111 15 7	2,861 16 10
HAMPDEN SHIRE— Berry Bank Road .. .. . Mount Bute Road .. .. .	.. ..	.. ..	.. 2,368 2 11	2,368 2 11
HEALESVILLE SHIRE— Healesville-Toolangi Road .. .. . Myer's Creek Road .. .. .	.. ..	.. ..	1,403 9 4 516 11 6	1,920 0 10
HEALESVILLE AND UPPER YARRA SHIRES (Joint Works)— Healesville-Woori Yallock Road .. .. . Woori Yallock-Cockatoo (Bridge) .. .. .	.. ..	.. ..	48 11 10 ..	48 11 10
HEYTESBURY SHIRE— Ayersford Road .. .. . Carpentait South .. .. . Cowley's Creek-Timboon Road .. .. . Kennedy's Creek Road .. .. . Timboon-Nirranda Road .. .. . Timboon-Port Campbell Road .. .. . Timboon-Scott's Creek Road .. .. . Princetown Road .. .. . Timboon-Digney's Bridge Road .. .. .	.. .. .. .. .. .. .. .. ..	.. .. .. .. .. .. .. .. ..	1,350 3 0 100 15 2 504 3 9 1,821 11 4 6,238 7 8 1,759 19 3 .. 787 15 0	12,562 15 2
HEYTESBURY AND WARRENAMBOOL SHIRES (Joint Works)— Ayersford Road .. .. .	.. ..	.. ..	624 6 5	624 6 5
HUNTLY SHIRE— Dromartin Road .. .. .	.. ..	.. ..	1,707 11 7	1,707 11 7
KERANG SHIRE— Murrabit Road .. .. .	.. ..	.. ..	6,426 15 6	6,426 15 6
KORUMBURRA SHIRE— Arawata Road .. .. . Bena-Kongwak Road .. .. . Clancy's Road .. .. . Ferrier's Road .. .. . Korumburra-Fairbank Road .. .. . Korumburra South Road .. .. .	.. .. .. .. .. ..	.. .. .. .. .. ..	2,031 3 5 2,805 1 8 1,630 12 2 510 4 1 362 5 1 934 1 10	..
Carried forward .. .. .	..	845 2 0	..	248,436 14 1

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.			
	Amount.		Total.			
	£	s. d.	£	s. d.		
Brought forward .. .. .	..	..	845	2 0	..	248,436 14 1
<b>KORUMBURRA SHIRE—<i>continued.</i></b>						
Loch-Nyora Road .. .. .	..	..	..	..	49	12 2
Poowong Estate Road .. .. .	..	..	..	..	..	..
Poowong-Olsen Road .. .. .	..	..	..	..	3,169	15 6
Poowong-Ranceby Road .. .. .	..	..	..	..	1	0 0
Sheepway's Road .. .. .	..	..	..	..	58	12 0
Stewart's Road .. .. .	..	..	..	..	626	15 7
Timm's Road .. .. .	..	..	..	..	1,938	10 2
Torwood-Topiram Road .. .. .	..	..	..	..	11	7 9
						14,129 1 5
<b>KORUMBURRA AND WARRAGUL SHIRES (Joint Works)—</b>						
Warragul-Korumburra Road .. .. .	..	..	..	..	..	..
<b>KARA KARA SHIRE—</b>						
Marnoo-St. Arnaud Road .. .. .	..	..	..	..	1,381	14 2
						1,381 14 2
<b>KARKAROOC SHIRE—</b>						
Hopetoun-Lascelles Road .. .. .	..	..	..	..	1,069	12 0
Hopetoun-Yaapect Road .. .. .	..	..	..	..	1,785	2 10
Rosebery West Road .. .. .	..	..	..	..	862	13 6
Yaapect-Nypo Road .. .. .	..	..	..	..	196	7 2
						3,913 15 6
<b>KORONG SHIRE—</b>						
Emu-Logan Road .. .. .	..	..	..	..	279	8 6
Kurting-Rheola Road .. .. .	..	..	..	..	179	0 0
Nine-mile Road .. .. .	..	..	..	..	346	0 0
Wedderburn-Spring Hill Road .. .. .	..	..	..	..	639	1 3
						1,443 9 9
<b>KOWREE SHIRE—</b>						
Benayeo Road .. .. .	..	..	..	..	532	10 0
Edenhope-Natimuk Road .. .. .	..	..	..	..	335	2 9
Elderslie Road .. .. .	..	..	..	..	721	14 10
Elderslie-Narracoorte Road .. .. .	..	..	..	..	..	..
Miga Lake-Gymbowen Road .. .. .	..	..	..	..	223	15 10
Minimay Road .. .. .	..	..	..	..	6	9 9
						1,819 13 2
<b>KYNETON SHIRE—</b>						
Lagoon Road .. .. .	..	..	..	..	340	11 3
						340 11 3
<b>LAWLOTT SHIRE—</b>						
Cove Estate Settlement Road .. .. .	..	..	..	..	1,992	7 0
Miram Road .. .. .	..	..	..	..	1,938	7 6
Serviceton North Road .. .. .	..	..	..	..	63	17 7
Serviceton South Road .. .. .	..	..	..	..	887	6 6
						4,881 18 7
<b>LEIGH SHIRE—</b>						
Cressy-Meadows Road .. .. .	..	..	..	..	984	16 10
Cressy-Shelford Road .. .. .	..	..	..	..	..	..
Gillett's Road .. .. .	..	..	..	..	32	9 6
Werneth Road .. .. .	..	..	..	..	141	12 10
Werneth East Road .. .. .	..	..	..	..	1,525	13 2
						2,684 12 4
<b>LEIGH AND COLAC SHIRES (Joint Works)—</b>						
Cressy-Shelford Road .. .. .	..	..	..	..	1,019	14 11
						1,019 14 11
<b>LEIGH AND GRENVILLE SHIRES (Joint Works)—</b>						
Gillett's Road .. .. .	..	..	..	..	162	0 0
						162 0 0
<b>LILYDALE SHIRE—</b>						
Beenak-Seville Road .. .. .	..	..	..	..	2,411	4 11
Emerald-Monbulk Road .. .. .	..	..	..	..	..	..
Monbulk-Seville Road .. .. .	..	..	..	..	1,523	0 1
Olinda Creek Road .. .. .	..	..	..	..	1,408	2 1
Swansea Road .. .. .	..	..	..	..	..	..
Wandin Road .. .. .	..	..	..	..	5,647	9 0
Wantirna Road .. .. .	..	..	..	..	4	2 0
						10,993 18 1
<b>LOWAN SHIRE—</b>						
Diapir-Yanac Road .. .. .	..	..	..	..	328	7 0
Netherby Road .. .. .	..	..	..	..	4,486	19 11
Woorak Road .. .. .	..	..	..	..	2,189	11 7
Yanac South Road .. .. .	..	..	..	..	51	18 0
Yanac Station Road .. .. .	..	..	..	..	608	13 7
						7,665 10 1
<b>MAFFRA SHIRE—</b>						
Bushy Park-Valencia Creek Road .. .. .	..	..	..	..	218	0 7
Maffra-Newty Road .. .. .	..	..	..	..	2,575	9 6
						2,793 10 1
<b>MANSFIELD SHIRE—</b>						
Benalla-Mansfield Road .. .. .	..	..	..	..	860	12 4
Merton-Strathbogie Road .. .. .	..	..	..	..	2,849	18 9
Tolmie Road .. .. .	..	..	..	..	2,560	15 8
						6,271 6 9
Carried forward .. .. .	..	..	845	2 0	..	307,937 10 2



STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	845 2 0	..	307,937 10 2
MANSFIELD AND BENALLA SHIRES (Joint Works)— Benalla—Mansfield Road .. .. .	..	..	175 14 5	175 14 5
MARONG SHIRE— Yarraberb Road .. .. .	..	..	3,025 2 9	3,025 2 9
MELTON SHIRE— Coburn's Road .. .. .	..	..	51 0 0	51 0 0
MILDURA SHIRE— Benetook Avenue .. .. . Brownport Road .. .. . Colignan Road .. .. . Merbein West Road .. .. .	.. .. .. ..	.. .. .. ..	.. 463 9 2 50 0 0 915 0 4	.. .. .. 1,428 9 6
MINHAMITE SHIRE— Bessie Belle—Yambuk Road .. .. . Heywood Road .. .. . Lake Gorrje Road .. .. . Nardoo Road .. .. . Orford—St. Helen's Road .. .. . Woodlands Road .. .. .	.. .. .. .. .. 431 11 6	.. .. .. .. .. 431 11 6	240 0 0 4,063 18 6 3,469 10 8 604 19 11 246 0 0 616 15 2	.. .. .. .. .. 9,241 4 3
MIRBOO SHIRE— Berry's Creek Road .. .. . Boolarra—South Mirboo Road .. .. . Boorool Road .. .. . Clear Creek Road .. .. . Darlimurla—Thorpdale Road .. .. . Leongatha—Mirboo Road .. .. . Mardan Road .. .. . Mirboo—Boolarra Road .. .. . Mirboo—Yarragon Road .. .. . Nichol's Road .. .. .	.. .. .. .. .. .. .. .. .. ..	.. .. .. .. .. .. .. .. .. ..	2,091 6 3 2,032 3 9 67 13 3 135 19 1 636 15 10 17 3 9 741 15 9 171 18 11 773 14 6 3,901 3 8	.. .. .. .. .. .. .. .. .. 10,569 14 9
MORNINGTON SHIRE— Tyabb Road .. .. .	..	..	649 13 0	649 13 0
MORWELL SHIRE— Boolarra—Foster Road .. .. . Boolarra—Morwell Road .. .. . Boolarra—Welshpool Road .. .. . Jeeralang West Road .. .. . Jumbuk Road .. .. . Livingston Road .. .. . Middle Creek Road .. .. . Mirboo—Boolarra Road .. .. . Shelcot's Road .. .. . Tyer's Road .. .. . Whitelaw's Track Road .. .. .	.. .. .. .. 102 15 0 .. .. .. .. .. ..	.. .. .. .. 102 15 0 .. .. .. .. .. ..	29 17 7 2,254 1 9 372 2 1 34 0 4 1,876 15 3 148 7 10 2,210 11 9 2,752 12 10 269 10 3 12 17 3 40 2 11	.. .. .. .. .. .. .. .. .. .. 10,000 19 10
MORWELL AND ALBERTON SHIRES (Joint Works)— Gunyah—Ryton Road .. .. . Jeeralang West Road .. .. .	.. ..	.. ..	357 4 6 36 17 11	.. 394 2 5
MORWELL AND SOUTH GIPPSLAND SHIRES (Joint Works)— Gunyah—Ryton Road .. .. .	..	..	7 9 2	7 9 2
MORWELL AND TRARALGON SHIRES (Joint Works)— Jeeralang West Road .. .. . Tyer's Road .. .. .	.. ..	.. ..	27 2 1 ..	.. 27 2 1
NARRACAN SHIRE— Allambee—Childers Road .. .. . Allambee Estate Road .. .. . Canal Road .. .. . Childers—Thorpdale Road .. .. . Coalville—Moe Road .. .. . Erica Road .. .. . Hazeldeane—Yarragon Road .. .. . Latrobe River Connection Road .. .. . Mirboo—Moondarra Road .. .. . Moe—Moondarra Road .. .. . Moe River Road .. .. . Moe—Westbury Road .. .. . Narracan—Connection Road .. .. . Shady Creek Road .. .. . Telbit Road .. .. . Thorpdale East Road .. .. . Thorpdale—Yarragon Road .. .. . Trafalgar East Road .. .. . Trafalgar—Willowgrove Road .. .. . Yarragon—Leongatha Road .. .. .	.. 125 0 0	.. 125 0 0	819 19 10 .. 2,008 4 8 194 17 6 689 0 11 515 10 0 .. 1,157 3 8 4,104 0 7 980 3 11 812 5 6 1 0 7 1,729 14 11 161 0 0 .. 2,700 1 1 1,001 5 2 30 7 3 172 13 4 4 12 0	.. 17,082 0 11
Carried forward .. .. .	..	1,504 8 6	..	360,590 3 3

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road,	Acts. Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	1,504 8 6	..	360,590 3 3
NEWHAM AND WOODEND SHIRE—				
Campaspe Road .. .. .	..		1,433 13 3	
Macedon Village Settlement Road .. .. .	..		1,631 10 2	
				3,065 3 5
NEWSTEAD AND MOUNT ALEXANDER SHIRE—				
Glengowar-Joyce's Creek Road .. .. .	..		830 16 9	
				830 16 9
NUNAWADING SHIRE—				
Terrara Road .. .. .	..		..	
OMEQ SHIRE—				
Benambra Road .. .. .	..		..	
Benambra-Corryong Road .. .. .	..		25 0 0	
Bindi Road .. .. .	..		1,873 6 2	
Brookville Road .. .. .	..		15 19 2	
Bruthen-Omeo Road .. .. .	..		550 18 5	
Mountain Ash Road .. .. .	..		..	
Reedy Creek Road .. .. .	..	63 13 0	1,592 2 8	
Swift's Creek-Cassilis Road .. .. .	..	..	..	
		63 13 0		4,057 6 5
ORBOST SHIRE—				
Bete-Bolong Road .. .. .	..		1,750 12 9	
Broome's Road .. .. .	..		1,304 17 10	
Cann Valley Road .. .. .	..		716 6 0	
Combiobar Road .. .. .	..		814 2 1	
Errinundra Valley Road .. .. .	..		..	
Groves Road .. .. .	..		7 10 2	
Jarrahmond Road .. .. .	..		37 11 1	
Lochend Road .. .. .	..		1 0 0	
Lower Bemm Road .. .. .	..		560 14 5	
Lower Bendock Road .. .. .	..		24 2 5	
Mallacoota West Road .. .. .	..		..	
Murrungowar Road .. .. .	..		93 10 2	
Orbost-Delegate Road .. .. .	..		47 15 0	
Prince's Highway .. .. .	..		1,990 18 4	
Tostaree Road .. .. .	..		..	
Wangrabelle Road .. .. .	..		517 3 9	
West Cann Road .. .. .	..		59 19 4	
				7,926 3 4
OTWAY SHIRE—				
Apollo Bay-Elliott River Road .. .. .	..		0 8 9	
Barham Valley Road .. .. .	..		306 19 0	
Beech Forest-Apollo Bay Road .. .. .	..		3,149 5 8	
Beech Forest-Laver's Hill Road .. .. .	..		365 5 7	
Beech Forest-Mount Sabine Road .. .. .	..		1,413 2 3	
Cape Patten Road .. .. .	..		8 8 8	
Colac-Forrest .. .. .	..		773 2 2	
Denhart's Road .. .. .	..		1,505 7 9	
Ferguson-Charley's Creek Road .. .. .	..		909 9 1	
Forrest-Apollo Bay Road .. .. .	..	0 15 9	..	
Gellibrand-Carlisle Road .. .. .	..		2,371 7 9	
Hordern Vale Road .. .. .	..		783 9 2	
Kennedy's Creek Road .. .. .	..	49 3 6	110 3 6	
Laver's Hill-Chapplevale-Devondale Road .. .. .	..		753 2 2	
Laver's Hill-Glenaire Road .. .. .	..		353 15 2	
Phillip's Track .. .. .	..		929 12 0	
Port Campbell-Princetown Road .. .. .	..		0 8 3	
Princetown Road .. .. .	..		2,654 6 10	
Wild Dog Valley Road .. .. .	..		141 15 10	
		64 12 6		16,529 9 7
OXLEY SHIRE—				
Abbeyard Road .. .. .	..		7 19 5	
Boggy Creek Road .. .. .	..		124 8 1	
Buffalo River Road .. .. .	..	44 3 3	4 12 3	
Carboor-Whorouly Road .. .. .	..		1,329 6 2	
Fifteen-mile Creek Road .. .. .	..		20 9 11	
King Valley Road .. .. .	..		3,352 11 4	
Rose River Road .. .. .	..		..	
Toombullup Road .. .. .	..		..	
Whitfield-Tolmie Road .. .. .	..		11 18 6	
		44 3 3		4,851 5 8
PHILLIP ISLAND AND WOOLAMAI SHIRE—				
Almurta-Glen Forbes Road .. .. .	..		211 8 8	
Bass Road .. .. .	..		87 0 0	
Dalyston-Glen Forbes Road .. .. .	..		2,046 14 10	
Glen Alvie Road .. .. .	..		55 13 5	
Kilcunda Road .. .. .	..		20 9 9	
Korumburra-Wonthaggi Road .. .. .	..		289 0 7	
Krowera-Kernot Road .. .. .	..		2,289 9 3	
Phillip Island Road .. .. .	..		..	
Sheepways Road .. .. .	..		43 4 6	
Wonthaggi-Loch Road .. .. .	..		2,584 18 2	
				7,627 19 2
Carried forward .. .. .	..	1,676 17 3	..	405,478 7 7

STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—*continued.*

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	1,676 17 3	..	405,478 7 7
<b>PORTLAND SHIRE—</b>				
Bridgewater Road .. .. .	..		1,454 9 1	
Drik Drik—Winnap Road .. .. .	..		768 7 6	
Gorae Road .. .. .	..		1,290 6 6	
Heath Road .. .. .	..		..	
Prince's Highway .. .. .	..		2,105 2 11	5,618 6 0
<b>RIPON SHIRE—</b>				
Trawalla Road .. .. .	..		4,752 8 0	
Trawalla East Road .. .. .	..		2,582 0 4	
Trawalla West Road .. .. .	..		1,114 10 0	8,448 18 4
<b>ROCHESTER SHIRE—</b>				
Echuca West Road .. .. .	..		3,407 3 6	
Nanneella Road .. .. .	..		27 17 1	
Nanneella Extension Road .. .. .	..		21 19 6	3,457 0 1
<b>RODNEY SHIRE—</b>				
Lancaster—Udera Road .. .. .	..		2,611 4 6	
Tatura—Rushworth Road .. .. .	..		1,368 13 4	
Tatura—Toolamba Road .. .. .	..		2,048 7 4	6,028 5 2
<b>ROMSEY SHIRE—</b>				
Baynton Road .. .. .	..		11 15 0	
Konagadera Road .. .. .	..		..	11 15 0
<b>ROMSEY AND KYNETON SHIRES (Joint Works)—</b>				
Baynton Road .. .. .	..		35 5 0	35 5 0
<b>ROMSEY AND GISBORNE SHIRES (Joint Works)—</b>				
Riddell—Sunbury Road .. .. .	..		..	
<b>SHEPPARTON SHIRE—</b>				
Congupna Road .. .. .	..		1,714 7 6	
Cosgrove Road .. .. .	..		717 2 10	
Graham's Vale Road .. .. .	..		81 13 7	
Nathalia Road .. .. .	..		2,323 6 1	
Pine Lodge Road .. .. .	..		2,481 17 5	
Stover's Road .. .. .	..		200 3 3	
Violet Town Road .. .. .	..		798 18 4	8,317 9 0
<b>SOUTH GIPPSLAND SHIRE—</b>				
Agnes Falls Road .. .. .	..		474 2 2	
Amey's Track .. .. .	..		208 14 10	
Binginwarri South Road .. .. .	..		204 6 9	
Boolarra—Foster Road .. .. .	..		1,903 3 2	
Boy's Road .. .. .	..		1,011 7 1	
Chadwick's Road .. .. .	..		..	
Dollar—Woorarra West Road .. .. .	..		15 0 0	
Foster—Dollar Road .. .. .	..		63 7 4	
Foster—Mount Best Road .. .. .	..		2,890 9 0	
Foster—Stoney Creek Road .. .. .	..	231 7 6	6,312 16 11	
Lawson—Harding Road .. .. .	..		354 8 4	
O'Grady's Ridge Road .. .. .	..		8 7 4	
Rumbug's Hill Road .. .. .	..		2 10 0	
Stony Creek—Dollar Road .. .. .	..		415 15 4	
Toora—Gonyah Road .. .. .	..		1 0 0	
Toora—Wonyip Road .. .. .	..		1 15 0	
Turton's Creek Road .. .. .	..		26 12 10	
Waratah Road .. .. .	..		123 15 1	
Whitelaw's Track .. .. .	..		313 15 4	
Woorarra West Road .. .. .	..		98 8 5	
Yanakie Road .. .. .	..	35 0 0	24 17 7	14,454 12 6
<b>SOUTH GIPPSLAND AND WOORAYL SHIRES (Joint Works)—</b>		266 7 6		
Dollar—Dumbalk Road .. .. .	..		3,843 8 2	3,843 8 2
<b>STAWELL SHIRE—</b>				
Marnoo—St. Arnaud Road .. .. .	..		63 12 9	
Pomonal Road .. .. .	..		408 8 0	472 0 9
<b>SWAN HILL SHIRE—</b>				
Fish Point Road .. .. .	..		1,643 10 11	
Manangatang—Euston Road .. .. .	..		865 5 10	
Manangatang—Ouyen Road .. .. .	..		1,419 6 8	
Manangatang—Prooinga Road .. .. .	..		1,212 4 0	
Nyah South Road .. .. .	..		1,226 12 3	
Prooinga—Piangil Road .. .. .	..		292 12 3	
Tresco Road .. .. .	..		228 18 9	
Tyntynder Road .. .. .	..		240 9 11	
Woorinen Settlement Road .. .. .	..		1,312 4 6	8,441 5 1
Carried forward .. .. .	..	1,943 4 9	..	464,606 12 8

## STATEMENT OF EXPENDITURE IN CONNEXION WITH CONSTRUCTION OF DEVELOPMENTAL ROADS, ETC.—continued.

Municipality and Road.	Acts Nos. 2944 and 2985.		Act No. 3255.	
	Amount.	Total.	Amount.	Total.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Brought forward .. .. .	..	1,943 4 9	..	464,606 12 8
<b>TAMBO SHIRE—</b>				
Basin Road .. .. .	..	..	403 8 1	
Buchan-Gelantipy Road .. .. .	..	..	177 6 9	
Metung Road .. .. .	..	..	3,852 7 10	
Nowa Nowa-Buchan Road .. .. .	..	..	168 16 3	
Nowa Nowa-Buchan-Gelantipy Road .. .. .	..	..	7,265 18 10	
Prince's Highway .. .. .	..	..	3,488 6 4	
Upper Tambo Road .. .. .	..	..	2,310 12 3	17,666 16 4
<b>TOWONG SHIRE—</b>				
Eskdale Road .. .. .	..	..	9 8 11	
Granya-Tallangatta Road .. .. .	..	..	39 2 0	
Murray Valley Road .. .. .	..	..	28 6 8	
Shelley-Jingellic Road .. .. .	..	..	5,381 13 0	
Tallangatta Creek Road .. .. .	..	..	3,261 0 0	
Yabba Road .. .. .	..	..	1,548 3 11	10,267 14 6
<b>TRARALGON SHIRE—</b>				
Balook-Traralgon Road .. .. .	..	..	..	
Callignee Road .. .. .	..	..	186 10 0	
Callignee Factory Road .. .. .	..	..	..	
Traralgon Creek Road .. .. .	..	..	5,964 14 2	
Traralgon-Jeeralang Road .. .. .	..	..	1,103 10 5	
Tyer's Road .. .. .	..	..	1,271 6 6	8,526 1 1
<b>UPPER MURRAY SHIRE—</b>				
Beetoomba Road .. .. .	..	..	1,327 0 4	
Benambra-Corryong Road .. .. .	..	..	2,161 5 11	
Kancobin Road .. .. .	..	..	236 10 0	
Murray Valley Road .. .. .	..	..	1,984 12 4	
Thowgla Road .. .. .	..	..	3,508 8 1	9,217 16 8
<b>UPPER YARRA SHIRE—</b>				
Britannia Creek Road .. .. .	..	..	..	
Don Road .. .. .	..	..	3,510 1 7	
Little Yarra Road .. .. .	..	..	2,399 8 5	
Wood's Point Road .. .. .	..	..	..	
Wood's Point Road (Special Account) .. .. .	..	..	101 5 3	
Woori Yallock-Cockatoo Road .. .. .	..	..	161 7 8	6,172 2 11
<b>VIOLET TOWN SHIRE—</b>				
Harry's Creek Road .. .. .	..	..	2,990 1 11	2,990 1 11
<b>WALPEUP SHIRE—</b>				
Boinka South Road .. .. .	..	..	644 16 6	
Boulka-Timberoo Road .. .. .	..	..	635 0 0	
Boorongie North Road .. .. .	..	..	393 0 0	
Carina North Road .. .. .	..	..	643 7 6	
Cowangie Road .. .. .	..	..	..	
Kattyong Road .. .. .	..	..	164 0 0	
Mildura Road .. .. .	..	..	82 13 9	
Murrayville North Road .. .. .	..	..	402 1 0	
Murrayville South Road .. .. .	..	..	6 11 8	
Nyang South Road .. .. .	..	..	758 0 0	
Ouyen-Kulwin Road .. .. .	..	..	314 18 4	
Ouyen-Patchewollock Road .. .. .	..	..	..	
Ouyen-Tempy Road .. .. .	..	..	41 0 8	
Panitya North Road .. .. .	..	..	634 0 0	
Tiega North-east Road .. .. .	..	..	390 0 0	
Underbool-Namengorook Road .. .. .	..	..	483 0 0	
Walpeup-Patchewollock Road .. .. .	..	..	8 0 3	5,600 9 8
<b>WANGARATTA SHIRE—</b>				
Boorhaman-Springhurst Road .. .. .	..	..	1,386 17 11	1,386 17 11
<b>WARANGA SHIRE—</b>				
Lauderdale Road .. .. .	..	..	2,794 5 4	
Rushworth-Stanhope Road .. .. .	..	..	722 0 0	3,516 5 4
<b>WARRAGUL SHIRE—</b>				
Allambee Estate Road .. .. .	..	..	..	
Bona Vista Road .. .. .	..	..	7,121 3 0	
Darnum-Allambee Road .. .. .	..	..	512 8 0	
Ellinbank Road .. .. .	..	..	..	
Ferndale Road .. .. .	..	..	1,589 9 2	
Ferndale-Strezlecki Road .. .. .	..	..	2,570 4 1	
Korumburra-Seaview Road .. .. .	..	..	942 18 5	
Lardner's Track .. .. .	..	..	..	
Mountain View Road .. .. .	..	..	..	
Mountain View-McDonald's Track .. .. .	..	..	27 0 4	
Nilma-Shady Creek Road .. .. .	..	..	..	
Old Sale Road .. .. .	..	..	532 1 1	
Telegraph Road .. .. .	..	..	..	
Warragul-Lardner Road .. .. .	..	162 11 0	224 9 0	
Warragul-Leongatha Road .. .. .	..	..	882 8 11	
		162 11 0		14,402 2 0
Carried forward .. .. .	..	2,105 15 9	..	544,353 1 0



## APPENDIX F.

## COUNTRY ROADS BOARD.

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS UNDER THE PROVISIONS OF THE COUNTRY ROADS ACT DURING THE YEAR ENDED 30TH JUNE, 1925.

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER MUNICIPALITIES.					
ALBERTON SHIRE— Carrajung—Gormandale Road ..	About 1 mile north of Carrajung ..	..	..	·92	·92
Balook—Traralgon Road ..	Between Pattinson's Saddle and Balook P.O. ..	..	·92	·92	·92
ARARAT SHIRE— Ballarat—Hamilton Road ..	Near Westmere Railway Station ..	·87	·87	..	·87
ARAPILES SHIRE— Horsham—Natimuk—Edenhope Road ..	Natimuk to Wimmera Bridge ..	..	..	..	1·80
AVON SHIRE— Dargo Road ..	.. Township ..	..	·35	..	..
BACCHUS MARSH SHIRE— Gisborne Road ..	From Maffra Shire boundary to Freestone Creek cutting ..	..	..	·93	·93
BALLAN SHIRE— Mount Wallace Road ..	Gorman's Hill ..	..	..	·37	·37
BEECHWORTH SHIRE— Stanley (Barwidgee) Road ..	Ballan ..	1·02	1·02	·90	·90
BENALLA SHIRE— Mansfield—Benalla Road ..	Myrtle Creek Section beyond Stanley ..	·62	·62	·62	Works being carried out by day labour
Sydney Road ..	Reinforced concrete and approaches at Lima Bridges ..	·20	..	..	..
Berwick Shire— Woori Yallock—Pakenham—Kooweerup Road ..	Reinforced concrete Box Culvert at Gorge Bridge, north of Glenrowan ..	·05	..	..	..
Berwick Shire— Prince's Highway ..	At Mount Burnett ..	..	..	·84	·84
Pakenham—Gembrook Road ..	At south boundary of shire ..	..	..	·89	·89
BIRCHIP SHIRE— Benliah Birchip—Wycheproof Road ..	Timber Bridge over Bunyip River, Bunyip ..	..	Bridge	..	..
BORUNG SHIRE— Birchip Road ..	Black's deviation (south end) Upper Pakenham ..	..	..	1·38	·71
Rainbow Road ..	4½ miles east of Birchip ..	·66	·66	·33	·33
Minyip Road ..	Between Allotments 81 and 87, Bangerang ..	..	..	..	·13
Hopetoun Road ..	Between Allotments 144 and 174, Werrigar ..	..	..	..	1·18
Minyip Road ..	From Allotment 28, Yallangip, to 9 and 14 Willenabrina ..	..	..	..	1·50
Dimboola Road ..	From Allotments 141 and 126 to 69A Sheep Hills, Kellalac ..	..	..	..	·90
Hopetoun Road ..	Between Allotment 184, Werrigar, and 64, Warracknabeal ..	..	..	..	·93
Minyip Road ..	Between Allotments 59 and 58, Batchica ..	..	..	..	·14
Dimboola Road ..	Lyle-street, Warracknabeal ..	..	..	..	·41
Hopetoun Road ..	Between Allotments 37 and 36, Werrigar ..	..	..	..	·43
BRIGHT SHIRE— Bright Road ..	Allotments 1 and 6, 125, 43, Batchica ..	..	..	..	·41
BULN BULN SHIRE— Neerim "A" Road ..	Myrtleford—Reforming and metalling from 9,200 to 12,200 feet ..	1	1	·57	·57
Western Port Road ..	At Tarago P.O. ..	..	..	..	1·44
Neerim "B" Road ..	Section 3 ..	..	..	..	1·05
COLAC SHIRE— Colac—Beech Forest Road ..	Neerim North Deviation ..	..	..	..	1·08
CRANBOURNE SHIRE— Westernport Road ..	Methuen's Hill to Kaurerren, Sections 1 to 8—43,100 lineal feet ..	..	..	The whole	4·8
BOROUGH OF CRESWICK— Ballarat—Castlemaine Road ..	Fairchild's Hill, forming and metalling ..	1	1	1	1
DIMBOOLA SHIRE— Rainbow Road ..	Near south boundary 150 cubic yards gravel ..	..	..	·25	·25
Warracknabeal Road ..	From 1 to 3 miles north of Jeparit ..	·74	·74	..	·55
DONALD SHIRE— Donald—Charlton Road ..	About 4½ miles north-east of Dimboola ..	·55	·55	·55	·55
DUNDAS SHIRE— Hamilton—Warrnambool Road ..	3 miles north of Donald ..	·19	·19	·19	·19
Hamilton—Port Fairy Road ..	At from 2¼ to 3¼ and 5 to 5½ miles, and at 7½ miles from Hamilton, Parishes of South Hamilton and Croxton West ..	1·07	..	1·07 Tenders for spalls	1·07 Crushing by shire plant
Hamilton—Portland Road ..	At from 1½ to 2¼, 14½ to 15, and at 18 and 19½ miles from Hamilton, Parishes South Hamilton, Monivae, Byaduk and Warrabrook ..	2·06	..	2·06 Tenders for spalls	2·06 Crushing by shire plant
Hamilton—Mt. Gambier Road ..	At from 1½ to 2·4½ to 5 at 5½, and from 6½ to 7 and from 9½ to 13 miles from Hamilton, Parishes of South Hamilton and Yulecart ..	5·24	3·33	Tenders for spalls and 3·70	3·70 Crushing by shire plant
Hamilton—Horsham Road ..	At from 2½ to 3½, 6½ to 7, 12½ to 13½ miles from Hamilton, Parishes of North Hamilton, Bochara and Redruth ..	1·95	..	Tenders for spalls and ·75 of gravel	·75 Crushing by shire plant
DUNMUNKLE SHIRE— Rupanyup—Murtoa Road ..	At from 17 to 17½ miles from Hamilton, parish of Cavendish ..	·64	..	·64	·64
Stawell—Warracknabeal Road ..	2½ miles East of Murtoa (metalling) ..	·57	·57	·57	·57
FLINDERS SHIRE— Dromana—Sorrento Road ..	1½ miles south-east of Minyip (metal) ..	·57	·57	·57	·57
GOULBURN SHIRE— Goulburn Valley Road ..	Between Rye and Sorrento ..	5	5	..	..
GRENVILLE SHIRE— Cressy Road ..	South of Nagambie ..	2·5	2·5	2·5	2·5
HAMPDEN SHIRE— Prince's Highway ..	At McLennan's, Darnett ..	..	..	Tenders for spalls	Supply of spalls
BOROUGH OF HORSHAM— Natimuk Road ..	Bridge over Emu Creek ..	..	Bridge	..	..
SHIRE OF KARA KARA— St. Arnaud—Donald ..	West of Wilson Street ..	..	..	..	·5
Avoca—St. Arnaud Road ..	Near Sutherlands and Cope Cope ..	13·35	13·35	·58	·58
Ngaire Road ..	Between Redbank and Stuart Mill ..	·60	..	..	..
Carried forward ..	Near St. Arnaud ..	1·23	1·23	1·23	1·23
		41·68	35·36	24·33	40·61



PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>					
	Brought forward .. .. .	41·68	35·36	24·33	40·61
KARKAROO SHIRE—					
Hopetoun—Warracknabeal Road	At Goyura Township .. .. .	·83	·83	..	..
Hopetoun—Rainbow Road ..	2 miles south-west of Hopetoun .. .. .	·64	·64	·64	·64
Rainbow—Beulah—Birchip Road	West of Kenmare. Being carried out by day labour with shire plant	..	..	2·08	2·08
KORONG SHIRE—					
Bendigo—Charlton Road ..	Jersey Hill, Rubble .. .. .	·70	·70	..	..
Serpentine Road .. .. .	Between Coutts and Lancell. 63 chains, gravel ..	·79	·79	·79	·79
	Broadbent and Vanston. 33 chains, gravel ..	·41	·41	·41	·41
	O'Malley's Hill, 40 chains, gravel .. .. .	·50	·50	..	..
	Between Bell and Leach. 64 chains, formation	·80	·80	·80	·80
KORUMBURRA SHIRE—					
Korumburra—Warragul Road	At Strezlecke .. .. .	..	..	1·32	1·32
Bena—Poowong Road .. .. .	Poowong end .. .. .	1·51	1·51	1·51	1·51
LAWLOTT SHIRE—					
Nhill—Kaniva—Border Road ..	Metalling between Kaniva and Lillimur .. .. .	..	·87	·87	·87
Broughton Road .. .. .	Metalling between Kaniva and Broughton .. .. .	..	·39	·39	·39
Yearinga Road .. .. .	Metalling between Kaniva and Yearinga .. .. .	..	·26	·26	·26
South Lillimur Road .. .. .	Metalling between Kaniva and South Lillimur ..	..	·24	·24	·24
LEIGH SHIRE—					
Cressy—Rokewood Road .. .. .	South from Rokewood Railway Station .. .. .	..	..	..	2·45 By day labour
LILLYDALE SHIRE—					
Monbulk Road .. .. .	Silvan to Monbulk, metalling, partly completed 1·59 miles, balance in hand	5	5	5	5
LOWAN SHIRE—					
Goroke Road .. .. .	Between lots 5 and 14·10, &c., Balrootan .. .. .	2·20	2·20	2·20	2·20
MANSFIELD SHIRE—					
Tolmie Road .. .. .	Boxing and metalling, McKenzies to near Broken River .. .. .	..	1·67	1·67	1·67
Mansfield Road .. .. .	Formation and metalling from 5 to 6 miles east of Mansfield	·9	·9	·9	·9
MARONG SHIRE—					
Bendigo—Eddington Road .. .. .	South-east of Lots 6A and 5A2, Section V., Parish of Shelbourne	·77	·77	..	..
Bendigo—Bridgewater Road .. .. .	From Railway Crossing at Derby Station East (constructed by day labour)	..	..	·75	·75
MILDURA SHIRE—					
Wentworth Road .. .. .	Between Deakin Avenue and River Murray .. .. .	3·25	3·25	..	..
Melbourne Road .. .. .	Between 15th Street, Irymple, and Red Cliffs—Yatpool Boundary	5·55	5·55	5·55	..
	" " " " " "	1·10	1·10	..	..
MIRBOO SHIRE—					
Mardan Road .. .. .	Elliots and Campbells, metalling .. .. .	·41	·41	·41	·41
MORWELL SHIRE—					
Prince's Highway .. .. .	Yallourn Road towards Morwell .. .. .	..	..	·51	·51
	Narracan Shire Boundary to Yallourn Road .. .. .	1·56	1·56	1·56	1·56
MOUNT ROUSE SHIRE—					
Ballarat—Hamilton Road .. .. .	Between Dunkeld and 1 mile east of Glenthompson	..	..	1·30	1·30
Hamilton—Dunkeld Road .. .. .	Between Dunkeld and 3 mile post .. .. .	..	..	1·00	1·00
Hamilton—Penshurst Road .. .. .	Between Shire boundary on the south to the 8 mile post on the north	..	..	1·12	1·12
Maroona—Glenthompson Road	Between Glenthompson and the shire boundary	..	..	·31	·31
Penshurst—Caramut Road .. .. .	Between Penshurst and the 15 mile post .. .. .	..	..	·93	·93
MCIVOR SHIRE—					
Kilmore—Heathcote—Bendigo Road	Knowsley .. .. .	..	·72	..	..
NEWHAM AND WOODEND SHIRE—					
Tylden Road (Joint with Kynton Shire)	.. .. .	·5	·5	·5	·5
NUMURKAH SHIRE—					
Shepparton—Numurkah—Cobram Road	Main Street, Township of Strathmerton .. .. .	·93	·93	·93	·93
	North and south, extending south of Katunga Township	·88	·88	·88	·88
	North and south, road extending north of Numurkah Township	·44	·44	·44	·44
	North and south, road extending from Railway Crossing, Strathmerton, southwards	2·25	2·00	2·00	2·00
Numurkah—Nathalia Road .. .. .	Blake Street, Nathalia Township .. .. .	·50	·42	·38	·38
	East and west, road to west of Carlands' Bridge, Parish of Waaia	1·49	1·49	1·49	1·49
Nathalia—Kyabram Road .. .. .	Between First Camel-back Bridge and McCoy's Bridge, Parish of Kotupna	3·00	3·00	3·00	3·00
Echuca—Picola Road .. .. .	Extending to west of Wright's Bridge, Parish of Barmah	·75	·75	·75	·7
OMEQ SHIRE—					
Omeo Road .. .. .	Ensay .. .. .	1	1	1	1
	Mountain Ash .. .. .	5	5	..	..
	Doctor's Flat .. .. .	1	1	..	..
	Benambra .. .. .	1·5	1·5	..	..
Benambra Road .. .. .	.. .. .	..	..	..	..
ORBOST SHIRE—					
Prince's Highway .. .. .	Gravelling on Newmerella Flat .. .. .	..	..	·59	..
OTWAY SHIRE—					
Forrest—Apollo Bay .. .. .	Low Level Bridge over Skene's Creek .. .. .	·01	·01	·01	·01
OXLEY SHIRE—					
Sydney Road .. .. .	Whole length in shire .. .. .	·69	·69	·69	·69
PHILLIP ISLAND AND WOOLAMAI SHIRE—					
Korumburra—Wonthaggi Road	Buffalo Bank .. .. .	·56	·56	·56	·56
Korumburra—Wonthaggi Road	Watson's Hill .. .. .	·64	·64	·64	·64
Wonthaggi—Loch Road .. .. .	Schilling's Hill .. .. .	·43	·43	·43	·43
PORTLAND SHIRE—					
Portland—Casterton Road .. .. .	Near Sawmill between Digby and Hotspur .. .. .	·87	·87	·87	·87
PYALONG SHIRE—					
Kilmore—Heathcote—Bendigo Road	Opposite Torborac P.R. .. .. .	..	·37	·37	·37
RIPON SHIRE—					
Ballarat—Ararat Road .. .. .	3 miles west of Beaufort .. .. .	·60	·60	..	..
ROCHESTER SHIRE—					
Rochester—Bamawm—Prairie Road	Between Allotment 9A, 34 and 37·35, Parish of Rochester West	·41	·41	·41	·41
" " "	Between Allotments 173 and 175B, Parish of Bamawm	·60	·60	·60	(being carried out by day labour) ·60 (being carried out by day labour)
	Carried forward .. .. .	92·01	80·88	73·39	85·98

PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads.			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles
UNDER MUNICIPALITIES— <i>continued.</i>					
	Brought forward .. .. .	92·01	89·88	73·39	85·98
ROCHESTER SHIRE—					
Shepparton-Rochester-Wycheproof Road	Portions between Allotments 18 and 16, Allotments 19A and 14, Allotments 21A and 11, and Allotments 9A and 7, Parish of Rochester West Between Allotments 152, 148 and 149, 172, 171, Parish of Bamawm	1·02	1·02	1·02	1·02
		1·10	1·10	1·10	1·10 (being carried out by day labour)
RODNEY SHIRE—					
Mooroopna—Udera Road ..	2½ miles north of Mooroopna .. .. .	·45	·45	·45	·45
Shepparton-Tatura Road ..	4½ miles west of Mooroopna .. .. .	·44	·44	·44	·44
ROMSEY SHIRE—					
Melbourne Lancefield Road ..	New superstructure to bridge over Bolinda Creek	..	..	New superstructure to bridge	..
ROSEDALE SHIRE—					
Seaspray Road .. .. .	From end of existing gravel, near Murphy's, to Seaspray Township (in 7 sections)	5	5	..	..
Gormandale-Carrajung Road	Gormandale Causeway (construction of timber bridge)	·005	·005	·005	·005
BOROUGH OF ST. ARNAUD—					
Avoca-St. Arnaud Road .. ..	From Napier Street to New Borough boundary	·68	..	..	..
SEYMOUR SHIRE—					
Upper Goulburn Road .. .. .	Reinforced concrete pipe culvert at Paterson's	·01	·01	·01	·01
Seymour-Yea Road .. .. .	Tarring surface of road in Seymour .. ..	·5	·5	·5	·5
SHEPPARTON SHIRE—					
Shepparton-Numurkah Road	South-west from Congupna Road .. .. .	·47	·47	·47	Carried out by day labour
Dookle-Nalinga Road .. .. .	South of Union Church .. .. .	·64	·64	·64	..
SOUTH GIPPSLAND SHIRE—					
Dollar-Stony Creek Road .. ..	Between Leishman's and Dollar (metal) ..	1·45	1·45	1·45	..
Falls Road .. .. .	Between Sheedy's Hill and Synan's (gravel) ..	1·17	1·17	..	..
Welshpool-Boolarra Road .. ..	The "Cliffs," near Welshpool .. .. .	·71	·71	·71	·71
Foster-Boolarra Road .. .. .	Foster-Yarram Road to Deep Creek .. ..	·87	·87	·87	·87
STAWELL SHIRE—					
Navarre Road .. .. .	Near Borough boundary .. .. .	·95	·95	·95	·75
Marnoo Road .. .. .	Metalling .. .. .	1	1	1	·21
Stawell-Warracknabeal .. .. .	Metalling .. .. .	·2	·2	·2	·2
STRATHFIELDSAYE SHIRE—					
Strathfieldsaye Road .. .. .	Timber bridge over Sheepwash Creek .. ..	Bridge	Bridge	Bridge	Bridge
SWAN HILL SHIRE—					
Nyah-Ouyen Road .. .. .	To east of Chinkapook .. .. .	2·03	2·03	In progress by day labour	..
TOWONG SHIRE—					
Onco Road .. .. .	End section of Rocky Cuttings near Tallandoon	1·08	1·08	..	..
Murray Valley Road .. .. .	20 culverts (Cont. 35P.) .. .. .	..	..	..	..
TRARALGON SHIRE—					
Callignee Road .. .. .	Saw Pit Gully Section .. .. .	..	1·13	1·13	1·13
TUNGAMAH SHIRE—					
Yarrawonga-Cobram Road .. ..	On the Sandhills east of Cobram. Contract 166P/3	·47	·47	·47	·47
Yarrawonga-Cobram Road .. ..	3 Sections east of Cobram Railway Station. Contract 166P/4	·57	·57	·57	·57
Numurkah-Tungamah-Wilby Road	West of Tungamah Railway Station, 166P/7	1·5	1·5	1·5	1·5
St. James Road .. .. .	West of St. James Railway Station, 166P/8 ..	·31	·31	·31	·31
UPPER MURRAY SHIRE—					
Tintaldra Road .. .. .	3 miles north of Cudgewa Railway Station ..	1·52	1·52	1·52	1·52
WALPEUP SHIRE—					
Ouyen-Pinaroo Road .. .. .	Between Tiega and Galah .. .. .	·32	·32	·32	·32
	Between Torrita and Underbool .. .. .	·76	·76	·76	·76
	Between Linga and Bolnka .. .. .	·34	·34	·34	·34
BOROUGH OF WANGARATTA—					
Beechworth Road .. .. .	Whole section of road in borough .. .. .	·22	·22	·22	·22
WANNON SHIRE—					
Hamilton-Coleraine-Casterton Road	Construction of reinforced concrete culverts and approaches at Sypotts	..	..	..	..
WARANGA SHIRE—					
Elmore-Colbinabbin Road .. ..	Deviation at McTaggart P.O. .. .. .	1·81 and reinforced concrete culvert	1·81 and reinforced concrete culvert	1·17 and reinforced concrete culvert	1·17 and reinforced concrete culvert
WARRAGUL SHIRE—					
Warragul-Leongatha Road .. ..	Bull Swamp Deviation through C.A. 15, 16, and 4, Parish of Warragul	1·76	1·76	..	..
	Howitt Street, Township of Warragul .. ..	..	Concrete culvert	..	..
WIMMERA SHIRE—					
Horsham-Hamilton (joint with Arapiles)	Bridge (timber) and approaches over the Bungally	..	·19	·19	·19
Horsham-Wal Wal Road .. .. .	Construction between One Tree Road and Warlock Road	..	..	1·23	Being done by day labour
	Claying along metal, north of Burnt Creek ..	..	·40	..	..
WINCHELSEA SHIRE—					
Birregurra-Forrest Road .. ..	Section Hill .. .. .	..	..	·36	·36
WODONGA SHIRE—					
Wodonga-Yaekandandah Road	Kiewa Road Junction to shire boundary .. ..	·69	·69	..	..
WONTHAGGI SHIRE—					
Wonthaggi-Korumburra Road	From Loch-Wonthaggi Road, easterly .. ..	·21	·21	·21	·21
WOORAYL SHIRE—					
Leongatha-Yarragon Road .. ..	Near "Tangle" .. .. .	·41	·41	·41	·41
Inverloch-Leongatha Road .. ..	Near Kelly's and Inverloch Racecourse .. ..	·53	·53	·53	·53
Lower Tarwin Road .. .. .	Near Brown's and Bolton's .. .. .	3·31	3·31	·82	·82
Lower Tarwin Road .. .. .	Near Middle Tarwin and Cashin's .. .. .	·98	·98	·98	·98
Mardan Road .. .. .	Through Hyde's and Evans' .. .. .	..	..	1·55	1·55
WYCHEPROOF SHIRE—					
Wycheproof-Sea Lake Road .. ..	Between Wycheproof and Sea Lake .. .. .	2·63	2·63	2·42	2·42
Wycheproof-Birchip Road .. ..	Between Wycheproof and Birchip .. .. .	1·10	1·10	1·10	1·10
Sea Lake-Ouyen Road .. .. .	.. .. .	·71	·71	·71	·71
Sea Lake-Birchip Road .. .. .	.. .. .	·68	·68	·68	·68
Sea Lake-Woomelang Road .. ..	.. .. .	·38	·38	·38	·38
YARRAWONGA SHIRE—					
Yarrawonga-Cobram Road .. ..	Reforming and metalling from Peg at 9,060 feet to peg at 11,700 feet (Cont. No. 190P/3)	..	·50	·50	·50
YACKANDANDAH SHIRE—					
Dederang Road .. .. .	House Creek-Keegan's Sections .. .. .	1·02	..	..	..
	Bridges and approaches Pioneer (Drapers) ..	·63	..	..	..
	Drapers, Kinchington's Creek .. .. .	2·01	2·01	1·34	1·34
	Totals .. .. .	136·655	134·415	104·925	112·735

## PARTICULARS OF SURVEYS, ETC., ON ROADS DECLARED MAIN ROADS, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD.					
ALEXANDRA SHIRE— Healesville—Alexandra Road	Little River breakaway, two 20-ft. spans, reinforced concrete bridge	0·008	0·008	0·008	0·008
Upper Goulburn Road .. ..	Dry Creek, two 10-ft. span culvert .. ..	0·003	0·003	..	..
" " " " .. ..	Concrete culvert near Christie's, Thornton .. ..	0·003	0·003	..	..
AVON SHIRE— Prince's Highway .. ..	Steel and concrete bridge over Avon River .. ..	0·07	0·07	0·07	0·07
BACCHUS MARSH SHIRE— Bacchus Marsh—Geelong Road	Maddingley Bridge .. ..	0·02	0·02	0·02	0·02
BULLA SHIRE— North-Western Highway .. ..	Survey throughout Shire .. ..	6·50	6·50	..	..
CHARLTON SHIRE— Bendigo Road .. ..	Reinforced bridge over Avoca River at Charlton	0·04	0·04	0·04	0·04
Wycheproof Road .. ..	Alteration bridge approaches and reinforced concrete culvert	..	..	..	..
CORIO AND WERRIBEE SHIRES— Prince's Highway .. ..	Regrading over culverts .. ..	0·17	..	..	..
CRANBOURNE SHIRE— Koo-wee-rup—Pakenham Road	Bridge over Main Canal, timber, four 28-ft. 6-in. spans	0·02	0·02	0·02	0·02
Main Coast Road .. ..	Bridge near Koo-wee-rup over Main Canal .. ..	0·04	0·04	0·04	0·04
DEAKIN AND NUMURKAH SHIRES— Echuca—Picola Road .. ..	Repairs to timber bridge (Stewart's) .. ..	..	..	..	..
DUNDAS SHIRE— Hamilton—Mt. Gambier Road	Reinforced concrete bridge over McKinnon's Creek	0·004	..	..	..
ECHUCA BOROUGH— Echuca West Road .. ..	Over Campaspe River, Crossenvale, timber bridge and concrete culvert	0·004	0·004	0·004	0·004
ELTHAM SHIRE— Eltham—Yarra Glen Road .. ..	Over Diamond Creek, Eltham, reinforced concrete bridge	..	..	..	..
FOOTSCRAY CITY— Prince's Highway .. ..	Experimental coldspray penetration $\frac{1}{4}$ mile west of Stony Creek	..	..	0·10	Day labour 0·10
" " " " .. ..	Experimental Coldfix penetration macadam .. ..	..	..	0·07	Day labour 0·07
GISBORNE SHIRE— North-Western Highway .. ..	Survey through Shire .. ..	9·50	9·50	..	..
KEILOR SHIRE— North-Western Highway .. ..	" " " " .. ..	10·0	10·0	..	..
KYNETON SHIRE— North-Western Highway .. ..	" " " " .. ..	10·0	10·0	..	..
LILLYDALE SHIRE— Main Healesville Road .. ..	At Yarra Grange, concrete culvert, two 9-ft. x 8-ft. spans	0·003	0·003	0·003	0·003
Mt. Dandenong Road .. ..	From Jeeve's Saddle to Olinda Road, re-forming and metalling	..	2·62	0·57	0·57
MARONG SHIRE— North-Western Highway .. ..	Survey through Shire .. ..	10·0	10·0	..	..
MALDON AND METCALFE SHIRES— North-Western Highway .. ..	Survey along boundaries .. ..	9·0	9·0	..	..
METCALFE SHIRE— North-Western Highway .. ..	" " " " .. ..	11·0	11·0	..	..
MELTON SHIRE— North-Western Highway .. ..	" " " " .. ..	5·0	5·0	..	..
MILDURA SHIRE— North-Western Highway .. ..	From Lake Hattah going south, Sections 13, 14, 15, 16	7·5	7·5	7·5	7·50
MORWELL SHIRE— Boolarra—Foster Road .. ..	Sanding Austin's Hill Deviation .. ..	0·80	0·80	0·80	Day labour 0·80
" " " " .. ..	Sanding Bickerton's and Cox's Hills .. ..	2·00	2·00	2·0	2·0
MORWELL AND ALBERTON SHIRES— Boolarra—Wetshpool Road .. ..	Formation Swan's and Savage's Sections .. ..	2·20	2·20	2·2	2·2
MORWELL SHIRE— Jeeralang West Road .. ..	Quarrying, crushing, and metalling from Traralgon-road to Jeeralang School	2·0	2·0	2·0	Day labour 2·0
MULGRAVE SHIRE— Prince's Highway .. ..	Re-sheeting between Box Hill-road and Spring Vale Post Office	0·83	0·83	0·83	Day labour 0·83
NEWHAM AND WOODEND SHIRES— North-Western Highway .. ..	Survey through Shire .. ..	7·50	..	..	..
ORPOST SHIRE— Prince's Highway .. ..	Timber and concrete bridge at Genoa .. ..	0·06	0·06	0·06	Day labour 0·06
" " " " .. ..	Renewal of superstructure Bemm River Bridge	..	..	..	..
OTWAY SHIRE— Forrest—Apollo Bay Road .. ..	Re-sheeting with basalt metal between Forrest and Barramunga, 3 miles	..	3·0	..	Day labour
" " " " .. ..	Metalling from Barramunga southerly towards Apollo Bay	..	2·2	2·2	(metal crushed by contract) 2·2
" " " " .. ..	Metalling Mt. Sabine Deviation .. ..	..	0·9	0·9	(metal crushed by contract) 0·9
" " " " .. ..	Formation and side-cutting south from Experimental Farm	..	0·25	0·25	0·25
" " " " .. ..	Widening existing formation for 3 miles, Mt. Sabine Section	..	3·0	..	Day labour
" " " " .. ..	Formation Mt. Sabine Section .. ..	..	1·5	1·50	1·50
" " " " .. ..	Formation 1·7 miles between Smith's Saddle and Mt. Sabine	1·7	1·7	1·70	1·70
ROCHESTER SHIRE— Bendigo—Echuca Road .. ..	Grubbing and clearing from intersection with Echuca West Road southerly 9,000 lin. feet	1·7	1·7	1·70	1·70
RUTHERGLEN SHIRE— Chiltern—Howlong Road .. ..	Construction of 30-ft. reinforced concrete culvert	0·006	0·006	0·006	0·006
SOUTH BARWON SHIRE— Prince's Highway .. ..	Bridge over Barwon River .. ..	..	..	..	In progress
SWAN HILL SHIRE— Tooleybuc Road .. ..	Bridge at Tooleybuc, approach to new bridge, Victorian side	0·2	0·20	0·20	0·20
					Day labour
	Carried forward .. ..	97·881	103·677	24·791	24·791

PARTICULARS OF SURVEYS, ETC., OF ROADS DECLARED MAIN ROADS, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works.	Lengths of Roads—			
		For which Permanent Surveys have been Made.	For which Plans have been Prepared.	For which Tenders have been Invited.	For which Tenders have been Accepted.
		Miles.	Miles.	Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>					
	Brought forward .. .. .	97·881	103·677	24·791	24·791
TAMBO SHIRE—					
Bruthen-Omeo Road .. .. .	Deviation of main road at St. Patricks	1·50	1·50	1·50	Day labour 1·50
"Prince's Highway" .. .. .	Deviation at Monkey Creek and Mountain Ash	6·0	6·0	6·00	Day labour 6·00
	Repairs to reinforced concrete bridge at North Arm	..	..	..	Day labour ..
TULLAROOP SHIRE—					
Castlemaine-Maryborough Road	Construction of reinforced concrete bridge over Deep Creek and approaches	0·2	0·20	0·20	0·20
UPPER YARRA SHIRE—					
Wood's Point Road .. .. .	Timber bridge at McVeigh's over Yarra ..	0·01	0·01	..	..
WANGARATTA SHIRE—					
Yatta-wonga Road .. .. .	10,300 feet across Ovens River serving Peechelba Soldier Settlement	1·90	1·90	..	..
	Totals .. .. .	107·491	113·287	32·491	32·491

## APPENDIX G.

## COUNTRY ROADS BOARD.

## MAIN ROADS.

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED AND ROADS MAINTAINED DURING THE YEAR ENDED 30TH JUNE, 1925.

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES.</b>				
ALBERTON SHIRE—				
Balook-Traralgon Road ..		General repairs .. .. .	..	4
Carrajung-Gormandale Road ..	928 miles, 1 mile north of Carrajung	" " " " " " " "	92	17.5
Yarram-Pt. Albert Road ..		" " " " " " " "		8.7
Yarram-Boolarra Road ..		" " " " " " " "		18.1
Yarram-Sale Road ..		" " " " " " " "		28
Foster-Yarram Road ..		" " " " " " " "		8
ALEXANDRA SHIRE—				
Healesville-Alexandra Road ..	Formation and metalling	General repairs .. .. .	93	16
Cathkin-Mansfield Road ..	Formation, &c. .. .. .	" " " " " " " "	25	12
Yarck Road ..		Supply of gravel and general repairs ..		2
Upper Goulburn Road ..		General .. .. .		6
ARAPILES SHIRE—				
Horsham-Hamilton Road ..		Seven sections along road to link up		2
Horsham-Natimuk-Edenhope Road ..		Gravelling near Mount Arapiles ..		1.25
ARARAT BOROUGH—				
Ballarat-Stawell Road ..		General maintenance and tar painting ..		3.68
ARARAT SHIRE—				
Ballarat-Hamilton Road ..	Near Westmere Railway Station	Re-building bridge at Wickliffe and general repairs throughout	87	..
Ballarat-Stawell Road ..		Tarring 4 miles near Ararat and general repairs throughout		4
Ararat-Warrnambool Road ..		Tarring 4½ miles, 6 miles from Ararat; gravelling 1.93 miles at Lake Bolac; and general repairs throughout whole length		6.43
Maroona-Glenthompson Road ..		Gravelling .90 mile, 2 miles from Willaura; grading 3 miles between Kiora and Willaura; and general repairs throughout		3.9
Ararat-Elmhurst Road ..		Repairing bridge over Wimmera River on boundary. Gravelling .26 miles about 18 miles from Ararat, and general repairs throughout whole length		.26
AVOCA SHIRE—				
Ballarat-St. Arnaud Road ..		General maintenance over whole length ..		22
Ararat Road ..		" " " " " " " "		7
Landsborough Road ..		" " " " " " " "		2
Bealiba Road ..		" " " " " " " "		9
Maryborough Road ..		" " " " " " " "		5
AVON SHIRE—				
Prince's Highway ..		General repairs throughout whole length, about 26 miles		26
Sale-Maffra Road ..		General repairs throughout whole length, about 2½ miles		2.5
Dargo Road ..		General repairs throughout whole length, about 45 miles		45
BACCHUS MARSH SHIRE—				
Geelong-Bacchus Marsh Road ..		Sheeting, 9M34, Gaynor's Hill ..		.51
Gisborne Road ..		Sheeting, 9M35, Parwan, and general repairs ..		.27
Melbourne-Ballarat Road ..		Gravelling, 9M36 and general repairs ..		.63
General repairs .. .. .				..
BAIRNSDALE SHIRE—				
Prince's Highway ..		General maintenance .. .. .		25
Bulumwaal-Tabberabbera Road ..		" " " " " " " "		8
Bairnsdale-Bruthen Road ..		" " " " " " " "		6
BALLAN SHIRE—				
Mount Wallace Road ..	Near Ballan ..	General maintenance from Ballan to Mt. Wallace	90	10
Ballarat Road ..		General maintenance .. .. .		15
Daylesford Road ..		" " " " " " " "		8
Gordon-Meredith "A" and "B" ..		" " " " " " " "		3
BALLARAT SHIRE—				
Ballarat-Ararat Road ..		Metal sheeting carried out— 1,620 lin. ft. west of old Crown and Sceptre Hotel 1,110 lin. ft. east of White's Bridge 1,308 lin. ft. near Burrumbeet Park 1,080 lin. ft. near Burrumbeet Park State School		.97
Ballarat-Lexton Road ..		Ravelled sections raked in and rolled Between Howitt Street, Wendouree, and the Old Junction Hotel, 3,000 lin. feet metal-sheeted, and 1,520 lin. ft. metal-sheeted near Murray's Bridge, Miner's Rest		1.75 .85
Ballarat-Maryborough Road ..		8,155 lin. ft. metal-sheeted in sections as follows— 2,300 lin. ft. north of Ascot 1,545 lin. ft. south of Ascot 2,100 lin. ft. north of Blowhard Railway Line 2,210 lin. ft. north of Miner's Rest		1.54
BANNOCKBURN SHIRE—				
Geelong-Ballarat Road ..		General maintenance .. .. .		12
Inverleigh Road ..		" " " " " " " "		6
BARRARBOOL SHIRE—				
Angelsea Road ..		General maintenance throughout .. .. .		17
Prince's Highway ..		" " " " " " " "		13
Hendy Main Road ..		" " " " " " " "		10
BEECHWORTH SHIRE—				
Beechworth Road ..		Tarrawingee-Indigo Gap .. .. .		23
Bright Road ..		Rocky Point-Everton-Myrtleford Road ..		1.5
Everton-Myrtleford Road ..		Everton-Myrtleford .. .. .		15
Stanley Road ..		Stanley Section .. .. .		5
		Carried forward .. .. .	8.87	444.34



STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Work Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>				
		Brought forward .. ..	8·5	761·81
CRESWICK BOROUGH—				
Ballarat-Castlemaine Road .. ..		Putting out 300 cubic yards metal on south boundary .. ..		·18
" " " " .. ..		Putting out 150 cubic yards gravel .. ..		·25
" " " " .. ..		Tarring metal .. ..		·18
" " " " .. ..		Re-sheeting with metal sections near Forest Hill, near Kingstoa, at Smeaton, Marshall's Corner to Moorookyle, and near Stoney Rises .. ..		·75
" " " " .. ..		General repairs with gravel—Sulky to Borough boundary, and Borough to Spring Mount .. ..		4
Daylesford-Ballarat Road .. ..		General repairs to ruts and pot holes .. ..		·15
" " " " .. ..		Re-sheeting with metal sections between Newlyt and Blampied, and at Dean .. ..		·3
" " " " .. ..		General repairs to ruts .. ..		·12
DANDENONG SHIRE—				
Prince's Highway .. ..		Between Springvale and Dandenong .. ..		5
Cheltenham Road .. ..		Between Dandenong and West Boundary Road .. ..		6
DAYLESFORD BOROUGH—				
Ballarat Road .. ..		General maintenance only .. ..		·8
Ballan Road .. ..		Scarifying, and re-forming, and general maintenance .. ..		·22
Malmsbury Road .. ..		Re-sheeting .. ..		·1
Castlemaine Road .. ..		General maintenance only .. ..		1
Daylesford-Hepburn Road .. ..		Scarifying, re-forming, and treating surface with tar .. ..		·53
DEAKIN SHIRE—				
Kyabram-Nathalia Road .. ..		Commencing 3½ miles north of Kyabram, thence north .. ..		2
Kyabram-Tonaglia Road .. ..		Commencing 1½ miles south of Tongala, thence south on east-west road between Parishes of Tongala and Kyabram .. ..		2·
Rochester-Kyabram Road .. ..		In parishes of Kyabram and Timmering .. ..		1·
Rochester-Kyabram (Jt. Rodney) .. ..		On Kyabram-Rushworth Road, starting 1 mile south of Kyabram corner .. ..		·31
Echuca-Cornelia Road .. ..		Parish of Echuca South .. ..		·24
DIMBOOLA SHIRE—				
Rainbow Rises Road .. ..		About 1 mile east of Outlet Creek .. ..		·33
Rainbow Road .. ..		Between Antwerp and Tarranyurk .. ..		·15
Nhill Road .. ..		Between Dimboola and Lochiel .. ..		·12
Hopetoun-Rainbow Road .. ..		About 3 miles north of Rainbow .. ..		·25
DONALD SHIRE—				
Donald-Charlton Road .. ..	3 miles north of Donald	2½ miles north of Donald .. ..	·19	3
" " " " .. ..		3 miles to 6 miles north of Donald, re-sheeting .. ..		4·25
St. Arnaud-Birchip Road .. ..		North-west and south-east of Donald, re-sheeting and binding .. ..		2·25
DONCASTER SHIRE—				
Doncaster Road .. ..		General repairs .. ..		2
Heidelberg-Warrandyte Road .. ..		" " " " .. ..		1
Warrandyte-Ringwood Road .. ..		" " " " .. ..		1·07
DUNDAS SHIRE—				
Hamilton-Warmambool Road .. ..		At from 2½ to 3½, and 5 to 5½, and 7½ miles from Hamilton, Parishes of South Hamilton, and Croxton West .. ..		2·06
Hamilton-Port Fairy Road .. ..		At from 1½ to 2½, 14½ to 15, and at 18 and 19½ miles from Hamilton, Parishes of South Hamilton, Monivae, Byaduk, and Warrabkook .. ..		2·61
Hamilton-Portland Road .. ..	Between 9½ and 13 miles from Hamilton, parish of Yulecart	At from 1½ to 2, 4½ to 5, at 5½, and from 6½ to 7 miles from Hamilton, Parishes of South Hamilton and Yulecart .. ..		1·95
Hamilton-Mount Gambier Road .. ..		At from 2½ to 3½, 6½ to 7, 12½ to 13½ miles from Hamilton, Parishes of North Hamilton, Bochara, and Redruth .. ..		·64
Hamilton-Horsham Road .. ..		At from 17 to 17½ miles from Hamilton, Parish of Cavendish .. ..		2·5
DUNMUNKLE SHIRE—				
Rupanyup-Murtoa Road .. ..	·57 miles of metalling east of Murtoa	General maintenance 2·5 miles gravel .. ..	·57	1·38
" " " " .. ..		General maintenance 1·38 miles metal .. ..		5
" " " " .. ..		General maintenance 5 miles earth formation .. ..		2·3
Minyip-Donald Road .. ..		General maintenance 2·3 miles metal .. ..		·38
Stawell-Warracknabeal Road .. ..	·57 miles metal south-east of Minyip	·38 miles gravel south of Rupanyup .. ..	·57	1
" " " " .. ..		General repairs and maintenance to—		4·5
" " " " .. ..		1 mile gravel .. ..		20
" " " " .. ..		4½ miles metal .. ..		·10
" " " " .. ..		20 miles earth formations .. ..		·05
EAST LODDON SHIRE—				
Prairie-Borong Road .. ..	Metal and formation, Prairie	Gravelling and rolling near Prairie .. ..	·34	·2
Mitiamo Road .. ..		Gravelling and rolling near Mitiamo .. ..		1·6
Dingee Road .. ..		Gravelling and rolling near Dingee .. ..		·
ECHUCA BOROUGH—				
Echuca West Road .. ..		General repairs .. ..		·
ELTHAM SHIRE—				
Eltham-Yarra Glen Road .. ..		General maintenance, Plenty Bridge to Yarra Glen .. ..		·
Hurstbridge-Kinglake Road .. ..		General maintenance Hurstbridge to Kinglake .. ..		·
EUROA SHIRE—				
Arcadia Road .. ..	Formation, metalling, &c.	Repairs to roadway .. ..	1·3	4
Murchison-Shepparton Road .. ..		" " " " .. ..		6
Euroa-Arcadia Road .. ..		Repairs to roadway and sheeting with gravel .. ..		16
Euroa-Strathbogie Road .. ..	Formation, side-cutting, &c.	" " " " .. ..	·25	·18
Euroa-Mansfield Road .. ..		Repairs to roadway " " " " .. ..		20
Sydney Road .. ..		" " " " .. ..		15
FERNTREE GULLY SHIRE—				
Main Ferntree Gully Road .. ..		Wheeler's Hill to Belgrave (Tar-spraying 4 miles Belgrave to Upper Ferntree Gully included herein) .. ..		10·81
Monbulk Road .. ..		Belgrave to Monbulk .. ..		5
Olinda Road .. ..		Upper Ferntree Gully to Olinda .. ..		6·25
FLINDERS SHIRE—				
Hastings-Flinders Road .. ..	At Merrick's Railway Station .. ..	Bittern to Flinders .. ..	·64	14
Mornington-Flinders Road .. ..	Between Chapman's and Wiseman's .. ..	Hearne's Bridge to Hastings-Flinders Road .. ..	·5	12
Dromana Road .. ..		Moat's Corner to Dromana .. ..		4
Dromana-Sorrento Road .. ..		Dromana to Sorrento .. ..		12·5
FRANKSTON AND HASTINGS SHIRE—				
Point Nepean Road .. ..		Repairs and bitumen painting .. ..		7
Frankston-Hastings Road .. ..		" " " " .. ..		8
		Carried forward .. ..	15·47	988·2





STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works. Miles.	Maintenance. Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>				
		Brought forward .. ..	26·14	1,294·16
KOWREE SHIRE—		Sheeting and general maintenance .. ..		·58
Boorookpi Road .. ..		General maintenance .. ..		28
Edenhope—Goroke Road .. ..		" .. ..		41
Hamilton—Edenhope—Apsley Road .. ..		" .. ..		
KYNETON SHIRE—		General repairs .. ..		·81
Melbourne—Bendigo Road .. ..		" .. ..		·22
Redesdale Road .. ..		" .. ..		·7
Trentham Road .. ..		" .. ..		·11
Tylden—Woodend Road .. ..		" .. ..		
LAWLOTT SHIRE—		Generally throughout .. ..	·87	4
Nhill—Kaniva—Border Road .. ..	Metalling between Kaniva and Lillimur	" .. ..	·39	1·25
Broughton Road .. ..	Metalling between Kaniva and Broughton	" .. ..	·26	1
Yearinga Road .. ..	Metalling between Kaniva and Yearinga	" .. ..	·24	1
South Lillimur Road .. ..	Metalling between Kaniva and South Lillimur	" .. ..		
LEIGH SHIRE—		Between Shelford and Teesdale .. ..		1·03
Bannockburn—Shelford Road .. ..		Pot holes and drains along full length .. ..		6
Inverleigh—Shelford Road .. ..		Near Blair's Hill .. ..		·83
Shelford—Rokewood Road .. ..		General dressing up and repairs .. ..		6·66
Cressy—Rokewood Road .. ..		" .. ..		2·4
Werneth Road .. ..		" .. ..		
LEXTON SHIRE—		Between Lexton and Wanbra .. ..		·6
Ballarat—Avoca Road .. ..		Adjoining Amphitheatre Township .. ..		·4
Ararat—Avoca Road .. ..				
LILYDALE SHIRE—		Healesville Road to Woori Yallock Creek .. ..		8
Warburton Road .. ..		Ringwood to Yarra River .. ..		18
Healesville Road .. ..		Healesville Road to Yarra Glen .. ..		4·75
Yarra Glen Road .. ..		Ringwood to top of mountain .. ..		8
Mount Dandenong Road .. ..		Mount Evelyn to Monbulk .. ..	1·59	8
Monbulk Road .. ..	Metalling Silvan to Monbulk (partly completed)			
LOWAN SHIRE—		Between Lots 5, 14, 10, &c., Balrootan .. ..	2·2	
Goroke Road .. ..		Between Lots 149 and 148, Woorak .. ..		·11
Lorquon West Road .. ..		Between Lots 134 and 73, Woorak .. ..		·25
" .. ..		Between Lots 66 and 67, Woorak .. ..		·22
Yanac Road .. ..		Between Lots 90 and 91, Tarranginnie .. ..		·37
" .. ..		Between Lots 125 and 223, Tarranginnie .. ..		·20
MAFFRA SHIRE—		Re-forming and gravelling at Valencia Creek .. ..		·51
Bushy Park—Valencia Creek .. ..				
MALDON SHIRE—		Gravelling—Gowar, Muckleford and Castlemaine .. ..		1·56
Castlemaine—Maldon Road .. ..		Gravelling near Sandy Creek .. ..		·56
Newstead Road .. ..		Metalling—Maldon, Baringhup East, Dennis's and Baringhup .. ..		1·04
Baringhup Road .. ..		Metalling, gravelling and general repairs—Maldon, Sandhills and Eddington .. ..		14
Maldon—Eddington Road .. ..		Filling holes with gravel .. ..		·8
Castlemaine—Newstead Road .. ..				
MANSFIELD SHIRE—		General maintenance .. ..		6
Tolmie Road .. ..		" .. ..		38
Mansfield Road .. ..	Formation and metalling from 5 to 6 miles east of Mansfield	" .. ..	·9	
Merton—Enroa Road .. ..		" .. ..		4
Jameson Road .. ..		" .. ..		13
Mansfield—Woods Point Road .. ..		" .. ..		9
MARONG SHIRE—		Bendigo City boundary to Big Hill, sections tar and bitumen treated .. ..		1·5
Bendigo—Melbourne Road .. ..		General maintenance .. ..		5
Bendigo—Eddington Road .. ..		" .. ..		26·3
Bendigo—Bridgewater Road .. ..	From railway crossing at the Derby Station, eastwards	Re-construction at Bausell's, between Marong and Leichardt .. ..	·77	·57
Bendigo—Serpentine Road .. ..		General maintenance .. ..		21·5
		Re-construction at Campbell's Forest .. ..		·28
		Construction at Daly's Forest .. ..		·10
		General maintenance .. ..		8·25
MARYBOROUGH BOROUGH—		Re-sheeting with metal .. ..		·10
Eddington Road .. ..				
MELTON SHIRE—		From Melton to Toolern .. ..		6
Toolern Road .. ..		Portions between Deep Creek and Rockbank .. ..		11
Ballarat Road .. ..				
MILDURA SHIRE—		Re-surfacing with bitumen between Irymple and Sandalong Avenues .. ..	·54	1
Irymple Road .. ..	Metalling between Karadoc and Etiwanda Avenues and construction of 19 concrete culverts over irrigation channels	" .. ..		
" .. ..	Construction of multiple pipe culvert over main irrigation channels at Morpung Avenue	" .. ..		
Deakin Avenue .. ..	Construction of multiple pipe culvert between 14th and 15th Streets	Re-surfacing with bitumen between 14th and 15th Streets .. ..		·8
Wentworth Road .. ..	Construction of 4 concrete syphons on irrigation channels	" .. ..		
" .. ..	Formation in 17th Street and Mena Road	Re-surfacing with bitumen between 15th and 16th Streets .. ..	3·08	·25
" .. ..	Metalling between Channel Road and 5th Street	Re-forming in Main Avenue .. ..	·7	·75
MILDURA TOWN—				
Melbourne Road .. ..	Deakin Avenue, from 10th to 14th Streets, bitumen spraying	" .. ..	1	
Mildura Road .. ..	Langtree Avenue from 7th to 8th Streets, bitumen spraying	" .. ..	·14	
MINHAMITE SHIRE—		Re-sheeting with metal .. ..		2
Hamilton—Macarthur—Port Fairy Road .. ..		" .. ..		2·19
Warrnambool—Hawkesdale—Penshurst Road .. ..				
MIRBOO SHIRE—		General maintenance over whole length .. ..	·73	5·55
Mirboo—Allambee East Road .. ..	Mirboo North to Wenke's, metalling	" .. ..		5
Mirboo—Leongatha Road .. ..	" .. ..	" .. ..		5
Mardan Road .. ..	" .. ..	General maintenance, including sections re-sheeted .. ..		8·5
Mirboo South Road .. ..	" .. ..			
MOORABBIN SHIRE—		General repairs, bitumen spraying .. ..		3·19
Point Nepean Road .. ..		Metalling and tar painting .. ..		2·06
Centre Dandenong Road .. ..				
		Carried forward .. ..	39·55	1,644·61



## STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed	
	Permanent Works.	Maintenance.	Permanent Works. Miles.	Maintenance. Miles.
<b>UNDER MUNICIPALITIES—continued</b>				
		Brought forward	53·38	2,054·29
PRESTON TOWN— Whittlesea Road		Maintenance throughout		
PYALONG SHIRE— Kilmore-Heathcote-Bendigo Road		General repairs		14
QUEENSCLIFFE BOROUGH— Queenscliffe-Geelong Road		2,000 lineal feet reconstructed and general repairs		
RINGWOOD BOROUGH— Ringwood-Warrandyte Road		Whitehorse Road to Wonga Road		73
RIPON SHIRE— Ballarat-Ararat Road		Generally throughout		22
		" " "		18
		" " "		17
ROCHESTER SHIRE— Ballarat-Hamilton Road		" " "		
ROCHESTER SHIRE— Rochester-Bamawm-Prairie Road	Between Allotments 9A, 34 and 37·35, Parish of Rochester West	From Rochester to Waranga Channel	41	2·5
	Between Allotments 173 and 175B, Parish of Bamawm	" " "	6	
	Portions between Allotments 18 and 19, 19A and 14, 21A and 11, and 9A and 7, Parish of Rochester West	" " "	1·02	
Shepparton-Rochester-Wycheproof Road	Between Allotments 152, 148 and 149, 172, 171, Parish of Bamawm	" " "	1·1	
" " "		From Rochester to Shire boundary		4·2
Rochester-Timmering Road		General maintenance over whole length		1
RODNEY SHIRE— Kyabram-Nathalia Road		Between Merrigum and Kyabram		8·5
Tatura-Byrneside-Kyabram Road		Between Tatura and Merrigum		2·03
" " "		Full length		1
Kyabram-Tongala Road		South from Tatura		1·02
Tatura-Murchison Road		At south end		·86
Shepparton-Tatura Road	4½ miles west of Mooroopna	Generally throughout	44	7·46
Mooroopna-Undera Road	2½ miles north of Mooroopna	North from Mooroopna	45	1·82
" " "		At North Mooroopna		1·14
ROMSEY SHIRE— Melbourne-Lancefield Road		General		15·85
Woodend-Lancefield Road		" " "		5·62
Lancefield-Kilmore Road		" " "		9·71
Lancefield-Kilmore Road (joint with Kilmore Shire)		" " "		2·25
ROSEDALE SHIRE— Gormandale-Carrajuj Road	Construction of bridge on causeway at Gormandale	General throughout	·005	
Prince's Highway		Construction of culvert at Shiel's Gully, Rosedale. General maintenance throughout		·002
Prospect Road		Generally throughout		
Sale-Yarram Road		" " "		
Traralgon-Gormandale Road		" " "		
Willung Road		" " "		
Seaspray Road		" " "		
RUTHERGLEN SHIRE— Chiltern-Howlong Road		Generally throughout		4·5
Rutherglen-Wahgunyah Road		" " "		5·25
Springhurst-Rutherglen Road		" " "		7·31
Sydney Road		" " "		1·27
Wodonga Road		" " "		10·25
Yarrowonga Road		" " "		14·3
ST. ARNAUD BOROUGH— Avoca-St. Arnaud Road		Contract work in hand		·68
		Tarring Napier Street		·48
St. Arnaud-Donald Road		Tarring Napier Street		·35
		General maintenance		1·58
St. Arnaud-Charlton Road		" " "		·5
Navarre Road		" " "		·92
SALE TOWN Prince's Highway		From Rosedale Shire boundary to Sale P.O.		1
Sale-Longford Road		From Sale P.O. to Swing Bridge		3
SEYMOUR SHIRE— Goulburn Valley Road		Repairing portion from Lobb's to Hughes Creek		5
Sydney Road		General repairs throughout		20
Seymour-Yea Road		Tarring surface of road		·5
Upper Goulburn		Culvert at Patterson's		·01
" " "		General repairs		10
SHEPPARTON SHIRE— Shepparton-Numurkah Road	Forming and metalling south-west from Congupna Road	Generally throughout	47	13·1
Dookie-Nalinga Road	Forming and metalling south of Union Church	" " "	64	9·3
Shepparton-Nagambie Road	Forming and metalling	" " "	1·77	10·25
" " "	Construction of timber bridge at Jones' Creek	" " "	·01	
" " "	Construction of timber bridge at Seven Creeks	" " "	·01	
Shepparton-Mooroopna Road	From Shepparton westwards towards Mooroopna	Generally throughout		·6
Shepparton-Nalinga Road		" " "		18·62
SOUTH BARWON SHIRE— Prince's Highway		From Barwon Bridge to Shire boundary at Wauru Ponds Bridge		4
Barwon Heads Road		From junction of road with Prince's Highway to Barwon Heads		12·8
SOUTH GIPPSLAND SHIRE— Foster-Yarram Road		Generally throughout		21
Dollar-Stony Creek Road		" " "		10
Toora-Gunyah Road		" " "		10
Falls Road		" " "		5
Foster-Boolarra Road	Between Foster-Yarram Road and Deep Creek	" " "	57	15
Welshpool-Boolarra Road	"The Cliffs," near Welshpool	" " "	71	7
STAWELL BOROUGH— Ararat-Stawell Road	Metalling London Road	London Road and Patriek Street throughout	1·13	1·54
Glenorchy Road		Repairs throughout		1·36
STAWELL SHIRE— Navarre Road		20 miles general repairs		20
Marnoo Road		30 chains and repairs over whole length		10·25
Stawell-Glenorchy-Horsham Road		Repairs		
Ballarat-Stawell Road		Repairs generally		15
		Carried forward	62·715	2,462·722

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>				
		Brought forward .. ..	62·715	2,462·722
STRATHFIELDSAYE SHIRE— Strathfieldsaye Road .. ..	Timber bridge over Sheepwash Creek	Contract 158M/38—Pieper's Hill .. ..	Bridge	·15
.. ..	.. ..	.. 158M/39—Near Hay's and Milling's properties	..	·15
.. ..	.. ..	.. 158M/40—Near McMahon's and Tower-villes	..	·2
.. ..	.. ..	.. 158M/45—Near Shire Hall .. ..	..	·1
Mandurang Road .. ..	.. ..	.. 158M/41—Conway's Hill .. ..	..	·05
Bendigo-Heathcote Road .. ..	.. ..	.. 158M/42—Bull's Head to Bennett's .. ..	..	·3
.. ..	.. ..	.. 158M/43—Bennett's to Axedale Station .. ..	..	·3
.. ..	.. ..	.. 158M/44—Axedale Township .. ..	..	·05
.. ..	.. ..	.. 158M/46—Grand View Hill .. ..	..	·8
SWAN HILL SHIRE— Euston Road .. ..	Construction through Nyah Set- tlement	New formations and general maintenance .. ..	2·39	29·25
Murray Valley Road .. ..	.. .. at Burra Swamp .. ..	.. ..	1·59	..
Tooleybuc Road .. ..	Construction of approach to bridge	.. ..	·23	..
Nyah-Ouyen Road .. ..	.. ..	Blue metalling between Nyah and Nyah West, and general maintenance	..	26·
Swan Hill Road .. ..	.. ..	New formations and general maintenance .. ..	..	14·94
Ultima Road .. ..	.. ..	.. ..	..	14·40
Ultima-Sea Lake Road .. ..	.. ..	General maintenance throughout .. ..	..	10·
TALBOT SHIRE— Maryborough-Ballarat Road .. ..	.. ..	Gravelling sections between Dunach and Clunes	..	·5
TAMBO SHIRE— Prince's Highway .. ..	Swan Reach to Lakes Entrance	Nicholson to Swan Reach .. ..	8·	6·
Bruthen-Omeo Road .. ..	.. ..	Bruthen to Ramrod Creek .. ..	..	4·
Nowa Nowa-Buchan-Gelantipy road .. ..	.. ..	Canal Creek to Gelantipy .. ..	..	15·
Bairnsdale-Bruthen Road .. ..	.. ..	Bruthen to Nicholson River .. ..	..	2·
TOWONG SHIRE— Omeo Highway .. ..	.. ..	2,530 cubic yards of gravel supplied and repairs effected between Huon and Lightning Creek	..	70·
Murray Valley Road .. ..	Culverts only .. ..	850 cubic yards of gravel supplied and main- tenance repairs (including 2 miles of road grader work—new formation)	..	45·
TRARALGON SHIRE— Traralgon-Jeeralang Road .. ..	.. ..	From Traralgon Township along Traralgon Creek	..	7·5
Prince's Highway .. ..	.. ..	Morwell Shire to Rosedale Shire .. ..	..	14·5
TULLAROOP SHIRE— Avoca Road .. ..	.. ..	Supply ironstone spalls .. ..	..	·5
Ballarat Road .. ..	.. ..	.. ..	..	·25
Castlemaine Road .. ..	.. ..	Re-sheeting with metal .. ..	..	1·5
Eddington Road .. ..	.. ..	Supply of ironstone spalls .. ..	..	·25
Natte Yallock Road .. ..	.. ..	Re-sheeting with metal .. ..	..	·35
TUNGAMAH SHIRE— Yarrawonga-Cobram Road .. ..	East of Cobram .. ..	.. ..	·47	..
UPPER MURRAY SHIRE— Corryong Road .. ..	.. ..	Patching, general repairs, and draining .. ..	..	15·
Tintaldra Road .. ..	Contracts 168P/48 and 49—3 miles north of Cudgewa Railway Sta- tion	.. ..	·35	14·
UPPER YARRA SHIRE— Main Warburton Road .. ..	.. ..	General maintenance .. ..	..	12·5
Don Road .. ..	.. ..	Re-sheeting, Warburton .. ..	..	·18
.. ..	.. ..	General repairs .. ..	..	1·
VIOLET TOWN SHIRE— Sydney Road .. ..	Formation and gravelling .. ..	General repairs and construction of culverts .. ..	1·25	12·
Violet Town-Dookie Road .. ..	.. ..	.. ..	·5	16·
Violet Town-Shepparton Road .. ..	.. ..	.. ..	..	4·
WALPEUP SHIRE— Ouyen-Pinaroo Road .. ..	Between Tiega and Boinka .. ..	Between Ouyen and South Australian border .. ..	1·42	7·3
Ouyen-Sea Lake Road .. ..	.. ..	Between Ouyen and Eastern Shire boundary .. ..	..	·45
WANGARATTA BOROUGH— Sydney Road .. ..	Tutty's Bridge and approaches between pegs 16350 and 16700	Scarifying and sheeting 6 inches deep and tar painting with two coats between 6,500 and 9,100	·06	·49
.. ..	.. ..	Top dressing and tar-painting with two coats between 9,100 and 12,800	..	·62
.. ..	.. ..	General maintenance of whole road .. ..	..	5·39
Beechworth Road .. ..	Flood crossing and metalling whole portion in borough	General maintenance of bridges and culverts .. ..	..	·22
WANGARATTA SHIRE— Main Yarrawonga Road .. ..	.. ..	Spreading maintenance gravel along portions throughout	..	..
Main Sydney Road .. ..	.. ..	Spreading maintenance gravel between Towser and Springhurst	..	..
Main Beechworth Road .. ..	.. ..	Spreading maintenance gravel between Tarra- wungee and Everton	..	..
WANNON SHIRE— Wannon Bridge Road .. ..	.. ..	Metalling .. ..	..	·6
Coleraine-Harrow Road .. ..	.. ..	Metalling and gravelling .. ..	..	2·7
Hamilton-Casterton Road .. ..	.. ..	Gravelling .. ..	..	1·
WARANGA SHIRE— Murchison Rushworth Road .. ..	.. ..	General maintenance .. ..	..	16·
Tatura Road .. ..	.. ..	General maintenance at Murchison .. ..	..	·97
Elmore-Colbinabbin Road .. ..	Fencing deviation at McTaggart .. ..	General maintenance .. ..	1·81	9·
Colbinabbin-Corop Road .. ..	.. ..	General maintenance at Colbinabbin .. ..	..	2·
Heathcote-Elmore Road .. ..	.. ..	General maintenance between Toolleen and El- more	..	17·
WARRAGUL SHIRE— Warragul-Korumburra Road .. ..	From Gourley's to Brew's, through allotments 8 and 7B, Parish of Allambee	.. ..	1·35	..
.. ..	At Strezlecki Railway Station (Jt.)	.. ..	·26	..
Warragul-Leongatha Road .. ..	.. ..	Bull Swamp (opposite C.A. 16, Parish of War- ragul)	..	·2
Prince's Highway .. ..	.. ..	Generally throughout .. ..	..	7·
WARRNAMBOOL SHIRE— Prince's Highway .. ..	.. ..	Bitumen carpeting .. ..	..	5·
.. ..	.. ..	Water bound macadam sheeting .. ..	..	5·
Mortlake-Warrnambool Road .. ..	.. ..	.. ..	..	6·
Allansford-Nirrandra Road .. ..	.. ..	.. ..	..	3·
.. ..	.. ..	Gravel sheeting .. ..	..	3·
Garvoc-Lang Road .. ..	.. ..	Water-bound macadam sheeting .. ..	..	·5
WERRIBEE SHIRE— Prince's Highway .. ..	.. ..	Werribee Township Section .. ..	..	·99
Geeelong-Bacchus Marsb Road .. ..	.. ..	From Werribee Road towards Balliang Creek .. ..	..	·75
		Carried forward .. ..	82·395	2,896·122

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>				
		Brought forward .. ..	82·395	2,896·122
WHITTLESEA SHIRE— Epping-Woodstock Road .. ..	.. .. .	General maintenance of tarred surface from Preston boundary at Thomastown to Woodstock	..	10·5
Main Whittlesea Road .. ..	.. .. .	From Bundoora to Whittlesea Railway Station Sheeting and patching—Sections at Janefield and Yan Yean and general maintenance Tar dressing—1 mile at South Morang Bitumen dressing—2 miles South Morang to Mernda, 2 miles Yan Yean towards Whittlesea	..	14·
Whittlesea-Kinglake Road .. ..	Contract 181P/7—6th and 7th sections (Scrubby Creek deviation)	From Whittlesea Station to Scrubby Creek deviation—general maintenance	1·92	5·
.. .. .	From end of Contract 181P/7 to end of Scrubby Creek deviation	.. .. .	·11	..
Whittlesea-Wallan Road .. ..	.. .. .	From Whittlesea township to Shire boundary—General maintenance	..	5·75
WIMMERA SHIRE— Horsham-Dimboola Road .. ..	.. .. .	Machine work (re-forming) .. ..	..	5·71
.. .. .	.. .. .	Gravelling east of 84 and 85, Vectis East .. ..	..	·19
Horsham-Hamilton .. ..	.. .. .	Bungalally Bridge and approaches .. ..	..	1·
Horsham-Natimuk Road .. ..	.. .. .	Sheeting with metal .. ..	..	·3
Horsham-Wal Wal Road .. ..	.. .. .	Re-forming with machine .. ..	..	·42
.. .. .	Construction between One Tree Road and Wartook Road	Sheeting east of the borough boundary .. ..	·5	..
.. .. .	Metalling and gravelling through allotment 8, Drung Drung (on deviation)	.. .. .	·69	..
WINCHELSEA SHIRE— Birregurra-Forrest Road .. ..	Section Hill .. ..	.. .. .	·36	..
Lorne Road .. ..	.. .. .	Near Dean's Marsh .. ..	..	·25
Prince's Highway .. ..	.. .. .	In sundry short lengths .. ..	..	·7
WODONGA SHIRE— Murray Valley Road .. ..	.. .. .	Generally throughout .. ..	..	1·1
Sydney Road .. ..	.. .. .	.. .. .	..	8·93
Tallangatta Road .. ..	.. .. .	.. .. .	..	11·7
Wodonga-Yackandandah Road .. ..	.. .. .	.. .. .	..	3·25
WONTHAGGI BOROUGH— Wonthaggi-Inverloch Road .. ..	.. .. .	McKenzie to Borough boundary .. ..	..	2·5
Wonthaggi-Korumburra Road .. ..	.. .. .	Loch-Wonthaggi road to Borough boundary ..	..	·75
Loch-Wonthaggi road .. ..	.. .. .	Railway crossing, McKenzie-Street to Borough boundary	..	·75
WOORAYL SHIRE— Main Gippsland South Road .. ..	.. .. .	Generally throughout .. ..	..	..
Wild Dog Valley Road .. ..	.. .. .	.. .. .	..	..
Leongatha-Yarragon Road .. ..	.. .. .	.. .. .	..	..
Warragul-Leongatha Road .. ..	.. .. .	.. .. .	..	..
Inverloch-Leongatha Road .. ..	.. .. .	.. .. .	..	..
Inverloch-Wonthaggi Road .. ..	.. .. .	.. .. .	..	..
Lower Tarwin Road .. ..	.. .. .	.. .. .	..	..
Farmer's Road .. ..	Near Brown's and Bollom's .. ..	.. .. .	·82	..
Mardan Road .. ..	.. .. .	.. .. .	..	..
WYCHEPROOF SHIRE— Wycheproof-Sea Lake Road .. ..	North and south of Dumosa .. ..	At Warne Bridge, south of Dumosa, north of Nullawill, and north of Berriwillock Formation near Thalia .. ..	·66	1·76
Wycheproof-Birchip Road .. ..	.. .. .	.. .. .	..	1·37
Wycheproof-Corack Road .. ..	.. .. .	.. .. .	..	..
Sea Lake-Ouyen Road .. ..	.. .. .	North of Sea Lake .. ..	..	·42
Sea Lake-Birchip Road .. ..	.. .. .	South of Sea Lake .. ..	..	·33
YARRAWONGA SHIRE— Wangaratta-Yarrawonga Road .. ..	.. .. .	Re-grading and gravelling from Crawford's Corner to Peechelba Township .. ..	..	18·
Yarrawonga-Cobram Road .. ..	.. .. .	Re-grading from Pierce's Corner to the Burramine North State School .. ..	..	6·
Yarrawonga-Rutherglen Road .. ..	.. .. .	Surfacing and repairs to the approaches to the Ovens Bridge .. ..	..	·2
YACKANDANDAH SHIRE— Dederang Road .. ..	Briggs' to Ivers' .. ..	Yackandandah to Dederang .. ..	1·32	26·
Gundowring Road .. ..	Springvale section .. ..	Huon R. S. Keegan's bridge and Tawanga ..	1·81	30·
Yackandandah-Wodonga Road .. ..	Allan's Flat Road junction .. ..	Indigo Gap-Baranduda .. ..	R.C. bridge	16·
Tallangatta-Wodonga Road .. ..	.. .. .	Ebden Huon .. ..	..	6·
YEA SHIRE— Upper Goulburn Road .. ..	.. .. .	Between Township and Cotton's Pinch .. ..	..	2·5
Yea-Glenburn Road .. ..	.. .. .	From Murphy's Corner to Glenburn .. ..	..	3·
		Total .. ..	90·585	3,081·002

## UNDER DIRECT SUPERVISION OF THE BOARD.

AVON SHIRE— Prince's Highway .. ..	Timber and steel bridge over Avon River at Stratford .. ..	.. .. .	..	..
ALBERTON SHIRE— Boolarra-Welshpool Road .. ..	.. .. .	Generally throughout .. ..	..	13·50
BULN BULN SHIRE— Fumina Road .. ..	Erection of timber bridge over Toorong River .. ..	.. .. .	..	..
.. .. .	Fencing 142 chains .. ..	.. .. .	..	..
BRAYBROOK SHIRE— Prince's Highway .. ..	.. .. .	Generally throughout .. ..	..	2·00
BRIGHT SHIRE— Bright-Omeo Road .. ..	.. .. .	.. .. .	..	14·00
CORIO AND WERRIBEE SHIRE— Prince's Highway .. ..	.. .. .	Cartage of 1,800 cubic yards gravel .. ..	..	..
.. .. .	.. .. .	Supply of 1,800 cubic yards gravel on rails, Talbot .. ..	..	..
CORIO SHIRE— Prince's Highway .. ..	.. .. .	Generally throughout .. ..	..	15·00
ECHUCA BOROUGH— Echuca West Road .. ..	Crossen Vale bridge over Campaspe River .. ..	.. .. .	..	..
FRANKSTON AND HASTINGS SHIRE— Frankston-Flinders Road .. ..	.. .. .	Supply of 1,000 cubic yards gravel .. ..	..	..
FOOTSCRAY CITY— Prince's Highway .. ..	.. .. .	Generally throughout .. ..	..	1·63
LILYDALE SHIRE— Mt. Dandenong Road .. ..	Supply of 500 cubic yards spalls between Jeeves, Saddle, and Olinda .. ..	.. .. .	..	..
.. .. .	1,300 feet grading, boxing, and trimming .. ..	.. .. .	·25	..
		Carried forward .. ..	0·25	46·13

STATEMENT SHOWING MILEAGE AND LOCALITY OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Road.	Particulars as to Locality of Works Constructed.		Mileage of Works Constructed.	
	Permanent Works.	Maintenance.	Permanent Works.	Maintenance.
			Miles.	Miles.
UNDER DIRECT SUPERVISION OF THE BOARD— <i>continued.</i>				
		Brought forward .. .. .	0.25	46.13
MALDON SHIRE— Melbourne-Bendigo Road .. .. .		Between Harcourt and Ravenswood .. .. .		8.50
MANFIELD SHIRE— Mansfield-Wood's Point Road .. .. .	Supply and delivery of 500 cubic yards of diorite metal .. .. .	Generally throughout, Jamieson to Matlock .. .. .		40.00
MARONG SHIRE— Melbourne-Bendigo Road .. .. .		Between Ravenswood and Big Hill .. .. .		3.5
MILDURA SHIRE— Mildura Road .. .. .	5,400 lin. feet clearing, 3,100 lin. feet forming, 2,300 lin. feet gravelling .. .. .		1.0	
" .. .. .	4,500 feet forming and loaming between Nowingi and Kiah .. .. .		0.80	
" .. .. .	10,500 feet construction between Nowingi and Kiah .. .. .		2.00	
" .. .. .	Construction of sections 13, 14, and 15 .. .. .		1.80	
" .. .. .	1,500 lin. feet of clearing and gravelling (sections 3, 3A, and 4) .. .. .		0.30	
" .. .. .	8,400 lin. feet clearing, forming, marling, and rolling section 1 .. .. .		1.60	
" .. .. .	7,500 lin. feet clearing and forming section 2 .. .. .		1.40	
MORWELL SHIRE— Boolarra-Welshpool Road .. .. .		Generally throughout .. .. .		16.1
Boolarra-Poster Road .. .. .	Boxing-out and sanding Cock's and Bickerton's Hill deviations .. .. .		2.30	
MULGRAVE SHIRE— Prince's Highway .. .. .		Generally throughout .. .. .		3.5
NARRACAN SHIRE— Walhalla Road .. .. .		Supply of 800 cubic yards screenings .. .. .		
OMBO SHIRE— Bright-Omeo Road .. .. .		Generally throughout .. .. .		12.00
Omeo-Glen Wills Road .. .. .		" .. .. .		28.00
ORBOST SHIRE— Prince's Highway .. .. .		" .. .. .		36.00
OTWAY SHIRE— Forrest-Apollo Bay Road .. .. .	5,700 lin. feet construction of Devil's Elbow section .. .. .		1.10	
" .. .. .	6,500 lin. feet grubbing and clearing only .. .. .		1.30	
" .. .. .	1,367 lin. feet formation .. .. .		0.25	
" .. .. .	Supply of 5,500 cubic yards of metal .. .. .			
" .. .. .	Supply of 5,000 cubic yards metal and screenings .. .. .			
PORTLAND SHIRE— Portland-Nelson Road .. .. .	6,500 lin. feet re-forming and metalling .. .. .		1.30	
" .. .. .	9,400 lin. feet grubbing, clearing, forming, and metalling .. .. .		1.75	
Prince's Highway .. .. .	12,950 lin. feet clearing, forming, and metalling between Port Fairy and Portland .. .. .		2.50	
" .. .. .	2,900 lin. feet re-forming and metalling .. .. .		0.70	
" .. .. .	2,200 lin. feet re-forming and metalling near Mumbamar .. .. .		0.40	
" .. .. .	2,100 lin. feet forming and metalling near Tyrendarra .. .. .		0.40	
RUTHERGLEN SHIRE— Chiltern-Howlong Road .. .. .	Reinforced concrete bridge at Simmons .. .. .			
ROCHESTER SHIRE— Rochester-Echuca Road .. .. .	Quarrying and loading 560 cubic yards of gravel .. .. .			
" .. .. .	Forming, boxing, and gravelling 1,500 lin. feet .. .. .		0.30	
SOUTH BARWON SHIRE— Prince's Highway .. .. .	Bridge over Barwon River .. .. .		In progress	
SWAN HILL SHIRE— Murray River Valley Road .. .. .	8,400 lin. feet clearing, forming, and metalling .. .. .		1.60	
" .. .. .	7,500 lin. feet grubbing, clearing, forming, gravelling, and fencing .. .. .		1.40	
TAMBO SHIRE— Nowa Nowa-Buchan-Gelantipy Road .. .. .	Reinforced concrete bridge over Ironstone Creek .. .. .			
TOWONG SHIRE— Glen Wills-Granite Flat Road .. .. .		Generally throughout .. .. .		31.00
UPPER YARRA AND ALEXANDRA SHIRES— Wood's Point Road .. .. .		McVeigh's to Matlock .. .. .		30.00
WALPEUP SHIRE— Mildura Road .. .. .	8,400 lin. feet clearing, forming, and marling .. .. .		1.70	
WERRIBEE SHIRE— Prince's Highway .. .. .		Supply of 1,800 cubic yards gravel .. .. .		
" .. .. .		Supply of 1,000 cubic yards screenings near Laverton .. .. .		
" .. .. .		Generally throughout .. .. .		17.50
		Total .. .. .	26.15	364.23



## APPENDIX H.

## COUNTRY ROADS BOARD.

## STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED ON DEVELOPMENTAL ROADS FOR THE YEAR ENDED 30TH JUNE, 1925.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES.</b>						
ALBERTON SHIRE—						
Balook-Traralgon	Through Bulga Park—Balook	4·25	·64	·64	·64	·64
Carrabung—Gormandale			1·01	2·63	1·01	1·01
Lays Road	For full length	1·32			1·32	1·32
Carrabung—Balook			1·55	1·55	4·18	4·18
Whitelaw's	At North Devon and at Womerah	4·04			1·25	1·25
Jenkins	For full length	1·97			1·97	1·97
Binginwarri—Welshpool	At Binginwarri	1·32				
Albert River	At Stacey's Bridge	1·23	·81	·81	2·04	2·04
Gellondale	At Albert River	1·1			1·31	1·31
ALEXANDRA SHIRE—						
Terip Terip	Between Yarek and Terip Terip—Formation, &c.	·08				
	Between Yarek and Terip Terip—Fencing	3·37				
ARAPILES SHIRE—						
Miga Lake—Gymbowen	Parish of Kalingur	2·0				2·0
AVOCA SHIRE—						
Maryborough—Natte Yallock	Between 15,375 feet and 42,714 feet on continuous chainage. Parish of Rathscar					4·28 Supply of spalls only
AVON SHIRE—						
Clydebank	Glover's Corner to Lake Wellington	2·48				
	Punt Lane to Glover's Corner				2·46	2·46
Clydebank—Bengworden	Clydebank Road to Avon River				2·50	2·50
Dargo	Long's Hill deviation				·61	·61
Stratford—Bengworden	Tom's Creek section				·61	·61
BACCHUS MARSH SHIRE—						
Balliang	At Balliang	2·42				
BAIRNSDALE SHIRE—						
Glenaladale—Lindenow	Station-road, Lindenow			1·23	1·23	1·23
Calulu—Boggy Creek	South side of Calulu Bridge, and Calulu Bridge to Stone House			2·76	2·76	2·76
Sarsfield—Waterholes	Bee Farm to Cole's		3·71	3·71		
Bairnsdale—Bengworden	Bengworden Hall—Tom's Creek, Titree section, near Neill's, north of surveyed township		2·09	1·23	1·23	1·23
BALLAN SHIRE—						
Back Settlement	Korweinguboorra	1·98	1·98	1·98	1·98	1·98
Blakeville	Blakeville	·90	·90	·90	·90	·90
Moorabool West (east section)	Bunding	·81	·81	·81	·81	·81
Daylesford	Korweinguboorra		2·67	2·67	1·89	1·89
Bungeelap	Ballan		1·05	1·05	1·05	1·05
Ballan—Egerton	Ballan		1·50	1·50	·72	0·72
BANNOCKBURN SHIRE—						
Burnside	From Maroua railway line to Inverleigh Road	1·75		1·75	1·75	1·75
BEECHWORTH SHIRE—						
Bowman's Forest	Bowman R.S. to Ovens River	2·0				
Everton	Everton R.S. towards Everton	1·19			·61	·61
	Two bridges over Ovens River overflow	·04				·04 (Bridge section)
BEECHWORTH SHIRE—						
Myrtleford—Yackandandah	Bridge, Barwidgee Creek		Bridge, 1 mile in progress		Bridge over Barwidgee Creek	Bridge, Barwidgee Creek
Stanley (Hillsborough)					·74	Works in progress, day labour
BELFAST SHIRE—						
Spencer's	11,302—26,424	1·12				
Badhams	0—9,955		1·30	1·30		
BENALLA SHIRE—						
Toombullup	Truss bridge and approaches at Ford's	·15				
	From Ford's Bridge towards Crowley's Gap	2·19				
	Clearing and grubbing from 7,516 feet to Crowley's Gap	3·03				
	Fencing Clark's deviation	2·5				
Molyullah—Tatong	Forming, side-cutting, and grading, and reinforced concrete pipes from 11,516-ft. peg to 19,451-ft. peg					1·5
Toombullup	From Dodd's Crossing to Ford's, boxing and metalling			3·63		
BERWICK SHIRE—						
Bunyip—Tonimbuk	At Cannibal Creek	·95				
Longwarry—Modella	At Modella	1·61			1·61	1·61
Upper Beaconsfield—Upper Pakenham	At Upper Beaconsfield			1·8	1·8	1·48
Hallam—Emerald	Timber bridge north of Old Narre Warren			Bridge	Bridge	
Gembrook—Nar Nar Goon	At Nar Nar Goon North	·68		·68	·68	·68
Tynong—Tonimbuk	At Tynong	·49			·49	·49
Beaconsfield—Emerald	Near Emerald		1·31	1·31	1·31	1·31
BRIGHT SHIRE—						
Kiewa Valley	Between 6,000 and 23,500 feet—Forming and metalling	·57			2·75	2·75
Buffalo River	From 30,500 to 36,500 feet—Forming and metalling	·56			1·14	
Myrtleford—Yackandandah	Between 15,900 and 28,500 feet—Forming and metalling	1·36				
Happy Valley	From 17,900 to 23,900 feet—Metalling	1·14			1·14	1·14
BULLA SHIRE—						
Riddell	From boundary of Shire of Romsey at Jackson's Hill	1·00	2·71	2·71		
Konagaderra	From junction with Melbourne—Lancefield Road, near Lancefield Junction, across Emu Creek	·61	1·7	1·7		
BULN BULN SHIRE—						
Neerim North—Noojee	Sections 1 and 2, Neerim North to Noojee	1·64			3·40	3·40
	Section 1—Metalling		2·58	2·58	2·58	
Poowong	Athlone Flat	·76			·76	·76
Rokeby—North Jindivick	Section 1 at Jindivick Post Office		1·87	1·87	2·14	2·14
Torwood—Topiram	Section 1 at Torwood	·82				
	Carried forward	55·87	30·19	42·8	55·37	57·8





## STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—continued.

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES—continued.</b>						
	Brought forward	167·452	115·96	136·77	125·9	123·99
<b>LILLYDALE SHIRE—</b>						
Monbulk-Seville .. .. .	Silvan area .. .. .	..	..	..	..	1·75
Olinda Creek-Deviation .. .. .	Mt. Dandenong .. .. .	..	..	..	..	1·65
Yark .. .. .	Montrose .. .. .	..	..	..	..	1·04
Wandin .. .. .	Wandin .. .. .	7·5	..	..	..	..
Beenak-Seville .. .. .	Seville .. .. .	5·5	..	..	..	..
<b>LOWAN SHIRE—</b>						
Diapur-Yanac .. .. .	18A and 18, Tarranginnie .. .. .	..11	..	..	..	..
Netherby .. .. .	Between 44, 38, and 40, Warragul .. .. .	..94	..	..	..	..
Woorak .. .. .	Between 7, 32 and 8b, 31A, &c. .. .. .	1	1	1	1	1
Yanac Station .. .. .	Between 32 and 33, Yanac .. .. .	..	..	..	..	..
<b>MAFFRA SHIRE—</b>						
Maffra-Newry .. .. .	At Bell Bird Corner .. .. .	..93	..93	..93	..93	..93
<b>MANSFIELD SHIRE—</b>						
Tolmie .. .. .	Boxing and metalling at Tolmie .. .. .	1·5	2·5	2·5	2·5	2·5
	Re-formation, metalling from 9 to 15 miles .. .. .	..	2·25	2·25	2·25	2·25
	Metalling One Mile Stump Hill .. .. .	1	1	1	1	1
	Metalling clay section .. .. .	2	2·6	2·6	2·6	2·6
Merton-Strathbozie .. .. .	Daw's deviation formation and surfacing .. .. .	..7	1·63	1·63	1·63	1·63
Benalla-Mansfield .. .. .	Bridge over Running Creek .. .. .	..	..	..	..	..
	Fencing Daw's deviation .. .. .	..	..	..	..	..
<b>MARONG SHIRE—</b>						
Yarraberb .. .. .	From western end of road .. .. .	..	..47	..	1·42	1·42
	From 11,500 lin. ft. from western end .. .. .	..	..47	..	..	..
Kamarooka .. .. .	From western end .. .. .	..	..77	..	..	..
<b>MELTON SHIRE—</b>						
Coburns .. .. .	.. .. .	..	..	..	1·43	1·43
<b>MILDURA SHIRE—</b>						
Merbein West .. .. .	East from Passchendale Avenue .. .. .	..64	..75	..75	..	..
Colignan .. .. .	Between Melbourne-Mildura railway and River Murray .. .. .	..	..58	..58	..58	..58 under construction
<b>MINHAMITE SHIRE—</b>						
Woodlands .. .. .	Between Woolsthorpe and boundary of Warrong Estate .. .. .	7·8	7·8	7·8	7·8	7·8
Lake Gorrie .. .. .	Between Mt. Eccles Road and Heywood Road .. .. .	5·2	7·3	5·2	5·2	5·2
Heywood .. .. .	Between Main Road and Bessibelle Post Office .. .. .	3·92	9·00	9·00	3·92	3·92
Orford-St. Helens .. .. .	Opposite Forest's .. .. .	..92	1·92	..92	..92	..92
Bessibelle-Yambuk .. .. .	Between Heywood Road and Bourke's Corner .. .. .	2·00	2·00	2·00	2·00	2·00
Condah-Macarthur .. .. .	.. .. .	7·69	162	..	..	..
<b>MIRBOO SHIRE—</b>						
Nichols .. .. .	Cox's to Mayall's—metalling .. .. .	..74	3·5	3·5	..74	..74
Boolarra South .. .. .	First Section—metalling .. .. .	1·04	3·97	3·97	1·04	1·04
Darlimurla-Thorpdale .. .. .	Francome's to Pfall's—grading .. .. .	..	3	3	1·25	1·25
Berry's Creek .. .. .	Moir's to Austin's—grading .. .. .	..	2	..	1·58	1·58
Lleola .. .. .	Cox's—metalling .. .. .	..19	..19	..19	..19	..19
Boorool .. .. .	North's to boundary—grading .. .. .	..	2·23	2·23	2·23	2·23
Mirboo-Yarragon .. .. .	Dyke's to Brown's .. .. .	2·0	2·0	2·0	2·0	..
<b>MORNINGTON SHIRE—</b>						
Tyabb Road .. .. .	Approach to Quinu's Bridge .. .. .	..57	..	..	..	..
<b>MORWELL SHIRE—</b>						
Morwell-Boolarra .. .. .	Timber Bridge, near Yinnar .. .. .	..	..	..	..89	..89
" " .. .. .	Metalling Silcock's Hill deviation .. .. .	..	..	..	..77	..77
" " .. .. .	Metalling Cross Roads to Budgerce .. .. .	..	..	..	..	..
Morwell-Boolarra and Boolarra-Welshpool .. .. .	Grading and Metalling .. .. .	..	..66	..66	..	..
Boolarra-Welshpool .. .. .	Timber Bridge, Morwell River flat .. .. .	..	..	..	..74	..74
Jumbuk Road .. .. .	Metalling O'Reilly's Hill deviation .. .. .	..	..74	..74	..	..74
Tyer's Road .. .. .	Timber and metal flood crossing .. .. .	..	..	..	..	..
<b>NARRACAN SHIRE—</b>						
Trafalgar-Willow Grove .. .. .	Near Latrobe River—gravelling .. .. .	..23	..23	..23	..23	..23
Moe-River (also Paved invert) .. .. .	Gravelling near Yarragon .. .. .	..59	..	..	..	..
Allambee-Childers (also for metalling same section) .. .. .	Near corner Leviston's .. .. .	1·14	1·14	1·14	..	..
Thorpdale-Childers .. .. .	Gravelling near Childers .. .. .	2·23	..	..	..	..
Yarragon-Thorpdale (also fencing) .. .. .	Near Yarragon .. .. .	1·16	..	..	1·16	1·16
Main Canal (metalling and gravelling) .. .. .	Between Yarragon and Trafalgar East .. .. .	1·2	..	..	1·2	1·2
Latrobe River connexion .. .. .	(Also Fencing) South of Latrobe River .. .. .	1·35	..	..	1·35	1·35
Thorpdale East (also fencing) .. .. .	Construction at Narracan East .. .. .	1·78	..	..	1·78	1·78
	Metalling at Harris' gully .. .. .	..11	..11	..11	..11	..11
Erica .. .. .	Metalling through Erica Township .. .. .	..47	..47	..47	..47	..47
Moe-Moondarra .. .. .	Construction around Moondarra Hill (also fencing) .. .. .	1·1	..	..	1·1	1·1
Narracan Connexion .. .. .	Metalling, &c., near Cr. McGregor's .. .. .	..	..	..	..86	..86
Shady Creek - Buln Buln East .. .. .	Metalling, &c., near Lawrence's .. .. .	..26	..26	..26	..26	..26
Moe-Coalville .. .. .	Construction near Two-Mile (also fencing) .. .. .	1·12	..	..	1·12	1·12
Cooper's Creek-Platina .. .. .	Construction from Cooper's Creek to Walhalla Road .. .. .	..	1·53	..	..	..
<b>NEWHAM AND WOODEND SHIRE—</b>						
Macedon Village Settlement .. .. .	Under construction—full length .. .. .	..	3·25	3·25	3·25	3·25
Campaspe .. .. .	Under construction—full length .. .. .	..	4·25	4·25	4·25	4·25
<b>NEWSTEAD AND MOUNT ALEXANDER SHIRE—</b>						
Glenrower-Joyce's Creek .. .. .	South of Joyce's Creek Hall .. .. .	..	..76	..76	..76	..76
<b>OMBO SHIRE—</b>						
Reedy Creek .. .. .	Ensay .. .. .	..5	2·5	1·5	..5	..5
Bindi .. .. .	Bindi .. .. .	1	1·5	1·5	1·5	1·5
Swift's Creek-Walnuts .. .. .	Swift's Creek .. .. .	..	2	2	..	..
Brookville .. .. .	Brookville .. .. .	..	2	2	..5	..5
<b>ORBOST SHIRE—</b>						
Princes' Highway .. .. .	Bridge over Hospital Creek and regrading .. .. .	..08	..	..	..08	..08
" " .. .. .	Loaming near Tostaree .. .. .	..39	2·29	2·29	2·29	2·29
" " .. .. .	Gravelling near Simpson's Creek .. .. .	..	1·43	1·43	1·43	1·43
" " .. .. .	Construction Section 6 .. .. .	..	..	..	2·16	2·16
Wangarabelle .. .. .	Widening northwards from Erriandra .. .. .	..	4·32	3·69	3·69	3·69
Combiobar .. .. .	Formation part Section 1, Grubbing Section 2 .. .. .	..	8·58	8·58	6·69	6·69
Lower Benm .. .. .	From Craigie Bog to Big Flat .. .. .	..	2·27	2·27	..	..
Lower Bendoc .. .. .	From Township boundary to Gunn's .. .. .	..	4·55	4·55	..	..
Jarrahmond .. .. .	From Cann Valley Road to Trcome's .. .. .	1·59	..	..	1·59	1·59
Broome's .. .. .	.. .. .	..	..	..	..	..
Carried forward		237·642	220·81	233·5	210·84	211·37

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>						
	Brought forward .. .. .	237·642	220·81	233·5	210·84	211·37
ORROST SHIRE—						
Bete Bolong .. .. .	Gravelling near Russell's Bridge .. .. .	·57	2·65	2·65	·57	·57
" .. .. .	Relocation at Bete Bolong North .. .. .		3·62			
" .. .. .	Deviation through Mundy's .. .. .	2·10			2·10	2·10
Cann Valley .. .. .	Bridge over Flat Rock Creek .. .. .	·004				
West Cann .. .. .	Construction—Walker's to Vickerman's .. .. .				4·05	4·05
OTWAY SHIRE—						
Carlisle-Gellibrand .. .. .	Part sections 5 and 6, gravelling, 1,000 lineal feet .. .. .	·19	·19	·19	·19	·19
" .. .. .	Boxing out and metalling 800 lineal feet, part sections 5 and 6 .. .. .		·15	·15	·15	·15
" .. .. .	Boxing out and gravelling near Boggy Creek .. .. .		·57	·57	·57	·57
Laver's Hill-Chapple Vale-Devondale .. .. .	Boxing out and gravelling at Gellibrand .. .. .	1·10				
" .. .. .	Boxing out and metalling .. .. .		·73	·73	·73	·73
" .. .. .	Bridge at chainage 3254 .. .. .					
" .. .. .	Forming and grading part section 7 .. .. .		·80	·80	·80	·80
Glen Aire-Laver's Hill .. .. .	Boxing-out and metalling .. .. .		·76	·76	·76	·76
Colac-Forrest .. .. .	Bridge over Roadknight's Creek .. .. .					
Port Campbell-Princetown .. .. .	Forming, grading, and metalling .. .. .		·47	·47	·47	·47
Hordern Vale .. .. .	Forming and grading east of Aire River .. .. .			·89	·89	·89
Princetown Road .. .. .	Forming and grading west of Aire River .. .. .	1·27				
" .. .. .	Forming and grading Section 2 .. .. .	1·70				
" .. .. .	Forming and grading Section 5 .. .. .	1·70				
" .. .. .	Forming and grading Section 6 .. .. .	·08				
Beech Forest-Apollo Bay .. .. .	Construction pile timber bridge and approach .. .. .	·43				
Beech Forest Laver's Hill .. .. .	Forming and grading near Berry's .. .. .	1·33				
Beech Forest-Mt. Sabine .. .. .	Forming and grading Section 7 .. .. .	1·03				
" .. .. .	Clearing and grubbing Section 2 .. .. .	·96				
" .. .. .	Clearing and grubbing Section 4 .. .. .	·53				
Ferguson-Charley's Creek .. .. .	Boxing out and metalling part Section 11 .. .. .	·93				
OXLEY SHIRE—						
Whorouly-Carbour .. .. .	Section No. 2, Parish of Whorouly .. .. .	1·24	1·24	1·24	1·24	1·24
Boggy Creek .. .. .	Section No. 2, Myrbee .. .. .	·18	1·89	1·89	·18	·18
" .. .. .	Near Moyhu .. .. .		·18	·18		
King Valley .. .. .	Section No. 1, 10,700 feet at Whitfield .. .. .	2·02				
" .. .. .	Section No. 2, south of Whitfield, under construction .. .. .	·95	1·83	1·83	·95	·95
Whitfield-Tobnie .. .. .	From 00 to 15,000 ft. metalling .. .. .		2·84	2·84		
Rose River .. .. .	Bridge site at Schlue's between pegs 56,000-55,900 .. .. .		·01			
" .. .. .	Side cutting, &c., 63,000-76,000 .. .. .		2·46	2·46		
PHILIP ISLAND AND WOOLAMAI SHIRE—						
Sheepway's .. .. .	From Wonthaggi-Loch Road towards Kongwak .. .. .				·75	·75
Kernot-Krowera .. .. .	Through Smyth's towards Kernot .. .. .	·96			1·09	1·09
Loch-Wonthaggi .. .. .	Stanlake's and Harris' deviation .. .. .	·95			·95	·95
Wonthaggi-Korumburra .. .. .	From Bank's Flat north .. .. .	·15			1·13	1·13
Dalyston-Glen Forbes .. .. .	Riddell's Hill .. .. .	·56				
Bass .. .. .	Lang's floodway—Construction of concrete culvert .. .. .					
PORTLAND SHIRE—						
Prince's Highway .. .. .	Dartmoor to Mumbannar .. .. .	1·88	7·29			
" .. .. .	Dartmoor to Greenwald .. .. .		6·62	3·52		
" .. .. .	From Tyrendarra towards Port Fairy .. .. .	5·39				
Bridgewater .. .. .	From Bare Hills .. .. .	1				
Gorae Road, No. 1 Section .. .. .	From railway station north .. .. .	1·27			Day work	Day work
Gorae Road, No. 2 Section .. .. .	From 6,700 to 14,950 (day work in progress) .. .. .		1·56	1·56	Day work	Day work
Gorae Road, No. 3 Section .. .. .	From railway station to Hamilton Road .. .. .		1·13			
Grubbed Road .. .. .	From Portland towards Nelson .. .. .		3·10	3·10		
Winnap-Drik Drik .. .. .	From Winnap towards Drik .. .. .	1·00	3·47	1·80	1·06	1·06
Portland-Nelson .. .. .	Cashmore Heath .. .. .	3·50				
RIPON SHIRE—						
Trawalla .. .. .	Centre length of road .. .. .	3·70				
" .. .. .	Balance of length .. .. .		4·56	3·02	·47	·47
Trawalla East .. .. .	West end .. .. .	2·27				
" .. .. .	Balance of road .. .. .		1·26			
Trawalla West .. .. .	North end forming only .. .. .	·91			Spalls only	Spalls only
" .. .. .	Omitting centre portion .. .. .		5·29	5·29	2·60	2·60
ROCHESTER SHIRE—						
Echuca West .. .. .	From north-east corner of allotment 32A, Parish of Millewa, west 7,100 lineal feet .. .. .	1·34	1·34	1·34	1·34	1·34 Being carried out by day work
Nanneella .. .. .	From north-west corner of allotment 37A1, Parish of Nanneella East, to Shire boundary .. .. .	1·02	1·02	1·02	1·02	1·02
RODNEY SHIRE—						
Tatura-Rushworth .. .. .	4 miles west from Tatura .. .. .	·91	·57	·57	·49	·49
Tatura-Toolamba .. .. .	4 mile west of Toolamba .. .. .	1·70			1·23	1·23
Lancaster-Undera .. .. .	Eastward from Lancaster .. .. .	1·59				
ROMSEY SHIRE—						
Lancefield-Baynton .. .. .			1·51	1·51	1·51	1·51
SEYMOUR SHIRE—						
Hughes Creek .. .. .	From Scott's house to second crossing over Hughes Creek .. .. .		1·45	1·45		
SHEPPARTON SHIRE—						
Congupna .. .. .	North-east from Shepparton-Numurkah Road .. .. .	·75	·98	·98	·98	Carried out by day labour
Cosgrove .. .. .	North from railway station .. .. .	·55	1·13	1·13	1·13	
Nathalia .. .. .	North-west from Shepparton-Numurkah Road .. .. .	·76	·96	·96	·96	
Pine Lodge .. .. .	Parish of Pine Lodge .. .. .	2·15	2·15	2·15	2·15	2·15
Grahamvale .. .. .	East from Shepparton Cannery .. .. .		11	1		
Violet Town .. .. .	South from Shepparton-Nalinga Road .. .. .	1·14	1·14	1·14	1·14	1·14
SOUTH GIPPSLAND SHIRE—						
Harding Lawson .. .. .	Through Cochran's .. .. .		·60	·60		
Boys .. .. .	Between Fish Creek and Eldridge's .. .. .	1·63				
Agnes Falls .. .. .	Through Jackson's and Morris' .. .. .	·36				
Foster-Stony Creek .. .. .	Foster North and Section 9 .. .. .	2·70				
Harding-Lawson .. .. .	Through Buckley's and Cotter's .. .. .	1·13				
Foster-Boolarra .. .. .	Metalling between Munro's and Little Franklin .. .. .	1·27				
McCartin's .. .. .	Between Williams' and Reserve .. .. .	·26			·81	·81
Dingwarri South .. .. .	Granite Bar to Hunt's .. .. .	·26				
Waratah .. .. .	Between Smith's and Griffin's .. .. .	·11			1·78	1·78
Foster-Mt. Best .. .. .	Henwood's Section .. .. .	1·13			1·15	1·15
Woorara West .. .. .	Munro's to Ferguson's .. .. .					
STAWELL SHIRE—						
Marnoo-St. Arnaud .. .. .	Railway crossing towards D. McKenzie's, Parish of Marnoo .. .. .	·5	·26	·26	·26	·26
Pomonal .. .. .	Section 1 .. .. .	1	1	1	1	1
	Carried forward .. .. .	301·836	300·59	284·55	249·49	246·95

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>						
	Brought forward .. ..	301·836	300·59	284·55	249·49	246·95
<b>SWAN HILL SHIRE—</b>						
Woorineen Settlement .. ..	Extension north and west .. ..	1·83				
Manangatang—Prooinga .. ..	Eastward from Manangatang .. ..	2·19	1·16	1·16	1·16	1·16
Manangatang—Ouyen .. ..	Kulwyne Tank to Kulwin .. ..	1·86				
Manangatang—Euston .. ..	Fencing and clearing (only) .. ..	2				
Fish Point .. ..	Portion near Fish Point .. ..	1·47				
Nyah South .. ..	Nyah West southward .. ..	1·70	2·64	2·64		
Spewa .. ..			2·68	2·68		
Piangil—Prooinga .. ..	Bailey's plain and approach Piangil .. ..		1·57	1·57		
<b>TAMBO SHIRE—</b>						
Nowa Nowa—Buchan .. ..	Between Nowa Nowa turnoff and Cannia Creek .. ..	1·5		1·5	1·5	1·5
Prince's Highway .. ..	Between Nowa Nowa and Lakes Entrance .. ..	2		2	2	2
Tambo Upper .. ..	From Buchan Road to punt across River .. ..	3·5				
Buchan—Gellantyp .. ..	Diekson's Hill to the Murrindal Mt. .. ..			2		
Metung .. ..	Between Swan Reach and Metung .. ..	2				
<b>TOWONG SHIRE—</b>						
Yabba .. ..	Between Bullhead and Tallendoon .. ..	·25	1·64	1·64	·47	·47
Tallangatta Creek .. ..	Between Wyebo Post Office and Cravensville .. ..	3·25			3·25	1·30
Shelley—Jingellic .. ..	Metalling 1·5 miles at Pheasant Creek; formation 2·6 miles at Shelley .. ..	4·10	·41	·41	4·35	2·60
<b>TRARALGON SHIRE—</b>						
Traralgon—Jeeralang .. ..	Along Traralgon Creek .. ..	1·7				
Taylor's .. ..	Whole of road .. ..	1·75				
Tyers .. ..	From Latrobe River northward .. ..	·78				
<b>UPPER MURRAY SHIRE—</b>						
Kancobin .. ..	Section through Upper Towong Soldier Settlement .. ..		2·31	2·31	1	1
Thowgla .. ..	Section south from Thowgla S.S.; section east from Corryong main road .. ..	2·50	·95	·95	3·98	3·98
Corryong—Benambra .. ..		1·72				
Beetoomba South .. ..	Section south from Beetoomba railway station .. ..	1·43				
Murray Valley .. ..	Section 1 mile west of Tintaldra .. ..	1·65				
<b>UPPER YARRA SHIRE—</b>						
Little Yarra .. ..	Powelltown and Gilderoy .. ..	1				
Don .. ..	Section 2 .. ..	2				
Woori Yallock—Cockatoo .. ..	Fencing deviation .. ..	1·25				
Little Yarra .. ..	Erection of two culverts .. ..					
<b>VIOLET TOWN SHIRE—</b>						
Harry's Creek .. ..	Deviation near Hammond's .. ..	2·25				
<b>WALPEUP SHIRE—</b>						
Nyang South .. ..	South of Nyang .. ..	·65	·65	·65	·65	·65
Underbool—Mamengoroock .. ..	North of Underbool .. ..	·70	·70	·70	·70	·70
Boinka South .. ..	South of Boinka .. ..	·69	·69	·69	·69	·69
Boningle North .. ..	North of Bonongie .. ..	·48	·48	·48	·48	·48
Murrayville North .. ..	North of Murrayville .. ..	·40	·40	·40	·40	·40
Carina North .. ..	North of Carina .. ..	·60	·60	·60	·60	·60
Panitya North .. ..	North of Panitya .. ..	·40	·40	·40	·40	·40
Nyang—Kattyoon .. ..	North of Nyang .. ..	·19	·19	·19	·19	·19
Ouyen—Kulwin .. ..	East of Ouyen .. ..	·34	·34	·34	·34	·34
Boulka—Timbroo .. ..	West of Bronzewing .. ..	1·08	1·08	1·08	1·08	1·08
Underbool—Gnarr .. ..	North-east of Underbool .. ..	·65	·65	·65	·65	·65
Tiega North .. ..	North of Tiega .. ..	·50	·50	·50	·50	·50
Danyo North .. ..	North of Danyo .. ..	·25	·25	·25	·25	·25
<b>WARANGA SHIRE—</b>						
Lauderdale .. ..	Forming and metalling .. ..	1·72			·87	·87
Bona Vista (Duncan's) .. ..	Through Crown Allotment 102, Parish of Warragul .. ..	·73				
Ferndale Road, Sections 4 and 5 .. ..	Ferndale .. ..	1·53			2·05	2·05
Old Telegraph .. ..					2·37	
Sea View—Korumburra .. ..	Sea View .. ..	1·46			3·05	3·05
Warragul—Leongatha .. ..	Ferndale .. ..	·42			·75	·75
Bona Vista (Sections 3 and 4) .. ..	Bona Vista .. ..	1·20				
Yarragon—Hazeldene .. ..	Cloverlea (Parish of Warragul) .. ..		1·46	1·46		
Nilma—Shady Creek .. ..	Nilma North, Parish of Darnum .. ..		3·11	3·11		
Nilma—Bona Vista .. ..	Bona Vista, Parish of Warragul .. ..		1·21	1·21		
Lardner's Track—Bridges .. ..	Lardner, Parish of Longwarry .. ..					
Ferndale—Strezlecki .. ..	Wild Dog Creek, Crown Allotment 11, Parish of Allambie .. ..				One bridge	One bridge
Ellinbank .. ..	Bear Creek Bridge .. ..				One bridge	
<b>WARRNAMBOOL SHIRE—</b>						
Nullawarre—Timboon .. ..	Near Nullawarre—Sections 1, 2, and 6 .. ..	1·25	·75	·75		
Naringal .. ..	Near Naringal, Sections 1 and part of 2 .. ..	·60	Sections 3, 4, 5 ·80	Sections 3, 4, 5 ·80		·80
Pannure .. ..			Sections 3, 4, and part of 2 2·5		2·5	
<b>WERRIBEE SHIRE—</b>						
Aviation .. ..	From Duncan's Road towards Flying School .. ..	·76				
Exford .. ..	From Telephone Road towards Parwan railway station .. ..	1·04				
<b>WHITTLESEA SHIRE—</b>						
Whittlesea—Kinglelake .. ..	From end of Scrubby Creek deviation to Joyce's store .. ..	·97				
" .. ..	(Work carried out by day labour)					
" .. ..	From Joyce's store thence easterly towards Kinglelake 9,250 feet .. ..		1·75	1·75		
<b>WIMMERA SHIRE—</b>						
Horsham—Wonwondah East .. ..	Commencing between Allotments 37 and 52, Parish of Bungalally, and extending north to the Horsham—Wal Wal Road (gravelling 13 feet wide, 9 inches deep) (say) .. ..	1	2·29	2·29	2·29	2·29
<b>WINCHELSEA SHIRE—</b>						
Conn's Lane .. ..	Middle Section .. ..	1·14			1·14	1·14
Boonah .. ..	Near Dean's Marsh .. ..				1·79	1·79
Dean's Marsh—Retreat .. ..	Near Dean's Marsh .. ..				·94	·94
Lorne .. ..	Near Lorne .. ..		1·36	1·36	1·36	1·36
<b>WODONGA SHIRE—</b>						
Beechworth—Wodonga .. ..			2·16	2·16		
	Carried forward .. ..	367·396	338·27	325·23	293·24	282·93

STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Lengths of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<b>UNDER MUNICIPALITIES—<i>continued.</i></b>						
	Brought forward .. ..	367·396	338·27	325·23	293·24	282·93
<b>WOORAYL SHIRE—</b>						
Buffalo-Waratah .. ..	.. ..	.. ..	.. ..	8·26	.. ..	.. ..
Nerrena .. ..	Through Pearson's .. ..	1·04	.. ..	.. ..	.. ..	.. ..
Koonwarra-Tarwin .. ..	From Koonwarra to Tarwin .. ..	·60	.. ..	.. ..	.. ..	.. ..
Koonwarra-Tarwin .. ..	Near Koonwarra railway station .. ..	·43	.. ..	.. ..	.. ..	.. ..
Meenyan-Tarwin .. ..	From Meenyan to Tarwin .. ..	1·72	.. ..	.. ..	1·72	1·72
Canavan's .. ..	Through Riseley's .. ..	·70	·70	·70	·70	·70
Gywyther Siding .. ..	Through Springdale .. ..	1·01	.. ..	.. ..	1·01	1·01
Boorool .. ..	Through "Boorool" and "Authoringa" .. ..	.. ..	3·26	3·26	3·26	3·26
<b>WYCHEPROOF SHIRE—</b>						
Wycheproof-Glenloch .. ..	.. ..	.. ..	·85	·85	·41	·41
Nullawill-Winston .. ..	North of Nullawill .. ..	1	.. ..	.. ..	.. ..	.. ..
Culgoa-Lalbert .. ..	East of Culgoa .. ..	·85	4·32	4·32	4·32	1·07
Meridian .. ..	North of Berrillock .. ..	1·25	1·02	1·02	1·02	1·02
Sea Lake-Myall .. ..	West of Sea Lake .. ..	1·63	1·36	1·36	1·36	1·36
Sea Lake-Tryell Down .. ..	North of Sea Lake .. ..	1·25	1·18	1·18	1·18	1·18
Nyarrin .. ..	West of Nyarrin .. ..	·19	1·90	1·90	1·90	1·90
Sea Lake-Utima .. ..	Taggart's Crossing .. ..	.. ..	·13	·13	·13	·13
<b>YACKANDANDAH SHIRE—</b>						
Dederang-Tawanga .. ..	Keegan's Bridge-Running Creek Road junction .. ..	·51	.. ..	.. ..	.. ..	.. ..
" .. ..	Running Creek junction-Blacks .. ..	1	.. ..	.. ..	2·12	2·12
		(1·12 in progress)				
Kiewa-Wodonga .. ..	Yackandandah Creek .. ..	.. ..	.. ..	R.C. bridge	.. ..	.. ..
Kergnyah-Gundowring .. ..	Deviations-Case's to Crosthwaite's .. ..	.. ..	.. ..	.. ..	1·74	·66
						Balance to be constructed by day labour
<b>YEA SHIRE—</b>						
Molesworth-Droymore .. ..	Allotments 16, 17, and 18, Parish of Switzerland .. ..	3·63	.. ..	.. ..	.. ..	.. ..
	Totals .. ..	385·326	352·99	348·21	314·11	299·47
<b>UNDER DIRECT SUPERVISION OF THE BOARD.</b>						
<b>ALBERTON AND MORWELL SHIRES—</b>						
Jeeralang West Road .. ..	Construction Sections 200F, 2 and 3 .. ..	1·0	.. ..	.. ..	.. ..	.. ..
Jeeralang West .. ..	Construction 5,200 lineal feet, Section 4, at Brown's Saddle .. ..	1·0	.. ..	.. ..	.. ..	.. ..
<b>ALBERTON AND SOUTH GIPPSLAND SHIRES—</b>						
Devil's Pinch .. ..	Construction 6,230 lineal feet to McElroy's .. ..	1·2	.. ..	.. ..	.. ..	.. ..
<b>HEALESVILLE SHIRE—</b>						
Healesville-Toolangi .. ..	From chainage 1,573 to West Healesville Saw-mill at Chum Creek .. ..	2·5	2·0	2·0	2·5	2·5
" .. ..	Timber bridge, 30-ft. span, Chum Creek .. ..	0·006	0·006	0·006	0·006	(Day labour)
" .. ..	Fencing from Heath Road to allotment 128, Part Tarra warra North (133 chains) .. ..	.. ..	.. ..	.. ..	.. ..	(Day labour)
" .. ..	Grubbing and clearing from Heath Road, 00 feet to 15,000 feet .. ..	.. ..	2·9	2·9	2·9	2·9
						(Day labour)
<b>HEYTESBURY SHIRE—</b>						
Timboon-Nirranda .. ..	Curdie's River bridge (timber) and approaches .. ..	0·17	0·17	0·17	0·17	0·17
<b>LILLYDALE SHIRE—</b>						
Monbulk-Seville .. ..	Re-forming and metalling from Wandin Road .. ..	.. ..	.. ..	0·86	0·86	0·86
" .. ..	Re-forming and metalling from Edwards' south-west to main road .. ..	0·45	0·45	0·45	0·45	0·45
<b>MAFFRA SHIRE—</b>						
Bushy Park .. ..	Approaches to and fencing for bridge over Avon River .. ..	0·85	0·85	0·85	0·85	0·85
<b>MORWELL SHIRE—</b>						
Jeeralang West .. ..	Clearing and forming between Brown's and Hart's Saddles .. ..	1·30	.. ..	.. ..	2·80	2·80
Middle Creek .. ..	Clearing and formation between Coleman's and Moysey's .. ..	2·80	.. ..	2·20	2·20	2·20
Morwell River .. ..	Clearing and forming 4,600-10,000 feet .. ..	1·10	.. ..	1·10	1·10	1·10
" .. ..	Three-span timber bridge at 4,500 feet, and 240 feet approacher .. ..	·017	0·017	0·017	0·017	0·017
" .. ..	50 chains post-and-wire fencing through Siggins', parish of Gonyah .. ..	.. ..	.. ..	.. ..	.. ..	.. ..
" .. ..	Forming and gravelling Kelleher's Deviation .. ..	0·74	0·74	0·74	0·74	0·74
<b>NARRACAN SHIRE—</b>						
Allambee Estate .. ..	Single-span timber bridge over Tarwin River, and approaches .. ..	0·004	0·004	.. ..	.. ..	.. ..
Childers .. ..	Allambee-Childers Road to Diver's Saddle .. ..	.. ..	6·75	6·75	.. ..	.. ..
Darnum-Allambee .. ..	Clearing and formation through Cameron's, and across Clancy's Cutting towards McDonald's Track .. ..	1·80	.. ..	.. ..	1·80	1·80
						(Day labour)
<b>ORBOST SHIRE—</b>						
Erinundra Valley .. ..	Clearing and forming Hewat's at 14,000 feet to 20,400 feet, including three timber bridges .. ..	1·30	2·50	2·50	1·30	1·30
Orbost-Delegate .. ..	Clearing and forming between Sardine Creek and Little Bill, including three timber bridges .. ..	5·41	.. ..	.. ..	5·41	5·41
Murrungowar .. ..	Forming and grading along Arte Road to Pike's .. ..	0·50	.. ..	.. ..	0·50	0·50
" .. ..	Boxing and gravelling from turnoff at Prince's Highway to Buggs' .. ..	.. ..	.. ..	.. ..	5·10	5·10
<b>OTWAY SHIRE—</b>						
Apollo Bay-Wye River .. ..	Widening previously constructed 10-ft. track, Cape Patten to Grey River .. ..	2·0	.. ..	2·0	2·0	2·0
" .. ..	Formation and side-cutting Kennet River section .. ..	2·0	.. ..	2·0	2·0	2·0
" .. ..	Formation and side-cutting Wye River section .. ..	1·0	.. ..	1·0	1·0	1·0
" .. ..	Formation and side-cutting, commencing at Wye River and extending towards Apollo Bay .. ..	3·2	.. ..	3·2	3·2	3·2
" .. ..	Formation and side-cutting. Extension of above .. ..	1·0	.. ..	1·0	1·0	1·0
Wild Dog Valley .. ..	Formation of 2·3 miles at northern end of road .. ..	2·3	.. ..	2·3	2·3	2·3
						(Day labour)
<b>PORTLAND SHIRE—</b>						
Heath .. ..	13,900 feet of clearing and forming .. ..	.. ..	2·60	2·60	.. ..	.. ..
Prince's Highway .. ..	Metalling with limestone metal between Mumbannar and Limestone Ridge .. ..	2·4	.. ..	.. ..	.. ..	.. ..
<b>ROSEDALE SHIRE—</b>						
Callignee South (not gazetted) .. ..	Investigation survey from Dawson's Bridge to Callignee South School .. ..	.. ..	6·2	.. ..	.. ..	.. ..
	Carried forward .. ..	35·647	25·187	34·643	39·703	39·703



STATEMENT SHOWING MILEAGE, LOCALITY, AND PARTICULARS OF WORKS CONSTRUCTED, ETC.—*continued.*

Name of Municipality and Name of Road.	Locality of Works Constructed.	Mileage of Works Constructed.	Length of Roads.			
			For which Permanent Surveys have been made.	For which Plans have been prepared.	For which Tenders have been invited.	For which Tenders have been accepted.
		miles.	miles.	miles.	miles.	miles.
<i>UNDER DIRECT SUPERVISION OF THE BOARD—continued.</i>						
	Brought forward .. .. .	35·647	25·187	34·643	39·703	39·703
SOUTH GIPPSLAND AND WOORAYL SHIRES—						
Turton's Creek .. .. .	Metalling from Lodge's Bridge southerly towards Woorarra West Road .. .. .	2·0	..	..	2·0 (Day labour)	2·0
SOUTH GIPPSLAND—						
Quarries (general) .. .. .	Constructing tramway from Fish Creek Station easterly to gravel pits .. .. .	2·00	..	..	2·75	2·75
TAMBO SHIRE—						
Basin .. .. .	Forming and side-cutting going north-east from Mount McCloud .. .. .	1·87	1·87	1·87	1·87	1·87
" .. .. .	From Woods' across Murrindal River .. .. .	1·25	1·25	..	1·25 (Day labour)	1·25
" .. .. .	Erection of timber subway in Whelan's Block .. .. .	..	..	..	..	..
Nowa Nowa—Buchan—Gelantipy .. .. .	Timber and concrete bridge (302 feet) and approaches (550 feet) over Buchan River .. .. .	·16	·16	·16	·16	·16
" .. .. .	Survey from Canni Creek to South Buchan .. .. .	..	5·90	..	..	..
UPPER YARRA SHIRE—						
Woori Yallock—Cockatoo .. .. .	From Shepherd's Creek Bridge to Parslow's .. .. .	..	3·13	3·13	..	..
UPPER YARRA AND HEALESVILLE SHIRES—						
Healesville—Woori Yallock .. .. .	Six timber bridges, totalling 307 feet, over River Yarra and flats .. .. .	..	..	0·06	..	..
WANGARATTA SHIRE—						
Boorhaman—Springhurst .. .. .	Reinforced concrete bridge over Diddah Creek .. .. .	..	..	..	..	..
WOORAYL AND SOUTH GIPPSLAND SHIRES—						
Dollar—Dumbalk .. .. .	At Dollar—Clearing, forming and side-cutting .. .. .	2·75	2·75	2·75	2·75	2·75
YEA SHIRE—						
Yarra Glen—Glenburn .. .. .	Clearing, forming, culverts, &c., 4,300 feet .. .. .	0·80	0·80	0·80	0·80 (Day labour)	0·80
" .. .. .	Construction of deviation from Bradley's to Brown's Hill, 9,650 feet .. .. .	1·80	1·80	1·80	1·80 (Day labour)	1·80
" .. .. .	100 chains clearing west slide deviation .. .. .	1·25	1·25	1·25	1·25 (Day labour)	1·25
	Totals .. .. .	49·927	44·097	46·463	54·833	54·833

## APPENDIX J.

## SUMMARY OF TAR AND BITUMEN SPRAYING FOR YEAR ENDED 30TH JUNE, 1925.

Municipality.	Name of Road.	Mileage.	Materials used.	Supervision.	Permanent or Maintenance.
Berwick .. ..	Prince's Highway ..	1·6	Bitumen and gravel .. ..	Shire ..	M.
Braybrook .. ..	" " ..	2·0	" " .. ..	Board ..	M.
" .. ..	Ballarat .. ..	3·5	Tar and screenings .. ..	Shire ..	M.
Carrum .. ..	Point Nepean .. ..	6·0	Bitumen and screenings .. ..	" ..	M.
Corio .. ..	Prince's Highway ..	19·0	{ Bitumen and gravel, 17 .. ..	Board ..	M.
			{ Tar and gravel, 2 .. ..		
Dandenong .. ..	" " ..	4·0	Bitumen and screenings .. ..	Shire ..	M.
" .. ..	Dandenong-Cheltenham ..	5·0	" " .. ..	" ..	M.
Flinders .. ..	Point Nepean .. ..	3·5	" " .. ..	" ..	M.
Footscray .. ..	Prince's Highway .. ..	2·0	Bitumen and gravel .. ..	Board ..	M.
Frankston and Hastings	Point Nepean .. ..	7·8	" " .. ..	" ..	M.
" .. ..	Flinders .. ..	7·0	" " .. ..	" ..	M.
Healesville .. ..	Healesville-Alexandra ..	1·8	Tar and screenings .. ..	Shire ..	M.
Heidelberg .. ..	Heidelberg-Eltham ..	6·0	{ Tar and screenings, 2 .. ..	" ..	M.
			{ Bitumen and screenings, 4 .. ..		
Lilydale .. ..	Mount Dandenong .. ..	5·8	Tar and screenings .. ..	Board ..	P.
" .. ..	Main Healesville .. ..	12·6	{ Tar and screenings, 6·3 .. ..	" ..	M.
			{ Bitumen and screenings, 6·3 .. ..		
Maldon, Metcalfe, and Marong	Bendigo .. ..	16·0	Bitumen and screenings, gravel and sand	" ..	M.
Moorabbin .. ..	Point Nepean .. ..	3·0	Bitumen and screenings .. ..	Shire ..	M.
Mordialloc .. ..	" " .. ..	2·8	Bitumen and gravel .. ..	" ..	M.
Mornington .. ..	" " .. ..	6·0	" " .. ..	" ..	M.
Mulgrave .. ..	Ferntree Gully .. ..	1·3	Bitumen and screenings .. ..	" ..	M.
" .. ..	Prince's Highway .. ..	3·5	Bitumen and gravel .. ..	Board ..	M.
Nunawading .. ..	Main Healesville .. ..	3·5	" " .. ..	" ..	M.
Werribee .. ..	Prince's Highway .. ..	17·0	Bitumen and screenings .. ..	" ..	M.
" .. ..	Point Cook .. ..	5·0	" " .. ..	" ..	P.
Whittlesea .. ..	Main Whittlesea .. ..	5·0	{ Bitumen and screenings, 4 .. ..	Shire ..	M.
			{ Tar and screenings, 1 .. ..		
		150·7			

Miles sprayed with tar .. ..	23·4
Miles sprayed with bitumen .. ..	127·3
Total mileage .. ..	150·7

## APPENDIX K.

## SUMMARY OF MILEAGE FOR TYPES OF ROAD CONSTRUCTION COMPLETED DURING THE YEAR ENDED 30TH JUNE, 1925.

## UNDER DIRECT SUPERVISION OF THE BOARD.

Macadamized Roads.	Gravel or Sand.	Formation only.
25·50	12·49	90·14